



CITY OF
HAYWARD
HEART OF THE BAY

PLANNING COMMISSION

JANUARY 30, 2014

Table of Contents

Agenda	2
Request for adoption of a Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program and approval of a Conditional Use Permit (Application No. PL-2012-0069) and Vesting Tentative Tract Map (Application No. PL-2013-0070) associated with 194 townhomes and 16,800 square feet of commercial space on an 11.33 acre site located at 22301 Foothill Boulevard. Integral Communities (Applicant); MDS Realty II & 22301 Foothill Hayward, LLC (Owners)	
Staff Report	4
Attachment I - Area and Zoning Map	24
Attachment II - Initial Study, Mitigated Negative Declaration & MMRP	25
Attachment III - Recommended Findings for Approval	60
Attachment IV - Recommended Conditions of Approval	66
Attachment V - Project Plans	91
Attachment VI - General Plan Map for the Project Site & Surrounding Area	146
Attachment VII - Comments Received as of December 6, 2013	147
Attachment VIII - Proponent's Responses to Findings for Approval	224
Attachment IX - Support Cards & Petitions Submitted by the Applicant	229
Attachment X - E-mail Request Dated January 21, 2014	362
Capitol Corridor Train Service	
Staff Report	363
Attachment I	367
Attachment II	368
Attachment III	372
Attachment IV	397
Attachment V	398
Approval of minutes	
December 5, 2013	399



CITY OF HAYWARD
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AGENDA
SPECIAL HAYWARD PLANNING COMMISSION MEETING
THURSDAY, JANUARY 30, 2014 , AT 7:00 PM
COUNCIL CHAMBERS

MEMBERS OF THE AUDIENCE WISHING TO ADDRESS THE PLANNING COMMISSION:
 Obtain a speaker’s identification card, fill in the requested information, and give the card to the Commission Secretary. The Secretary will give the card to the Commission Chair who will call on you when the item in which you are interested is being considered. When your name is called, walk to the rostrum, state your name and address for the record and proceed with your comments. The Chair may, at the beginning of the hearing, limit testimony to three (3) minutes per individual and five (5) minutes per an individual representing a group of citizens for organization. Speakers are expected to honor the allotted time.

ROLL CALL

SALUTE TO FLAG

PUBLIC COMMENT: (The PUBLIC COMMENTS section provides an opportunity to address the Planning Commission on items not listed on the agenda. The Commission welcomes your comments and requests that speakers present their remarks in a respectful manner, within established time limits and focus on issues which directly affect the City or are within the jurisdiction of the City. As the Commission is prohibited by State law from discussing items not listed on the agenda, your item will be taken under consideration and may be referred to staff for further action).

ACTION ITEMS: (The Commission will permit comment as each item is called for Public Hearing. **Please submit a speaker card to the Secretary if you wish to speak on a public hearing item).**

PUBLIC HEARINGS: For agenda item No. 1, the decision of the Planning Commission is final unless appealed. The appeal period is 10 days from the date of the decision. If appealed, a public hearing will be scheduled before the City Council for final decision. For agenda item No. 2, no decision is required, this is for information only.

1. Request for adoption of a Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program and approval of a Conditional Use Permit (Application No. PL-2012-0069) and Vesting Tentative Tract Map (Application No. PL-2013-0070) associated with 194 townhomes and 16,800 square feet of commercial space on an 11.33 acre site located at 22301 Foothill Boulevard. Integral Communities (Applicant); MDS Realty II & 22301 Foothill Hayward, LLC (Owners)

[Staff Report](#)



Assistance will be provided to persons requiring accommodations for disabilities in compliance with the Americans with Disabilities Act of 1990. Persons needing accommodation should contact Sonja Dal Bianco 48 hours in advance of the meeting at (510) 583-4204, or by using the TDD line for those with speech and hearing disabilities at (510) 247-3340.

- [Attachment I - Area and Zoning Map](#)
- [Attachment II - Initial Study, Mitigated Negative Declaration & MMRP](#)
- [Attachment III - Recommended Findings for Approval](#)
- [Attachment IV - Recommended Conditions of Approval](#)
- [Attachment V - Project Plans](#)
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- [Attachment X - E-mail Request Dated January 21, 2014](#)

INFORMATIONAL REPORTS:

2. Capitol Corridor Train Service

- [Staff Report](#)
- [Attachment I](#)
- [Attachment II](#)
- [Attachment III](#)
- [Attachment IV](#)
- [Attachment V](#)

COMMISSION REPORTS:

3. Oral Report on Planning and Zoning Matters
4. Commissioners' Announcements, Referrals

APPROVAL OF MINUTES

5. [December 5, 2013](#)

ADJOURNMENT

PLEASE TAKE NOTICE that if you file a lawsuit challenging any final decision on any public hearing item listed in this agenda, the issues in the lawsuit may be limited to the issues which were raised at the City's public hearing or presented in writing to the City Clerk at or before the public hearing. **PLEASE TAKE FURTHER NOTICE** that the City Council has adopted Resolution No. 87-181 C.S., which imposes the 90 day deadline set forth in Code of Civil Procedure section 1094.6 for filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure section 1094.5.

NOTE: Materials related to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the Permit Center, first floor at the above address. Copies of staff reports for agenda items are available from the Commission Secretary and on the City's website the Friday before the meeting.



DATE: January 30, 2014

TO: Planning Commission

FROM: Damon Golubics, Senior Planner

SUBJECT: **Request for adoption of a Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program and approval of a Conditional Use Permit (Application No. PL-2012-0069) and Vesting Tentative Tract Map (Application No. PL-2013-0070) associated with 194 townhomes and 16,800 square feet of commercial space on an 11.33 acre site located at 22301 Foothill Boulevard. Integral Communities (Applicant); MDS Realty II & 22301 Foothill Hayward, LLC (Owners)**

RECOMMENDATION

Staff recommends that the Planning Commission adopt the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program (Attachment II) that identify mitigation measures to eliminate or reduce to insignificant levels all potentially significant environmental impacts, and approve the related conditional use permit and vesting tentative tract map applications, subject to the attached findings (Attachment III) and conditions of approval (Attachment IV).

SUMMARY

The applicant requests a conditional use permit to allow ground-floor residential units and a vesting tentative tract map to create 194 condominium parcels, in order to permit the construction of one hundred and ninety four (194) townhomes and 16,800 square feet of commercial space in two separate buildings at the former Mervyn’s headquarters site along the west side of Foothill Boulevard in the Central City-Commercial (CC-C) zoning district.

Staff supports the project because:

- ✓ It will provide a significant number of new high-quality residential units to be occupied by owners with middle incomes in the downtown area. It is anticipated that those residents will help contribute to a more vibrant downtown and help support existing businesses while also attracting new desired businesses to the downtown.
- ✓ The project will incorporate “green” components. The project will include an option for rooftop solar photovoltaic cells with all wiring, etc. in place for future installation.
- ✓ The project complies with all development standards of the zoning district and is consistent with Policy Number 3 (Downtown Area) Strategy No. 5 in the Land Use Chapter of the General Plan, which encourages “residential development in the downtown area to increase market support for business and to extend the hours of downtown activity.”
- ✓ The project’s economic consultant anticipates that the residents of the proposed units would

spend several million dollars annually in Hayward, and the commercial space in the project would generate annual sales of several million dollars. Recent market analyses show that the potential for a non-residential development on the site is limited.

- ✓ Significant environmental impacts, including those related to traffic, are not anticipated with this project with proposed mitigation measures.
- ✓ New high-quality residential units to be occupied by owners with middle incomes are currently missing in the downtown area.
- ✓ The proposed 16,800 square feet of commercial space will add additional shopping options to nearby neighborhood residents, the commercial space fronts onto Foothill Boulevard as envisioned by the General Plan, and
- ✓ The revised project addresses many of the issues raised by the Planning Commission during their prior review of the project. Design details for the townhomes along with more specific details on the roofing, paint colors, parking, first floor living space addition and amenities.

BACKGROUND

Local Setting & Context - The Project is located on the northern edge of the downtown area. This location consists of a mix of office, commercial and residential development, with the bulk of the office and retail uses situated along Foothill Boulevard. Residential uses are located behind the commercial and offices away from the Foothill Boulevard corridor. Much of the surrounding residential subdivisions were constructed after World War II. With the closure of the Mervyn's retail chain of department stores in 2008, there was no need for the headquarters building anymore. Since the closure and reflective of the sluggish economy, the building has remained vacant and no other interested parties have leased the site. Current access to the property is from Hazel Avenue (two access points), City Center Drive (one entrance), and right turn in and out from/to Foothill Boulevard (one access point).

The Project site is surrounded on all sides by urbanized development consisting of residential, office and commercial land uses. Most residential properties near the project site include single-family and multi-family homes one (1) to two (2) stories in height with adjacent surface parking and landscaped areas. The San Lorenzo Creek flood control channel, maintained and owned by the Alameda County Flood Control and Water Conservation District, physically separates the project from existing residential properties to the west. Located immediately south of the project site are a variety of retail establishments, which provide retail shopping and personal services for the neighborhood. East of the project site across Foothill Boulevard is a mix of retail, commercial and office uses set back from Foothill Boulevard. Further east and behind these uses are some high density residential complexes, along with the former City Hall building and garage and the now-deconstructed Centennial Hall facility (City Center site). North of the project site is a mix of uses with commercial establishments fronting Foothill Boulevard and behind these businesses are single family and multi-family housing. The adjacent gas station at the corner of Hazel Avenue and Foothill Boulevard would remain. Carlos Bee Park, a neighborhood park in an unincorporated area nearby, is about a one-mile walk from the project site. AC Transit Route 48 stops hourly on City Center Drive about 500 feet from the project site (corner of Foothill Boulevard and Hazel Avenue) on the other side of Foothill Boulevard.

Existing Project Site Setting - The Project site consists of two developed parcels, irregularly-shaped and approximately 11.33 acres in size. The Project site gently slopes downward and west towards the middle of the site from Foothill Boulevard towards the San Lorenzo Creek concrete flood control channel. Two hundred and thirty three (233) trees of varying sizes, species and health are dispersed across the project site, all but five of which (located along Hazel Ave.) are proposed to be removed and replaced with new trees and landscaping (see later discussion in the report).

Past Planning Commission Actions – On October 17, 2013, the Planning Commission reviewed the first version of the development request, which was 194 condominiums and 16,800 square feet of commercial space, and approved a motion to deny the project without prejudice pending staff returning to the Commission with the appropriate legal findings to substantial the their decision. Staff returned to the Commission on November 7, 2013 with findings to support the decision to deny the project. The Commission took additional public testimony and input from the applicant and rescinded their previous motion to deny the project and recommended that the project come back to the Commission at future meeting and a revised version of the project be brought back for Commission review that addressed most Commission’s concerns with the project. The previous staff report for the October 17, 2013 Commission meeting can be found via the following link: <http://www.hayward-ca.gov/CITY-GOVERNMENT/BOARDS-COMMISSIONS-COMMITTEES/PLANNING-COMMISSION/2013/PCA13PDF/pca101713full.pdf>. The prior staff report for November 7, 2013 Commission meeting can be found through the following link: <http://www.hayward-ca.gov/CITY-GOVERNMENT/BOARDS-COMMISSIONS-COMMITTEES/PLANNING-COMMISSION/2013/PCA13PDF/pca110713full.pdf>.

Meeting minutes for either of these meeting can be found pursuant to the following link: <http://citydocuments.hayward-ca.gov/WebLink8/Browse.aspx?startid=124108>.

Planning Commission Comments on the Previous Proposal – The Commission provided some generalized, specific and targeted Project comments at the October 17, 2013 and the November 7, 2013 meetings. Listed below is a summary of most comments and/or suggested changes sought by the Commission:

- Add text to the conditions that the Project would demonstrate meeting the City’s housing goal of providing diverse housing opportunities.
- Details need to be added to Project plans showing window details such as awnings, shutters, and other window designs.
- The roofing of the townhomes should consist of varying materials and colors.
- Add the following text to one for the conditions that “there be no pink, orange, or purple paint on the exterior of any of the buildings.”
- Address the underwhelming design of the townhomes.
- Address the lack of Project amenities proposed.
- Proposed site is a key location in the downtown area and a there should be a business entity at this commercial site.
- Incorporate a design that would have ground-floor retail with renters above the first floor.
- Also, a mixture of for sale and rentals units would be beneficial to the area.
- A revised development could include university and retail inclusion in addition to the project creating jobs for members of the community.
- Making public transportation easily accessible to residents would be beneficial to the

project.

- Parking in the development area was mentioned as a concern.
- Previous design plans better addressed community concerns.
- Some units have up to a three car garage alleviating impacts to the surrounding neighborhood but future residents would be more inclined to drive instead of using public transportation.
- Downsize the proposed three car garage/ground floor space that might be designed used as an in-law unit or an extra bedroom might reduce the number of vehicles being parked on streets.
- Need for jobs and businesses but is it really possible to locate a new large retail building or mall at the site?

DISCUSSION AND STAFF ANALYSIS

Project Description – The project entails construction of 194 dwelling units, as well as 16,800 square feet of commercial space in two buildings along Foothill Boulevard: one located at the corner of City Center Drive and Foothill and the other adjacent to the existing gas station located at Foothill and Hazel Avenue. The existing office building, parking garage and auto center will be demolished and removed from the site to make room for the project.

Site Plan - A parking area between the two commercial buildings along Foothill would be for exclusive use of customers and employees of the new commercial structures. There are no existing structures on the site that have any significant historic significance. The applicant states that the 194 dwelling units would be for-sale units which are encouraged in the Hayward General Plan. A condominium map has been filed with this project submittal (see Attachment V). The resulting project density would be 20.9 dwelling units per net acre, below the allowable maximum density indicated in the Zoning Ordinance of 65 dwelling units per net acre.

Building Elevations – As shown in Attachment V, the commercial portion of the Project is a contemporary style compatible with other newer commercial structures in the downtown area. The building colors proposed are harmonious with the surroundings. Offsetting wall planes are used effectively to break up the building mass into different components, consistent with the City’s Design Guidelines that encourage breaking up bulky buildings into components that relate to interior and exterior functions with variations in height, color and texture. Stone veneer, decorative cladding and stucco dominate the exterior treatment of the buildings. Simple awnings break up the mass of the building and add interest to exterior elevations. Placement of trees and landscaping along the Foothill Boulevard frontage is done in a way to not hide any of the buildings’ architecture.

The revised “conceptual retail perspective” is more refined than previously proposed for the two (2) commercial buildings. Previously, the commercial building elevations consisted of a suburban design that mirrored the existing commercial buildings across Foothill Boulevard (Safeway, etc.). This new architectural style is more urban and ties in nicely with the downtown area. Staff supports the proposed design changes. Additional specific project details have been included related to exterior materials used, material colors, landscaping, window specification, or other architectural building features. A condition of approval has been added to require that final missing construction details be provided for staff approval prior to building permits being issued. The rear elevations of

the commercial building are still quite austere, and staff has added a condition of approval that the rear elevation of the retail buildings be broken up or architecturally treated to be more interesting as this will be in the direct view shed of the residents. It will be especially important to treat this current monotone wall effectively since it is on a retaining wall and will be the predominant visual element seen from the residences abutting the rear of the commercial uses. Condition of approval #51 has been included to address this issue.

The exterior designs of the townhomes employ five different and distinct styles: “Agrarian Contemporary”, “Contemporary Craftsman”, “Contemporary Monterey”, “Contemporary Spanish” and “Coastal Contemporary” styles. Each is unique and can be seen as a transition to the existing neighborhood beyond Hazel Avenue. The proposed exterior color selection for each design style is warm, not harsh or glaring.

Sheet TM-3 in Attachment V shows the layout of the condominiums. Attachment V also has detailed condominium elevation drawings, sample photos of similarly finished units by the project architect highlighting architectural detailing, and new exterior commercial building elevations for Commission consideration.

Floor Plans and Quality of Housing Design and Materials – Besides ensuring amenities exist in the immediate area, the residential unit types and the quality of design and amenities to be provided within the project are key to attracting the targeted population for the townhomes. The basic attributes of part of the proposed condominiums are summarized in the following table.

Unit Type	# of Bed-rooms	# of Bath-rooms	Floor Area (sq. ft.)	Number Of Units	Percent of Total (by type)
Townhomes					
Unit 1	2	2.5	1,485	28	14%
Unit 2	3	2.5	1,570	62	32%
Unit 3	3 + Den	2.5	1,850	62	32%
Unit 4	4 + Multi-Purpose Room ¹	4	2,115	42	22%
GRAND TOTAL				194	100%

The project proponents have indicated that the unit mix for this project, suggested floor plan layouts, fit and finishes of the construction, and amenities provided within each unit will ensure that these units are seen as “higher end” dwellings. The condominiums will have tile entries, wood cabinets, pre-wiring, etc. There will be optional upgrades typical of today's new homes, such as

¹ Multi-purpose room to be conditioned such that there will be no cooking facilities, gas lines or plumbing to avoid potential for creation of second units.

granite counter tops, hard wood flooring, upgraded fixtures, solar roof panels, etc. Pricing for the townhomes, which are expected to be available for sale mid to late 2015, is expected to range from \$518,000 to \$608,000.

The applicant has submitted revised floor plans showing additional options for the ground floors of some floor plans addressing previous Commission concerns and comments. Specifically, the Commission asked that some unit floor plans have “flexible space” so the residents/homeowners could “age in place” or possibly accommodate the living arrangement of local university students. The applicant has revised some of the ground floor plans, including the inclusion of a formal den or game room, multipurpose rooms with full bathroom, tech rooms and living suites that include a library/study area. Conditions Number 17 and 18 speak to the allowance of proposed ground-floor living space.

Grading and Site Work –The preliminary grading plan is depicted in Attachment V (Preliminary Grading Plan – Sheet TM-4). The project civil engineer estimates that no off-site soils would be imported for grading activities. The final site topography after grading will entail a series of benches for road and structures, to slope down from along Foothill Boulevard toward the San Lorenzo Creek flood control channel. A retaining wall system will be built adjacent to internal “D” Alley, “E” Alley, “T” Alley, “J” Alley, “A” Street and City Center Drive.

Subdivision of Land - The Project includes a proposed vesting tentative tract map to create a condominium subdivision for the construction of 194 residential units, two commercial parcels and parcels held in common ownership for access, parking, open space and utilities. All public utilities necessary to serve the subdivision are located adjacent to the Project site and utility easements would run within the private road system within the project. No new public roads are proposed, meaning all roads and parking areas within the project will be maintained by the homeowners’ association or commercial business association. There will be two 5-plexes, one 6-plex, two 7-plexes, four 8-plexes, one 10-plex, ten 11-plexes and one 12-plex (for a total of 21 lots) creating saleable condominiums under the subdivision request. The commercial buildings will be on two separate parcels as part of the subdivision request. A commercial ownership association will be formed for the two commercial parcels and there will be an agreement or language in the Covenants, Conditions and Restrictions (CC&Rs) drafted that clearly outlines the maintenance duties of the homeowners’ association and the commercial business associations with regards to the project entrance at Foothill Boulevard.

Access and Circulation - On-site vehicular and pedestrian access would be provided by a series of new private roads, alleys and sidewalks internal to the development. Some curbside parking will be available on Foothill and Hazel. Previous development plans proposed “mountable median” islands to be installed in Hazel Avenue, forcing traffic exiting from the development site to turn right towards Foothill Boulevard. Mountable medians are used to stem the flow of traffic from the development into the existing neighborhood to the west and fire trucks would be able to drive over them in the event of an emergency. Staff is now recommending as a condition of approval that “pork chop” islands/features be installed on the property at the entrance points along Hazel Avenue, which will better reduce maintenance issues and enhance visual quality, while achieving the same effects as the proposed “mountable median” islands that would eliminate left turn movements out of the project site.

Parking for the commercial spaces will be provided by surface parking lots located adjacent to Foothill Boulevard. A commercial ownership association will be formed for the two parcels with language in the CC&Rs clearly outlining the maintenance duties for the commercial surface parking lots. Accessible parking spaces are strategically located throughout the development site.

Landscaping and Open Space - Proposed landscaping is proposed throughout the site (see sheet L1 of Attachment V), including along all street frontages. Most open areas of the Project site will be vegetated with trees, shrubs, sod, vines and other ground cover. The preliminary landscape plan shows approximately 275 replacement trees, related to the proposed removal of 228 trees from the property. A tree appraisal report was submitted for the Project that indicates that irrigation to the existing trees had been turned off, some trees have died and others are in a severe state of decline. The condition of the existing trees range from poor to good, but most suffer from a variety of problems that are listed in the report.

Twenty-two (22) stormwater detention/bio-retention areas are proposed throughout the project site, which are designed to collect water during rainstorm events and filter water back into the ground water ecosystem. These areas may not be counted as common open space. The applicant has proposed some new enhancements to the large open space area (“The Hangout”) located in the northwestern portion of the site (see Sheet L-1 and related sheets in Attachment V). Three (3) elevated roofed structures with lounge seating are proposed, one of which has barbeques for outdoor dining. There is also a new 1,245 square foot elevated area with play structures proposed for the middle part of this space.

The proposed “paseos” or passageways providing access to the front doors of each townhome are fifteen (15) feet wide throughout the project site. Each side of the “paseo” would be flanked by a townhome structure three (3) stories high. This will create a relatively dark space between buildings. This dark space may limit the types of plants used within each “paseo”. Staff would suggest that all proposed plants, shrubs, bushes, grasses and groundcover be appropriate for these special areas. The applicant must ensure final landscape plans take into account the special species of plants that require less light be planted in these paseo areas.

There was previous thought about adding “bulb outs” along the new San Lorenzo Creek trail to add some architectural interest along this bicycle/pedestrian pathway. To address this issue, the applicant has created one main bulb out area that has been provided along the pathway surrounding the park/bio-retention area (aka; The Hangout). A final analysis will be needed to ensure the required amount of bio-retention area has been provided for the development prior to issuance of a building permit for the Project. Staff was previously concerned that the new play area on the elevated island may take away needed bio-retention treatment area needed for the Project site but preliminary information submitted by the Project engineer ensures that the proper amount of functioning bio-retention areas for the Project site have been provided for.

Besides this largest group open space area,, there are five (5) additional group open space areas strategically located in the northern and southern portions of the site. These designated open space areas provide centralized nodes serving several residential buildings.

The City’s Zoning regulations require a total minimum of 19,400 square feet of usable open space for the Project (100 square feet of open space per dwelling unit). Also, the Project must provide a minimum of 5,820 square of group open space (30 square feet per unit). Each townhome has built-in private outdoor deck areas in excess of 100 square feet. As to private open space, most townhome units include deck space ranging in size from 104, 105, 120 and 166 square feet. Front porch features are included in some floor plans but are too small to include into the private/usable open space calculation. Group open spaces areas are shown throughout the development site add up to a total area of 5,885 square feet.

The total project open space is 25,220 sq. ft. The applicant is proposing a total of 45,770 sq. ft. of on-site open space, almost 21,000 sq. ft. more than they need to provide. A summary of the open space calculations is shown below in a table.

194 Residential Units	Minimum Open Space Required	Proposed Open Space	Amount of Additional Open Space Proposed
Usable Open Space	19,400 sq. ft. (100 sq. ft. per unit)	39,885 sq. ft.	20,485 sq. ft.
Group Open Space	5,820 sq. ft. (30 sq. ft. per unit)	5,885 sq. ft.	65 sq. ft.
Total Open Space	25,220 sq. ft. (130 sq. ft. per unit)	45,770 sq. ft.	20,550 sq. ft.

Lighting - A “preliminary lighting plan” has been submitted as part of the applicant’s recent plan submittal. There is a condition of approval that requires a final lighting plan be prepared by a qualified illumination engineer. The plan needs to show the exterior lighting design of all exterior and parking lot lighting and such lighting shall be in accordance with the Security Standards Ordinance (No. 90-26 C.S.).

All site lighting will need to be designed by a qualified lighting designer and erected and maintained so that light is confined to the property and will not cast direct light or glare upon adjacent properties or public rights-of-way. Site lighting shall also be designed such that it is decorative and in keeping with the design of the development and exterior lighting shall be erected and maintained so that adequate lighting is provided in all common areas. The Planning Director or a designated staff member shall approve the design and location of lighting fixtures, which shall reflect the architectural style of the buildings. Exterior lighting shall be shielded and deflected away from neighboring properties and from windows of proposed buildings. The preliminary lighting plan shows proposed bollard and post top light s that blend with the project architecture. Staff will request a light level analysis from a lighting specialist and review the proposed lighting levels prior to issuance of a building permit for the project. Final lighting plans would be approved by the Development Services Director.

Public Parkland – City regulations and State law allows the City to require dedication of parkland, payment of park dedication in-lieu fees, or a combination of both. Public parkland differs from typical on-site group or private open space because it is required to be available to the general public, versus the residents of a development project. Based upon the number of proposed dwelling

units and per City standards, if only parkland dedication were required with no payment of fees, the applicant would need to dedicate 3.2 acres of the 11.33-acre site for public park purposes. If only park dedication in-lieu fees were required, a payment of \$2,210,630 would be owed.

The project proponent is proposing to pay full in-lieu fees and also construct a public access trail along the rear of the property without receiving credit for such dedication and construction, as described below.

The public bicycle/pedestrian path and related public trail/access easement (County Flood Control maintenance easement necessary for maintenance of the flood control channel) is proposed at the rear of the project property along/above the San Lorenzo Creek flood control channel. This easement will be required to be dedicated to the public and the path will provide a needed link in this section of the San Lorenzo Creek trail and would allow a more attractive pedestrian and bike path away from Foothill Boulevard from Hazel Avenue to City Center Drive. The Hayward Area Recreation and Park District (HARD) staff is in support of this project amenity for use by the public, including the project residents. HARD has requested that any proposed path be wide enough to accommodate both pedestrian and bicycle access. The typical standard for a joint use path is a minimum of 8 feet in width. Staff is suggesting that standard be increased to a 10- foot path width as a condition of project approval since this is the standard HARD requires for similar pathways.

“Green” Aspects of the Project – The is required to be green point rated and obtain a green point rating score of at least 100, as confirmed by an independent qualified green point rater. The following green building features will be incorporated into the final project design: water efficient landscaping, use of engineered lumber, high efficiency shower heads, efficient bathroom fixtures and kitchen faucets, energy star appliances, high efficiency HVAC systems, use of low-voc paints, and installation of carbon monoxide detectors. Condition Number 52 specifically requires that all final green building details be reviewed and approved by the Planning and Building Divisions prior to issuance of building permits for the project. The applicant has also indicated that solar will be offered as an optional feature for each townhome style condominium.

Parking - Based on the City’s Off-Street Parking Regulations and as shown in the table below, the Project would comply with the City’s minimum parking standards.

Land Use	Units/Size	Requirement	Total Required	Total Provided
Retail/Commercial	16,800 sq.ft	1 space per 315 sq.ft.	53	53
Townhomes	194 Units	1.0 covered & 0.5 open spaces per dwelling unit	194 covered 97 open spaces	450 covered 87 open spaces
Credit for Two-Wheel Vehicles	Located in Groups of 4	Bike Parking Spaces Must be 2-feet by 7-feet	0	44 Bicycle Parking Spaces (Credit for 11 Vehicle Parking Spaces – Section 10-2.406)
Total Parking			344	601

The Project would provide more than double the required number of covered on-site parking spaces for the residences and the minimum number of spaces for the commercial buildings. There is a shortage of ten (10) open on-site vehicle parking spaces and the applicant has addressed this shortage by providing 44 bicycle parking spaces interspersed throughout the development. These proposed bike parking areas meet the size and grouping requirements and credit for eleven (11) vehicle parking spaces is allowed by the creation of the forty-four (44) bike spaces. With these bicycle parking spaces, the applicant meets the total number of required on-site parking spaces.

There had been some Commission comments that with all the parking designed into each townhome unit that residents will be more likely to drive. That idea might be true, however, given the closeness to downtown, BART and AC Transit as well as the CSUEB Shuttle service, it is hoped that many residents will find most products, goods and services within walking distance or take transit to other nearby retail opportunities within the City of Hayward. A condition of approval has been included that requires the applicant to disclose to all renters and/or owners of the townhome units of transit opportunities available to residents. This information shall be updated annually/yearly as noted in the conditions of approval.

Additionally, according to on-street parking surveys conducted by the project traffic consultant, there are 60 parking spaces available on public streets within one block of the project, which include Hazel Avenue between Main Street and Foothill Boulevard and McKeever Avenue between City Center Drive and Main Street. During the a.m. weekday peak period, the maximum parking occupancy within the project vicinity is approximately 45 percent and during the p.m. peak period, it is approximately 37 percent. Based on the parking survey, there are 33 on-street parking spaces

available during the a.m. peak period and 38 parking spaces available during the p.m. peak period. These available on-street parking spaces could be utilized by surrounding property owners and visitors, as well as project residents and visitors/guests to the proposed development.

Community Facilities District - As a standard condition of approval and related to adopted City Council policy, the City requires developers to pay the cost of providing public safety services to the proposed project through the formation of, or annexation to, a Community Facilities District (CFD), should the project generate the need for additional public safety services. This will require the project developer to post an initial deposit of \$20,000 with the City prior to or concurrently with the submittal of the final subdivision map and improvement plans, to offset the City's cost of analyzing the project's need for additional public safety services. If the analysis determines that the project creates a need for additional public safety services warranting the formation of, or annexation to, a Community Facilities District, the project developer shall be required to pay all costs of formation of, or annexation to, the district, which costs may be paid from the developer's deposit to the extent that funds remain after payment of the City's costs of analysis as described above.

Inclusionary Housing Requirements – Compliance with the City's affordable housing provisions will be required for the project. Pursuant to the City's Interim Relief Ordinance, 7.5 percent of attached residential ownership units in a project must be set aside and sold at affordable prices to moderate-income households (households earning 120 percent of Area Median Income or less). The Relief Ordinance also allows developers the option to pay an \$80,000 per affordable unit in-lieu fee prior to obtaining a certificate of occupancy for the new units. Therefore, the applicant has to set aside fifteen (15) units or pay a total of \$1,200,000.00. The applicant intends to pay the in-lieu fees to meet inclusionary ordinance obligations. In order to exercise this option and per the existing Relief Ordinance provisions, the project must obtain all discretionary approvals by June 30, 2014 and all building permits must be issued by June 30, 2016.

Summary of Proposed Project Revisions in Response to Commissioners' Previous Comments - Staff has reviewed the latest plan submittals and would like to provide some analysis to address specific Commission comments related to the Project.

Prior Condition Number 46 has been deleted and language added to Condition Number 12 that the proposed annual income level of new homeowners helps meet the need or goal of having diverse housing within the City.

Some Commissioners previously spoke to the need for the development to be a "commercial" project with more retail, one that has a mixture of "for sale" and rental units, and a Project that might cater to the university (Cal State East Bay). The applicant has not put forth any alternatives for consideration by the Commission, in response to these comments.

The centralized open space area for residents (aka; The Hangout) includes more amenities and some "decorative landscape boulders" strategically located in this area. Staff has included a Condition of Approval (Condition Number 23) that requires site amenities be disbursed throughout the Project site, preferably in the designated group open space areas, for the enjoyment of all Project residents and not just residents residing close to the main centralized gathering spot. It is doubtful residents

living at the southern end of the Project site will routinely use the centralized gathering spot given it would be so far away from their homes.

Building Details - The applicant has provided additional building elevations showing additional exterior details, including window treatments, for each townhome. Also, additional plans details were submitted showing materials and other exterior details associated with the proposed retail buildings. Condition Number 20 in Attachment IV requires that final details of all windows shall be reviewed and approved by the Planning Director prior to issuance of a building permit for the Project. The intent is to ensure that high quality window products, awnings and shutters complement the Project's architecture.

Revised project plans show a "concrete flat roof tile" used on each townhome in various colors. Condition Number 21 memorializes the need for roofing material used on each townhome to consist of varying materials and colors per Commission direction. Different roof tile colors are now shown on Project plans.

Condition Number 22 addresses the paints colors used for the townhome portion of the project. This condition also prohibits the use of pink, orange or purple paint on the exterior of any townhome.

As previously mentioned, the applicant has provided additional building elevations showing exterior details of each architectural style proposed for the condominiums. The level of detail associated with the townhome designs is much better than previous plans sets reviewed by the Commission. Staff believes that these revised plan details should ensure that the Project condominiums will be rich in exterior architectural design and detailing. This high level of architectural design also applies to the two (2) commercial buildings.

Findings for the Conditional Use Permit - In order for a Conditional Use Permit to be approved that would allow first floor residential units, the Planning Commission must make four (4) findings, per Section 10-1.3225 of the Zoning Ordinance. The following text conveys staff's analysis of the Project under those findings (see Attachment III) and Attachment VIII provides input on the findings from the project proponent.

(1) The proposed use is desirable for the public convenience or welfare;

The Project, and specifically, a residential use on the first floor of the Project, is desirable for the public convenience and welfare because the Project will convert a large, vacant commercial building into a mixed-use community, create economic stimulus and housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled. The design and features of the Project will attract middle-income residents who are expected to spend their incomes to support businesses in Hayward, particularly in the Downtown, and/or attract new businesses. The Project would provide higher end, aesthetically-pleasing ownership housing with on-site amenities (open spaces and the San Lorenzo Creek pathway) within walking distance of transit. Providing ground-floor residential units would provide more active "eyes on the street" later in evenings, in line with "crime prevention through environmental design" (CPTED) principles, which would not necessarily be realized with commercial ground floor development.

(2) The proposed use will not impair the character and integrity of the zoning district and surrounding area;

The Project site is surrounded by residential uses and similarly-zoned properties, and is in the vicinity of multi-storied residential complexes, and as such, the Project will not impair the character and integrity of the surrounding area. As conditioned, traffic leaving the project on Hazel Avenue would not be able to turn westward and drive through the neighborhoods to the west, directing traffic generated by the project onto Foothill Boulevard, a major arterial. The project would entail higher quality materials/finishes and architecture, and entail the planting of new, irrigated landscaping, including the planting of 278 new trees. The standard specification level for the townhomes will consist of tile entries, wood cabinets, pre-wiring, etc. There will be numerous optional upgrades typical of today's new homes, such as granite counter tops, hard wood flooring, upgraded fixtures, solar roof panels, etc. Pricing for the townhomes should range from \$518,000 to \$608,000.

(3) The proposed use will not be detrimental to the public health, safety, or general welfare;

The initial study/mitigated negative declaration prepared for the Project demonstrates that no substantial adverse environmental effects would occur after implementation of mitigation measures included therein, including no significant impacts on public services or hazards. Traffic impacts are not expected to be significant and would be less than peak-hour trips compared with the previously existing Mervyn's office building use. Therefore, the Project's proposed residential and commercial uses will not have a negative effect on the public health, safety, or general welfare. Specifically, a conditional use permit allowing first-floor residential units has no effect on the public health, safety or general welfare. If analysis demonstrates that public services demands warrant it, the Project proponent would be required to make a one-time payment for such costs or form/be annexed into an existing community facilities district, which through assessments, would pay for needed public safety services.

(4) The proposed use is in harmony with the applicable City policies and the intent and purpose of the zoning district involved.

The current General Plan designation of the site is Downtown - City Center / Retail and Office Commercial (CC-ROC). On page C-4 of Appendix C of the General Plan, the Downtown - City Center Area has the following text that explains the unique vision for this area:

“This area is a major activity center in the planning area. It contains major public facilities such as City Center and the Main Library, retail and office areas, and high-density residential areas. Mixed-use development is encouraged to promote the pedestrian orientation and to maintain the downtown area as an integrated living, working, shopping and recreational area. The boundary of this area is delineated in the Downtown Hayward Design Plan.”

Page C-3 of that General Plan appendix lays out the vision for areas with a Retail and Office Commercial land use designation:

“These areas include the regional shopping center (Southland Mall), community shopping centers, concentrations of offices and professional services, and portions of the downtown area and South Hayward BART Station area where mixed retail and office uses are encouraged. Not shown are neighborhood convenience centers that support and are compatible with residential areas.”

One additional section of the General Plan further supports the project as related to City policies:

“Recognize the importance of continuous retail frontage to pedestrian shopping areas by discouraging unwarranted intrusion of other uses that weaken the attractiveness of retail areas; encourage residential and office uses to locate above retail uses.”

These sections of the General Plan indicate the proposed project is consistent with the policies of the General Plan in that the project provides residential use with some commercial use in the Downtown in close proximity to the Downtown BART station. The current development, with the surface parking lot, unoccupied Mervyn’s office building and parking garage, does not create a continuous retail frontage interfacing with the more pedestrian-oriented part of Downtown. It should be noted that this section of Foothill Boulevard is quite different from other sections of Downtown Hayward. Specifically, this section of Foothill is a multi-lane arterial with high-speed, high-volume vehicular traffic that is not very pedestrian-friendly. B Street is considered an example of a more pedestrian-friendly environment with a continuous retail frontage and presence, with lower volumes of traffic traveling at lower speeds in just two lanes.

The zoning designation of the project site is Central City Commercial (CC-C). Allowable permitted uses not requiring a use permit include residential dwelling units above the first floor and a variety of commercial uses (as is proposed at the southeast and northeast corners of the project site). Approval of a conditional use permit (CUP) is required for ground-floor residential use. Note that ground-floor residential is not outright prohibited by the CC-C regulations.

The purpose of the Central City – Commercial (CC-C) is, “to establish a mix of business and other activities which will enhance the economic vitality of the downtown area. Permitted activities include, but are not limited to, retail, office, service, lodging, entertainment, education, and multi-family residential.”

The project could be viewed as one that adds synergy to Downtown. Adding townhomes along with commercial space would contribute to the goal of Downtown being an active and vibrant area as referenced in the General Plan and Zoning Ordinance. Recent economic studies done for Downtown and this specific project show that this type of project would add to Hayward’s revitalization of Downtown by providing housing to attract middle-income households that would spend disposable income in Downtown. The Project also fulfills the intent and purpose of the CC-C zone by replacing an underutilized site with a vibrant, pedestrian-friendly mixed use development, and as a result, revitalizing the Central City and creating economic stimulus.

Findings for the Vesting Tentative Tract Map - - In order for a vesting tentative map to be approved, seven (7) findings are required to be made. The following text conveys staff's analysis of the Project under those findings (see Attachment III) and Attachment VIII provides input on the findings from the project proponent.

(1) That the proposed map is consistent with applicable general and specific plans as specified in Section 65451. [Subdivision Map Act §66474(a)]

The proposed subdivision is consistent with the Hayward General Plan and allows a development project that is consistent with allowed uses and densities designated by the "City Commercial – Residential Office Commercial (CC – ROC)" land use category of the General Plan. No Specific Plan applies to the Project.

(2) That the design or improvement of the proposed subdivision is consistent with applicable general and specific plans. [Subdivision Map Act §66474(b)]

The proposed subdivision is of a design consistent with the Hayward General Plan in that circulation design and roadways are provided to accommodate the anticipated traffic, and utilities, including water, sewer, and stormdrain facilities, will be provided to accommodate the proposed development. As demonstrated by the project initial study/mitigated negative declaration, the Project will have no significant impacts on aesthetics or land use.

(3) That the site is physically suitable for the type of development. [Subdivision Map Act §66474(c)]

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012), which is referenced in the project initial study/mitigated negative declaration, demonstrates that the proposed subdivision would occur on a site suitable for the proposed development.

(4) That the site is physically suitable for the proposed density of development. [Subdivision Map Act §66474(d)]

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012) demonstrates that the proposed subdivision would occur on a site suitable for the proposed density, in compliance with the City's parking, open space, and traffic impact standards.

(5) That the design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. [Subdivision Map Act §66474(e)]

The initial study/mitigated negative declaration prepared for the Project demonstrates that substantial adverse environmental damage, including to fish or wildlife and their habitat, would not result from the proposed subdivision, with incorporation of required mitigation measures. Moreover, the Project site has already been fully developed, and as a result, no fish or wildlife habitats exist on the Project site.

(6) That the design of the subdivision or type of improvements is not likely to cause serious public health problems. [Subdivision Map Act §66474(f)]

Adequate capacity exists to provide sanitary sewer service to the Project site, nor are air quality impacts to future residents considered significant, as analyzed in the initial study/mitigated negative declaration. The Project also adds housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled, which reduces impacts on air quality and greenhouses gases.

(7) That the design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. [Subdivision Map Act §66474(g)]

There are no existing public easements within the boundary of the proposed subdivision, nor are any easements necessary. The Project site is fully developed and currently consists of a 336,000 square foot unused office building and parking facilities, and therefore, there is currently no public access though the property.

Environmental Review - Staff prepared an Initial Study/Mitigated Negative Declaration (IS/MND) (Attachment II) that identifies potentially significant impacts under the environmental topics of: Aesthetics, Air Quality, Biological Resources, Cultural Resources and Noise. However, the IS/MND identifies mitigation measures, agreed to by the Project sponsor, that would reduce those impacts to a *less than significant level*.

The IS/MND was made available for public review from September 27, 2013 through October 16, 2013. One comments was received on the IS/MND from the Alameda County Flood Control and Water Conservation District/Public Works Agency stating that the project as proposed may result in increased runoff from increased impervious surface, an increased runoff discharge into the flood control channel may compromise the capacity of the channel, the MND did not identify how runoff will be treated prior to discharge into the flood control channel and, the removal and demolition of the existing structures may contain hazardous materials such as lead/asbestos. All issues have been addressed through specific conditions of approval. No other comments were received.

The Mitigation Monitoring and Reporting Program identifies responsibility for mitigation implementation and oversight (see Attachment II). The Initial Study, Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program were also posted at the Alameda County Clerk's Office on September 26, 2013, in compliance with the California Environmental Quality Act (CEQA) Guidelines. The documents were also posted on the City's website for review. Links to supporting materials used to assemble the IS/MNS are on the City's website under <http://www.hayward-ca.gov/city-government/departments/development-services/project-permit-status/projects-under-environmental-review/@-the-boulevard>

TJKM Transportation Consultants, the project traffic consultant, also reviewed the project site plan to evaluate on-site circulation and access. The plan shows that primary access will be provided using the right-in/right-out only driveway on Foothill Boulevard. Secondary accesses will be

provided through driveways on Hazel Avenue and City Center Drive, with the Hazel driveways prohibiting outbound left turns, as indicated previously. All three accesses are expected to be adequate for the project site. Related to safe traffic movements, staff is recommending as a condition of approval that “STOP” signs be installed facing exiting vehicles at the three project exit driveways.

Traffic – The proposed development is expected to generate approximately 2,680 daily trips on a typical weekday, including 117 trips (39 inbound, 78 outbound) during the a.m. peak hour and 257 trips (143 inbound and 114 outbound) during the p.m. peak hour. Such figures incorporate application of internal trip and BART-related reductions, reflective of commonly accepted assumptions that fewer vehicle trips will occur due to Project residents walking to/from BART, riding buses, and walking to the adjacent and nearby commercial businesses.

Hayward General Plan’s Circulation Element contains an established environmental impact threshold policy for roadway intersection levels of service (LOS). The policy states, “Seek a minimum Level of Service D at intersections during the peak commute periods, except when a LOS E may be acceptable due to costs of mitigation or when there would be other unacceptable impacts.” LOS D equates to delays at an intersection of between 15 and 25 seconds. For situations where there exists a LOS E or F, the City’s threshold for environmental impact significance is an additional delay of five or more seconds; meaning that a project impact would not be considered significant if an additional delay caused by the project was less than five seconds.

Prior to the Hayward Downtown One-way Loop (Loop) Project implementation, all the Project study intersections operated at LOS D or better during both the a.m. and p.m. peak hours. With the Loop Project, the traffic impact analysis indicates that all of the study intersections remain at LOS D or better and the A Street/Foothill Boulevard intersection improves to LOS B during the peak commute hours.

Under projected future intersection levels of service *without* the project, the intersection of Foothill Boulevard / City Center Drive is expected to operate at LOS E during the p.m. peak hour. The intersection of A Street / Mission Boulevard is expected to operate at LOS F during both the a.m. and p.m. peak hours. With the proposed project, the future intersection of Foothill Boulevard / City Center Drive is expected to continue operating at LOS E during the p.m. peak hour, while the intersection of A Street / Mission Boulevard is expected to continue operating at LOS F during both the a.m. and p.m. peak hours. The increases in delays at both intersections are expected to be less than five seconds. This is expected due to the beneficial effects of the adaptive signal system recently implemented for the Loop Project. Therefore, the addition of project traffic is not expected to result in a significant impact at these locations in the future.

A link to the traffic impact analysis prepared for the project by TJKM Transportation Consultants is on the City’s website under <http://www.hayward-ca.gov/city-government/departments/development-services/project-permit-status/projects-under-environmental-review/@-the-boulevard>.

ECONOMIC AND FISCAL IMPACTS

Market Analyses – According to analysis provided by the applicant’s consultant, the Concord Group, this project seeks to meet the needs of a new Hayward resident who is interested in a downtown-proximate product with upscale features and amenities of a newly constructed community. Because of Hayward’s central location, the applicant is confident the Project will attract young couples with dual commutes to different job centers. The units will appeal to young professionals and local families. Unit features will include upgraded flooring, some with master bedrooms and walk-in closets, loft space, large decks, large garage space for two cars with extra storage space, and some units will have multi-purpose rooms.

Per the Concord Group, annual sales of spending by Project residents in Hayward is estimated to be up to almost \$7M annually (assumes an optimistic capture rate of 75 percent of total spending to occur in Hayward). The capture rate of 75 percent is considered high by the City’s economic development staff. Given the type of retail goods and services that Hayward has to offer consumers, a more realistic capture rate of 60 percent is more appropriate. The proposed commercial space in the project is expected to involve up to \$3.9M in sales annually, which is also considered optimistic by staff.

Staff also has concerns about the potential type of commercial uses that could occupy the building spaces along Foothill Boulevard, related to their potential impact to existing local businesses. Neighborhood serving retail may compete with similar established businesses, thus reducing sales of those businesses and projected revenue generated by the commercial component of the project. The project consultant’s analysis assumes that the project retail sales would be \$290 a square foot, which is similar to mall levels. A lesser amount would be more reflective of neighborhood-serving establishments in the City.

The applicant’s economic impact analysis assumes that 20 percent of all sales will be generated by new residents. Since the type of future tenants that will lease the project’s commercial spaces had yet to be determined, the 20 percent sales generation number may not be accurate.

A link to the economic impact analysis is on the City’s website under <http://www.hayward-ca.gov/city-government/departments/development-services/project-permit-status/projects-under-environmental-review/@-the-boulevard>

Fiscal Impacts - Staff has conducted a revised fiscal impact analysis of the project, which estimates that the project will generate \$463,494 of new revenue annually; however, the project is projected to cost the City \$464,659 for a net annual cost of \$1,165 (\$6.00 per unit) – essentially, fiscally neutral. This analysis does not include any revenue from a community facilities district. The analysis used an average sales price of \$563,000 since the applicant provided a price range of \$518,000 to \$608,000 for the new townhomes.

PUBLIC NOTICE & OUTREACH

A notice of this public hearing and availability of the draft Mitigated Negative Declaration for the project was sent to all property owners within a 300-foot radius of the Project site and published in *The Daily Review* newspaper. Staff has also included previous correspondence related to the

project, which is included as Attachment VI. The applicant has submitted additional support cards/petitions for Commission consideration, included as Attachment IX.

No other new comments had yet been received as of the writing of this report. Any comments that are received before the Planning Commission meeting will be forwarded to the Commission for consideration.

NEXT STEPS

Should the Planning Commission's decision be appealed by an interested party or called up by a member of the City Council, a future hearing and decision by the City Council would then become the final action on this Project. If the Project is approved, the applicant will subsequently submit a final map and related subdivision improvement plans for processing, with the final map to be approved by the City Council. A vesting tentative tract map provides, for a period of three years after the date of approval or conditional approval of the vesting tentative map, the right to proceed with the proposed development in substantial compliance with the ordinances, policies, and standards in effect on the date on which the vesting tentative map application was deemed complete. However, the Project will be required to meet the building codes in effect at the time building permit applications are submitted. Subsequent to filing of the final map, building, grading and encroachment permit applications will be processed and issued, allowing for Project construction.

Prepared by: Damon Golubics, Senior Planner

Reviewed by:



Pat Siefers
Planning Manager

Approved by:

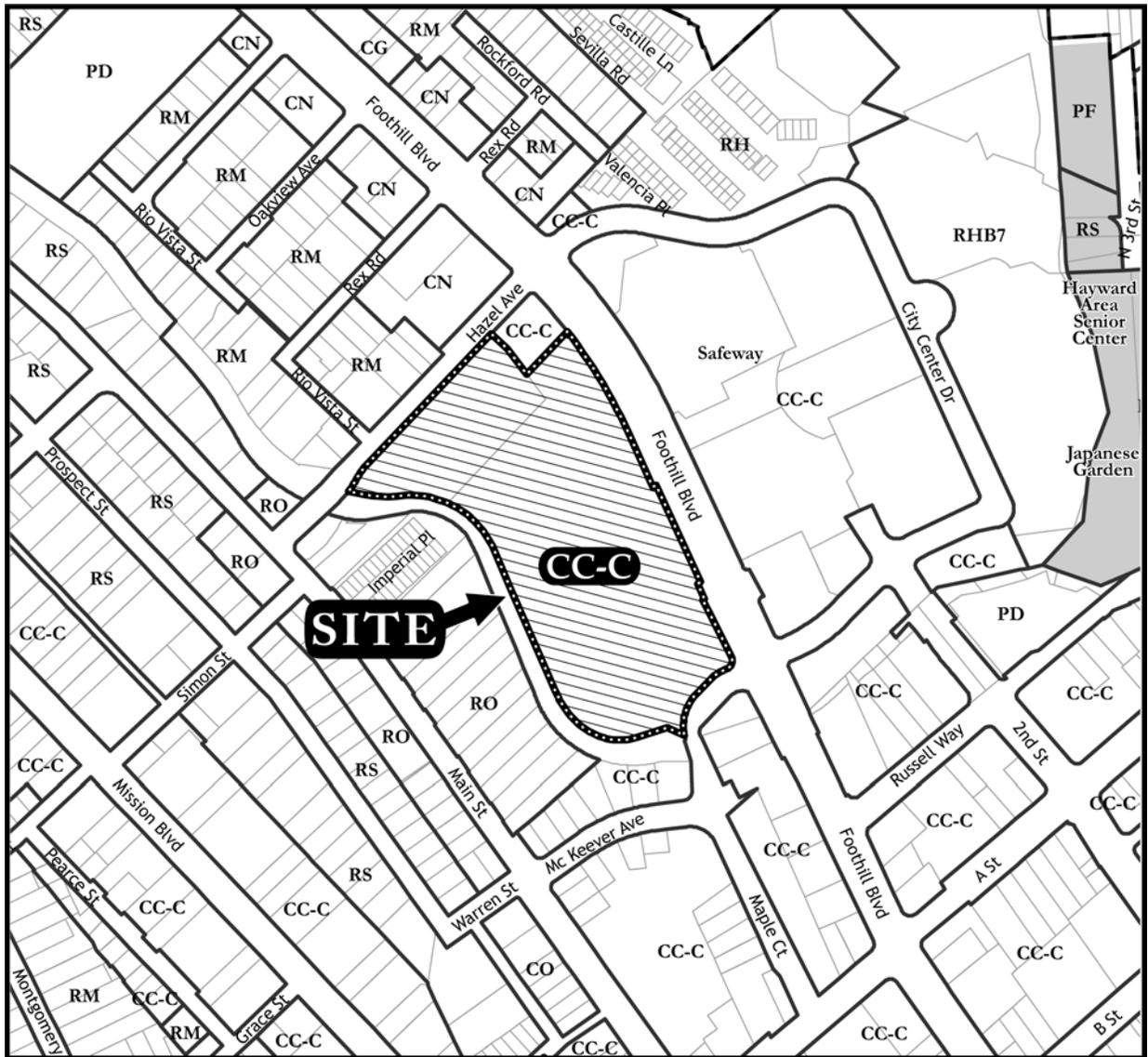


David Rizk, AICP
Development Services Director

Attachments:

Attachment I	Area and Zoning Map
Attachment II	Initial Study Checklist/Mitigated Negative Declaration/Mitigation Monitoring & Reporting Program

Attachment III	Recommended Findings for Approval
Attachment IV	Recommended Conditions of Approval
Attachment V	Project Plans
Attachment VI	General Plan Map for the Project Site & Surrounding Area
Attachment VII	Comments Received as of December 6, 2013
Attachment VIII	Proponent's Responses to Findings for Approval
Attachment IX	Support Cards/Petitions Letters Submitted by the Applicant as of January 23, 2014
Attachment X	E-mail Request Dated January 21, 2014



Area & Zoning Map

PL-2012-0068

PL-2012-0069

Address: 22301 Foothill Boulevard

Applicant: Integral Communities

Owner: 22301 Foothill Hayward, LLC
& MDS Realty II, LLC

Zoning Classifications

RESIDENTIAL

RH High Density Residential, min lot size 1250 sqft

RHB7 High Density Residential, min lot size 750 sqft

RM Medium Density Residential, min lot size 2500 sqft

RS Single Family Residential, min lot size 5000 sqft

COMMERCIAL

CG General Commercial

CN Neighborhood Commercial

CO Commercial Office

CENTRAL CITY

CC-C Central City - Commercial

OTHER

PD Planned Development





CITY OF
HAYWARD
HEART OF THE BAY

**ENDORSED
FILED**
ALAMEDA COUNTY
SEP 26 2013

PATRICK O'CONNELL, County Clerk
By  Deputy

September 26, 2013

Alameda County Clerk
1106 Madison Street, 1st Floor
Oakland, CA 94607

Subject: Notice of Intent to Adopt a Mitigated Negative Declaration for Conditional Use Permit Application No. PL-2012-0069 and Vesting Tentative Tract Map Application No. PL-2013-0070 (VTM 8129) – Located on Two (2) Parcels Totaling 11.33 Acres and Located at 22301 Foothill Boulevard Between City Center Drive and Hazel Avenue in the Downtown Area, Hayward

Dear Mr. O'Connell,

Please post this letter with the attached Mitigated Negative Declaration and Initial Study for a period of 20 days to conform to CEQA Guideline Section 15072.

The Planning Commission of the City of Hayward has scheduled a public hearing on Thursday, October 17, 2013, at 7:00 p.m., Council Chambers, 2nd Floor, City Hall, 777 B Street, Hayward, to obtain citizen input on the proposed project and the Mitigated Negative Declaration and Initial Study. A copy of the staff report can be viewed on the City's website at www.hayward-ca.gov after October 11, 2013. Planning Commission action at the hearing will be the final decision in this matter unless appealed to the City Council or called up by a Councilmember.

If the Mitigated Negative Declaration is approved, a copy will be sent to the General Business Division of your office for recordation. If you have any questions, please contact me at (510) 583-4210 or e-mail me at damon.golubics@hayward-ca.gov.

Sincerely,

Damon Golubics
Senior Planner

**DEVELOPMENT SERVICES DEPARTMENT
PLANNING DIVISION**



**CITY OF HAYWARD
MITIGATED NEGATIVE DECLARATION**

Attachment II
**ENDORSED
FILED**
ALAMEDA COUNTY

SEP 26 2013

PATRICK O'CONNELL, County Clerk
By  Deputy

Notice is hereby given that the City of Hayward finds that could not have a significant effect on the environment as prescribed by the California Environmental Quality Act of 1970, as amended will occur for the following proposed project:

I. PROJECT DESCRIPTION:

Project title: @ The Boulevard; Conditional Use Permit Application No. PL-2012-0069 and Vesting Tentative Map Application No. PL-2013-0070 (Map No. 8129).

Description of project: The project calls for a mixed-use development with 194 townhome units and 16,800 square feet of retail on 11.33 acres of land. The project is an in-fill development, and the project site currently consists of paved surface parking lots, a parking garage, and a vacant commercial office building. The surface lots, existing office building and existing parking structure will be removed as part of the construction of the Project.

Project review involves consideration of a vesting tentative map, conditional use permit and site plan review.

II. FINDING PROJECT WILL NOT SIGNIFICANTLY AFFECT ENVIRONMENT:

The proposed project, with the mitigation measures identified in the attached initial study checklist, will not have a significant effect on the environment.

FINDINGS SUPPORTING DECLARATION:

1. The proposed project has been reviewed according to the standards and requirements of the California Environmental Quality Act (CEQA) and an Initial Study Environmental Evaluation Checklist has been prepared for the proposed project. The Initial Study has determined that the proposed project, with the recommended mitigation measures, could not result in significant effects on the environment.
2. The project will not adversely affect any scenic resources. A lighting plan will be required to ensure that light and glare do not affect area views. Also, compliance with the City's Design Guidelines will ensure visual impacts are minimized. Landscape plans will also be required to ensure that structures are appropriately screened.
3. The project will not have an adverse effect on agricultural land since the subject site is not used for such purposes, does not contain prime, unique or Statewide important farmland.
4. The project will not result in significant impacts related to changes in air quality. When the property is developed the City will require the developer to submit a construction Best Management Practice (BMP) program prior to the issuance of any grading or building permit.

5. The project, proposed on properties surrounded by other residential development and within an urbanized area, will not result in significant impacts to biological resources. Any trees removed are required to be replaced as per the City's Tree Preservation ordinance.
6. The project will not result in significant impacts to known cultural resources including historical resources, archaeological resources, paleontological resources, unique topography or disturb human remains.
7. The project will not result in significant impacts to geology and soils. The project is located west of the Hayward fault, which poses potential risk to any development in the city of Hayward. Recommendations of the project geotechnical engineer will be required to be incorporated into project design and implemented throughout construction, to address such items as seismic shaking. Construction will also be required to comply with the California Building Code standards to minimize seismic risk due to ground shaking.
8. The project will not lead to the exposure of people to hazardous materials.
9. The project will be required to meet all water quality standards as part of the normal development review and construction process, to be addressed in a Stormwater Pollution Prevention Plan and Erosion Control Plan that utilize best management practices. Drainage improvements will be required to accommodate stormwater runoff, so as not to negatively impact the existing downstream drainage system of the Alameda County Flood Control and Water Conservation District.
10. The project is consistent with the overall density supported by the Hayward General Plan. In addition, the project will be required to be consistent with the City of Hayward's Design Guidelines.
11. The project will not result in any long-term noise impacts. Construction noise will be mitigated through restriction on construction hours, mufflers, etc., to be approved as part of the future building permits for the homes and commercial structures..
12. The project will not result in significant impacts related to population and housing in that the amount of development proposed is within the range of development analyzed in the Hayward General Plan.
13. The project will not result in a significant impact to public services in that development is at least as intensive as that proposed was analyzed in the Hayward General Plan EIR and found to have less-than-significant impacts.

ENDORSED
FILED
ALAMEDA COUNTY
SEP 26 2013

PATRICK O'DONNELL, County Clerk
Deputy

III. PERSON WHO PREPARED INITIAL STUDY:



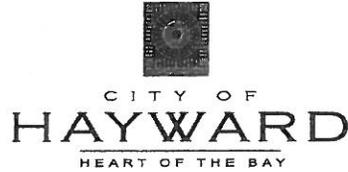
Damon Golubics, Senior Planner
Dated: September 26, 2013

I. COPY OF ENVIRONMENTAL CHECKLIST IS ATTACHED

For additional information, please contact the City of Hayward, Planning Division, 777 B Street, Hayward, CA 94541-5007, telephone (510) 583-4200

DISTRIBUTION/POSTING

- Provide copies to all organizations and individuals requesting it in writing.
- Provide a copy to the Alameda County Clerk's Office.
- Reference in all public hearing notices to be distributed 20 days in advance of initial public hearing and/or published once in Daily Review 20 days prior to hearing.
- Project file.
- Post immediately upon receipt at the City Clerk's Office, the Main City Hall bulletin board, and in all City library branches, and do not remove until the date after the public hearing.



**DEPARTMENT OF DEVELOPMENT SERVICES
Planning Division**

INITIAL STUDY CHECKLIST

Project Title: @ The Boulevard

Lead agency name/address: City of Hayward / 777 B Street, Hayward, CA 94541

Contact person: Damon Golubics, Senior Planner

Project location: 22301 Foothill Boulevard, Hayward, CA 94541

Project sponsors

Name and Address: Mark Butler, Integral Communities, 675 Hartz Avenue, Suite 202, Danville, CA 94526

Existing General Plan Designation: CC-ROC

Existing Zoning: Central City – Commercial (CC - C)

Project description: The project calls for mixed-use development with 194 residential units and 16,800 square feet of retail on 11.33 acres of land. The 194 residential units will consist entirely of townhomes. The Project also provides a significant amount of open space, including a park. The project is an infill development, and the project site currently consists of paved surface parking lots, a parking garage, and a vacant commercial office building. All existing buildings (the surface lots, the parking garage and the office building) will be removed as part of the construction of the Project.

Requested Local Approvals: The following actions by the Lead Agency are necessary to carry out the project:

- Conditional Use Permit: The Central City – Commercial zoning permits retail uses and residential dwelling units above first-floor commercial by right, and conditionally permits residential development, including multi-family units, on the first floor. Processing of a conditional use permit is required in order to allow for residential dwelling units on the first floor.
- Site Plan Review: The zoning regulations require that when a project materially alters the appearance and character of the property or area or may be incompatible with City policies, standards and guidelines. Since the current site development is that of an office use, the proposed mixed use development of 16,800 square feet of retail and 194 townhomes on 11.33 acres of land requires review of the proposed site plan.

- Vesting Tentative Map: (Vesting Tentative Tract Map No. 8129) A condominium map for Lots 1 through 23. The total number of residential condominium dwelling units shall be no more than 194 units for lots 1 through 23.
- Building Permit: (Hayward Municipal Code 07-17) The City of Hayward Development Services Department would review the proposed construction activities.
- Encroachment Permit: [Hayward Municipal Code, Article 2 (Streets)] The City of Hayward Public Works Department would review proposed construction activities associated with the project's utility, driveway and traffic control improvements within Foothill Boulevard, Hazel Avenue and City Center Drive.

Surrounding land uses and setting: The project site is near other similarly-zoned properties, including residential, mixed use and commercial properties.

Other public agencies whose approval is required: Alameda County Flood Control and Water Conservation District and the Hayward Area Recreation and Park District.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

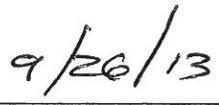
- | | | |
|---|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources | <input checked="" type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input checked="" type="checkbox"/> Geology /Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation/Traffic | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.


 Damon Golubics, Senior Planner


 Date

EVALUATION OF ENVIRONMENTAL IMPACTS:

ENVIRONMENTAL ISSUES:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
I. AESTHETICS -- Would the project:				
a) Have a substantial adverse effect on a scenic vista? Comment: <i>There are no designated scenic vistas in the vicinity of the project and the project is not located within or visible from a designated scenic vista; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? Comment: <i>The project is not located within a state scenic highway. No scenic resources exist in the area, and the project site is located in an urbanized setting, and the surrounding area is entirely developed; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings? Comment: <i>The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project will create a different massing of building that may be visible from existing neighborhoods surrounding the site. The project includes a proposed landscape plan that will result in more greenery than currently exists on the project site. The project site is located in an urbanized setting, and the surrounding area is entirely developed. The project will add a different visual character of the site and area but this aesthetic change is considered less than significant; no mitigation is required.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area? Comment: <i>The project site is fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building parking lot lighting and building lighting. The project will comply with the City's Municipal Code and design requirements relating to aesthetics, light and glare. The mixed use project proposes lighting to public streets abutting the project site, the internal street system of the project, interior pathways and each townhome</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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will have exterior building lights. The applicant's preliminary lighting plan strategically illuminates the project site with little light spillage onto adjacent properties, therefore the proposed project lighting will have a less than significant impact ; no mitigation is required.

II. AGRICULTURE AND FOREST

RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. -- Would the project:

a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Comment *The project site is in a substantially urbanized area, which includes residential and commercial land uses consistent with the Hayward General Plan and Zoning Map. The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project site is not zoned for agricultural uses, and there are no agricultural resources in the area. The project does not involve any Prime Farmland, Unique Farmland or Farmland of Statewide Importance; thus, no impact.*

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b) Conflict with existing zoning for agricultural use, or a Williamson Act contract? **Comment** *The project is not located in an agricultural zoning district nor is it subject to a Williamson Act contract. The project site is not zoned for agricultural uses nor*

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is it under a Williamson Act contract; thus, no impact.

c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))? **Comment:** *The project site is in a substantially urbanized area, which includes residential and commercial land uses consistent with the Hayward General Plan and Zoning Map. The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project site is not zoned for agricultural uses, and there are no agricultural resources in the area. The project does not involve the rezoning of forest land or timberland; thus, no impact.*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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d) Result in the loss of forest land or conversion of forest land to non-forest use? **Comment:** *There are no forest lands in this area, and the project does not involve the loss of forest land or involve conversion of forest land. Since the project does not involve the loss of forest land or involve conversion of forest lands, there is no impact.*

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e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use? **Comment:** *The project does not involve, nor is it located near, any commercially operated agricultural lands. The project is not located near any forest land. There is no impact to Farmland or forest land. The project does not involve changes to the environment that could result in conversion of Farmland or forest land; thus no impact.*

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III. AIR QUALITY -- Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a) Conflict with or obstruct implementation of the applicable air quality plan? **Comment:** *The Bay Area Air Quality Management District*

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Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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(BAAQMD) has established screening criteria as part of its CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria (thresholds of significance; 1999 and 2011), the proposed project screens below what would require additional evaluation; therefore the proposed project will not violate any air quality standard; thus no impact.

b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation? **Comment:** *The Bay Area Air Quality Management District (BAAQMD) has established screening criteria as part of their CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria (thresholds of significance; 1999 and 2011), the proposed project screens below what would require additional evaluation According to a September 10, 2013 air quality study performed by Urban Crossroads, there are two types of air quality impacts to evaluate with any development project; construction and operation air quality impacts. An evaluation of the operational aspects of the project reveals that the proposed development would not exceed any applicable threshold. Construction activities associated with the project would exceed the BAAQMD threshold for NOx. In order to reduce construction impacts to below the BAAQMD's threshold for NOx, the September 10, 2013 air quality study recommended that during construction activity, all diesel powered equipment (≥ 100 horsepower) shall be California Air Resources Board (CARB) Tier 3 Certified or better. The project will implement this mitigation measure, and as a result, all impacts will be less than significant with mitigation.*

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Mitigation Measure 1: *All diesel powered equipment (≥ 100 horsepower) shall be California Air Resources Board (CARB) Tier 3 Certified or better.*

c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? **Comment:** *The proposed project complies with the BAAQMD's CEQA Guidelines (thresholds of significance; 1999 and 2011). The*

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proposed project meets the screening criteria in Table 3-1 of the Air District's CEQA Guidelines; thus, it can be determined that the project would result in a less-than-significant cumulative impact to air quality from criteria air pollutants and precursor emissions.

d) Expose sensitive receptors to substantial pollutant concentrations? **Comment:** *The proposed project complies with the BAAQMD's CEQA Guidelines (thresholds of significance; 1999 and 2011). The mixed-use project is located in an already developed area that will not involve exposing sensitive receptors to substantial pollutant concentrations; thus the impact is less than significant.*

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e) Create objectionable odors affecting a substantial number of people? **Comment:** *The project is not considered a use that would create objectionable odors nor is it located in proximity to an existing source of objectionable odors. The mixed-use development will not create any objectionable odors; thus, no impact.*

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IV. BIOLOGICAL RESOURCES -- Would the project:

a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? **Comment:** *The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project will not cause any additional land within or outside the project site to be paved or otherwise developed. The site is not adjacent to or in the vicinity of any significant biological resources as it is an infill site and the flood control channel is a concrete culvert. The project will therefore not affect any listed species. The project site is located in an area that is largely developed and does not contain plant or wildlife special-status species; thus, no impact.*

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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service? Comment: <i>The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project will not cause any additional land within or outside the project site to be paved or otherwise developed. The site is not adjacent to or in the vicinity of any significant biological resources as it is an infill site. The project will not affect any habitats. The project area is largely developed and the flood control channel is a concrete culvert which does not contain any riparian habitat or sensitive natural communities; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? Comment: <i>The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project will not cause any additional land within or outside the project site to be paved or otherwise developed. The site is not adjacent to or in the vicinity of any significant biological resources as it is an infill site. The project will not affect any wetlands since the project site is located in an urban setting, which contains no wetlands; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites? Comment: <i>The project site is not adjacent to or in the vicinity of any significant biological resources, as it is an infill site. The project site, located in an urban setting, will not interfere with the movement of any migratory fish or wildlife species; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? Comment: <i>The project will comply with all local policies and ordinances, and considering the project site is a fully</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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developed site, the project will not affect any biological resources; thus, no impact.

f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

Comment: *In order to accommodate the development request, only five (5) existing on-site trees located on the project site along Hazel Avenue will be saved. All other trees on the site will be removed. A tree appraisal report has been submitted in conjunction with the project and "the majority of the mature trees on the site are in various levels of decline due to a number of factors." Some of those factors include lack of water to trees, poor maintenance and disease. A "tree mitigation plan" has also been submitted pursuant to the City's Tree Preservation ordinance along with a "preliminary landscape plan." The landscape plan shows replace tree type, species and locations for planning on the site. Lastly, the tree mitigation plan includes an appraisal of trees to be removed and remain on-site consistent with the Tree Preservation ordinance. Consistent with this ordinance, an application for a Protected Tree Removal or Cutting permit shall be required as a condition of approval for the use permit and subdivision request. All replacement trees shall be equal in size and species or value as required by ordinance. Also, there are no habitat conservation plans affecting the property, specifically, the project site is not located in an area covered by an adopted Habitat Conservation Plan or Natural Community Conservation Plan. Since the project proponent will be required to comply with all provisions of the City's Tree Ordinance, the proposed impact is less than significant.*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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V. CULTURAL RESOURCES -- Would the project:

a) Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5? **Comment:** *There are no historical resources associated with the improvements on the site or the affected parcels. Moreover, the project site has already been fully developed, and the existing buildings are of relatively recent origin and are of no significant historical or cultural significance. Due to extensive prior disturbance, there is a very low likelihood of impacting archeological or paleontological resources or disturbing human remains. In addition, the surrounding properties have no historical significance. Should any disturbance occur below developed areas, a remote possibility exists that historical or cultural resources might be discovered. If that should occur, standard measures should be taken to stop all work adjacent to the find and contact the City of Hayward Development Services Department for ways to preserve and record the uncovered materials. If standard procedures are followed in the event cultural/historical resources are uncovered at the project site, the proposed impact is less than significant.*

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b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5? **Comment:** *No known archaeological resources exist on the site, which has already been fully developed. Due to extensive prior disturbance, there is a very low likelihood of impacting archeological resources. Should any disturbance occur below developed areas, a remote possibility exists that historical or cultural resources might be discovered. If that should occur, standard measures should be taken to stop all work adjacent to the find and contact the City of Hayward Development Services Department for ways to preserve and record the uncovered materials. If standard procedures are followed in the event cultural/historical resources are uncovered at the project site, the proposed impact is less than significant.*

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c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature? **Comment:** *No known paleontological resources exist on the site, which has*

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Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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already been fully developed. Due to extensive prior disturbance, there is a very low likelihood of impacting paleontological resources. There are no unique geological features on or near the site; thus, no impact.

d) Disturb any human remains, including those interred outside of formal cemeteries?

Comment: *There are no records of any human remains located on the project site nor cemeteries nearby. In the event that human remains, archaeological resources, prehistoric or historic artifacts are discovered during construction or excavation, the following procedures shall be followed: Construction and/or excavation activities shall cease immediately and the Planning Division shall be notified. A qualified archaeologist shall be consulted to determine whether any such materials are significant prior to resuming groundbreaking construction activities. Standardized procedure for evaluating accidental finds and discovery of human remains shall be followed as prescribed in Sections 15064.f and 151236.4 of the California Environmental Quality Act. Due to extensive prior disturbance, there is a very low likelihood of disturbing human remains. Standard procedures for grading operations would be followed during development, which require that if any such remains or resources are discovered, grading operations are halted and the resources/remains are evaluated by a qualified professional and, if necessary, mitigation plans are formulated and implemented. These standard measures will be conditions of approval should the project be approved; thus, no impact.*

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VI. GEOLOGY AND SOILS -- Would the project:

a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:

i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

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Comment: *The State of California Fault Zone is located about 300 feet southwest of the nearest project site boundary. The*

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p><i>Hayward fault is mapped approximately 800 feet southwest of the site. A geotechnical investigation performed by Berlogar, Stevens & Associates on February 10, 2012 concluded that the project site shows no evidence of faulting and the likelihood of a surface fault rupture at the project site is low; thus, impacts related to fault rupture are expected to be less than significant.</i></p>				
<p>ii) Strong seismic ground shaking? <u>Comment:</u> <i>The project site is near, but not located in, both the California Fault Zone and the Hayward Fault. However, the proposed buildings will be designed and constructed to withstand ground shaking in the event of an earthquake; specifically, the project requires a building permit which would involve the mandatory implementation of design features to minimize seismic-related hazards. An earthquake of moderate to high magnitude could cause considerable ground shaking at the site; however, all structures will be designed using sound engineering judgment and adhere to the latest California Building Code (CBC) requirements, thus the impact is considered less than significant.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>iii) Seismic-related ground failure, including liquefaction? <u>Comment:</u> <i>The site is located within a State of California liquefaction seismic hazard zone. The site is underlain by Older Alluvium as shown in on Plate 3, Geologic Map (geotechnical investigation performed by Berlogar, Stevens & Associates dated February 10, 2012). Borings indicate the site is underlain predominately by very stiff to hard clayish soil. A lens of gravelly and silty sand was encountered at a depth of 20 feet in boring (B1). There is a potential that lens of gravelly and silty sand at the site could liquefy during an earthquake. However, the amount of settlement caused by liquefaction of these lenses should be muted at the ground surface due to the cap of clayish soil. Lateral spreading is unlikely since the sandy material is not</i></p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p><i>believed to be a continuous layer. A design level geotechnical evaluation shall be conducted and submitted for review and approval prior to issuance of building permits and if liquefaction is determined to be probable, measures as recommended by the project geotechnical consultant shall be implemented. Such measures, such as special foundation construction, will reduce the significance of liquefaction-related impacts to a level of insignificance.</i></p> <p>Mitigation Measure 2: <i>Prior to issuance of a Building Permit, the applicant shall conduct a design level geotechnical evaluation and submit that for review and approval and any recommendations shall be incorporated into the final design of the project.</i></p>				
<p>iv) Landslides? Comment: <i>The project site consists of flat lots not subject to landslides. Due to the relatively flat site topography, landslides are not likely; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b) Result in substantial soil erosion or the loss of topsoil? Comment: <i>Although the project would result in an increase in impervious surface, the project site is relatively flat and erosion control measures that are typically required for such projects, including but not limited to gravelling construction entrances and protecting drain inlets, will address such impacts. Therefore, the potential for substantial erosion or loss of topsoil is considered insignificant.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? Comment: <i>The site is relatively flat and such impacts are not anticipated.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? Comment: <i>According to the Due-Diligence Geotechnical Investigation, the site is underlain with predominately very stiff to hard clayish soil. The assessment recommends that a design-level geotechnical investigation be performed and recommendations thereof be incorporated into</i></p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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the project design and construction. Provided the recommendations of a design-level geotechnical assessment are followed, the impacts of the expansive soils will be mitigated to a less than significant level.

Mitigation Measure 3: *All recommendations outlined in a design-level geotechnical investigation shall be incorporated in the final design in order to mitigate for the presence of expansive soils on the project site.*

e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

Comment : *The project will be connected to an existing sewer system with sufficient capacity and does not involve septic tanks or other alternative wastewater; thus, no impact.*

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VII. GREENHOUSE GAS EMISSIONS --
Would the project:

a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Comment: *A September 10, 2013 study of the project performed by Urban Crossroads concluded that while the project would produce GHG emissions, these emissions will be significantly less than the currently entitled land use. This study used the California Emissions Estimator Model (CalEEMod) to evaluate the GHG impacts. The Bay Area Air Quality Management District (BAAQMD) recommends using the CalEEMod model in lieu of the Urban Land Use Emissions Model (URBEMIS) in calculating project greenhouse gas emission and evaluating air quality, as required by the BAAQMD. The BAAQMD has established screening criteria as part of their CEQA guidance to assist in determining if a proposed project could result in operational-related impacts to Greenhouse Gases. Based on the Urban Crossroads study, it has been determined that the project does not exceed the applicable threshold for operational greenhouse gas emissions using CalEEMod. Urban Crossroads used both the 1999 and 2011 BAAQMD thresholds of significance and the project will not exceed any of these thresholds. The operational threshold (impact) was below 4.6MT of CO²e/SP/year, which is less than the allowable maximum daily thresholds; thus the impact is*

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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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considered less-than-significant.

b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? **Comment:** *The September 10, 2013 Urban Crossroads study concluded that Project's GHG emissions will not exceed any applicable thresholds (1999 or 2011 thresholds) articulated by the BAAQMD. Moreover, the project will be in compliance with the City of Hayward Green Building Ordinance. As discussed in VIIa above, the project will not exceed the threshold for operation greenhouse gases; thus no impact.*

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VIII. HAZARDS AND HAZARDOUS MATERIALS -- Would the project:

a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? **Comment:** *The project is an infill residential project that does not involve the transport or use of hazardous materials; thus, no impact.*

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b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? **Comment:** *The site contains an underground fuel tank that will be removed during construction of the project. The applicant's Phase I and II environmental reports confirm that there has been no fuel leakage on the project site. Phase I and Phase II assessments were conducted on the subject property by Haley and Aldrich and although the property has an underground diesel storage tank used for powering a back-up generator for the previous office use and a former auto repair facility, no hydrocarbon-related compounds were detected in boring samples taken on-site. It is the opinion of Haley and Aldrich that the underground storage tank or the former auto repair facility has not impacted soil or groundwater quality at the site, therefore no further environmental assessment is warranted; therefore, no impact..*

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c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? **Comment:** *The project will not emit hazardous materials or substances, thus no impact.*

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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? Comment: <i>The project site has been analyzed through Phase I and Phase II environmental reports, which conclude that no contamination or hazardous substances are present on the project site. The project site is not on any list compiled pursuant to Government Code section 65962.5; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? Comment: <i>The project is not located within an airport land use plan area or within two miles of a public airport; therefore, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? Comment: <i>The site is not located within the vicinity of a private air strip and therefore, no such impacts would occur as a result of the project.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? Comment: <i>The project would not interfere with an adopted emergency response plan or emergency evacuation plan. In fact, the project would result in an improved on-site water system, thereby improving fire-fighting capabilities. Therefore, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? Comment: <i>The project site is located within an urban setting, away from areas with wildland fire potential, and outside the City's Urban Wildlife Interface zone. Therefore, no such impacts related to wildland fires are anticipated.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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IX. HYDROLOGY AND WATER QUALITY -- Would the project:

a) Violate any water quality standards or waste discharge requirements? **Comment:** *The project will comply with all water quality and wastewater discharge requirements of the city; thus, no impact.*

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b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? **Comment:** *The project will be connected to the existing water supply and will not involve the use of water wells and will not deplete groundwater supplies or substantially interfere with groundwater recharge; thus, no impact.*

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c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? **Comment:** *The project site is an infill site. All drainage from the site is required to be treated before it enters the storm drain system and managed such that post-development run-off rates do not exceed pre-development run-off rates; thus, no impact.*

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d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? **Comment:** *The project site is an infill site. All drainage from the site is required to be treated before it enters the storm drain system and managed such that post-development run-off rates do not exceed pre-development run-off rates; thus, no impact.*

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e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of

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polluted runoff? **Comment:** *The project site is a previously developed infill site. All drainage from the site is required to be treated before it enters the storm drain system and there is sufficient capacity to handle any drainage from the property; thus, the impact is considered less than significant.*

f) Otherwise substantially degrade water quality? **Comment:** *The project site has been analyzed through Phase I and Phase II environmental reports, which did not identify any impacts to surface or groundwater quality. There will be an increase in open space that currently exists on the site as part of the project, including implementation of a Provision C.3 storm water treatment system, which will actually improve groundwater quality. The project site is an infill. All drainage from the site is required to be treated before it enters the storm drain system; thus, no impact.*

g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? **Comment:** *The project site is not located within a 100-year flood hazard area; thus, no impact.*

h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows? **Comment:** *The project site is not located within a 100-year flood hazard area; thus, no impact.*

i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? **Comment:** *The project site is not located within a 100-year flood hazard area; thus, no impact.*

j) Inundation by seiche, tsunami, or mudflow? **Comment:** *The project site is not located within a 100-year flood hazard area; thus, no impact.*

X. LAND USE AND PLANNING -- Would the project:

a) Physically divide an established community? **Comment:** *The project is proposed in a developed urban setting and would not divide an established*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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community; thus, no impact

b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? **Comment:** *The project involves construction of 194 townhomes and 16,800 square feet of retail space, which is consistent with the General Plan and does not exceed the maximum permitted density. The Central City – Commercial zoning permits retail uses and residential dwelling units above first-floor commercial by right, and conditionally permits residential development on the first floor. Processing of a conditional use permit is currently underway allowing for residential dwelling units on the first floor. The proposed uses are also consistent with surrounding adjacent abutting uses, which consists of mixed-use, commercial and residential uses; thus, no impact.*

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c) Conflict with any applicable habitat conservation plan or natural community conservation plan? **Comment:** *The project site is not covered by any habitat conservation plan or natural community conservation plan; thus, no impact.*

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XI. MINERAL RESOURCES -- Would the project:

a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? **Comment:** *There are no known mineral resources on the project site; thus, no impact.*

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b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? **Comment:** *The project site is not identified as a site known to have mineral resources and there are no known mineral resources on the project site; thus, no impact.*

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Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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XII. NOISE -- Would the project result in:

a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Comment: *Temporary construction noise will be controlled by the Hayward Noise Ordinance, and specifically, the project will comply with the construction hours specified in the City's Noise Ordinance. Individual living units will need to be designed to standards called out in the Hayward General Plan for noise impacts. A qualified consultant will need to complete future noise readings, and if such readings result in indoor or outdoor noise levels that exceed the standards contained in Appendices M and N of the City of Hayward General Plan, then design of the units should incorporate sound attenuation features that are to be in accordance with the consultant's and/or architect's recommendations and be confirmed via actual readings prior to project finalization and/or C of Os on units. Efforts to reduce noise level of all dwelling units to be in compliance with standards in the General Plan will reduce the significance of noise-related impacts to a level of insignificance.*

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Mitigation Measure 4: *Prior to issuance of a Building Permit, the applicant shall conduct acoustical analysis by a qualified consultant to ensure that indoor or outdoor noise levels of each new residential unit does not that exceed the standards contained in Appendices M and N of the City of Hayward General Plan. If those standards are exceeded, the design of the units should incorporate sound attenuation features that are to be in accordance with the consultant's and/or architect's recommendations and be confirmed via actual readings prior to project finalization and/or C of Os on units.*

b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? **Comment:** *No significant vibration impacts are anticipated for the project site; thus, no impact.*

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c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? **Comment:** *The*

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project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. Under the project site's previous use, more than 1,000 individuals worked at the site. The proposed residential and retail uses will not produce noise levels in excess of the vehicle traffic produced by those using Foothill Boulevard. The mixed use development project is in the City Central – Commercial (CC-C) zoning district and will not involve an increase in the ambient noise levels in the area; thus, no impact.

d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?

Comment: Existing residential development nearby will experience a slight increase in ambient noise levels during the construction of the proposed project, construction is limited to the allowable hours per the City's Noise Ordinance; thus the impact is considered less-than-significant and no mitigation is required.

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e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? **Comment:** The project is not located within an airport land use plan area or within two miles of a public airport; thus, no impact.

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f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? **Comment:** The project is not located within the vicinity of a private air strip; thus, no impact.

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XIII. POPULATION AND HOUSING --

Would the project:

a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? **Comment:** The project will not, either directly or indirectly, induce substantial population growth. The project involves the construction of 194 new residential units, however, the residential development is consistent with the

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density established by the City's General Plan; thus, no impact.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? **Comment:** *The project will not displace any existing housing, as the project site currently consists of only commercial uses; thus, no impact.*

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c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? **Comment:** *The project will not displace any existing housing, as the project site does not currently consist of any residential uses; thus, no impact.*

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XIV. PUBLIC SERVICES --

a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

Fire protection? **Comment:** *No such facilities are required and therefore, no such impacts are expected to occur.*

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Police protection? **Comment:** *No such facilities are required and therefore, no such impacts are expected to occur.*

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Schools? **Comment:** *The project site is within the Strobbridge Elementary School, Bret Harte Middle School and Hayward High School attendance areas of the Hayward Unified School District. The developer will be required to pay school impact mitigation fees, which, per State law, is considered full mitigation. Such measures would reduce such impacts to levels of insignificance.*

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Parks? **Comment:** *The project proponent would be required to dedicate parkland and/or pay park dedication in-lieu fees. Such measures would reduce such impacts*

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to levels of insignificance.

Other public facilities? **Comment:** *The project's residents will not be numerous enough to have any material effect on the need for any other public facilities. Approval of the project may impact long-term maintenance of roads, streetlights and other public facilities; however, the project does not exceed density envisioned by the General Plan thus the impact is considered less than significant.*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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XV. RECREATION --

a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? **Comment:** *The project includes amenities and private spaces for residents, including a park. The project proposes to include some amenities and common areas within the development for residents. The developer will be required to pay applicable park in-lieu fees; thus the impact is considered less-than-significant.*

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b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment? **Comment:** *The project proposes to include some amenities and common areas within the developments, as well as a park. The developer will also be required to pay applicable park in-lieu fees. The project proposes a new bicycle and pedestrian pathway along the western boundary of the site adjacent to San Lorenzo Creek. This new recreational facility is well integrated into the project design and doesn't create any adverse physical effect on the environment on the adjacent creek; in fact, the proposed path respects the existing site topography and existing infrastructure controlling creek flow through this part of the City. Also, the Hayward Area Recreation and Park District (HARD) submitted project comments that the path provides a needed link in this section of San Lorenzo Creek and is pathway supported by their agency. Construction of the pathway/sidewalk may have minimal short-term environmental effects but once complete any impacts associated with this new pathway or recreational facility would be considered*

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Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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less-than-significant.

XVI. TRANSPORTATION/TRAFFIC --
Would the project:

a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?

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Comment: *The project will not conflict with any plan regarding the circulation system. The applicant commissioned a traffic study analyzing the project, which was completed by TJKM Transportation Consultants on September 26, 2013. This study concluded that the project will generate approximately 2,680 daily weekday trips, including 117 a.m. peak hour trips and 257 p.m. peak hour trips. The conclusion of the traffic study was that the project will not cause a significant impact to any study intersection and thus should not disrupt the existing transportation system; thus the impact is considered less than significant.*

b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

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Comment: *The applicant commissioned traffic study analyzing the project completed by TJKM Transportation Consultants on September 26, 2013, concluded that the project will not cause any significant impacts on traffic because all intersections will continue operating at the same level of service ("LOS") after the project that these intersections currently operate under the existing conditions. The same conclusion was reached under "near term plus project" and "cumulative plus project" conditions. Under "cumulative plus project" conditions, the Foothill Boulevard / City Center Drive intersection is expected to operate at LOS E during the p.m. peak hour, and the A Street / Mission Boulevard intersection is expected to operate at LOS F during both a.m. and p.m. peak hours. TJKM*

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p><i>concluded that the LOS E and F condition at these intersections are not significant impacts because the increases in delay due to project traffic is less than 5.0 second, which is the City's standard measure of significance. Therefore, the addition of project traffic is not expected to result in a significant impact. No level of service will be impacted by the construction of the new residential units and new retail/commercial space on an existing infill lot. The Alameda County Transportation Commission does not have an adopted level of service standard for intersections. In absence of such a standard the City has defaulted to the level of service standard in the General Plan. Using that standard as a guide, along with the SR 238 Corridor Improvement Project EIR, TJKM determined that there are less than significant traffic impacts. .</i></p>				

<p>c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? Comment: <i>The project involves no change to air traffic patterns; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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<p>d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? Comment : <i>The project has been designed to meet all City requirements, including site distance and will not increase any hazards; thus no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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<p>e) Result in inadequate emergency access? Comment: <i>The project is on a completely accessible infill site and will not result in inadequate emergency access; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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<p>f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities? Comment <i>The project does not involve any conflicts or changes to policies, plans or programs related to public transit, bicycle or pedestrian facilities; thus, no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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XVII. UTILITIES AND SERVICE SYSTEMS -- Would the project:

<p>a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? Comment: <i>The project will not exceed wastewater treatment requirements; thus no impact.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? Comment: <i>There is sufficient capacity to accommodate the proposed project; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? Comment: <i>There is sufficient capacity to accommodate the proposed project; thus, the impact is considered less than significant.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? Comment: <i>There is sufficient capacity to accommodate the proposed project; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project' s projected demand in addition to the provider' s existing commitments? Comment: <i>There is sufficient capacity to accommodate the proposed project; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project' s solid waste disposal needs? Comment: <i>There is sufficient capacity to accommodate the proposed project; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste? Comment: <i>The project will be subject to the regulations stipulated in Chapter 5, Article 1 Solid Waste Collection and Disposal in the City's Municipal Code. There is sufficient capacity to accommodate the proposed project; thus, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE --

a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels,	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Comment: *The project site has already been fully developed, and currently consists of paved parking lots, a parking garage, and a vacant office building. The project will not result in development of any currently undeveloped land. The project will have no impact on the environment, as this infill project exclusively calls for the development of land that has already been developed; thus, the project will have no impact and specifically will not degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory.*

b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

Comment: *The proposed mixed-use development is consistent with the density of development identified in the City's General Plan. An evaluation was done of past projects, the effects of other nearby current projects, and the effects of probable future projects in the immediate vicinity of the subject properties in crafting this Initial Study and it was determined and there were no foreseeable cumulatively considerable impacts associated with the development request and other adjacent projects (past, present and future); thus, no impact.*

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c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Comment: *The project will not have any environmental impacts therefore will not cause substantial adverse effects on human beings; thus, no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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@ The Boulevard

Mitigation Monitoring and Reporting Program

**Conditional Use Permit Application No. PL-2012-0069
Vesting Tentative Tract Map Application No. PL-2013-0070
(VTM 8129);
Integral Communities (Applicant/Project Sponsor)**

September 26, 2013

Mitigation 1

Significant environmental Impact: *The Bay Area Air Quality Management District (BAAQMD) has established screening criteria as part of their CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria (thresholds of significance; 1999 and 2011), the proposed project screens below what would require additional evaluation. According to a September 10, 2013 air quality study performed by Urban Crossroads, there are two types of air quality impacts to evaluate with any development project; construction and operation air quality impacts. An evaluation of the operational aspects of the project reveals that the proposed development would not exceed any applicable threshold. Construction activities associated with the project would exceed the BAAQMD threshold for NOx. In order to reduce construction impacts to below the BAAQMD's threshold for NOx, the September 10, 2013 air quality study recommended that during construction activity, all diesel powered equipment (≥ 100 horsepower) shall be California Air Resources Board (CARB) Tier 3 Certified or better. The project will implement this mitigation measure, and as a result, all impacts will be less than significant with mitigation.*

Mitigation Measure: All diesel powered equipment (≥ 100 horsepower) shall be California Air Resources Board (CARB) Tier 3 Certified or better.

Implementation Responsibility: Project developer

Monitoring Responsibility: City of Hayward Planning Division

Timing: During all phases of project construction

Mitigation 2

Significant environmental Impact: *The site is located within a State of California liquefaction seismic hazard zone. The site is underlain by Older Alluvium as shown in on Plate 3, Geologic Map (geotechnical investigation performed by Berlogar, Stevens & Associates dated February 10, 2012). Borings indicate the site is underlain predominately by very stiff to hard clayish soil. A lens of gravelly and silty sand was encountered at a depth of 20 feet in boring (B1). There is a*

potential that lense of gravelly and silty sand at the site could liquefy during an earthquake. However, the amount of settlement caused by liquefaction of these lenses should be muted at the ground surface due to the cap of clayish soil. Lateral spreading is unlikely since the sandy material is not believed to be a continuous layer. A design level geotechnical evaluation shall be conducted and submitted for review and approval prior to issuance of building permits and if liquefaction is determined to be probable, measures as recommended by the project geotechnical consultant shall be implemented. Such measures, such as special foundation construction, will reduce the significance of liquefaction-related impacts to a level of insignificance.

Mitigation Measure: Prior to issuance of a Building Permit, the applicant shall conduct a design level geotechnical evaluation and submit that for review and approval and any recommendations shall be incorporated into the final design of the project.

Implementation Responsibility: Project developer

Monitoring Responsibility: City of Hayward Planning Division

Timing: Prior to issuance of a Building Permit for the project

Mitigation 3

Significant environmental Impact: *According to the Due-Diligence Geotechnical Investigation, the site is underlain with predominately very stiff to hard clayey soil. The assessment recommends that a design-level geotechnical investigation is performed and recommendations thereof are incorporated into the project design and construction. Provided the recommendations of a design-level geotechnical assessment are followed, the impacts of the expansive soils will be mitigated to a less than significant level.*

Mitigation Measure: All recommendations outlined in a design-level geotechnical investigation shall be incorporated in the final design in order to mitigate for the presence of expansive soils on the project site.

Implementation Responsibility: Project developer

Monitoring Responsibility: City of Hayward Planning Division

Timing: Prior issuance of a Building Permit for the project

Mitigation 4

Significant environmental Impact: *Temporary construction noise will be controlled by the Hayward Noise Ordinance, and specifically, the project will comply with the construction hours specified in the City's Noise Ordinance; therefore, any impacts will be less than significant. Individual living units will need to be designed to standards called out in the Hayward General Plan for noise impacts. Future noise readings by a qualified consultant will need to be done and if such readings result in indoor or outdoor noise levels that exceed the standards contained in Appendices M and N of the City of Hayward General Plan, then design of the units should incorporate sound attenuation features that are to be in accordance with the consultant's and/or architect's recommendations and be confirmed via actual readings prior to project finalization and/or C of O's on units. Efforts to reduce noise level of all dwelling units to be in compliance with standards in the General Plan will reduce the significance of noise-related impacts to a level of insignificance.*

Mitigation Measure: Prior to issuance of a Building Permit, the applicant shall conduct acoustical analysis by a qualified consultant to ensure that indoor or outdoor noise levels of each new residential unit does not that exceed the standards contained in Appendices M and N of the City of Hayward General Plan. If those standards are exceeded, the design of the units should incorporate sound attenuation features that are to be in accordance with the consultant's and/or architect's recommendations and be confirmed via actual readings prior to project finalization and/or C of Os on units.

Implementation Responsibility: Project developer

Monitoring Responsibility: City of Hayward Planning Division

Timing: Prior to issuance of a Building Permit for the project

FINDINGS FOR APPROVAL

Conditional Use Permit Application No. PL-2012-0069, and Vesting Tentative Tract Map Application No. PL-2013-0070

Findings for Approval – California Environmental Quality Act:

1. Pursuant to CEQA Guidelines Section 15220, an Initial Study (“IS”) was prepared for this project with the finding that a Mitigated Negative Declaration (“MND”) was appropriate because all potentially significant impacts could be reduced to a level of insignificance.
2. That the proposed MND was prepared by the City of Hayward as the Lead Agency and was circulated with a twenty (20) day public review period, beginning on September 27, 2013 and ending on October 16, 2013.
3. That the proposed MND was independently reviewed, considered and analyzed by the Planning Commission and reflects the independent judgment of the Planning Commission; that such independent judgment is based on substantial evidence in the record (even though there may be differences between or among the different sources of information and opinions offered in the documents, testimony, public comments and such responses that make up the proposed MND and the administrative record as a whole); that the Planning Commission adopts the proposed MND and its findings and conclusions as its source of environmental information; and that the proposed MND is legally adequate and was completed in compliance with CEQA.
4. That the proposed MND identified all potential significant adverse impacts and feasible mitigation measures that would reduce these impacts to less-than-significant levels, and that all of the applicable mitigation measures identified in the MND and Mitigation Monitoring and Reporting Program will be adopted and implemented. Based on the MND and the whole record before the Planning Commission, there is no substantial evidence that the project will have a significant effect on the environment.
5. That the project complies with CEQA, and that the proposed MND was presented to the Planning Commission, which reviewed and considered the information contained therein prior approving the project. The custodian of the record of proceedings upon which this decision is based is the Development Services Department of the City of Hayward, located at 777 B Street, Hayward, CA 94544.
6. The monitoring and reporting of CEQA mitigation measures in connection with the project will be conducted in accordance with the attached Mitigation Monitoring Program, which is adopted as conditions of approval for the project. Adoption of this program will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in Section 21081.6 of CEQA. All proposed mitigation measures are capable of being fully implemented by the efforts of the project sponsor, City of Hayward or other identified public agencies of responsibility.

*Findings for Approval – Conditional Use Permit:***1. The proposed use is desirable for the public convenience or welfare;**

The Project, and specifically, a residential use on the first floor of the Project, is desirable for the public convenience and welfare because the Project will convert a large, vacant commercial building into a mixed-use community, create economic stimulus and housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled. The design and features of the Project will attract middle-income residents who are expected to spend their incomes to support businesses in Hayward, particularly in the Downtown, and/or attract new businesses. The Project would provide higher end, aesthetically-pleasing ownership housing with on-site amenities (open spaces and the San Lorenzo Creek pathway) within walking distance of transit. Providing ground-floor residential units would provide more active “eyes on the street” later in evenings, in line with “crime prevention through environmental design” (CPTED) principles, which would not necessarily be realized with commercial ground floor development.

2. The proposed use will not impair the character and integrity of the zoning district and surrounding area;

The Project site is surrounded by residential uses and similarly-zoned properties, and is in the vicinity of multi-storied residential complexes, and as such, the Project will not impair the character and integrity of the surrounding area. As conditioned, traffic leaving the project on Hazel Avenue would not be able to turn westward and drive through the neighborhoods to the west, directing traffic generated by the project onto Foothill Boulevard, a major arterial. The project would entail higher quality materials/finishes and architecture, and entail the planting of new, irrigated landscaping, including the planting of 278 new trees. The standard specification level for the townhomes will consist of tile entries, wood cabinets, pre-wiring, etc. There will be numerous optional upgrades typical of today's new homes, such as granite counter tops, hard wood flooring, upgraded fixtures, solar roof panels, etc. Pricing for the townhomes should range from \$518,000 to \$608,000.

3. The proposed use will not be detrimental to the public health, safety, or general welfare; and

The initial study/mitigated negative declaration prepared for the Project demonstrates that no substantial adverse environmental effects would occur after implementation of mitigation measures included therein, including no significant impacts on public services or hazards. Traffic impacts are not expected to be significant and would be less than peak-hour trips compared with the previously existing Mervyn’s office building use. Therefore, the Project’s proposed residential and commercial uses will not have a negative effect on the public health, safety, or general welfare. Specifically, a conditional use permit allowing first-floor residential units has no effect on the public health, safety or general welfare. If analysis demonstrates that public services demands warrant it, the Project proponent would be required to make a one-time payment for such costs or form/be annexed into an existing community facilities district, which through assessments, would pay for needed public safety services.

4. The proposed use is in harmony with the applicable City policies and the intent and purpose of the zoning district involved.

The current General Plan designation of the site is Downtown - City Center / Retail and Office Commercial (CC-ROC). On page C-4 of Appendix C of the General Plan, the Downtown - City Center Area has the following text that explains the unique vision for this area:

“This area is a major activity center in the planning area. It contains major public facilities such as City Center and the Main Library, retail and office areas, and high-density residential areas. Mixed-use development is encouraged to promote the pedestrian orientation and to maintain the downtown area as an integrated living, working, shopping and recreational area. The boundary of this area is delineated in the Downtown Hayward Design Plan.”

Page C-3 of that General Plan appendix lays out the vision for areas with a Retail and Office Commercial land use designation:

“These areas include the regional shopping center (Southland Mall), community shopping centers, concentrations of offices and professional services, and portions of the downtown area and South Hayward BART Station area where mixed retail and office uses are encouraged. Not shown are neighborhood convenience centers that support and are compatible with residential areas.”

One additional section of the General Plan further supports the project as related to City policies:

“Recognize the importance of continuous retail frontage to pedestrian shopping areas by discouraging unwarranted intrusion of other uses that weaken the attractiveness of retail areas; encourage residential and office uses to locate above retail uses.”

These sections of the General Plan indicate the proposed project is consistent with the policies of the General Plan in that the project provides residential use with some commercial use in the Downtown in close proximity to the Downtown BART station. The current development, with the surface parking lot, unoccupied Mervyn’s office building and parking garage, does not create a continuous retail frontage interfacing with the more pedestrian-oriented part of Downtown. It should be noted that this section of Foothill Boulevard is quite different from other sections of Downtown Hayward. Specifically, this section of Foothill is a multi-lane arterial with high-speed, high-volume vehicular traffic that is not very pedestrian-friendly. B Street is considered an example of a more pedestrian-friendly environment with a continuous retail frontage and presence, with lower volumes of traffic traveling at lower speeds in just two lanes.

The zoning designation of the project site is Central City Commercial (CC-C). Allowable permitted uses not requiring a use permit include residential dwelling units above the first floor and a variety of commercial uses (as is proposed at the southeast and northeast corners of the project site). Approval of a conditional use permit (CUP) is required for ground-floor residential

use. Note that ground-floor residential is not outright prohibited by the CC-C regulations.

The purpose of the Central City – Commercial (CC-C) is, “to establish a mix of business and other activities which will enhance the economic vitality of the downtown area. Permitted activities include, but are not limited to, retail, office, service, lodging, entertainment, education, and multi-family residential.”

The project could be viewed as one that adds synergy to Downtown. Adding townhomes along with commercial space would contribute to the goal of Downtown being an active and vibrant area as referenced in the General Plan and Zoning Ordinance. Recent economic studies done for Downtown and this specific project show that this type of project would add to Hayward’s revitalization of Downtown by providing housing to attract middle-income households that would spend disposable income in Downtown. The Project also fulfills the intent and purpose of the CC-C zone by replacing an underutilized site with a vibrant, pedestrian-friendly mixed use development, and as a result, revitalizing the Central City and creating economic stimulus.

Findings for the Vesting Tentative Tract Map - - In order for a vesting tentative map to be approved, seven findings are required to be made. The following text conveys staff’s analysis of the Project under those findings.

1. That the proposed map is consistent with applicable general and specific plans as specified in Section 65451. [Subdivision Map Act §66474(a)]

The proposed subdivision is consistent with the Hayward General Plan and allows a development project that is consistent with allowed uses and densities designated by the “City Commercial – Residential Office Commercial (CC – ROC)” land use category of the General Plan. No Specific Plan applies to the Project.

2. That the design or improvement of the proposed subdivision is consistent with applicable general and specific plans. [Subdivision Map Act §66474(b)]

The proposed subdivision is of a design consistent with the Hayward General Plan in that circulation design and roadways are provided to accommodate the anticipated traffic, and utilities, including water, sewer, and stormdrain facilities, will be provided to accommodate the proposed development. As demonstrated by the project initial study/mitigated negative declaration, the Project will have no significant impacts on aesthetics or land use.

3. That the site is physically suitable for the type of development. [Subdivision Map Act §66474(c)]

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012), which is referenced in the project initial study/mitigated negative declaration, demonstrates that the proposed subdivision would occur on a site suitable for the proposed development.

4. That the site is physically suitable for the proposed density of development. [Subdivision Map Act §66474(d)]

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012) demonstrates that the proposed subdivision would occur on a site suitable for the proposed density, in compliance with the City’s parking, open space, and traffic impact standards.

5. That the design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. [Subdivision Map Act §66474(e)]

The initial study/mitigated negative declaration prepared for the Project demonstrates that substantial adverse environmental damage, including to fish or wildlife and their habitat, would not result from the proposed subdivision, with incorporation of required mitigation measures. Moreover, the Project site has already been fully developed, and as a result, no fish or wildlife habitats exist on the Project site.

6. That the design of the subdivision or type of improvements is not likely to cause serious public health problems. [Subdivision Map Act §66474(f)]

Adequate capacity exists to provide sanitary sewer service to the Project site, nor are air quality impacts to future residents considered significant, as analyzed in the initial study/mitigated negative declaration. The Project also adds housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled, which reduces impacts on air quality and greenhouses gases.

7. That the design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. [Subdivision Map Act §66474(g)]

There are no existing public easements within the boundary of the proposed subdivision, nor are any easements necessary. The Project site is fully developed and currently consists of a 336,000 square foot unused office building and parking facilities, and therefore, there is currently no public access though the property.

CONDITIONS OF APPROVAL

Integral Communities (Applicant/Subdivider)

Conditional Use Permit Application No. PL-2012-0069 and Vesting Tentative Tract Map Application No. PL-2013-0070

Condominium Purposes for the Construction of 194 Townhomes and 16,800 square feet of Commercial Space on an 11.33-acre site located at 22301 Foothill Boulevard

Note: New or revised condition text is shown in **bold underlined italic** font.

General

1. In accordance with Zoning Ordinance §10-1.1520, subject to all conditions listed below, the approval is for the Conditional Use Permit and Vesting Tentative Tract Map Project as shown in the City's Project files as:

Exhibit A – Conditional Use Permit and Vesting Tentative Tract Map,” submitted by Integral Communities, dated September 10, 2013, Sheets T1, TM-1, TM-2, TM-3, TM-4, TM-5, TM-6, TM-7, TM-8, A0.1, A0.2, A2.0, A2.1, A3.0, A4.0, A.TH.1, A.TH.2, A.TH.3, A.TH.4, A.TH.5, A.TH.6, L1, L2, L3, L4, L5, and EXH, and labeled Conditional Use Permit (CUP) No. PL-2012-0069 and Tentative Tract Map No. PL-2013-0070 (TTM 8129).
2. Project approval shall be void two years after issuance of the building permits, or three years after approval of the conditional use permit and vesting tentative tract map applications, whichever is later, unless the construction authorized by the building permits has been substantially completed or substantial sums have been expended in reliance upon the project approval.
3. This approval is subject to the Mitigation Monitoring and Reporting Program included in the City's Project files as Exhibit B.
4. The developer/subdivider shall assume the defense of and shall pay on behalf of and hold harmless the City, its officers, employees, volunteers and agents from and against any or all loss, liability, expense, claim costs, suits and damages of every kind, nature and description directly or indirectly arising from the performance and action of this permit.

PRIOR TO SUBMITTAL OF IMPROVEMENT PLANS AND FINAL MAP

5. The applicant shall include the location of the public access easement to be located adjacent to the San Lorenzo Creek. This easement area shall be wide enough to incorporate some landscape area and a **ten (10)** foot wide pedestrian bicycle pathway within the easement area. **Should there be any areas that cannot accommodate the full ten (10) foot path width, the applicant will be allowed to narrow such areas to eight (8) feet, as approved by the Development Services Director. The ten (10) foot wide path shall extent from Hazel Avenue to City Center Drive.** All details related to the **dedicated** public access easement shall be included with the final map for the project. The final map shall be accepted by the City Council once all conditions have been met.

6. Unless otherwise stated, all necessary easements shall be dedicated, and all improvements shall be designed and installed, at no cost to the City of Hayward.
7. Unless indicated otherwise, the design for development shall comply with the following:
 - a) All improvements shall be designed and constructed in accordance with the City of Hayward Municipal Code – Chapter 10, Articles 1 and 3, and Standard Specifications and Details.
 - b) All construction shall meet the California Building Codes (CBC) and all applicable City of Hayward Building Codes and amendments, including Green Building standards.
 - c) Design and construction of all pertinent life safety and fire protection systems shall meet the California Fire Code and all applicable City of Hayward Fire Codes and amendments.
8. A Registered Civil Engineer shall prepare all Civil Engineering improvement plans, and a Licensed Architect shall prepare all architectural plans, unless otherwise indicated herein.

Subdivision Improvement Plans

9. The subdivider shall also submit proposed subdivision improvement plans and Final Map that are in substantial compliance with the approved Vesting Tentative Tract Map. Said plans and map shall meet all City standards and submittal requirements. The following information shall be submitted with or in conjunction with improvement plans and final map:
 - a. A detailed drainage plan, to be approved by the Alameda County Flood Control and Water Conservation District (ACFC&WCD) and the City Engineer, designing all on-site drainage facilities to accommodate the runoff associated with a ten (10) year storm and incorporating onsite storm water detention measures sufficient to reduce the peak runoff to a level that will not cause capacity of downstream channels to be exceeded. Existing offsite drainage patterns, i.e., tributary areas, drainage amount and velocity shall not be altered by the development. The detailed drainage plan shall be approved by the City Engineer and if necessary, the ACFC&WCD prior to issuance of any construction or grading permit.
 - b. A detailed Stormwater Treatment Plan and supporting documents, following City ordinances and conforming to Regional Water Quality Control Board's "Staff recommendation for new and redevelopment controls for storm water programs."

Final Tract Map

10. Prior to recordation, a proposed Final Tract Map shall be submitted for review by the City. The Final Tract Map shall be presented to the City Council for review and action. The City Council meeting will be scheduled approximately sixty (60) days after the Final Map is deemed technically correct, and Subdivision Improvement Plans with supporting documents, reports and agreements are approved by the City. Executed Final Map shall be returned to the City Public Works Department if Final Map has not been filed in the County Recorder's Office within ninety (90) days from the date of City Council's approval.
11. One Final Map shall be filed for the proposed Vesting Tentative Tract Map pursuant to the Government Code 66452.6(a) (1). The Developer/Applicant shall submit a proposed construction phasing and scheduling for the installation of improvements prior to the approval of Final Map.

12. Prior to issuance of a building permit for the project, the developer/subdivider shall submit expected and/or revised sales price information for all residential components of the project. The Applicants estimated pricing for the town homes based on current market condition ranges from approximately \$518,000 to \$608,000. However, pricing will ultimately be governed by market conditions. **Higher income households may generally be in the range of an average annual income of \$133,600. Households meeting this income criteria contribute to meeting the City's goal to have for diverse housing. Such information and documentation, shall include, but not be limited to, construction details and standard specifications that show that all residential units will employ high quality materials and finishes, including for the condominiums/apartments a variety of on-site amenities for all residents, and that each residential unit will incorporate the highest quality construction that caters to executive or higher income households. This information shall be submitted to the Development Services Department for review, consideration and approval.**
13. **Prior to approval of the Final Map, an Inclusionary Housing Agreement (IHA) shall be submitted and approved by the Planning Director related to providing affordable housing units. The Inclusionary Housing Agreement shall conform to the requirements of the City's Inclusionary Housing Ordinance, including possibly the option of paying required in-lieu fees pursuant to the ordinance.**
14. Prior to the recordation of the Final Tract Map, all documents that need to be recorded with the final map shall be approved by the City Engineer and any unpaid invoices or other outstanding charges accrued to the City for the processing of the subdivision application shall be paid.
15. The final map shall reflect all easements needed to accommodate the project development. The private streets and alleys shall be designated as a Public Utility Easement (PUE), Public Assess Easement (PAE), Water Line Easement (WLS), Sanitary Sewer Easement (SSE), and Emergency Vehicle Access Easement (EVAE).
16. The final map shall reflect dedication of a strip of land approximately 9.4-foot wide, and a request for quit claim of approximately 5-foot wide along Foothill Boulevard frontage., and dedication of a strip of land 0.5-foot wide as right-of-way, and 9.5-foot wide as Public Utilities, Sidewalk and Access Easement (PUS and PUE) encompassing a 5-foot wide sidewalk and 4.5-foot wide planter strip along City Center Drive frontage.

Planning Division

17. Any proposal for alterations to the proposed site plan and/or design, which does not require a variance to any zoning ordinance standard, must be approved by the Development Services Director or his/her designee, prior to implementation.
18. **The applicant shall provide evidence that some townhome first floor plans be design to be a flexible living space, specifically that the space could have a bedroom, a bathroom and/or a kitchenette. This first floor space shall be designed to the residents of that particular space could age in place. Final design details of the space shall be reviewed and approved by the Development Services Director amd Building Official.**

19. Pursuant to the Central City – Commercial (CC-C) zoning regulations Section 10-1.1555 (Minimum Design and Performance Standards for CC-C, CC-R and CC-P Subdistricts), all projects that contain multiple-family dwellings shall apply all Minimum Design Criteria and Performance Standards contained in the RH District and all applicable criteria and standards relating to multiple-family dwellings contained in the Minimum Design and Performance Standards for CC-C, CC-R and CC-P Subdistricts. All applicable sections of the building code shall apply to construction of all multiple-family dwelling units including code sections related to ingress and egress requirements, fire code separation standards, and electrical and plumbing requirements. Any attached second dwelling units shall comply with all standards for such units as outlined in Section 10-1.545q. At no time shall the maximum density for the site be exceeded. If such space is not properly permitted as an attached second dwelling unit, all townhome floor plans with ground floor den/game rooms, multi-purpose rooms, “tech” rooms or living suites shall not have either kitchen facilities, 220 watt power, gas lines installed or any other way to be turned into a separate living space.
20. As a prominent design feature of each residential townhome, specific details related to all windows shall be reviewed and approved by the Development Services Director or his or her designee prior to issuance of a building permit for the project. Detailed plans and specifications for each window, awnings, shutters and other window details (window trim, etc.) shall be included for review, consideration and approval.
21. The applicant shall submit final plans and specifications of all proposed roofing material uses on the residential component of the project for review and approval by the Development Services Director of his or her designee. Roofing materials for all residential structures shall consist of varying materials and colors.
22. Prior to issuance of a building permit for the project, all exterior paint colors shall be reviewed and approved by the Development Services Director and at no time shall any of the residential units be painted pink, orange or purple on any exterior elevation.
23. The applicant shall submit development plans for the site that clearly show site amenities for the townhome residents. The applicant shall make every attempt to evenly disburse project amenities throughout the development site. A final site amenity plan shall be reviewed and approved by the Development Services Director prior to issuance of a building permit for the project.
24. The applicant shall make an effort to work with AC Transit to locate a bus stop along one of the project frontages. These frontages are defined as Foothill Boulevard, Hazel Avenue and City Center Drive.
25. All commercial signage shall conform to Section 10-1.1555(q) of the Zoning Ordinance and Chapter 10 Article 7 of the Hayward Municipal Code.
26. All uses located in the 16,800 square feet of commercial space located adjacent to Foothill Boulevard shall conform to Zoning Ordinance Section 10-1.1522: CC-C Permitted Uses.
27. The applicant shall work with the City’s Landscape Architect and City Engineer to allow for the large bio-retention area located adjacent to San Lorenzo Creek to be used for a usable open space

area for project residents and trail users. The main function of the large bio-retention area is to collect water during rainstorm events where water is filtered back into the ground water ecosystem. This large bio-retention area is excluded from the group open space required on the project site.

28. The applicant or property-owners' association shall maintain all fencing, parking surfaces, common landscaping, lighting, trash enclosures, drainage facilities, project signs, exterior building elevations, etc. The CC&Rs shall include provisions as to a reasonable time period that the building shall be repainted, the limitations of work (modifications) allowed on the exterior of the buildings, and its power to review changes proposed on a building exterior and its color scheme, and the right of the property-owners' association to have necessary work done and to place a lien upon the property if maintenance and repair of the unit is not executed within a specified time frame. The premises shall be kept clean.
29. Any satellite dishes for retail use shall be located as near as possible to the center of roofs to limit visibility from the ground.
30. The residents shall not use parking spaces for storage of recreational vehicles, camper shells, boats or trailers. These parking spaces shall be monitored by the property-owners' association. The property-owners' association shall remove vehicles parked contrary to this provision. The developer shall include in the CC&Rs authority to tow illegally-parked vehicles.

Landscape

31. Both property owners' associations shall maintain the common area landscaping in a healthy, weed-free condition at all times, and the irrigation system with efficient irrigation water management practices to provide uniform distribution, reduce runoff and promote surface filtration. The landscape maintenance practices shall minimize the use of fertilizers and pesticides that can contribute to runoff pollution. Minimum three inches of organic recycled chipped wood mulch shall be maintained at all times. The owner's representative shall inspect the landscaping on a monthly basis and any dead or dying plants (plants that exhibit over thirty percent dieback) shall be replaced within ten days of the inspection. All trees planted by the developer are "Protected Trees" in accordance with the City's Tree Preservation Ordinance. A tree removal and a pruning permit are required prior to removal and pruning of all Protected Tree. All removed trees shall be replaced in accordance with the City's Tree Preservation Ordinance. Trees shall not be severely pruned, topped or pollarded. Any trees that are pruned in this manner shall be replaced with a tree species selected by, and size determined by the City Landscape Architect, within the timeframe established by the City and pursuant to the Municipal Code.

Storm Water Quality Requirements

32. The following materials related to the Storm water quality treatment facility requirements shall be submitted with improvement plans and/or grading permit application:
 - a) A Stormwater Treatment Measures Maintenance Agreement shall be submitted to Public Works - Engineering and Transportation Department staff for review and approval. Once

approved, the Maintenance Agreement shall be recorded with the Alameda County Recorder's Office to ensure that the maintenance is bound to the property in perpetuity.

- b) A Storm Water Pollution Prevention Plan (SWPPP) shall be submitted with a design to reduce discharge of pollutants and sediments into the downstream storm drain system. The plan shall meet the approval of the City Engineer.
- c) Before commencing any grading or construction activities at the project site, the developer shall obtain a National Pollutant Discharge Elimination System (NPDES) permit and provide evidence of filing of a Notice of Intent (NOI) with the State Water Resources Control Board.
- d) The project plans shall include the storm drain design in compliance with post-construction stormwater requirements to provide treatment of the stormwater according to the National Pollutant Discharge Elimination System (NPDES) permit's numeric criteria. The design shall comply with the C.3 established thresholds and shall incorporate measures to minimize pollutants to the maximum extent practicable (MEP).
- e) The project plans shall identify Best Management Practices (BMPs) appropriate to the uses conducted on-site to effectively prevent the entry of pollutants into storm water runoff. Roof leaders and direct runoff shall discharge into a landscaped area or a bioretention area prior to stormwater runoff entering an underground pipe system.
- f) The proposed BMPs shall be designed to comply with the hydraulic sizing criteria listed in Provision C.3 of the Alameda County Clean Water Program (ACCWP) NPDES permit.
- g) Landscaping shall be designed with efficient irrigation to reduce runoff, promote surface infiltration, and minimize the use of fertilizers and pesticides that can contribute to stormwater pollution. Where feasible, as determined by the City Engineer and Landscape Architect, landscaping should be designed and operated to treat stormwater runoff. Landscaping shall also comply with the City's "water efficient landscape ordinance."
- h) The bioretention treatment area shall be designed using a Bioretention Soil Mix (BSM) per Attachment L of the C.3 Technical Guidance dated May 14, 2013, with a minimum infiltration rate of 5 inches per hour. The proposed bioretention area shall not be used as a turf play field and shall have a decorative fence along the inside perimeter of the meandering sidewalk.
- i) The following documents pursuant to the Cleanwater Program requirements:
 - i. Hydromodification Management Worksheet;
 - ii. Infiltration/Rainwater Harvesting and Use Feasibility Screening Worksheet;
 - iii. Development and Building Application Information Impervious Surface Form;
 - iv. Project Applicant Checklist of Stormwater Requirements for Development Projects;
 - v. C.3 and C.6 Data Collection Form; and,
 - vi. Numeric Sizing Criteria used for stormwater treatment (Calculations).

33. The subdivider is responsible for ensuring that all contractors are aware of all storm water quality measures and implement such measures. Failure to comply with the approved construction BMPs will result in the issuance of correction notices, citations or a project stop order.

Public Streets: (Foothill Boulevard, Hazel Avenue and City Center Drive)

34. Improvements for public streets shall incorporate the following:

- a) The design and locations of street approaches including pedestrian ramps shall be approved by the City Engineer. Pedestrian ramps shall be installed at all street intersections and as where required by the City.

- b) The subdivider shall remove and replace any damaged and/or broken sidewalk associated with project demolition and construction, as determined by the City.
- c) The subdivider shall install additional LED illuminated street lights along Hazel Avenue and City Center Drive, of a design identical to the existing lights installed as part of the Route 238 Corridor Improvement Project improvements, at locations approved by the City Engineer. These new street lights shall be part of the City lighting system.
- d) The proposed project entrances off Foothill Boulevard, City Center Drive, and Hazel Avenue shall conform to the City Standard SD-110A and be enhanced with at least ten feet of raised decorative paving (e.g., interlocking pavers or stamped colored concrete, or bands of decorative paving, etc.). The Planning Director shall approve the material, color and design, and the City Engineer shall approve the pavement section for the decorative paving. Decorative pavements shall be capable of supporting a 75,000 lb. GVW load per Fire Department's requirement. Modifications to these requirements, however, may be made when documented by a geotechnical study providing alternative specifications which are necessary to construct and maintain the site in a safe and stable condition.
- e) Foothill Boulevard is on moratorium for planned work involving pavement cuts. If the applicant finds it necessary to cut into Foothill Boulevard to provide utility services and/or street improvements required for development, Foothill Boulevard pavement sections shall be reconstructed with a minimum of two inches of Hot Mix Asphalt (HMA) pavement after the installation of the proposed water main, and fire and irrigation service lines. The limits of pavement reconstruction shall be determined by the City Engineer.
- f) Existing street improvements along the City Center Drive project frontage shall be removed and replaced with a new five-foot wide Portland Cement Concrete sidewalk behind the planter strip and a minimum 4.5-foot wide planter strip behind the curb.
- g) Existing Portland Cement Concrete improvements on Hazel Avenue along the project frontage shall be removed and replaced with a five-foot wide sidewalk adjacent to the property line and a minimum 4.5-foot wide planter strip behind the curb.
- h) Raised medians shall be installed on Hazel Avenue to prohibit left-turn movements from the project site onto Hazel Avenue in a southbound direction. The design and location of such medians shall be approved by the City Engineer and Fire Chief.
- i) Existing pavement section along the Hazel Avenue project frontage shall be reconstructed with a minimum of two inches of Hot Mix Asphalt (HMA) pavement to the lane line.

Private Streets and Alleys

35. Improvements for private streets and alleys shall incorporate the following:

- a) Proposed Street 'A' and Foothill Boulevard intersection shall be redesigned to accommodate truck turning movements (ingress to and egress from retail parking areas.) The redesign shall be approved by the Fire Chief and City Engineer.
- b) Proposed private street and alley improvements and modifications shall be designed and approved by the Fire Chief and the City Engineer prior to the approval of the Final Map.
- c) Pavement Sections for proposed private street and alley improvements shall be designed with a Traffic Index (TI) of five and minimum Asphalt Concrete (AC) thickness of four inches.
- d) The minimum pavement width of "B" Street on the project site shall be twenty-two (22) feet, unless a lesser width is approved by the City Engineer and Fire Marshal.

- e) Except for designated open parking spaces, no curbside parking shall be allowed. “No Parking Fire Lane” (T29 – ‘No Parking Sign’ in a specific industry format) signs shall be installed and curbs shall be painted red in locations approved by the Fire Chief and City Engineer.
- f) The interior intersections shall be designed to meet Fire Department access and turning movements. Pedestrian ramps shall be installed to facilitate access and circulation throughout the development.
- g) L.E.D. luminaire lights shall be installed within the development and proposed walkway along San Lorenzo Creek. Locations and design shall be approved by the City Engineer and Planning Director.

Storm Drainage

36. Improvements for storm drain systems shall incorporate the following:

- a) The proposed realignment of the existing storm drain in Foothill Boulevard upstream of the subdivision shall not create adverse impacts to the existing upstream drainage system.
- b) The locations and design of storm drains shall meet the City’s standard design and be approved by the City Engineer and if necessary, the Alameda County Flood Control and Water Conservation District (ACFC&WCD). Any alternative design shall be approved by the City Engineer prior to installation.
- c) Storm drain pipes in streets and alleys shall be a minimum of twelve inches in diameter with a minimum cover of three feet over the pipe.
- d) The latest edition of the Alameda County Flood Control and Water Conservation District’s Hydrology and Hydraulics Criteria Summary shall be used to determine storm drainage runoff. A detailed grading and drainage plan with supporting calculations and a completed Drainage Review Checklist shall be submitted, which shall meet the approval of the Alameda County Flood Control and Water Conservation District (ACFC&WCD) and the City. Development of this site shall not augment runoff to the ACFC&WCD’s downstream flood control facilities. The hydrology calculations shall substantiate that there will be no net increases in the quantity of runoff from the site versus the flow rate derived from the original design of downstream facilities. If there is augmented project-generated runoff, off-site and/or on-site mitigation shall be provided.
- e) The project shall not block runoff from, or augment runoff to, adjacent properties. The drainage area map developed for the project hydrology design shall clearly indicate all areas tributary to the project area. The developer is required to mitigate unavoidable augmented runoffs with offsite and/or on-site improvements.
- f) No surface runoff is allowed to flow over the sidewalks and/or driveways. Area drains shall be installed behind the sidewalks to collect all runoff from the project site.
- g) All storm drain inlets must be labeled "No Dumping - Drains to Bay," using City-approved methods. Refer to City Standard SD-401A.
- h) An encroachment permit from ACFC&WCD is required for any modification and/or alteration of the existing outfall structures or connections to San Lorenzo Creek, or any work within District right-of-way and facilities. All workmanship, equipment, and materials shall conform to ACFC &WCD standards and specifications.
- i) The starting water surface elevation(s) for the proposed project’s hydraulic calculations and the corresponding determination of grate/rim elevations for all the on-site storm drainage

structures shall be based on Federal Emergency Management Agency's Flood Insurance Study for the 100-year storm event.

- j) Post-development flows should not exceed the existing flows. If the proposed development warrants a higher runoff coefficient or will generate greater flow, mitigation measures shall be implemented.

Sanitary Sewer System

- 37. The proposed sewer services shall be approved by the Oro Loma Sanitary District (OLSD), the utility purveyor for the project development.

Water System

- 38. The proposed water services shall be approved by the East Bay Municipal Utility District (EBMUD), the utility purveyor for the project development.

Fire Protection

- 39. A fire flow shall be provided in accordance with the 2010 California Fire Code Table B105.1 based on the construction type and building area when building exceeding 3,600 square feet. A fire flow reduction of up to 50 percent is allowed when the building is provided with automatic sprinkler system in accordance with NFPA 13. The resulting fire flow shall not be less than 1,500gpm.
- 40. The minimum number of fire hydrants shall be provided in accordance with the Hayward Fire Code Ordinance and the 2010 California Fire Code Table C105.1. The average spacing between hydrants is 300 feet. Any portion of the building or facility shall be within 400 feet of a fire hydrant. Spacing and locations of fire hydrants shall be subject to review and approval by the Hayward Fire Department.

All new fire hydrants shall be double steamer type, equipped with (2) 4-1/2" outlets and (1) 2-1/2" outlet. The capacity of each individual hydrant shall be 1,500 GPM. Vehicular protection may be required for the fire hydrants. Blue reflective fire hydrant blue dot markers shall be installed on the roadways indicating the location of the fire hydrants. Blue reflective pavement markers shall be installed at fire hydrant locations.

A fire apparatus access road 20 feet to 26 feet wide shall be posted on both sides as fire lanes; a fire apparatus access road 26 feet to 32 feet wide shall be posted on one side of the road as a fire lane. "No Parking" signs along fire lanes shall be installed and shall meet the City of Hayward Fire Department fire lane requirements.

Other Utilities

- 41. All service to dwellings shall be an "underground service" designed and installed in accordance with the Pacific Gas and Electric Company, AT&T (phone) Company and local cable company regulations. All facilities necessary to provide service to the dwellings, including transformers and switchgear, shall also be undergrounded.

42. All electric system, including transformers, shall be installed underground within the development. Design and installation shall be in accordance with Pacific Gas and Electric Company regulations.
43. The joint trench design and location shall meet the approval of the City Engineer.
44. All surface-mounted hardware (fire hydrants, electroliers, etc.) along the private streets and driveways shall be located outside of the sidewalk within the Public Utility Easement in accordance with the requirements of the City Engineer or, where applicable, the Hayward Fire Chief.
45. The developer/subdivider shall provide and install appropriate facilities such as conduit, junction boxes, individual stub-outs, etc., to allow for future installation of a City-owned and maintained fiber optic network within the subdivision.

PRIOR TO ISSUANCE OF BUILDING OR GRADING PERMITS

Planning Division

46. Prior to issuance of building permits, a final map that reflects and is in substantial compliance with the approved vesting tentative tract map, shall be approved by the City Engineer and filed in the office of the Alameda County Recorder.
47. Submit the following documents for review and approval, or for City project records/files:
 - a. Copy of the Notice of Intent filed with State Water Resources Control Board;
 - b. Engineer's estimate of costs, including landscape improvements;
 - c. Signed Final Map;
 - d. Signed Subdivision Agreement; and
 - e. Subdivision bonds.
48. Pursuant to the Municipal Code §10-3.332, the developer shall execute a subdivision agreement and post bonds with the City that shall secure the construction of the public improvements. Insurance shall be provided per the terms of the subdivision agreement.
49. Pursuant to the City of Hayward Design Guidelines, exposed or visible retaining walls shall be a maximum of six (6) feet in height. Walls abutting a public street shall be provided a ten (10) foot wide landscape area in front of the walls. Any retaining wall over the maximum six (6) foot height limit shall be screened with vegetation that is irrigated. All plan details associated with the retaining wall screening shall be reviewed and approved by the Development Services Director prior to issuance of a building permit for any retaining wall structure over six (6) feet in height.
50. All final exterior building finishes, paint colors and other architectural details shall be reviewed and approved by the Planning Division in accordance with the City of Hayward's Design Guidelines prior to issuance of a building permit for the project.
51. ***The applicant shall submit revised rear elevation drawings and details of each commercial buildings that clearly shows these rear elevations as having more articulation, architecturally***

broken up and/or architecturally treated to be more interesting as this will be in the view shed of some residents. These revised rear elevation drawings and details shall be reviewed and approved by the Planning Director prior to issuance of a Building Permit for the project.

52. **The project and units shall be green point rated and obtain a green point rating score of at least 100, as confirmed by an independent qualified green point rater. Also,** the following green building features shall be incorporated into the final project design: water efficient landscaping, use of engineered lumber, high efficiency shower heads, efficient bathroom fixtures and kitchen faucets, energy star appliances, high efficiency HVAC systems, use of low-voc paints, and installation of carbon monoxide detectors. All final green building details shall be reviewed and approved by the Planning and Building Divisions prior to issuance of building permits for the project. **The applicant shall offer solar as an optional feature for each townhome style condominium.**

53. Plans for building permit applications shall incorporate the following:

- a. A copy of these conditions of approval shall be included on a full-sized sheet(s) in the plan set.
- b. A lighting plan prepared by a qualified illumination engineer shall be included to show exterior lighting design. All exterior and parking lot lighting shall be provided in accordance with the Security Standards Ordinance (No. 90-26 C.S.) and be designed by a qualified lighting designer and erected and maintained so that light is confined to the property and will not cast direct light or glare upon adjacent properties or public rights-of-way. Such lighting shall also be designed such that it is decorative and in keeping with the design of the development. Exterior lighting shall be erected and maintained so that adequate lighting is provided in all common areas. The Planning Director or his/her designee shall approve the design and location of lighting fixtures, which shall reflect the architectural style of the buildings. Exterior lighting shall be shielded and deflected away from neighboring properties and from windows of proposed buildings.
- c. Plans shall show that all utilities will be installed underground.
- d. Each townhome dwelling unit shall be provided a minimum of 90 cubic feet of dedicated storage area, accessible from the exterior of the unit.

54. Prior to issuance of building permits:

- a. Documentation including, but not limited to, Covenants, Codes and Restrictions (CC&Rs) shall be recorded to establish the living units and the retail space(s) as condominiums. Before recordation, the CC&Rs shall be submitted to the City Attorney and Planning Director for review and approval.
- b. The developer shall submit a soils investigation report to the satisfaction of the City Engineer.

~~55. **(Condition to be deleted.)** Prior to submittal of building permit applications and plans, the developer/subdivider shall submit information showing that all proposed residential units will attract higher income households. Higher income households has been defined as having an average annual income of \$133,600. Such information and documentation, shall include, but not be limited to, construction details and standard specifications that show that all residential units will employ high quality materials and finishes, including for the condominiums/apartments a~~

~~variety of on-site amenities for all residents, and that each residential unit will incorporate the highest quality construction that caters to executive or higher income households. This information shall be submitted to the Development Services Department for review, consideration and approval.~~

54. Prior to issuance of a building permit for the project, the developer/subdivider shall submit expected and/or revised sales price information for all residential components of the project. Pricing for the townhomes range from \$518,000 to \$608,000. This information shall be reviewed and considered by the Development Services Department.
55. ~~(Condition to be deleted.)~~ The applicant shall include 15 additional on-site open uncovered parking spaces for the residential component of the project, or obtain approval of a variance, exception or some other resolution to address on-site parking deficiency, in accordance with City regulations, prior to issuance of a building permit for the Project.
55. The applicant shall provide a designated loading area(s) for the commercial buildings. The number and location for such areas shall be determined by the Development Services Director. All loading areas shall be designed to be visually-screened loading area(s) for the commercial component of the project. Details involving all loading areas shall be reviewed and approved by the Planning Division prior to issuance of a building permit for the Project.
56. **Mitigation Measure 4:** Prior to issuance of a Building Permit, the applicant shall conduct acoustical analysis by a qualified consultant to ensure that indoor or outdoor noise levels of each new residential unit does not that exceed the standards contained in Appendices M and N of the City of Hayward General Plan. If those standards are exceeded, the design of the units should incorporate sound attenuation features that are to be in accordance with the consultant's and/or architect's recommendations and be confirmed via actual readings prior to project finalization and/or Certificates of Occupancy for units.

Landscape

57. Prior to the approval of improvement plans or issuance of the first building permit, detailed landscape and irrigation plans shall be reviewed and approved by the City and shall be a part of approved improvement plans and the building permit submittal. The plans shall be prepared by a licensed landscape architect on an accurately surveyed base plan and shall comply with the City's *Design Guidelines, Bay-Friendly Water Efficient Landscape Ordinance, Hayward Environmentally Friendly Landscape Guidelines and Checklist for the landscape professional, and Municipal Codes*. Dripline of the existing trees to be saved shall be shown on the plan.
58. **A mylar of the approved landscape and irrigation improvement plans shall be submitted to the Public Works Department. The size of Mylar shall be twenty-four inches by thirty-six inches without an exception. A four-inch by four-inch blank signing block shall be provided in the low right side on each sheet of Mylar. The signing block shall contain two signature lines and dates for City of Hayward City Engineer and City Landscape Architect.**
59. A tree mitigation plan shall be submitted that identifies those trees to be removed and those that will remain, total dollar amount of mitigation and proposed mitigation trees with sizes and values.

Mitigation trees to offset the loss of removed trees shall be provided above and beyond trees required to comply with the City's standards for new development. All removed trees shall be mitigated by replacing them with new trees that are equal in value to removed trees, as established in the approved certified arborist's report. A bond will be required for all trees that are to remain or be relocated. Any trees that are removed or damaged during construction shall be replaced with trees of equal size and equal value.

60. A tree removal permit will be required for all trees that are to be removed, which can be obtained from the City Landscape Architect prior to site demolition.
61. Pedestrian Circulation and Experience: Adequate landscape buffers that meet the City's minimum design standards shall be provided for all walkways, including walkways to residential entrances located next to property lines, especially in regards to reducing visual impacts associated with the adjacent service station property. On-site retail uses shall have a landscape-enhanced pedestrian connection with the residential component of the development, to be approved by the City's Landscape Architect, in order to promote a safe pedestrian-oriented environment/village. The overall pedestrian-oriented experience shall be enhanced with sequencing of spaces in conjunction with walkways that avoids long stretches of sameness and overly large or lineal spaces, with focal elements and site enhancement to be provided offering places to rest and converse with visual interest, to be approved by the City's Landscape Architect.
62. Bicycle Path: A bicycle/pedestrian pathway shall be provided along San Lorenzo Creek.
63. Pedestrian Circulation for Service: Clear path of travel for using communal trash and recycling receptacles shall be provided.
64. Landscaping Plans shall incorporate the following:
- a) All submitted plans shall be in scale, and shall be provided with written and graphic scale.
 - b) Base Information: All underground utility information including water, storm drain, sewer, vaults and transformers in planting areas shall be provided in landscape plans to avoid conflicts with proposed tree planting.
 - c) Project data and associated calculations: Shall be provided on plan sheets with the following information: total project area, total irrigated landscape area, required private open space and provided private open space, required group open space and provided group open space, and Maximum Applied Water Allowance (MAWA).
 - d) Required and Proposed Landscape Setback: All setback dimensions shall be clearly provided on the plan. Pedestrian walkways and sidewalk shall not be encroached from proposed vehicular overhangs or required vehicular backup space. Vehicular back up or driveway or structure shall not abut walkways or sidewalks.
 - e) Public Sidewalk: Shall provide unobstructed width at all times in compliance with Americans with Disabilities Act.
 - f) Required Minimum Planting Area Dimension: Minimum planting area dimension shall be five feet measured from back of hardscape to back of hardscape. Hardscape shall include curb, paving, and structure.

- g) Underground Utilities: Locations and layout of all underground utilities lines, boxes and vaults shall be provided as base information on planting plans to minimize conflict with tree planting.
- h) Fire Hydrants: The City Standard Detail requires fire hydrants to be located on a six feet by six feet concrete pad. The minimum clearance for tree planting is seven feet from the edge of fire hydrants, not from the edge of the concrete pad. The actual size of the pad shall be shown on the planting plans.
- i) Required Street Tree: Per City standards, one twenty-four-inch-box street tree is required for every twenty to forty feet of street frontage within the public right-of-way planting strip or along the following public street frontages: ‘A’, ‘B’, ‘C’, ‘D’, ‘E’, ‘F’, ‘G’ and ‘H’ Streets.
- j) Required Private Front Yard Tree: One twenty-four-inch street tree is required for every unit; no unit should be without a tree, except where there are utilities that are located in the proposed planting location. Alternate tree locations shall be reviewed and considered by the Planning Division.
- k) Required Screening Tree: One fifteen-gallon evergreen tree at every twenty feet on center, or an equal/similar tree species approved by the City’s Landscape Architect, shall be planted in the setback area along those abutting property lines.
- l) Landscape Buffer: Different landscape buffer zones shall be established based on the adjacent use and site conditions such as public streets, alleys, neighboring commercial/retail and the Alameda County Flood Control and Water Conservation District’s concrete channel. A landscape buffer shall be provided between the flood control channel property line and the public pedestrian and bicycle pathway abutting it. The bicycle and pedestrian pathway along San Lorenzo Creek shall be interrupted with pockets of varying scale spaces to enhance the experience to be approved by the City’s Landscape Architect.
- m) Required Screening of Above-Ground Utilities including Trash Enclosures and Gas Station Pump Stations: Above ground utilities (e.g. gas or electric meters, backflow devices) and trash enclosures shall be located from public/street view, and shall be screened with trees, shrubs, groundcovers and vines on all three sides except the side where access is located.
- n) Required Parking Shade Tree: Parking areas shall include a minimum of one fifteen-gallon parking lot tree for every six parking stalls. Each parking bay shall end with endcap islands at both ends.
- o) Group Open Space and Site Amenities: A minimum one hundred square feet per unit shall be utilized for group open space. Each group open space shall be identified and square footage of each space shall be provided on building permit application plans. Group open space shall not be counted toward meeting the requirement where the noise level exceeds Ldn levels over sixty-five decibels (db), or where site gradient exceeds five percent slope. Group open space shall be centrally located for all residents and shall be visible. Group open space shall not include the required bio-retention areas, setback areas along the front, side and rear of the property.
- p) C.3 Stormwater Treatment in Landscape Areas:
- i. A minimum twelve-inch-wide leveled landscape area shall be provided around bio-treatment areas located adjacent to hardscape areas such as curbs, sidewalks, walkways and structures. The City will require a matched precipitation rotator type irrigation system on a separate valve for the stormwater treatment area irrigation. All spray irrigation systems shall be set back twenty-four inches from all impervious hardscape edges such as curbs, sidewalks, walkways and structures.
 - ii. Utility boxes and vaults, light fixtures and fire hydrants shall have minimum five feet of clearance from the edge of C.3 Stormwater Treatment areas.

- iii. Landscape areas could be used to comply with the C.3 Stormwater Treatment requirements; however, all tree planting requirements shall apply. A wider landscape area shall be provided if necessary to accommodate both bio-treatment and tree planting.
 - iv. Sod shall not be used in bio-treatment areas.
 - v. Turf shall not be provided unless provided for recreational purposes.
 - vi. Primary stormwater treatment area shall not be used for recreational purposes; therefore it shall not be counted toward meeting group open space requirements. Sandy-Loam soil type with high percolation rate that meets the C.3 Stormwater Treatment requirements is not suited for recreational surface.
- q) Plant Hydrozone shall be provided. *Alnus rhombifolia* and *Sequoia sempervirens* are listed for high water requiring plants in WUCOLS (Water Use Classifications of Landscape Species) , and shall not be grouped with low water requiring plants. WUCOLS listings in Planting Legend shall be verified again.
- r) Trees with invasive and shallow root systems such as *Magnolia grandiflora* shall not be used unless a minimum eight feet by eight feet of planting area can be provided.
- s) Coniferous trees, such as *Pinus canariensis* and *Sequoia sempervirens* shall not be proposed where those trees will block the views as well as sun exposure to the residential units. Those trees shall be replaced with another type of tree(s). These trees are large trees that shed needles, and require plenty of growing room. Plant these trees only where there would be adequate room to accommodate mature growth and natural growth patterns.
- t) All trees shall be planted twenty feet from a corner, a minimum of five feet away from any underground utilities, a minimum of fifteen feet from a light pole, and a minimum thirty feet from the face of a traffic signal, or as otherwise specified by the city. Root barrier shall be provided for all trees that are located within seven feet of paved edges or structure. Trees shall be planted according to the City Standard Detail SD-122.
- u) **Irrigation Meter:** A separate irrigation meter for the commercial development shall be provided from a dedicated irrigation meter(s) for the residential development. The adequate number of irrigation meters for the residential development shall be determined and provided by the developer.
The minimum dimension for all planting areas shall be five feet, including tree wells in parking lots or sidewalks measured from back of curb/paving.
- v) Class B Portland Cement concrete curb shall be constructed to a height of six inches above the adjacent finished pavement when landscape area adjoins driveways or parking areas.

Technical Reports

65. **Mitigation Measure 2:** Prior to issuance of a Building Permit, the applicant shall conduct a design level geotechnical evaluation and submit that for review and approval and any recommendations shall be incorporated into the final design of the project.
66. **Mitigation Measure 3:** All recommendations outlined in a design-level geotechnical investigation shall be incorporated in the final design in order to mitigate for the presence of expansive soils on the project site.

Fire Protection

67. Fire apparatus roads shall have unobstructed width of 26 feet in the immediate vicinity of buildings. At least one of the required access routes shall be located within a minimum of 15 feet and a maximum of 30 feet from the building and shall be positioned parallel to one entire side of the building.
68. Fire apparatus access roads shall be designed and maintained to support 75,000 pounds, the imposed load of fire apparatus, and shall be surfaced so as to provide all-weather driving capability. An unobstructed vertical clearance of not less than 13 feet 6 inches shall be provided for all fire apparatus accesses.
69. The proposed 'Extended Fire Access Area' at turning area/corner of "A Street" shall be designed to meet Fire Department's requirement so that Building TH-11 will be provided with a parallel fire apparatus access.

Dead-end fire apparatus access road in excess of 150 feet in length shall be provided with a turnaround that meets Hayward City standards.

70. Building permit plans shall incorporate the following:
- a) All buildings shall have automatic fire sprinkler systems installed in accordance with NFPA 13. Fire permits are required for sprinkler installation.
 - b) Underground fire service lines shall be installed in accordance with NFPA 24.
 - c) Fire sprinkler monitoring systems should be provided for multi-family residential townhouse buildings in accordance with the California Fire Code and NFPA 72. Each fire sprinkler system riser shall have exterior local alarm bell(s). Interior notification device(s) shall be installed within each residential unit.
 - d) Extinguisher placement shall conform to the California Fire Code.
 - e) Address and premise identification numbers shall be placed on all buildings in such a position as to be plainly visible and legible from the road or street fronting the property. Dimensions of address numbers or letters on the front of buildings shall be approved by the Fire Department.

Hazardous Materials

71. The developer/applicant shall comply to the following:
- a) Contact the Hazardous Materials office at (510) 583-4927 to obtain a Hazardous Materials permit for the removal of the underground fuel storage tank (UST).
 - b) Until such time as the existing underground fuel storage tank (UST) is removed, it shall be properly maintained by the property owner. The owner shall obtain and keep current all conditions of a valid City of Hayward Fire Department Hazardous Materials Consolidated Permit and Underground Storage Tank Operating Permit, including the submittal of all required paperwork, testing results and fees to the City of Hayward Fire Department.
 - c) Removal of the UST will require the submittal of formal work plans to the City of Hayward Fire Dept., Hazardous Materials Division. These plans shall include scope of work, and a site plan showing the physical layout of the facility and locations of UST and existing equipment. In addition, State of California UST forms shall be completed and submitted (State forms A/B/C). The tank shall be properly removed prior to obtaining a grading permit from the City of Hayward Fire Department.

- d) Prior to issuance of Building or Grading Permits, a final clearance shall be obtained from either the California Regional Water Quality Control Board or the Department of Toxic Substance Control and submitted to the Hayward Fire Department. The clearance certificate will ensure that the property meets investigation and cleanup standards for residential development. Allowance may be granted for some grading activities, if necessary, to ensure environmental clearances.
- e) Prior to grading, structures and their contents shall be removed or demolished under permit in an environmentally sensitive manner. Proper evaluation, analysis and disposal of materials shall be done by appropriate professional(s) to ensure that hazards posed to development construction workers, neighbors, the environment, future residents and other persons are mitigated. All hazardous materials and hazardous waste must be properly managed and disposed of in accordance with state, federal and local regulations.
- f) Any wells, septic tank systems and other subsurface structures - including hydraulic lifts for elevators - shall be removed properly in order not to pose a threat to the development construction workers, future residents or the environment. Notification shall be made to the Hayward Fire Department at least 24 hours prior to removal. Removal of these structures shall be documented and done under permit, as required by law.
- g) The Hayward Fire Department's Hazardous Materials Office shall be notified immediately at (510) 583-4910 if hazardous materials or associated structures are discovered during demolition or during grading. These shall include, but shall not be limited to, actual/suspected hazardous materials, underground tanks, or other vessels that contain or may have contained hazardous materials.
- h) During construction, hazardous materials used and hazardous waste generated shall be properly managed and disposed.
- i) Upon completion of construction, the Fire Department will complete a final walk-through inspection. An annual Consolidated Permit for hazardous materials storage may be required for hydraulic elevators, emergency generators, and the operation of general maintenance facilities.

Solid Waste

Applicants must comply with City standards to obtain building permits, as follows:

- 72. Roof Required on Trash Enclosures: Adequate indoor and outdoor storage space for recyclables is required by state law (California Public Resources Code 42910-42912 and Hayward Municipal Code 5-1.27). Federal provisions require a roof on all outdoor trash enclosures (Federal Clean Water Act).
- 73. Residential Collection of Garbage and Recyclables from Townhomes: All residential property owners are required to arrange for weekly collection of recyclables.

The four cubic-yard bins in each enclosure are appropriate for collection of trash and recyclables. However, none of the enclosures includes an interior curb to protect the walls of each enclosure from the metal bins, nor is there a divider to secure each bin in their respective location, as is required and further described below. To deter illegal dumping, a gate on each enclosure is required.

The locations of the enclosures require residents to transport their trash and recyclables for as much as 360 feet. As an alternative, staff recommends providing townhome residents with separate carts for garbage, recyclables and organics (i.e., food scraps, food-soiled paper) that can be stored in each resident's garage. The carts could be placed in front of each garage and serviced weekly by Waste Management. The enclosures accommodate recyclables and trash, not organics (i.e., food scraps or food-soiled paper). The market value of the property will be better maintained if cart services are provided, rather than bin service.

Enclosure design shall be submitted to the City for review and approval.

74. Access to Trash Enclosures by Residents with Physical Disability: Adequate provisions must be made by the property owner and manager to ensure that all residents, regardless of physical ability, are able to easily dispose of their garbage and recyclables in the bins. Any arrangements required to provide reasonable access to these containers is the sole responsibility of the property owner and manager and shall be included in any Covenants, Codes and Restrictions for the property.
75. Commercial Garbage and Recyclables Collection for Two 8,400 Square Foot Retail Buildings: All commercial properties with four cubic yards or more of weekly trash service are required to arrange for weekly collection of recyclables. The enclosures shown on the site plans are inside each of the two retail buildings. The two 10' x 18' enclosures must be retained. An eight-foot long roll-up door is required, rather than the three-foot length shown, to ensure sufficient access to service the bins for trash, recyclables and organics in each enclosure and due to the 5' wide x 7' long bin dimensions. The largest bin with wheels that will fit in each of the two enclosures is four cubic yards.
76. Collection Vehicle Access
- a) If collection vehicles must enter or exit under a structure, the minimum clearance is 14 feet.
 - b) If gates with locks are planned to limit access to the property, the applicant must provide keys or cards to the service provider, Waste Management of Alameda County (510) 537-5500. Keys and locks may also be obtained from Waste Management for a nominal fee
77. Requirements for Recycling Construction & Demolition Debris: City regulations require that applicants for all construction, demolition, and/or renovation projects, in excess of \$75,000 (or combination of projects at the same address with a cumulative value in excess of \$75,000) must recycle all asphalt and concrete and all other materials generated from the project. Applicants must complete the Construction & Demolition Debris Recycling Statement and obtain signature approval from the City's Solid Waste Manager prior to the issuance of a building permit.

During Construction

78. A Construction and Demolition Debris Recycling Statement must be submitted with the building permit application. A Construction and Demolition Debris Recycling Summary Report must be completed, including weigh tags, at the COMPLETION of the project.

Other Requirements

79. Community Facilities District for Public Services: The developer shall pay the costs of providing public safety services to the project should the project generate the need for additional public safety services. The developer may pay either the net present value of such costs prior to issuance of building permits, or the developer may elect to annex into a special tax district formed by the City and pay such costs in the form of an annual special tax. The developer shall post an initial deposit of \$20,000 with the City prior to submittal of improvement plans to offset the City's cost of analyzing the cost of public safety services to the property and district formation.
80. All utilities shall be designed in accordance with the requirements of the City of Hayward and applicable public agency standards.

PRIOR TO CONSTRUCTION WITH COMBUSTIBLE MATERIALS

81. Required water system improvements shall be completed and operational prior to the start of combustible construction.
82. The developer/subdivider shall be responsible to adhere to all aspects of the approved Storm Water Pollution Prevention Plan (SWPPP) per the aforementioned condition of approval.
83. A representative of the project soils engineer shall be on the site during grading operations and shall perform such testing as deemed necessary by the City Engineer. The representative of the soils engineer shall observe all grading operations and provide any recommended corrective measures to the contractor and the City Engineer.

PRIOR TO COMPLETION OF SITE IMPROVEMENTS

During Construction

84. The developer shall ensure that unpaved construction areas are sprinkled with water as necessary to reduce dust generation. Construction equipment shall be maintained and operated in such a way as to minimize exhaust emissions. If construction activity is postponed, graded or vacant land shall immediately be revegetated.
85. **Mitigation Measure 1**: All diesel powered equipment (≥ 100 horsepower) shall be California Air Resources Board (CARB) Tier 3 Certified or better.
86. Any transformer shall be located underground or screened from view by landscaping and shall be located outside any front or side street yard.
87. In the event that human remains', archaeological resources, prehistoric or historic artifacts are discovered during construction of excavation, the following procedures shall be followed: Construction and/or excavation activities shall cease immediately and the Planning Division shall be notified. A qualified archaeologist shall be retained to determine whether any such materials are significant prior to resuming groundbreaking construction activities. Standardized procedure for evaluation accidental finds and discovery of human remains shall be followed as prescribed in Sections 15064.f and 151236.4 of the California Environmental Quality Act.

88. The following control measures for construction noise, grading and construction activities shall be adhered to, unless otherwise approved by the Planning Director or City Engineer:

- a. Grading and site construction activities shall be limited to the hours 8:00 AM to 5:00 PM Monday through Friday with no work on weekends and Holidays unless revised hours and days are authorized by the City Engineer. Building construction hours are subject to Building Official's approval;
- b. Grading and construction equipment shall be properly muffled;
- c. Unnecessary idling of grading and construction equipment is prohibited;
- d. Stationary noise-generating construction equipment, such as compressors, shall be located as far as practical from occupied residential housing units;
- e. Applicant/developer shall designate a "noise disturbance coordinator" who will be responsible for responding to any local complaints about construction noise. Letters shall be mailed to surrounding property owners and residents within 300 feet of the project boundary with this information.
- f. The developer shall post the property with signs that shall indicate the names and phone number of individuals who may be contacted, including those of staff at the Bay Area Air Quality Management District, when occupants of adjacent residences find that construction is creating excessive dust or odors, or is otherwise objectionable. Letters shall also be mailed to surrounding property owners and residents with this information prior to commencement of construction.
- g. The developer shall participate in the City's recycling program during construction;
- h. Daily clean-up of trash and debris shall occur on City Center Drive, Hazel Avenue and Foothill Boulevard and other neighborhood streets utilized by construction equipment or vehicles making deliveries.
- i. The site shall be watered twice daily during site grading and earth removal work, or at other times as may be needed to control dust emissions;
- j. All grading and earth removal work shall follow remediation plan requirements, if soil contamination is found to exist on the site;
- k. Pave, apply water three times daily, or apply (non-toxic) soil stabilizers on all unpaved access roads, parking areas and staging areas at construction sites;
- l. Sweep daily (with water sweepers) all paved access roads, parking areas and staging areas at construction sites;
- m. Sweep public streets daily if visible soil material is carried onto adjacent public streets;
- n. Apply (non-toxic) soil stabilizers or hydroseed to inactive construction areas (previously graded areas inactive for 10-days or more);
- o. Enclose, cover, water twice daily or apply (non-toxic) soil binders to exposed stockpiles (dirt, sand, etc.).
- p. Gather all construction debris on a regular basis and place them in a dumpster or other container which is emptied or removed on a weekly basis. When appropriate, use tarps on the ground to collect fallen debris or splatters that could contribute to storm water pollution;
- q. Remove all dirt, gravel, rubbish, refuse and green waste from the sidewalk, street pavement, and storm drain system adjoining the project site. During wet weather, avoid driving vehicles off paved areas and other outdoor work;
- r. Broom sweep the sidewalk and public street pavement adjoining the project site on a daily basis. Caked on mud or dirt shall be scraped from these areas before sweeping;

- s. No site grading shall occur during the rainy season, between October 15 and April 15, unless approved erosion control measures are in place.
- t. Install filter materials (such as sandbags, filter fabric, etc.) at the storm drain inlet nearest the downstream side of the project site prior to: 1) start of the rainy season; 2) site dewatering activities; or 3) street washing activities; and 4) saw cutting asphalt or concrete, or in order to retain any debris or dirt flowing into the City storm drain system. Filter materials shall be maintained and/or replaced as necessary to ensure effectiveness and prevent street flooding. Dispose of filter particles in the trash;
- u. Create a contained and covered area on the site for the storage of bags of cement, paints, flammables, oils, fertilizers, pesticides or any other materials used on the project site that have the potential for being discharged to the storm drain system through being windblown or in the event of a material spill;
- v. Never clean machinery, tools, brushes, etc., or rinse containers into a street, gutter, storm drain or stream. See "Building Maintenance/Remodeling" flyer for more information;
- w. Ensure that concrete/gunite supply trucks or concrete/plasters finishing operations do not discharge washwater into street gutters or drains; and
- x. The developer shall immediately report any soil or water contamination noticed during construction to the City Fire Department Hazardous Materials Division, the Alameda County Department of Health and the Regional Water Quality Control Board.

89. The minimum soils sampling and testing frequency shall conform to Chapter 8 of the Caltrans Construction Manual. The subdivider shall require the soils engineer to daily submit all testing and sampling and reports to the City Engineer.

PRIOR TO CONSTRUCTION COMPLETION AND ISSUANCE OF CERTIFICATES OF OCCUPANCY

During Construction

90. The applicant shall comply with standards identified in General Plan Appendix N – Noise Guidelines for the Review of New Development. Measures to ensure compliance with such standards shall be developed by a state licensed acoustical engineer and incorporated into building permit plans, to be confirmed by the Planning and Building Divisions. Also, confirmation by a state licensed acoustical engineer that such standards are met shall be submitted after construction and prior to issuance of certificates of occupancy.
91. Prior to final inspections, all pertinent conditions of approval and all improvements shall be completed to the satisfaction of the Planning Director.

Landscape

92. ~~(Condition to be deleted.) A mylar of the approved landscape and irrigation improvement plans shall be submitted to the Public Works Department. The size of Mylar shall be twenty-four inches by thirty-six inches without an exception. A four-inch by four-inch blank signing block shall be provided in the low right side on each sheet of Mylar. The signing block shall contain two signature lines and dates for City of Hayward City Engineer and City Landscape Architect.~~

93. Landscape and ~~the~~ ***irrigation*** improvements shall be installed according to the approved plans prior to the occupancy of each building. All common area landscaping, irrigation and other required improvements shall be installed prior to acceptance of tract improvements, or occupancy of eighty percent of the dwelling units, whichever first occurs and a Certificate of Completion, as-built Mylar and an Irrigation Schedule shall be submitted prior to the Final Approval of the landscaping for the Tract to the Engineering Department by the developer.
94. Prior to the issuance of Certificate of Occupancy, all landscape and irrigation shall be completed in accordance to the approved plan and accepted by the project landscape architect prior to submitting a Certificate of Completion. The final acceptance form must be submitted prior to requesting an inspection to the City Landscape Architect. An Irrigation Schedule shall be submitted prior to the final inspection and acceptance of improvements.
95. As-built Mylar ***of the landscape and irrigation improvements***, and an Irrigation Schedule shall be submitted prior to Final Approval of the landscaping for the Tract to the Engineering Department by the developer.

Property-Owners' Association

96. Property-owners' association for the commercial and/or residential components of the property shall be created and shall be responsible for maintaining all private streets, alleys, parking bays, private street lights, private utilities, retaining walls and other privately owned common areas and facilities on the site, including, but not limited to landscaping, preservation and replacement of trees, as well as decorative paving that extends into public streets. For any necessary repairs done by the City in locations under the on-site decorative paved areas, the City shall not be responsible for the replacement cost of the decorative paving. The replacement cost shall be borne by the property-owners' association established to maintain the common areas within the subdivision boundary.
97. Prior to the sale of any parcel, or prior to the acceptance of site improvements, whichever occurs first, Condominium Plan, and Conditions, Covenants and Restrictions (CC&R's) creating property -owners association for the commercial and/or residential component of the property shall be reviewed and approved by the Planning Director and City Attorney and recorded. The CC&R's shall describe how the stormwater BMPs associated with privately owned improvements and landscaping shall be maintained by the association. The CC&Rs shall include the following provisions:
- a. Each owner shall automatically become a member of the association(s) and shall be subject to a proportionate share of maintenance expenses.
 - b. A reserve fund shall be maintained to cover the costs of improvements and landscaping to be maintained by the Association(s).
 - c. The association shall be managed and maintained by a professional property management company.
 - d. The property-owners' association(s) shall own and maintain on-site storm drain systems.
 - e. The property-owners' association(s) shall maintain the common area irrigation system and maintain the common area landscaping in a healthy, weed-free condition at all times. The property-owners' association(s) representative(s) shall inspect the landscaping on a

- monthly basis and any dead or dying plants (plants that exhibit over 30% die-back) shall be replaced within fifteen days of notification to the homeowner. Plants in the common areas shall be replaced within two weeks of the inspection. Trees shall not be severely pruned, topped or pollarded. Any trees that are pruned in this manner shall be replaced with a tree species selected and size determined by the City Landscape Architect, within the timeframe established by the City and pursuant to the Hayward Municipal Code.
- f. A provision that if the property-owners' association fails to maintain the decorative retaining walls, landscaping and irrigation in all common areas for which it is responsible so that owners, their families, tenants, or adjacent owners will be impacted in the enjoyment, use or property value of the project, the City shall have the right to enter upon the project and to commence and complete such work as is necessary to maintain the common areas and private streets, after reasonable notice, and lien the properties for their proportionate share of the costs, in accordance with Section 10-3.385 of the Hayward Subdivision Ordinance.
 - g. A requirement that the building exteriors and fences shall be maintained free of graffiti. The owner's representative shall inspect the premises on a weekly basis and any graffiti shall be removed within 72 hours of inspection or within 72 hours of notification by the City.
 - h. A tree removal permit is required prior to the removal of any protected tree, in accordance with the City's Tree Preservation Ordinance.
 - i. The garage of each unit shall be maintained for off-street parking of two vehicles and shall not be converted to living or storage areas. An automatic garage door opening mechanism shall be provided for all garage doors.
 - j. Individual homeowners shall maintain in good repair the exterior elevations of their dwelling. The CC&Rs shall include provisions as to a reasonable time period that a unit shall be repainted, the limitations of work (modifications) allowed on the exterior of the building, the formation of a design review committee and its power to review changes proposed on a building exterior and its color scheme, and the right of the property-owners' association to have necessary work done and to place a lien upon the property if maintenance and repair of the unit is not executed within a specified time frame. The premises shall be kept clean and free of debris at all times. Color change selections shall be compatible with the existing setting.
 - k. Utilities, meters, and mechanical equipment when not enclosed in a cabinet, shall be screened by either plant materials or decorative screen so that they are not visible from the street. Sufficient access for reading must be provided to meters.
 - l. Any transformer shall be located underground and shall be located within the right-of-way or public utility easement.
 - m. Any future major modification to the approved site plan shall require review and approval by the Planning Commission.
 - n. The CC&Rs shall specify the outdoor collection locations of trash and recycle containers. Adequate provisions shall be made to ensure that all residents, regardless of physical ability, are able to easily dispose of their garbage and recyclables in the centralized collection containers provided by the City's franchisee.
 - o. Streetlights and pedestrian lighting shall be owned and maintained by the property-owners' association and shall have a decorative design approved by the Planning Director and the City Engineer.

- p. Street sweeping of private streets, alleys and parking bays shall be conducted at least once a month.
- q. Balconies may not be used for storage and personal items may not be draped over the railings.
- r. The association shall ensure that no less than 75 percent of the units shall be owner-occupied. The CC&Rs shall further provide that the leasing of units as a regular practice for business, speculative investment or other similar purpose is not permitted. However, to address special situations and avoid unusual hardship or special circumstances, such as a loss of job, job transfer, military transfer, change of school or illness or injury that, according to a doctor, prevents the owner from being employed, the CC&Rs may authorize the governing body to grant its consent, which consent shall not be unreasonably withheld, to a unit owner who wishes to lease or otherwise assign occupancy rights to a specified lessee for a specified period.

Prior to the Issuance of Certificate of Occupancy or Final Report

- 98. All buildings shall be designed using the California Building Codes in effective at the time of submitting building permit applications.
- 99. ~~(**Condition to be deleted.**) All common area landscaping, irrigation and other required improvements shall be installed according to the approved plans.~~
- 100. All tract improvements, including the complete installation of all improvements relative to streets, fencing, sanitary sewer, storm drainage, water system, underground utilities, etc., shall be completed and attested to by the City Engineer before approval of occupancy of any unit. Where facilities of other agencies are involved, such installation shall be verified as having been completed and accepted by those agencies.
- 101. All common area landscaping, irrigation and other required improvements shall be installed according to the approved plans.
- 102. All tract improvements, including the complete installation of all improvements relative to streets, fencing, sanitary sewer, storm drainage, water system, underground utilities, etc., shall be completed and attested to by the City Engineer before approval of occupancy of any unit. Where facilities of other agencies are involved, such installation shall be verified as having been completed and accepted by those agencies.
- 103. Park Dedication In-Lieu Fees are required for all new dwelling units. Fees shall be those in effect at the time of the Vesting Tentative Tract Map is approved. All Park dedication in-lieu fees shall be paid prior to issuance of a Certificate of Occupancy for a residential unit.
- 104. The developer/subdivider shall be obligated for the following additional fees. The amount of the fee shall be in accordance with the fee schedule in effect at the time Vesting Tentative Tract Map was accepted as complete, unless otherwise indicated herein:
 - a. Supplemental Building Construction and Improvement Tax,
 - b. School Impact Fee

105. Final Hayward Fire Department inspection is required to verify that requirements for fire protection facilities have been met and actual construction of all fire protection equipment have been completed in accordance with the approved plan. Contact the Fire Marshal's Office at (510) 583-4910 at least 24 hours before the desired final inspection appointment.
106. The improvements associated with the Pacific Gas and Electric Company, AT&T (phone) company and local cable company shall be installed to the satisfaction of the respective companies.
107. The Stormwater Treatment Measures Maintenance Agreement for the project, prepared by Public Works Engineering and Transportation Division staff, shall be signed and recorded in concurrence with the Final Map at the Alameda County Recorder's Office to ensure that the maintenance is bound to the property in perpetuity.
108. The subdivider shall submit an Auto CAD file format (release 2010 or later) in a CD of approved final map and 'as-built' improvement plans showing lot and utility layouts that can be used to update the City's Base Maps.
109. The developer/subdivider shall submit an "as built" plans indicating the following:
 - a. Approved landscape and irrigation improvements;
 - b. All underground facilities, sanitary sewer mains and laterals, water services (including meter locations), Pacific Gas and Electric, AT&T (phone) facilities, local cable company, etc.;
 - c. All the site improvements, except landscaping species, buildings and appurtenant structures; and
 - d. Final Geotechnical Report.

















@ THE BOULEVARD



Integral Communities
 500 La Gonda Way, Suite 102
 Danville, CA 94526
 925.362.3749

CONCEPTUAL RETAIL PERSPECTIVES

HAYWARD, CALIFORNIA

KTGY # 2011-0597

12.19.2013

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Architecture+Planning
 580 Second St., Suite 200
 Oakland, CA 94607
 510.272.2910
 ktgy.com



AI.0



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CONCEPTUAL RETAIL PERSPECTIVES

HAYWARD, CALIFORNIA

KTGY # 2011-0597

12.19.2013

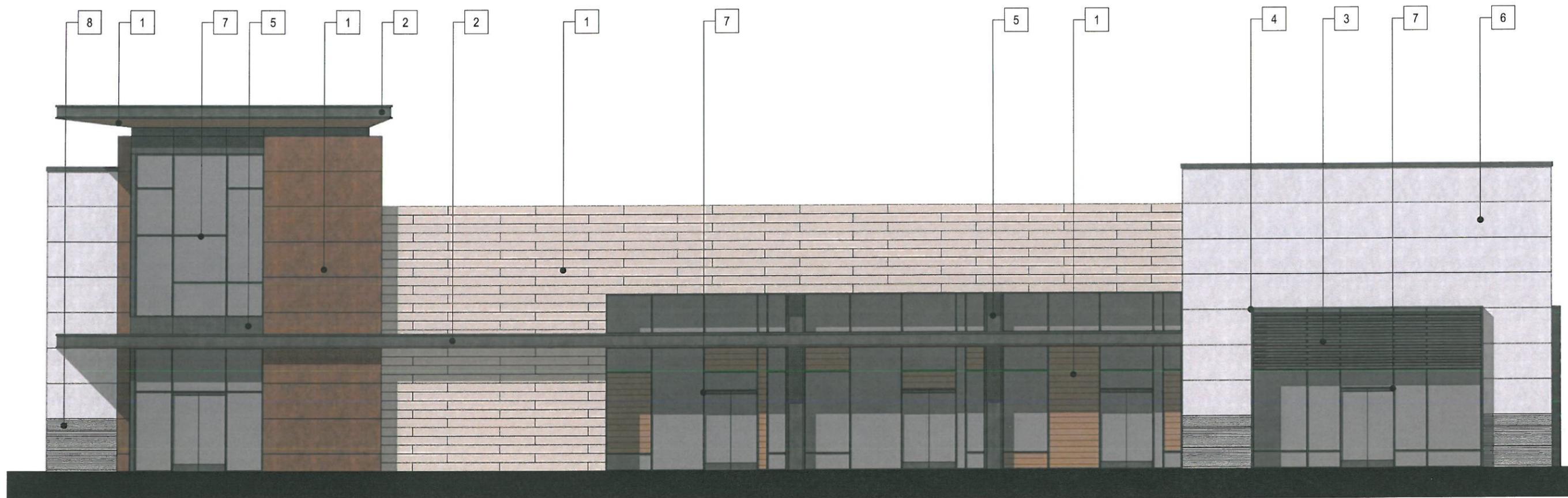
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Architecture+Planning
580 Second St., Suite 200
Oakland, CA 94607
510.272.2910
ktgy.com



AI.I

Material Legend

- 1. Flat Panel Decorative Cladding
- 2. Metal Awning
- 3. Metal Louvers
- 4. Metal Plate
- 5. Break Metal
- 6. Stucco
- 7. Storefront
- 8. Stone Veneer



Front Elevation

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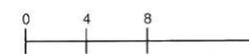
RETAIL BUILDING A CONCEPTUAL ELEVATIONS

HAYWARD, CALIFORNIA

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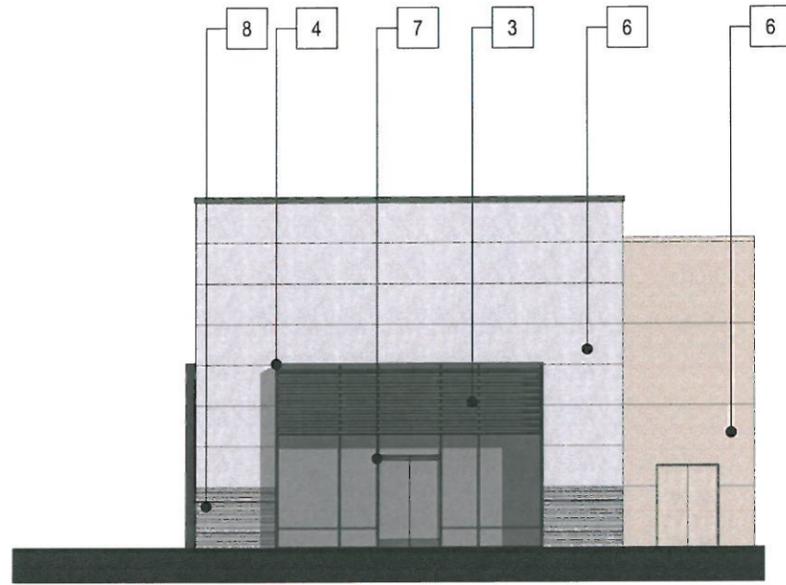


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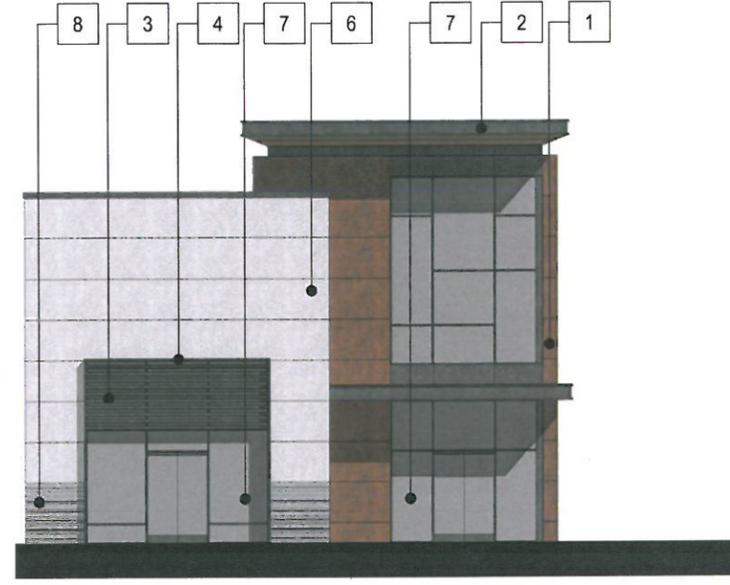


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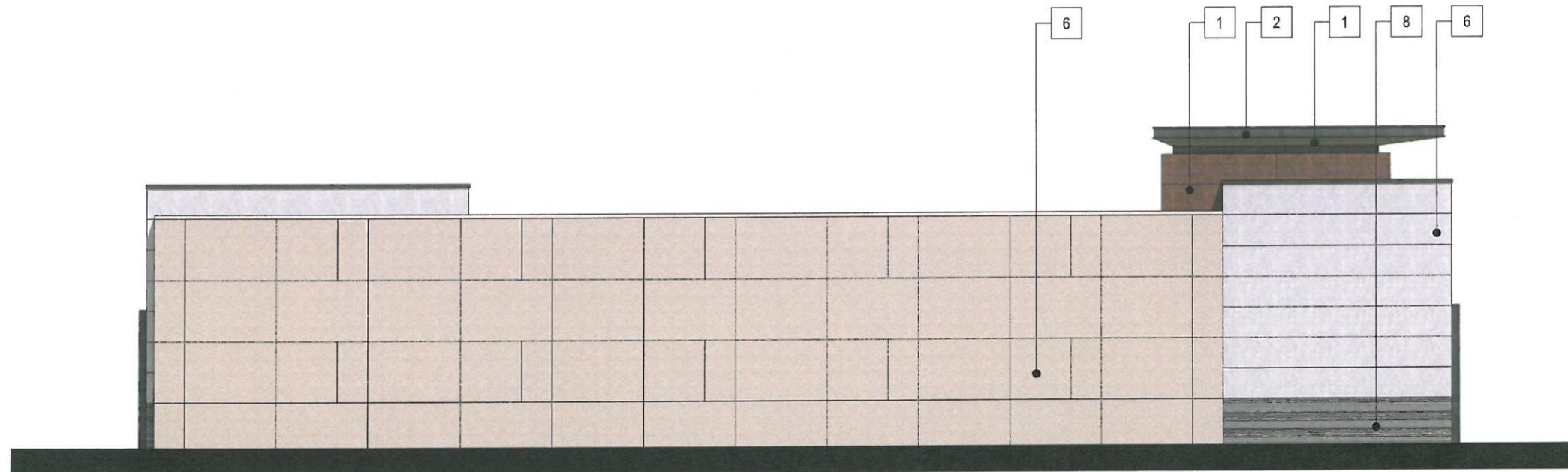
- 1. Flat Panel Decorative Cladding
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- 7. Storefront
- 8. Stone Veneer



Right Elevation



Left Elevation



Rear Elevation



A2.0B

@ THE BOULEVARD

RETAIL BUILDING A CONCEPTUAL ELEVATIONS

HAYWARD, CALIFORNIA

KTGY # 2011-0597

12.19.2013

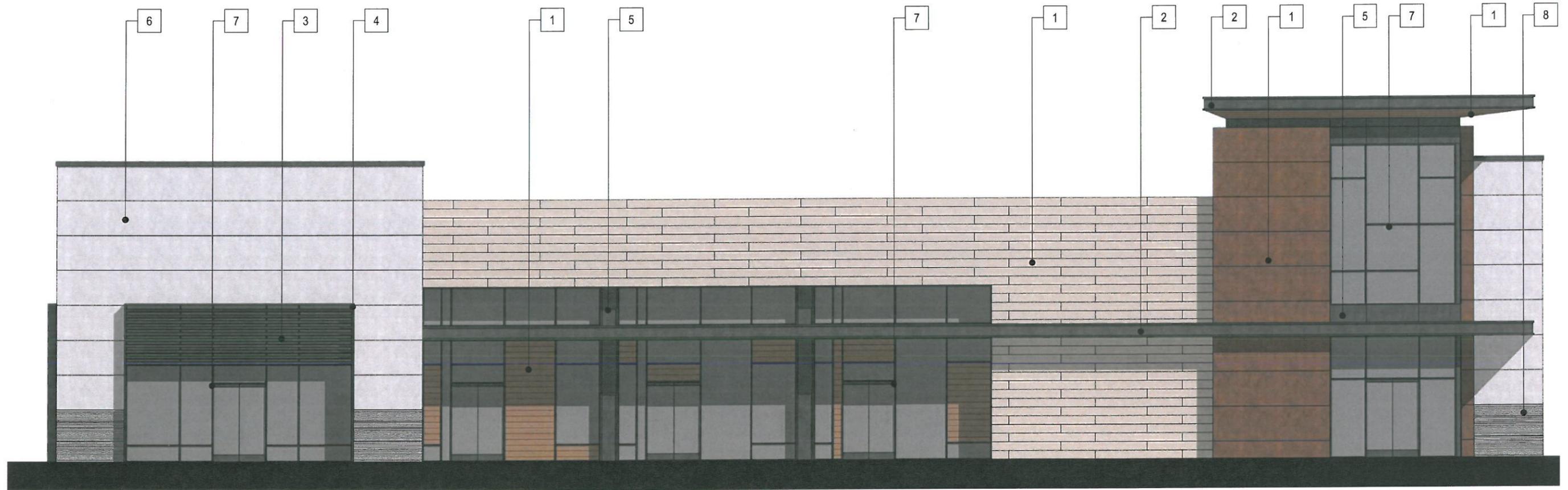
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Front Elevation

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RETAIL BUILDING B CONCEPTUAL ELEVATIONS

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A2.1A

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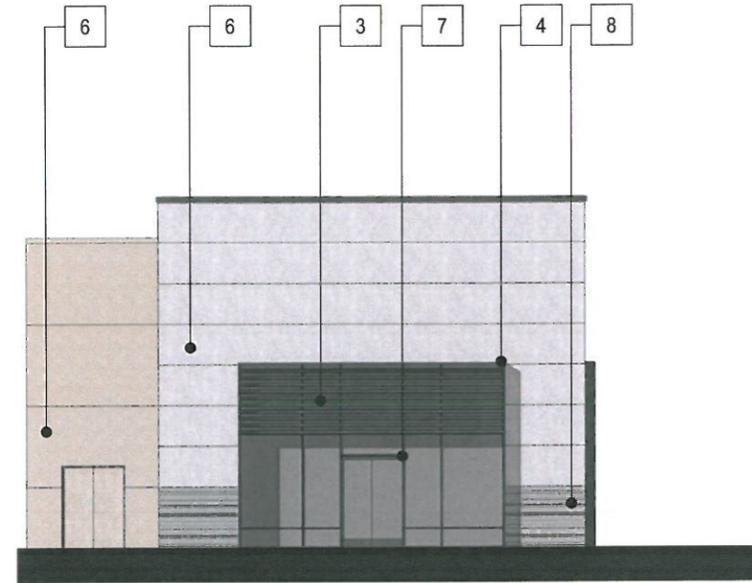


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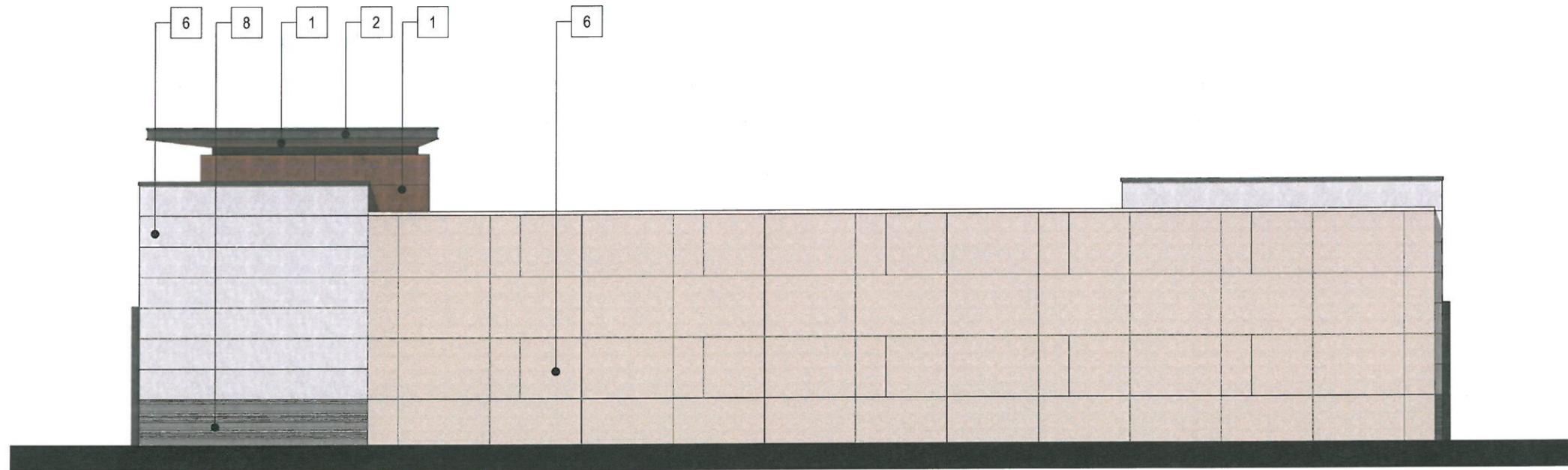
- 1. Flat Panel Decorative Cladding
- 2. Metal Awning
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- 4. Metal Plate
- 5. Break Metal
- 6. Stucco
- 7. Storefront
- 8. Stone Veneer



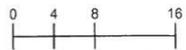
Right Elevation



Left Elevation



Rear Elevation



A2.1B

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RETAIL BUILDING B CONCEPTUAL ELEVATIONS



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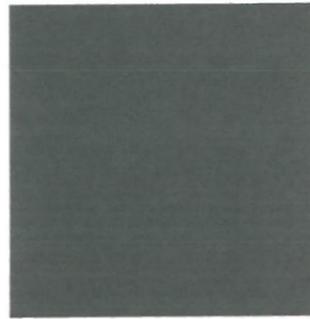


Flat Panel Decorative Cladding
Image represents material application
Size to be determined

Color :
Antique Copper
Anodized



Flat Panel Decorative Cladding
Color: Narrow Cherry

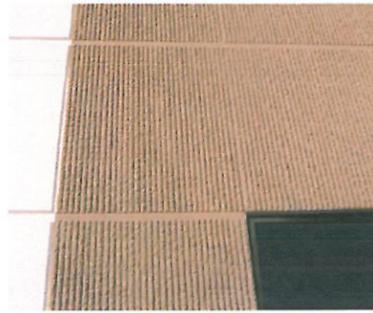


Typical Metal
SW #7069 Iron Ore



Flat Panel Decorative Cladding
Image represents material application
Size to be determined

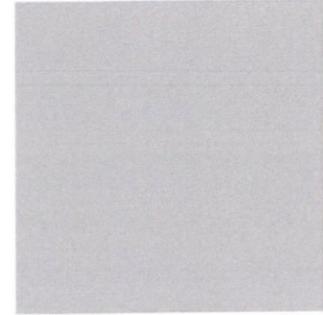
Color :
Bone White



Stone Veneer
Image represents material application
Color and size to be determined



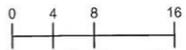
Typical Stucco
Sand Finish 30/30



SW #7065 Argos



Front Elevation



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CONCEPTUAL MATERIALS AND COLORS

A2.2



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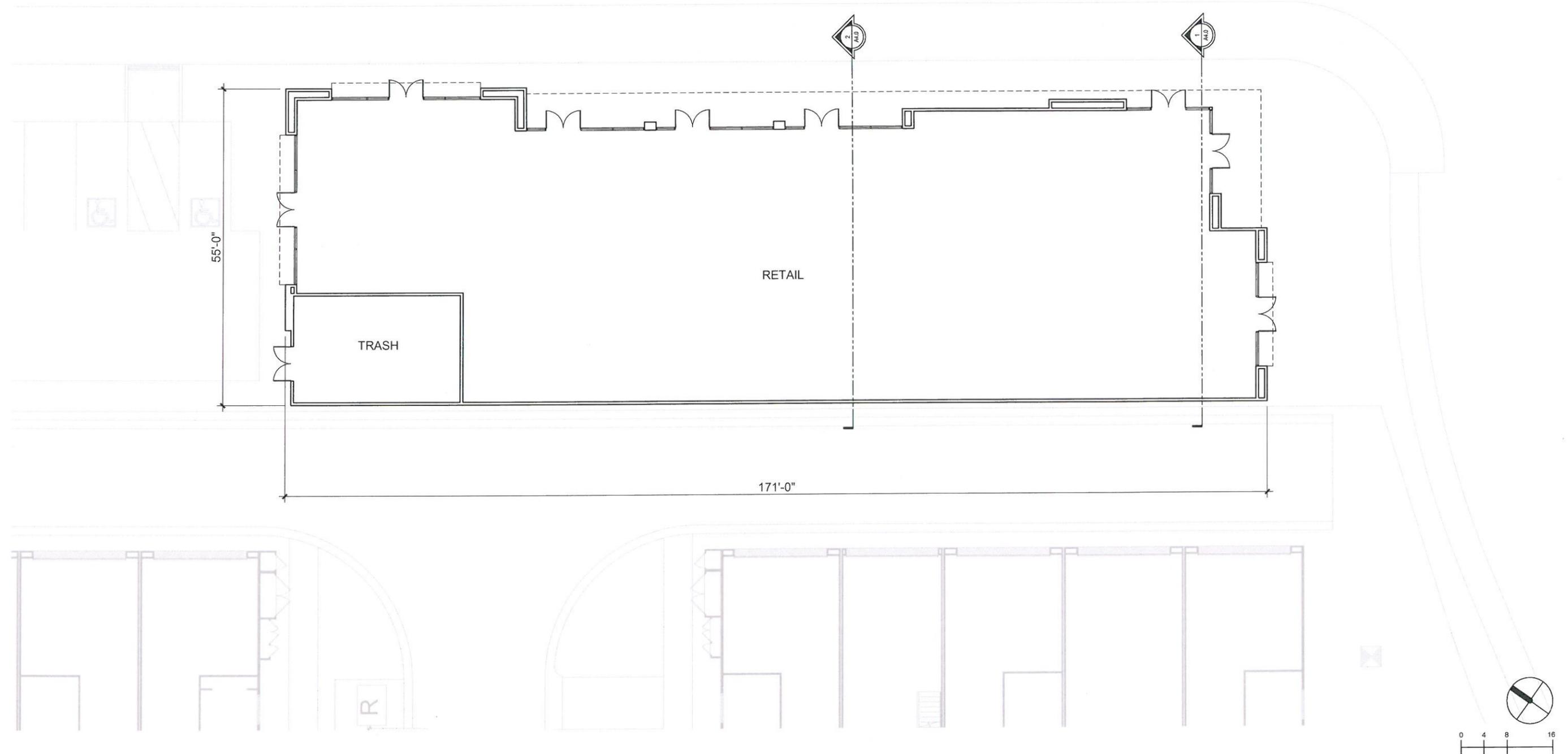
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RETAIL BUILDING A CONCEPT FLOOR PLAN

A3.0



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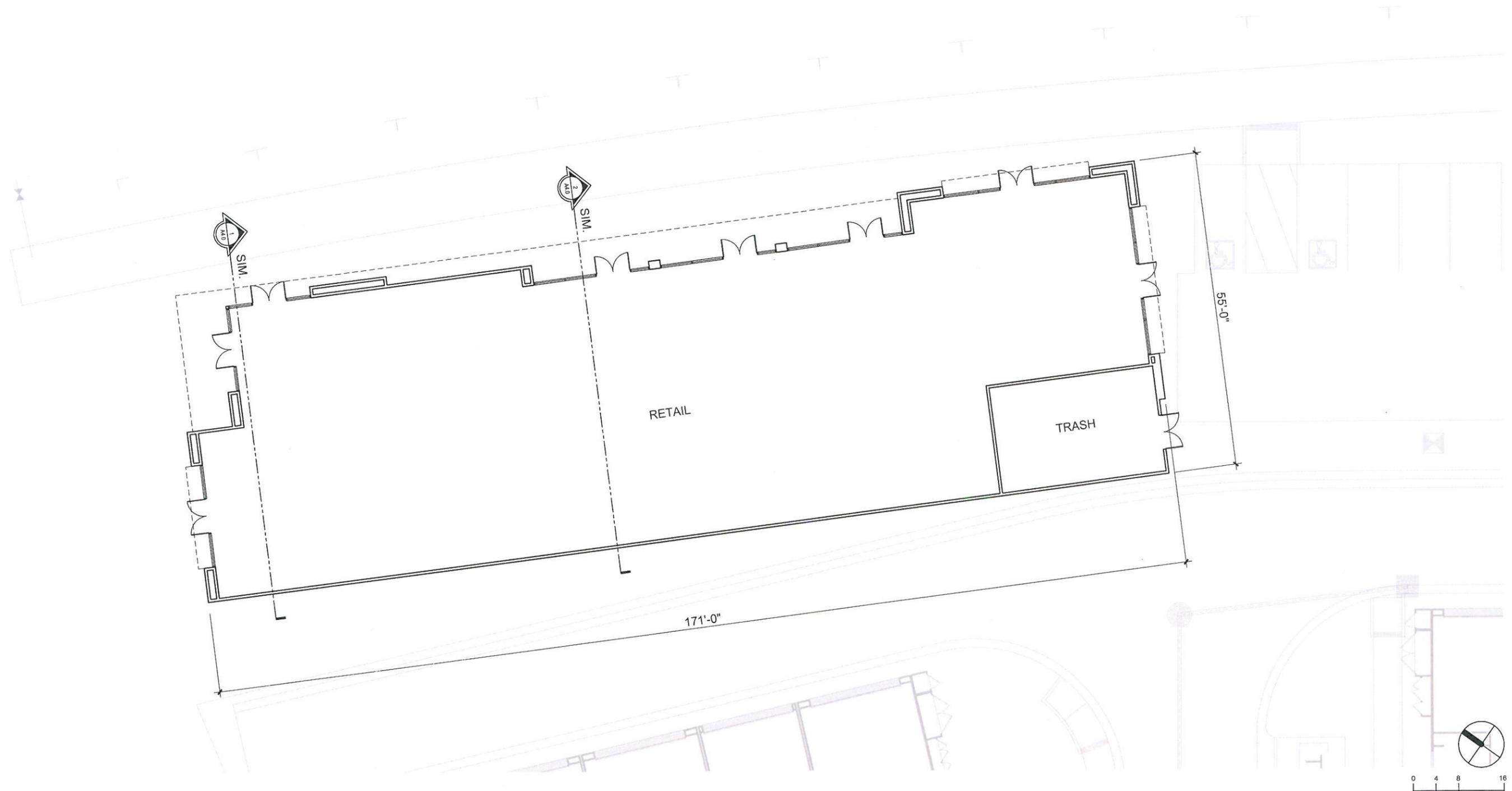
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RETAIL BUILDING B CONCEPT FLOOR PLAN

HAYWARD, CALIFORNIA

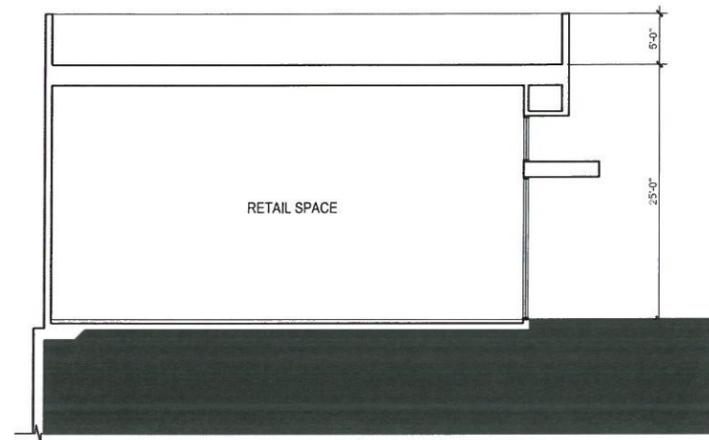
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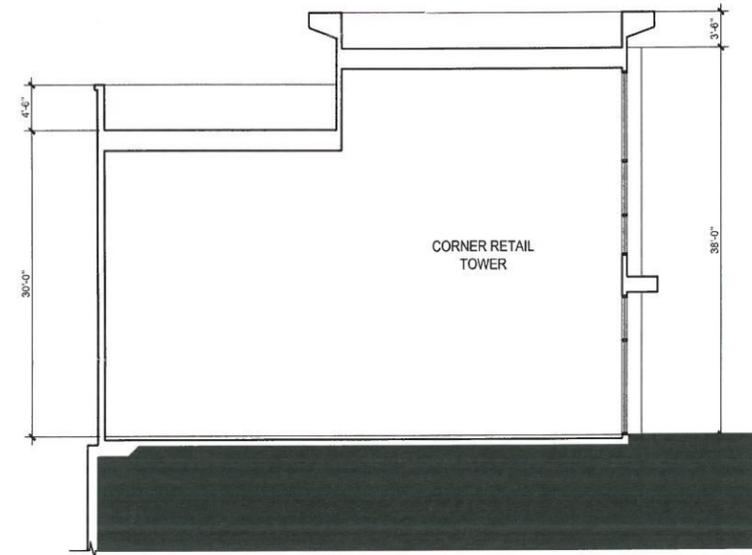
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A3.1



2. SECTION THROUGH RETAIL SPACE



1. SECTION THROUGH CORNER RETAIL TOWER

@ THE BOULEVARD



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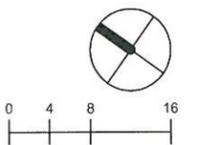
RETAIL BUILDING CONCEPTUAL SECTIONS

HAYWARD, CALIFORNIA

KTGY # 2011-0597

12.19.2013

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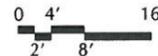


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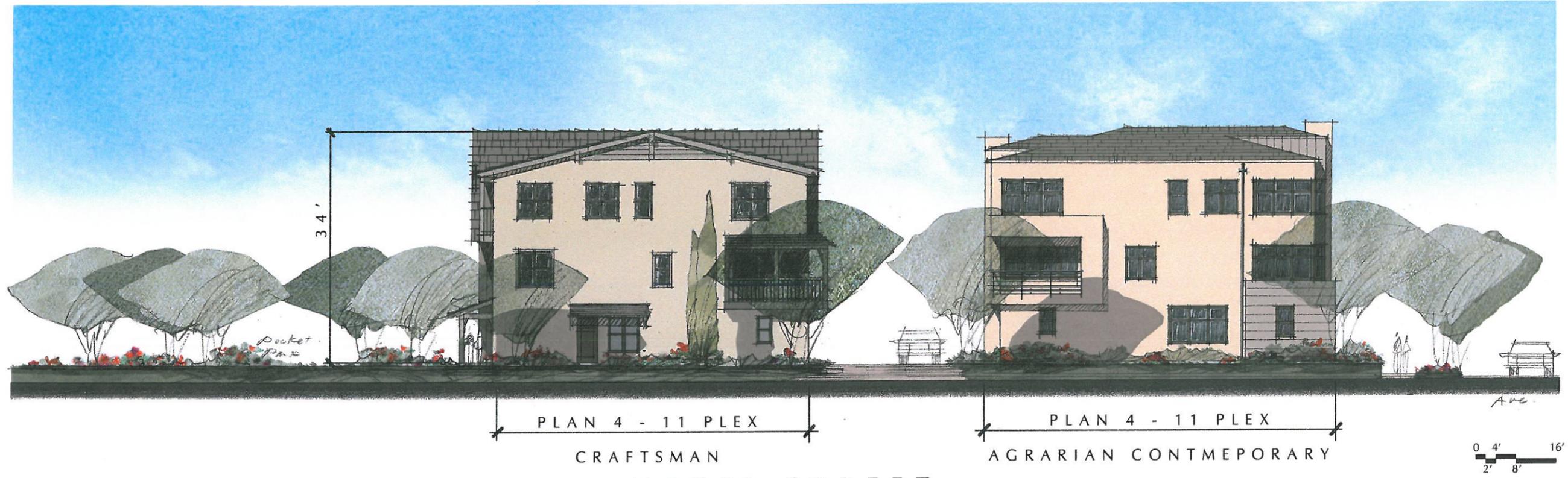


STREET ELEVATIONS

ELEVATIONS



@ **The Boulevard**
HAYWARD, CALIFORNIA



CRAFTSMAN

AGRARIAN CONTEMPORARY

HAZEL STREET ELEVATIONS



AGRARIAN RURAL

CRAFTSMAN

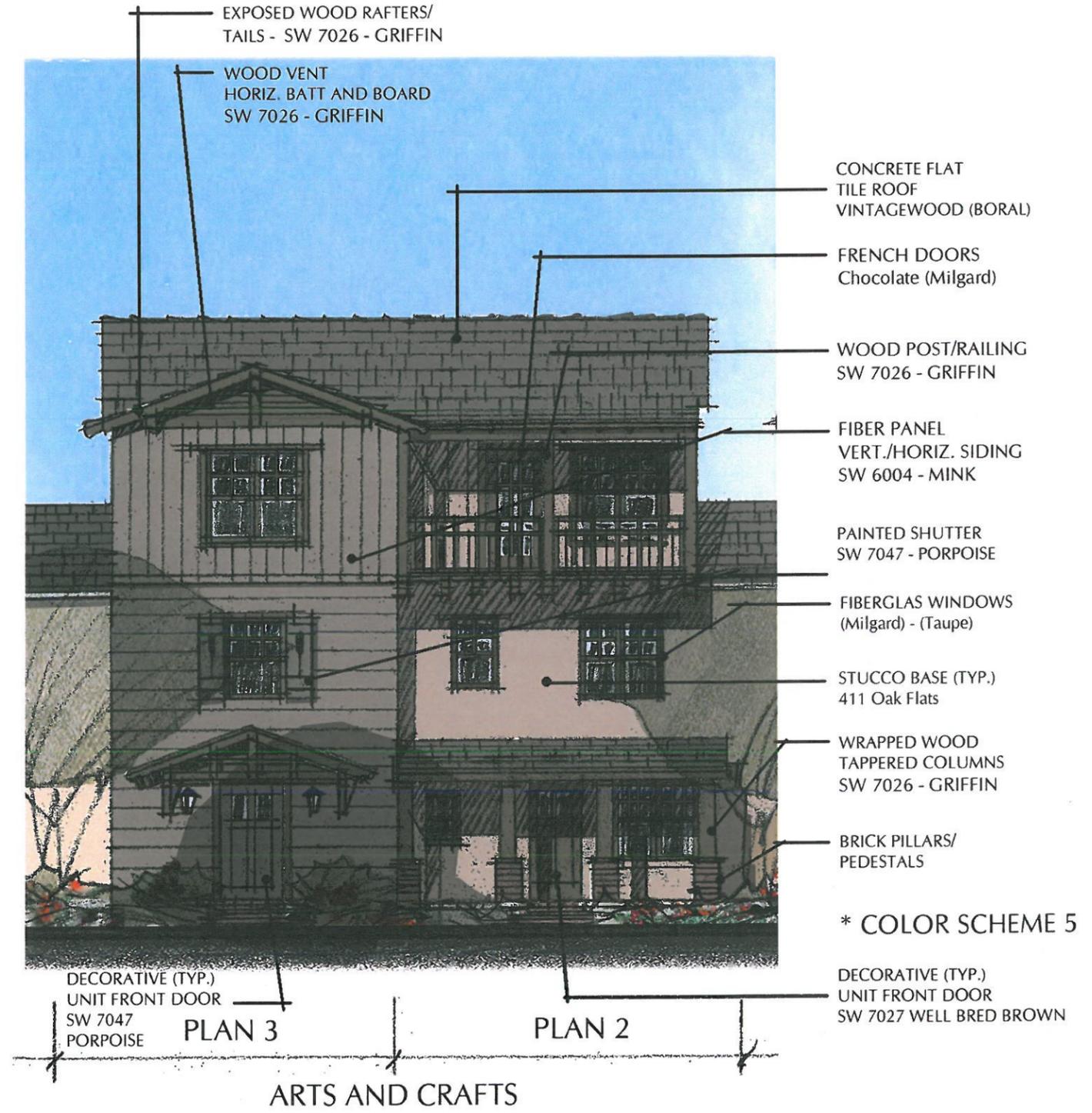
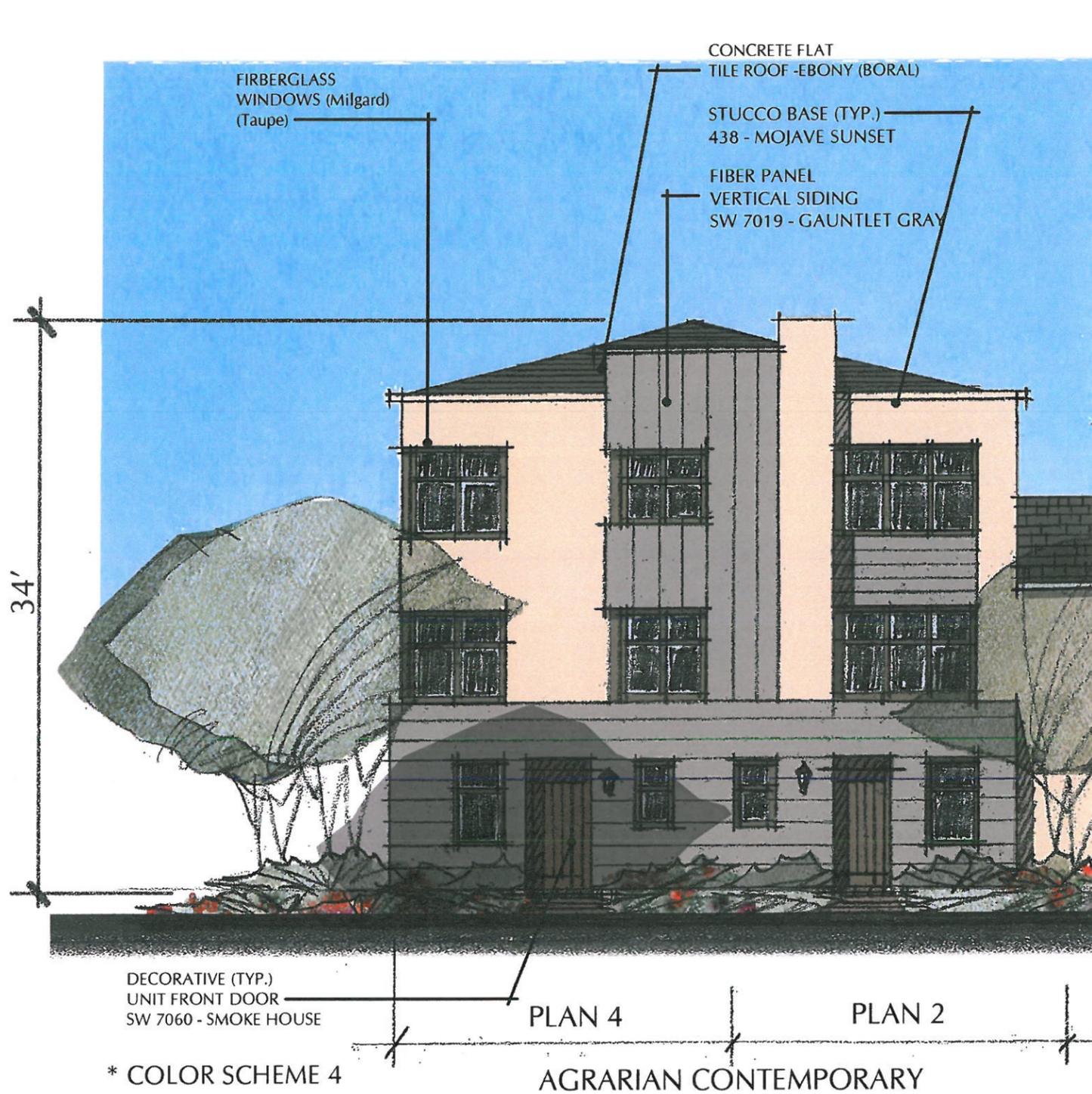
MONTEREY

ARTS & CRAFTS

AGRARIAN CONTEMPORARY

DRIVE ISLE ELEVATIONS
(NOT TO SCALE)

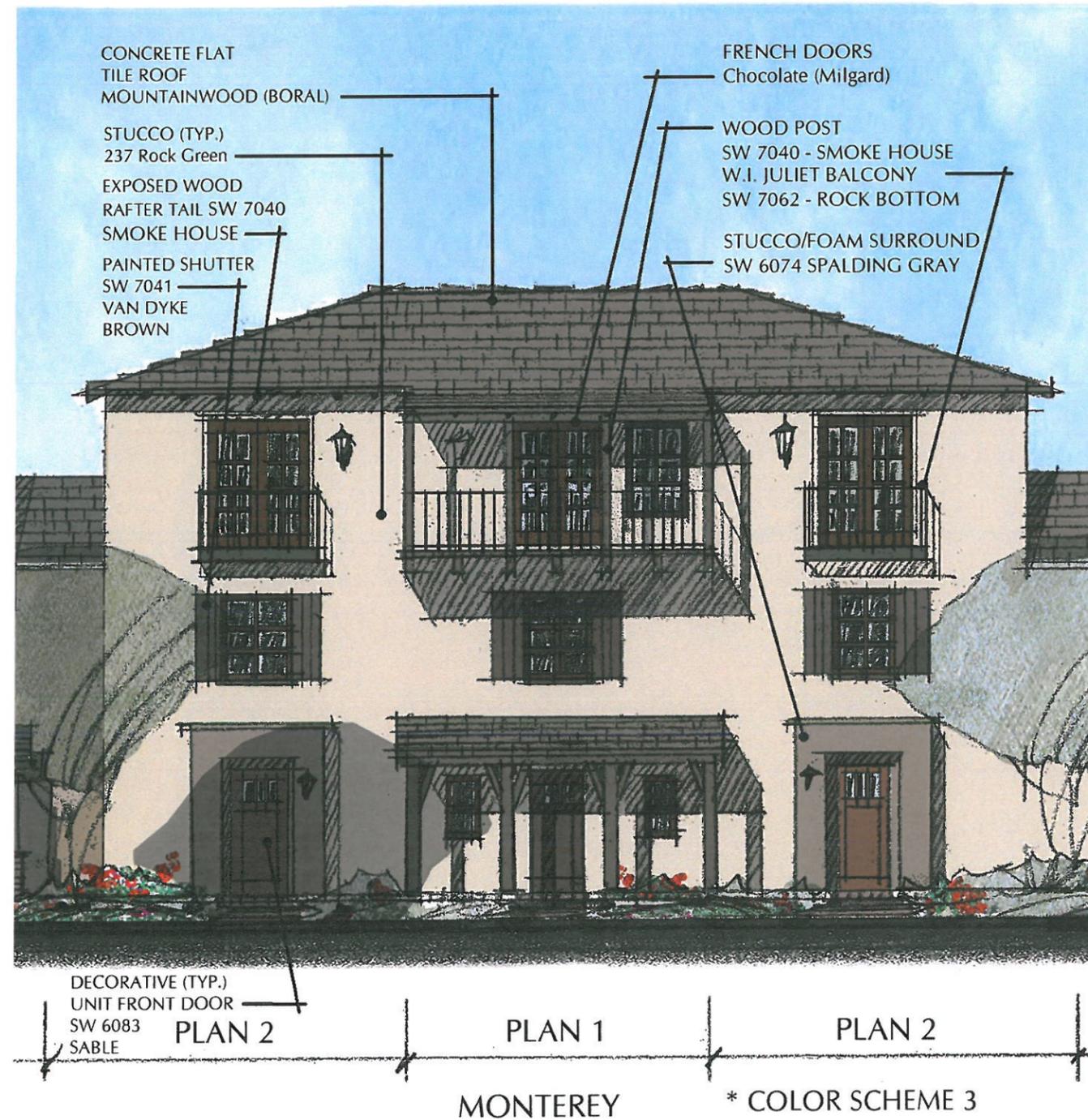
ELEVATIONS
@ The Boulevard
HAYWARD, CALIFORNIA



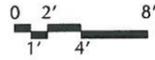
FRONT ELEVATIONS



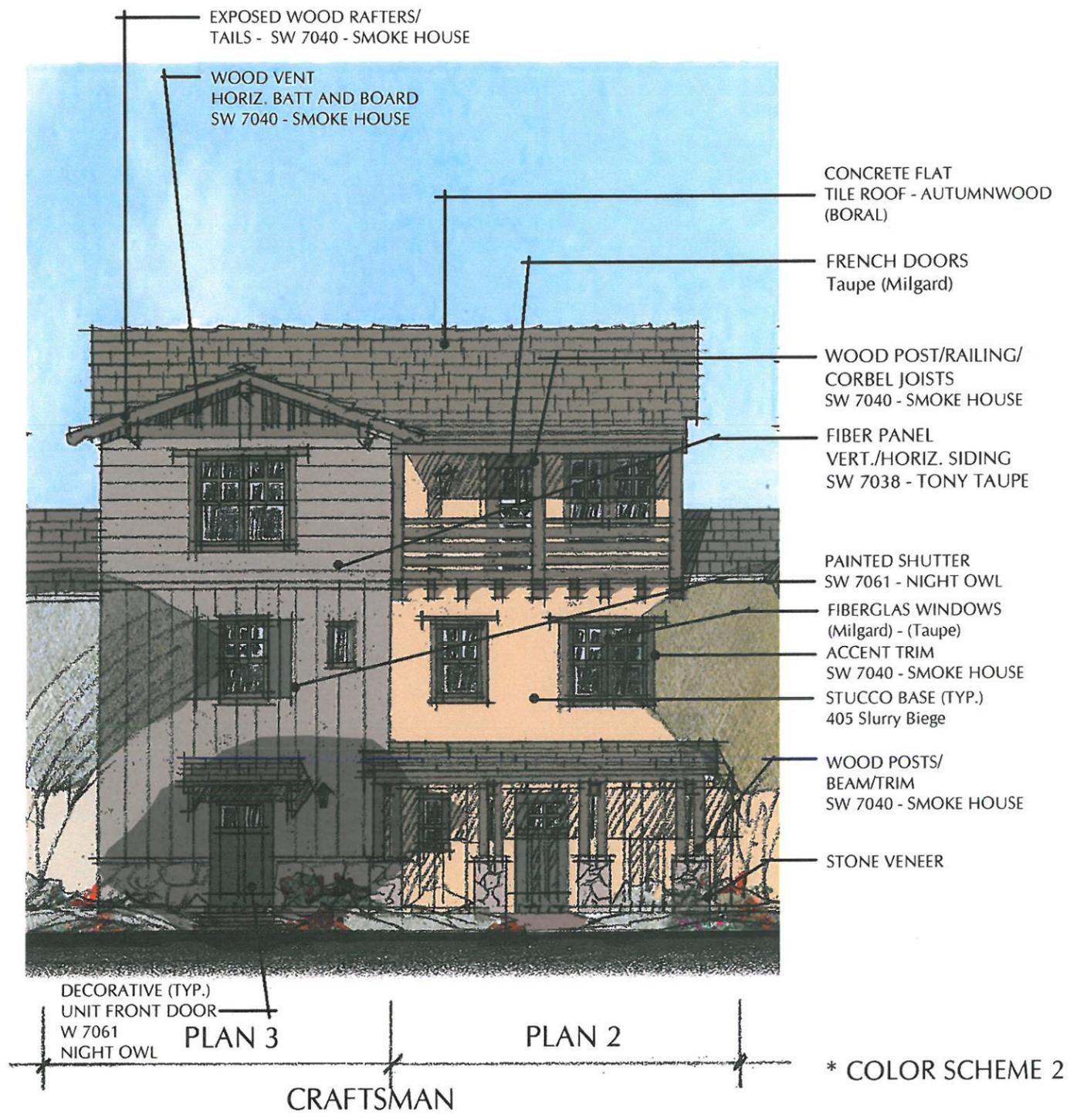
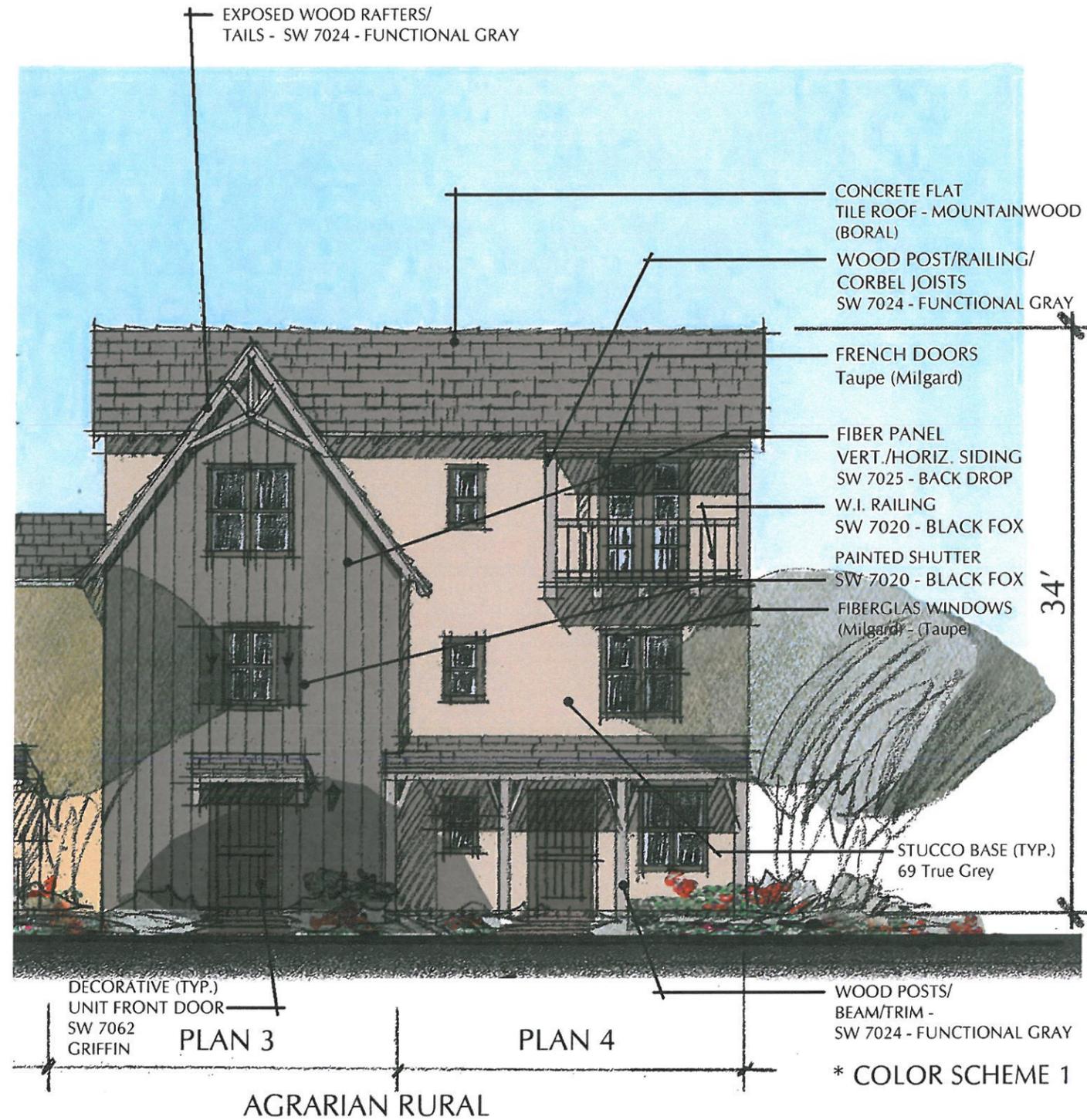
@ The Boulevard
HAYWARD, CALIFORNIA



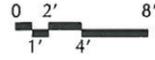
FRONT ELEVATIONS



@ The Boulevard
HAYWARD, CALIFORNIA



FRONT ELEVATIONS
 @ The Boulevard
 HAYWARD, CALIFORNIA





AC1 - Corner Windows



AC2 - Simple Forms with Plenty of Window Glass



Arts1 - Porch Corner Wood Wide Column and Beam



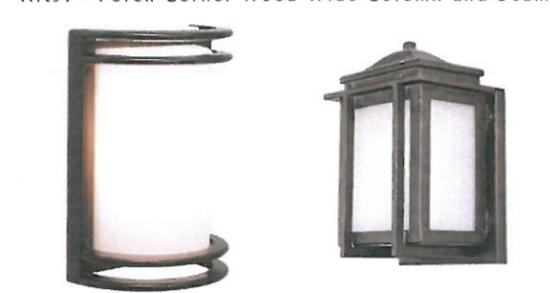
Arts3 - Deep Porch and Wrapped Wood Columns with Rails



AC3 - Wood and Stucco Massing



AC5 - Architectural Massing



AC6 - Light Fixture

Arts2 - Light Fixture



Arts4 - Steep Roofs, Wood Trim and Siding



Art5 - Shed Dormers



AC4 - Vertical Wood Shapes



Arts6 - Wood Beams and Outlookers

AGRARIAN
CONTEMPORARY

ARTS
AND
CRAFTS

IMAGES For AGRARIAN CONTEMPORARY
and ARTS AND CRAFTS
@ The Boulevard
HAYWARD, CALIFORNIA



M1 - Front door Stucco Surround



M2 - Front door and Stucco Surround



M4 - Wood Balcony



M3 - Exterior Light Fixture



M5 - Wood Sill/Stucco Jam



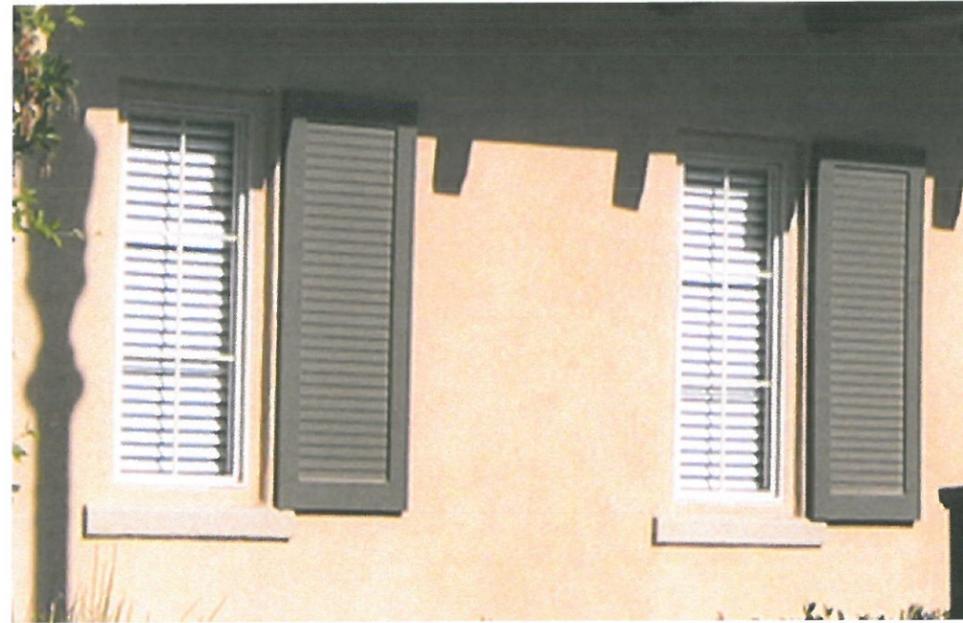
M6 - Wrought Iron Juliet Balcony



M7 - Window Trim and Faux Shutters



M8 - Wood Joists at Balcony



M9 - Window at Stucco Recess with Faux Shutters



M10 - Window Header and Stucco ecess

AGRARIAN CONTEMPORARY

MONTEREY

IMAGES For MONTEREY @ The Boulevard HAYWARD, CALIFORNIA

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INTEGRAL COMMUNITIES 500 LA GONDA WAY SUITE 102 DANVILLE, CA 94526

INTEGRAL COMMUNITIES 12.19.13.



AR1 - Gable End Wood Faux Vent



AR3 - Wood Outlooker and Bracket Detail



AR2 - Exterior Light Fixture



AR4 - Wood Outlooker Detail



C1 - Wood Rake Overhang and Outlooker



C3 - Typical Wood Trim at Wood Siding



C5 - Wood Window Trim and Sill



C2 - Wood "Gamble House" Style Railing and Deck



C4 - Gable End Outlooker and Faux Vent at Rake Overhang



AGRARIAN RURAL

AR5 - Steep Roofs with Flat Roof Tile Shingles, Shed Dormers, and Wood Siding



AR6 - Deep Porches with Wrapped Wood Columns, Brackets and railing

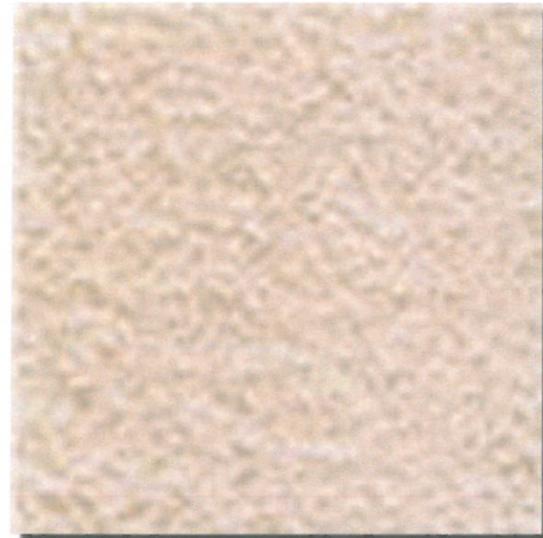


CRAFTSMAN

C6 - Wood Headers, Posts, Low Pillars and Wood Rail

IMAGES For AGRARIAN RURAL
And CRAFTSMAN
@ The Boulevard
HAYWARD, CALIFORNIA

SCHEME 1



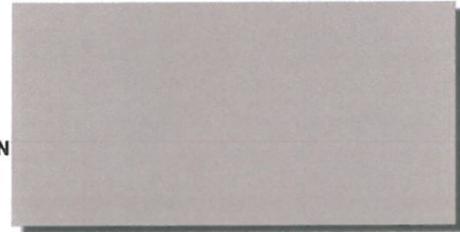
OMEGA STUCCO 69 TRUE GREY 20/30 SAND TEXTURE



SANDSTONE COURTYARD COLLECTION STEEL GARAGE DOORS



TAUPE - MILGARD FIBERGLAS WINDOW FRAME



SW 7024 - FUNCTIONAL GRAY



SW 7025 - BACKDROP

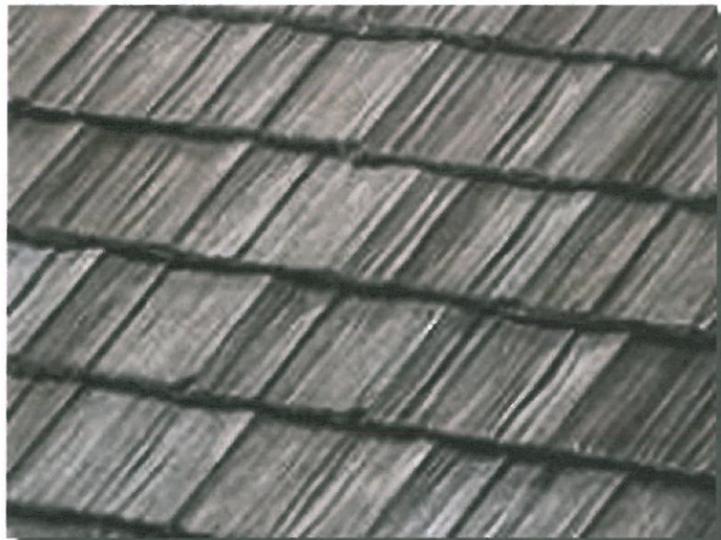


SW 7026 - GRIFFIN



SW 7020 - BLACK FOX

1MDCL5001 MOUNTAINWOOD BORAL TILE



**@ THE BOULEVARD
HAYWARD, CA**

SCHEME 2

EURO STONE CHAMPAGNE CORONADO STONE



1MDCL3002 AUTUMNWOOD BORAL TILE



TAUPE - MILGARD FIBERGLAS WINDOW FRAME



SW 7038 - TONY TAUPE



SW 7061 - NIGHT OWL



SW 7040 - SMOKE HOUSE



OMEGA STUCCO 405 SLURRY BIEGE 20/30 SAND TEXTURE



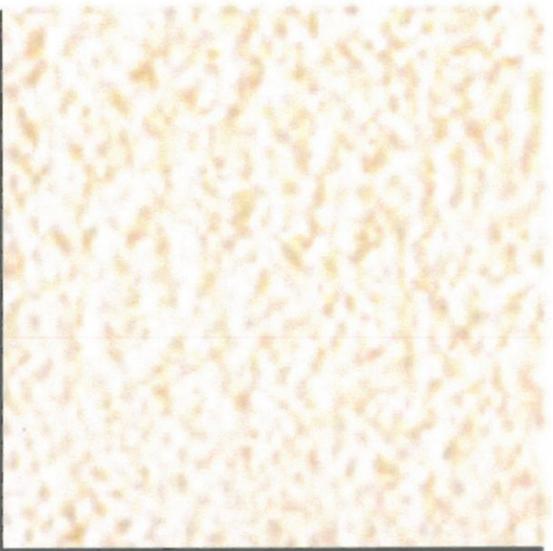
TERRA BRONZE COURTYARD COLLECTION STEEL GARAGE DOORS

COLORS AND MATERIALS BOARD

**INTEGRAL COMMUNITIES
DANVILLE, CA**

SCHEME 3 MATERIALS BOARD

COLORS AND MATERIALS BOARD



OMEGA STUCCO 237 ROCK GREEN
20/30 SAND TEXTURE



BROWN
COURTYARD COLLECTION
STEEL GARAGE DOORS



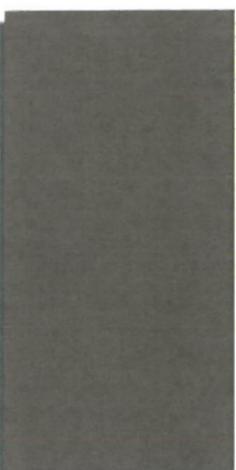
SW 6074 - SPALDING GRAY



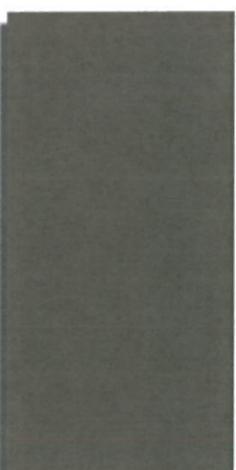
SW 7040 - SMOKE HOUSE



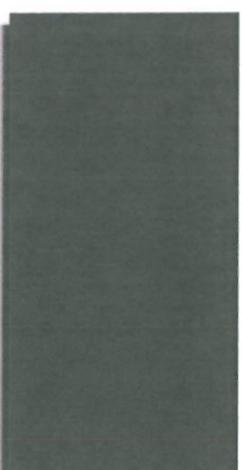
1MDCL5001 MOUNTAINWOOD BORAL TILE
CHOCOLATE - MILGARD FIBERGLAS WINDOW FRAME



SW 6083 - SABLE



SW 7041 - VAN DYKE BROWN



SW 7062 - ROCK BOTTOM

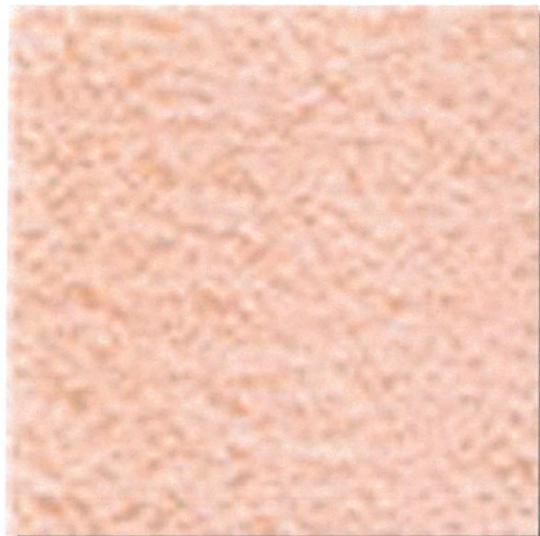
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HAYWARD, CA**

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DANVILLE, CA**

Updated: 12.12.13.

SCHEME 4



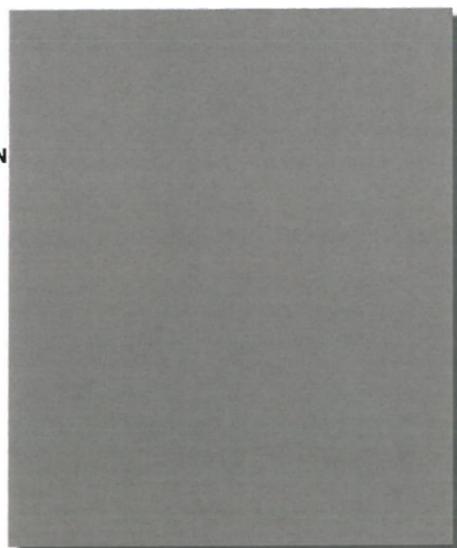
OMEGA STUCCO 438 MOJAVE SUNSET 20/30 SAND TEXTURE



SANDSTONE COURTYARD COLLECTION STEEL GARAGE DOORS

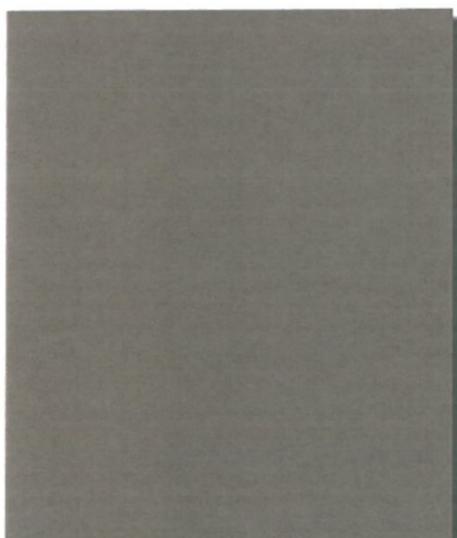
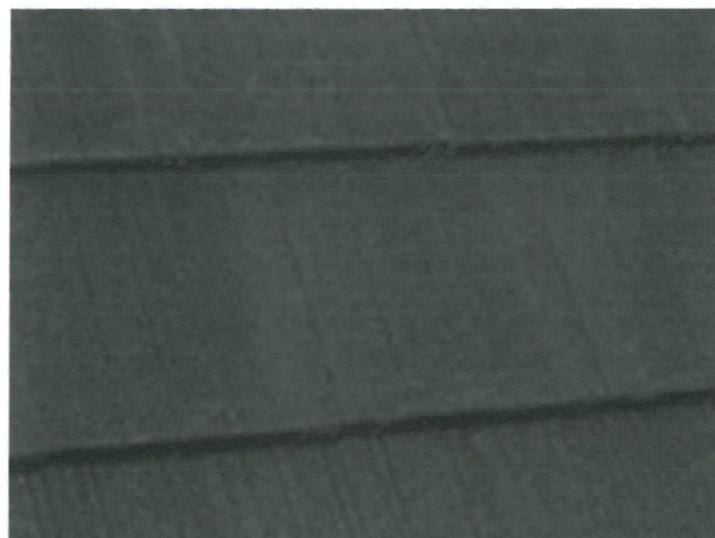


TAUPE - MILGARD FIBERGLAS WINDOW FRAME



SW 7019 - GAUNTLET GRAY

1LSCS5047 EBONY BORAL TILE



SW 7040 - SMOKE HOUSE

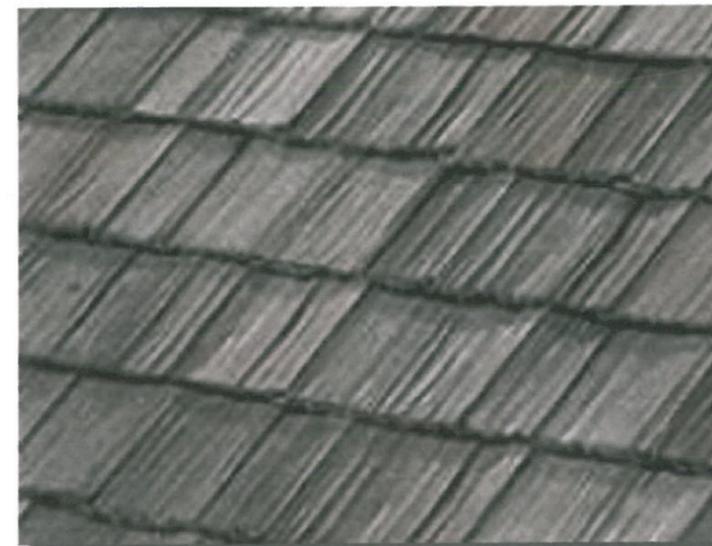
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SCHEME 5

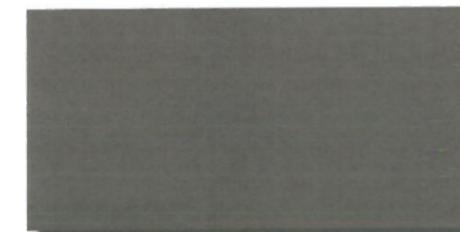
Special Used Country CORONADO STONE



1MDCL5011 VINTAGEWOOD BORAL TILE



SW 6004 - MINK



SW 7026 - GRIFFIN



OMEGA STUCCO 411 OAK FLATS 20/30 SAND TEXTURE



TAUPE - MILGARD FG WIN. FRM



SW 7047 - PROPOISE



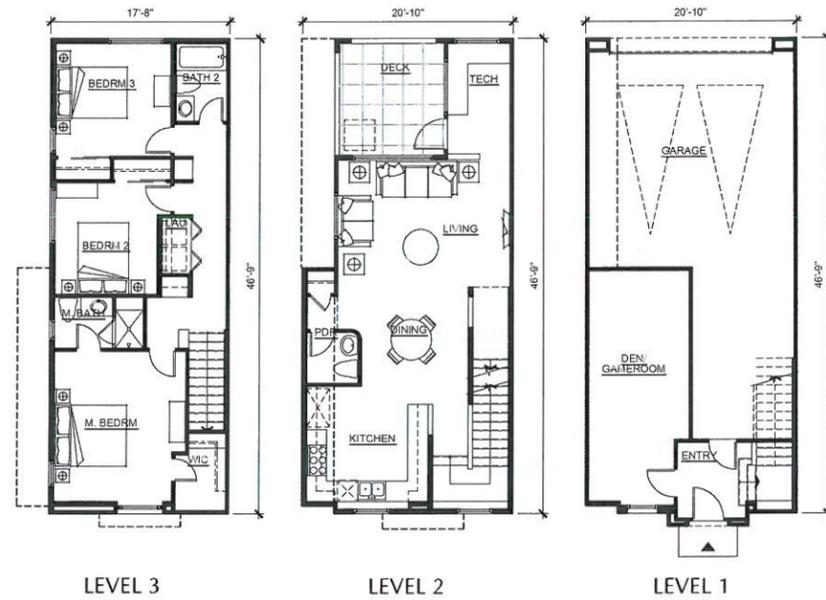
TERRA BRONZE COURTYARD COLLECTION STEEL GARAGE DOORS



SW 7027 - WELL BRED BROWN

COLORS AND MATERIALS BOARD

INTEGRAL COMMUNITIES DANVILLE, CA



LEVEL 3

LEVEL 2

LEVEL 1

UNIT 3

3 BED, DEN, 2.5 BA, 1,850 SF
 GARAGE 596 SF
 DECK 120 SF



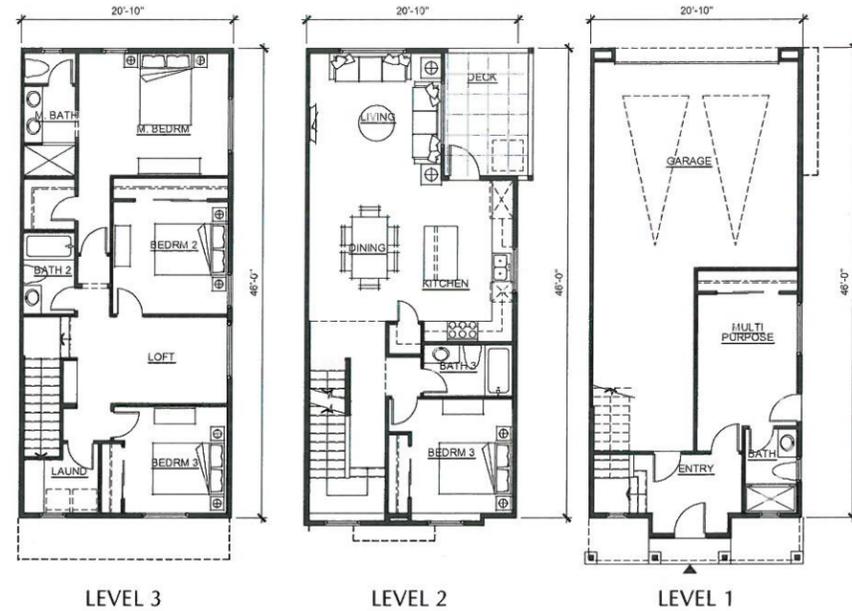
LEVEL 3

LEVEL 2

LEVEL 1

UNIT 1

2 BED, 2.5 BA, 1,485 SF
 GARAGE 640 SF
 DECK 104 SF
 PORCH 56 SF



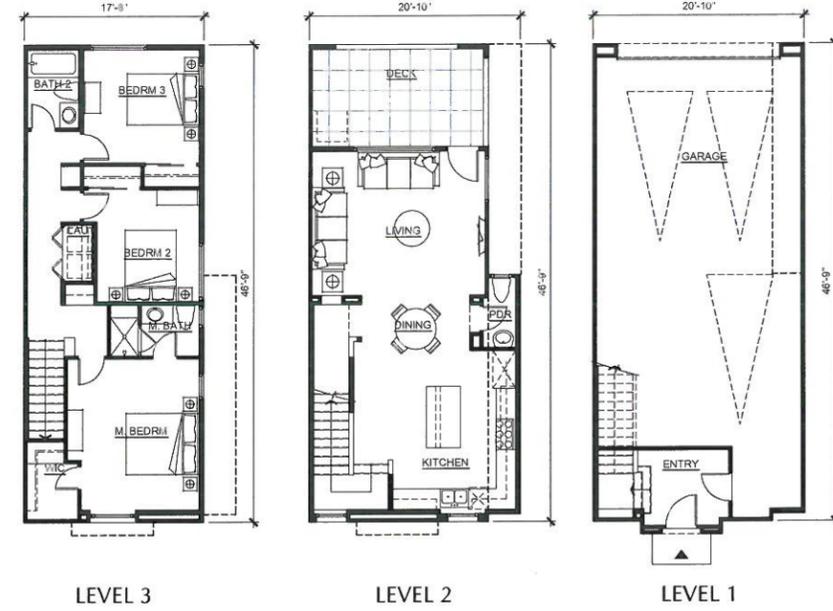
LEVEL 3

LEVEL 2

LEVEL 1

UNIT 4

4 BED, MULTI PURPOSE RM, 2,115 SF
 GARAGE 596 SF
 DECK 105 SF
 PORCH 78 SF



LEVEL 3

LEVEL 2

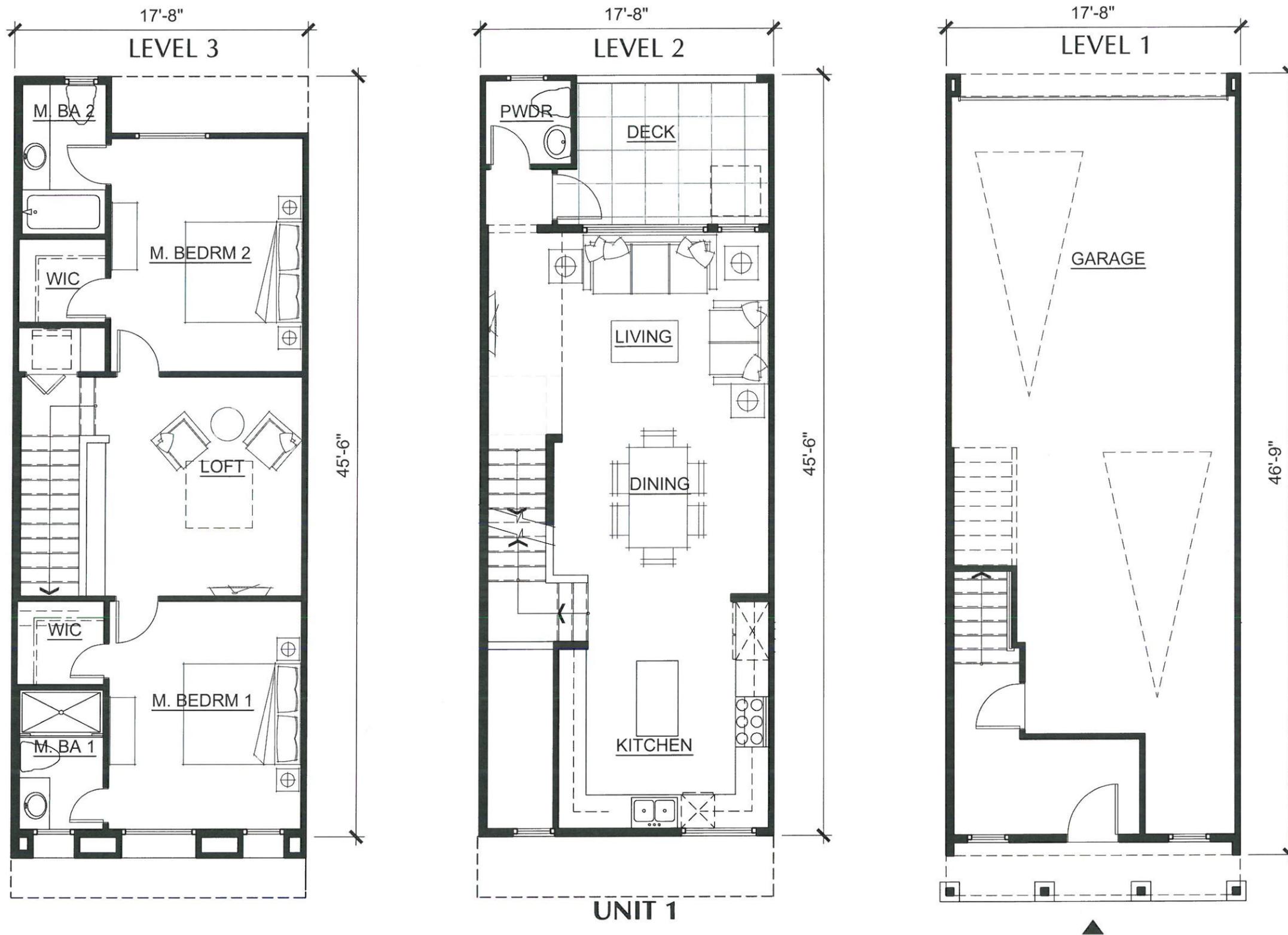
LEVEL 1

UNIT 2

3 BED, 2.5 BA, 1,570 SF
 GARAGE 814 SF
 DECK 166 SF

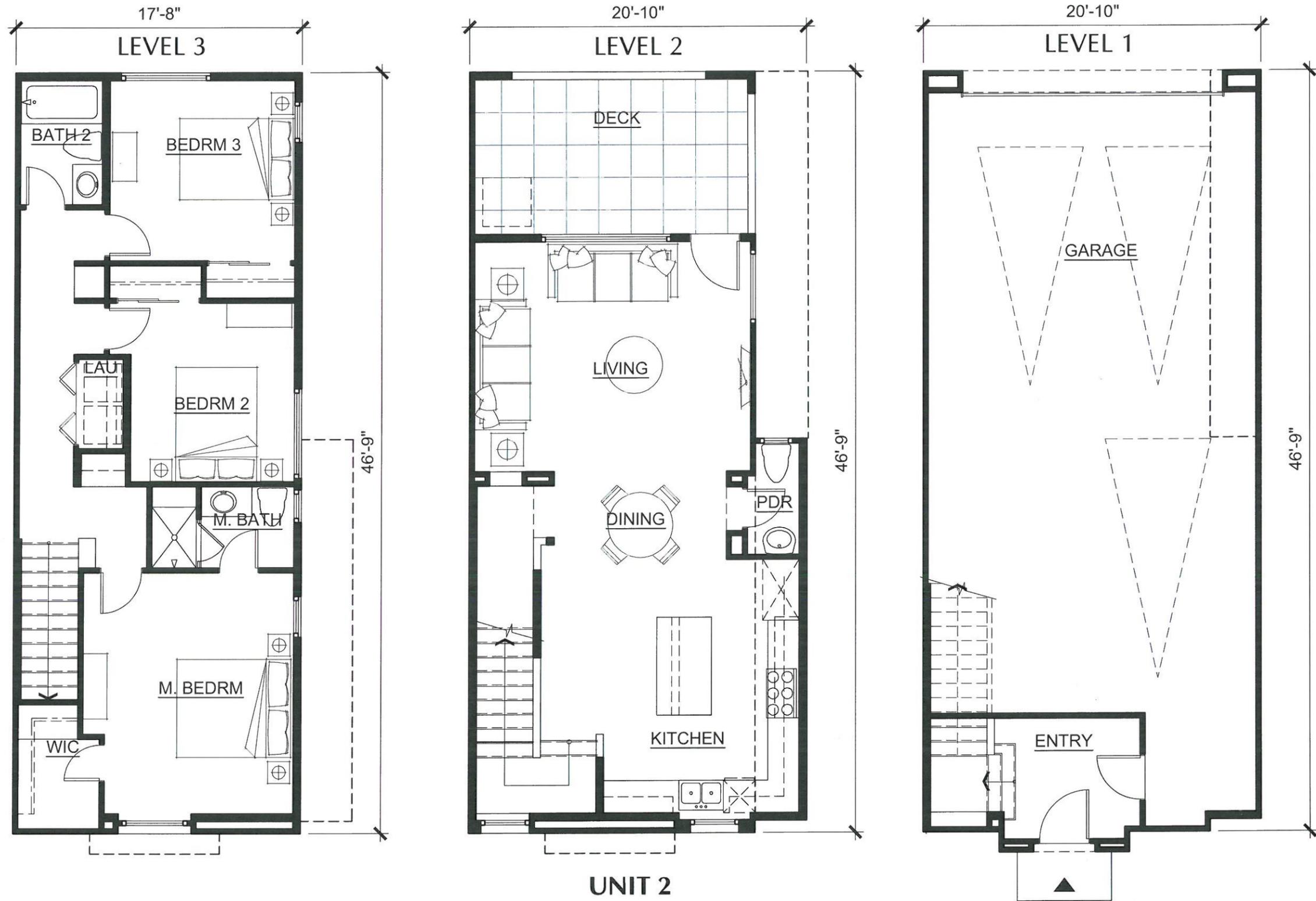


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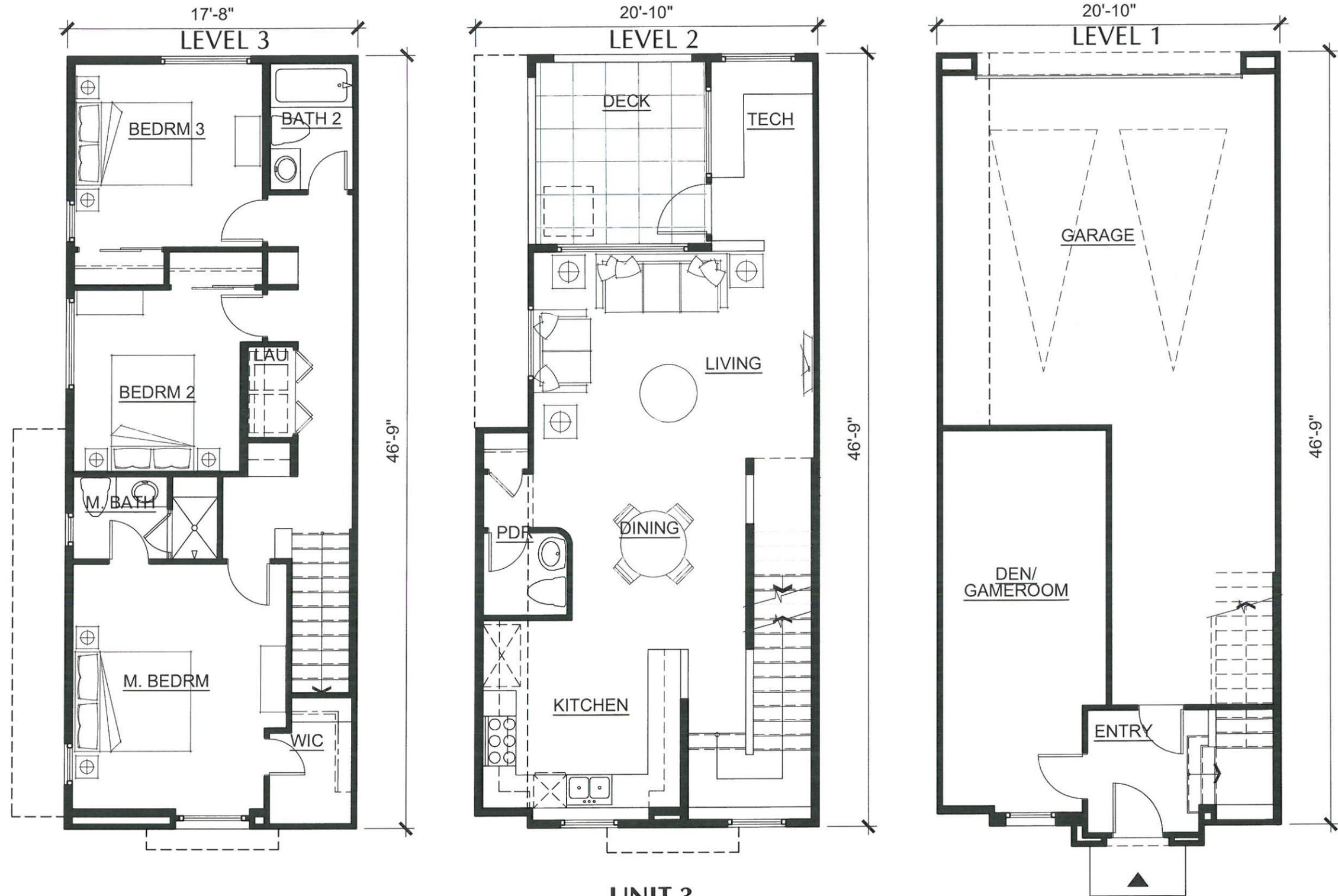
UNIT 1
2 BED, 2.5 BA - 1,485 SF
 GARAGE: 640 SF
 DECK: 104 SF
 PORCH: 56 SF

@ The Boulevard
 HAYWARD, CALIFORNIA



UNIT 2
3 BED, 2.5 BA - 1,570 SF
 GARAGE: 814 SF
 DECK: 166 SF

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UNIT 3
3 BED, DEN, 2.5 BA - 1,850 SF
 GARAGE: 596 SF
 DECK: 120 SF

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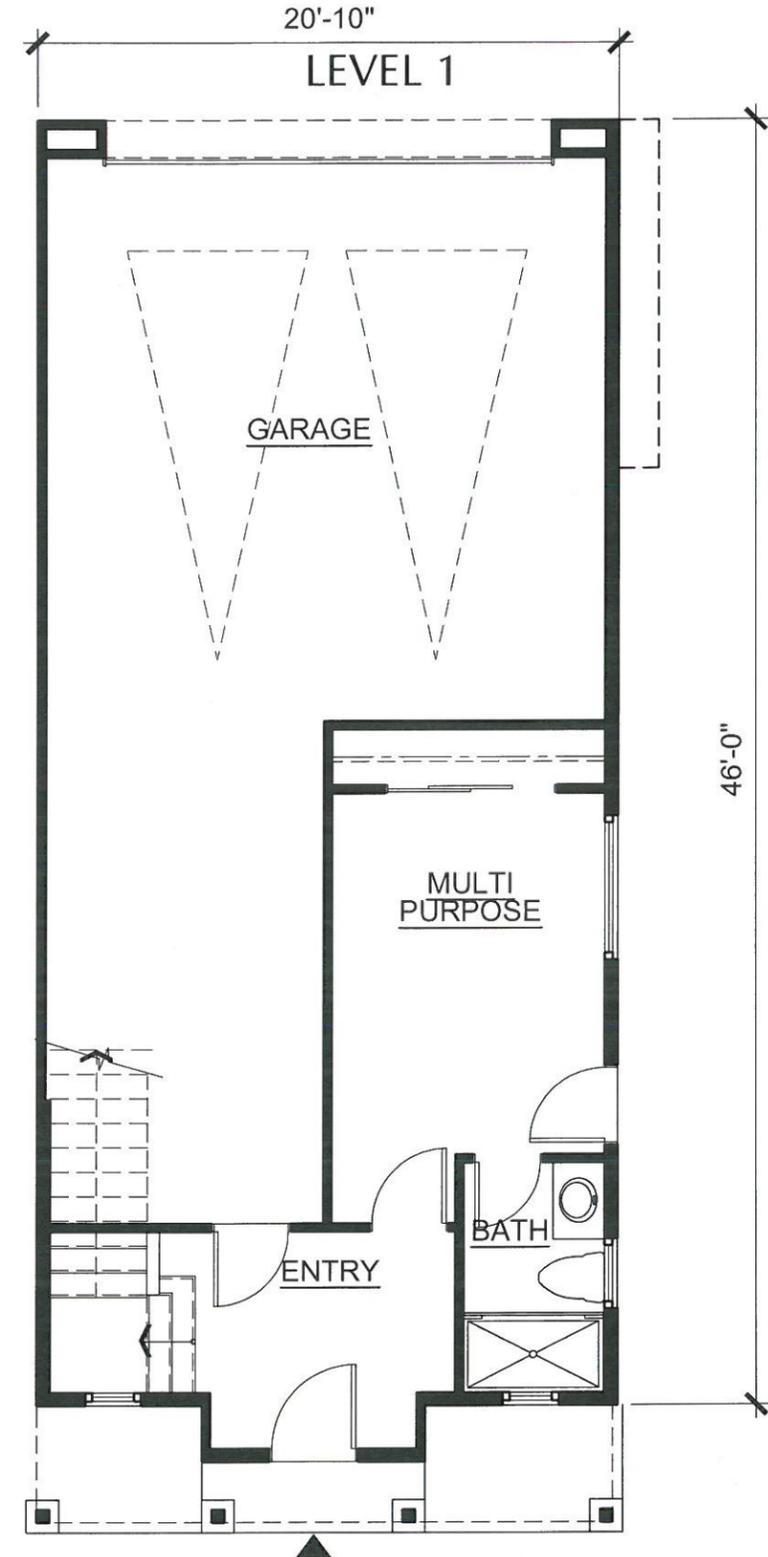
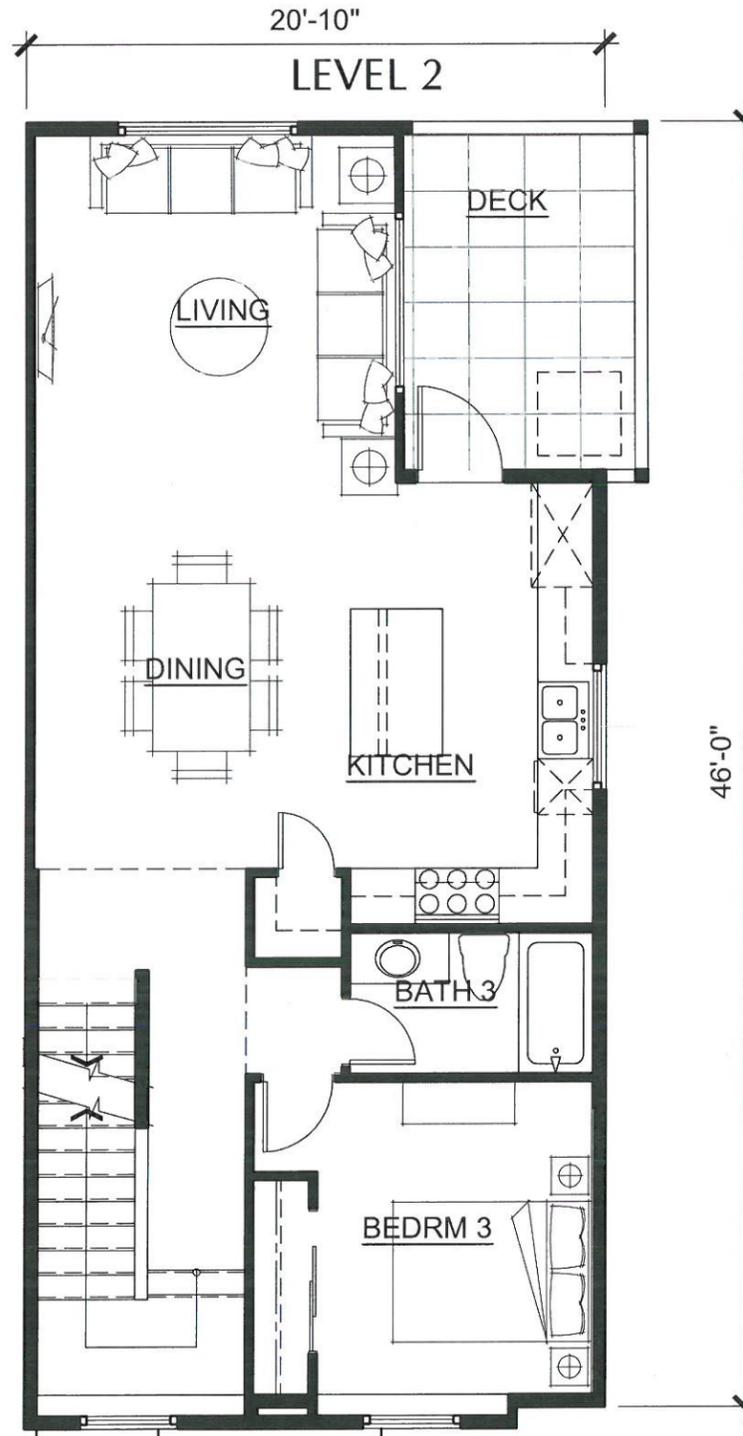
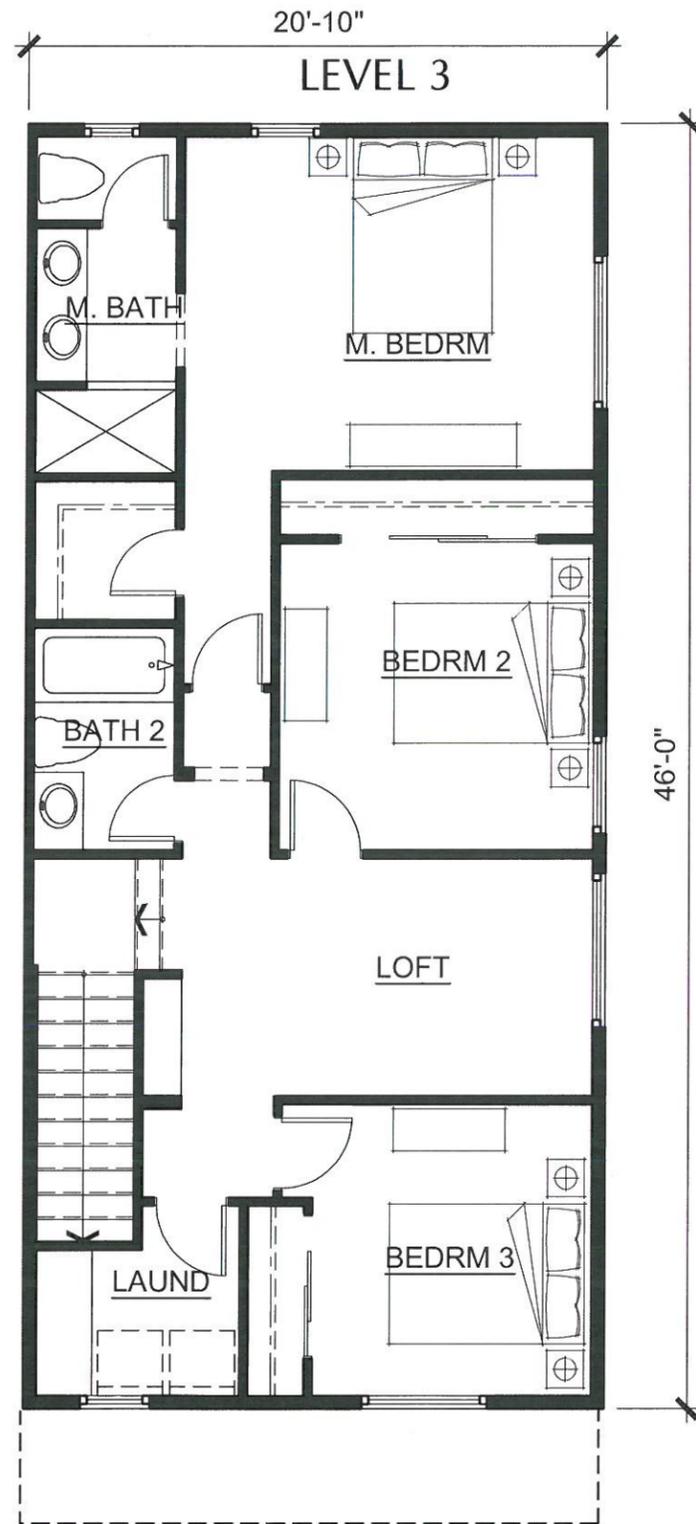
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A.TH.6-3
INTEGRAL
 Communities
A COMMITMENT TO EXCELLENCE

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UNIT 4
4 BED, MULTI PURPOSE
RM - 2,115 SF
 GARAGE: 596 SF
 DECK: 105 SF
 PORCH: 78 SF

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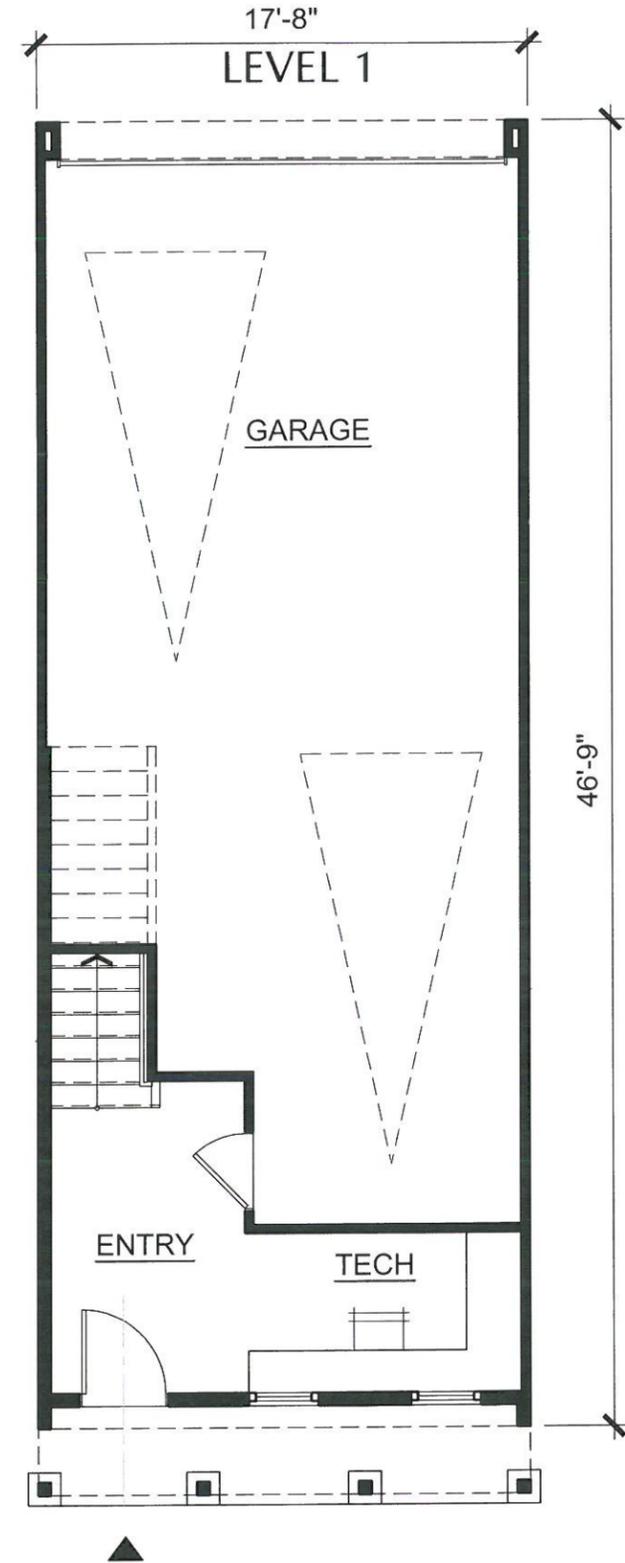
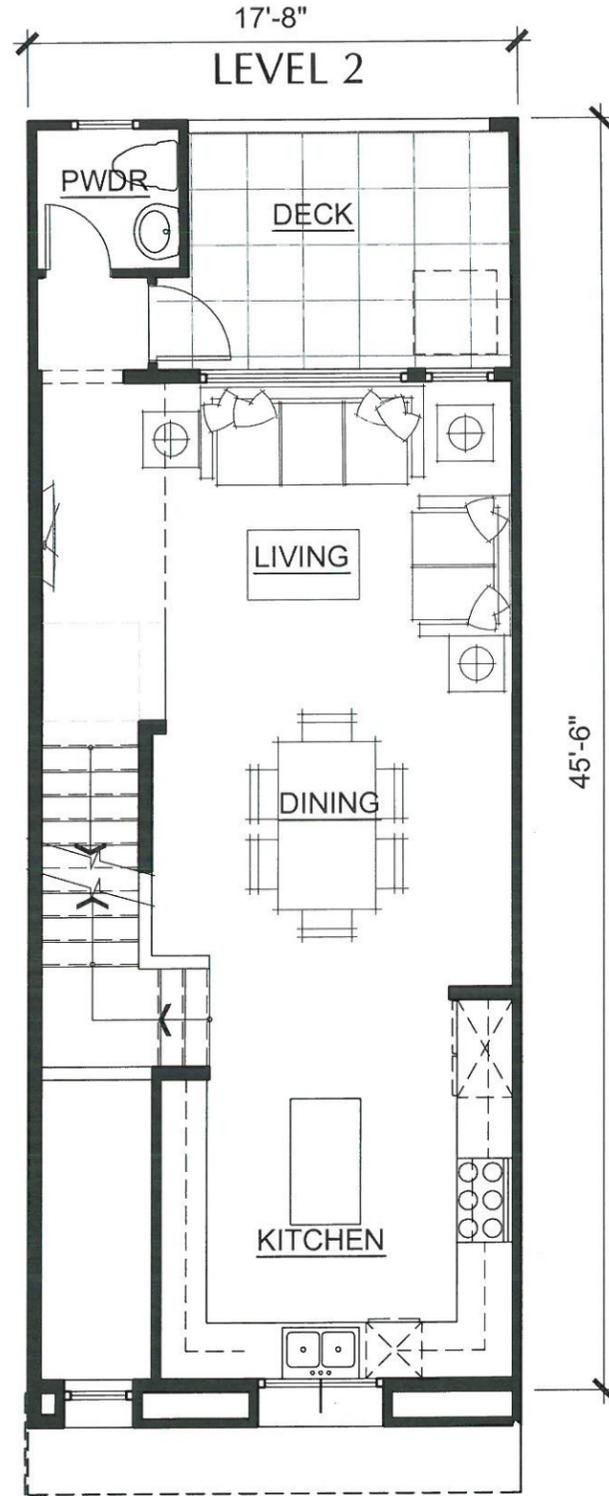
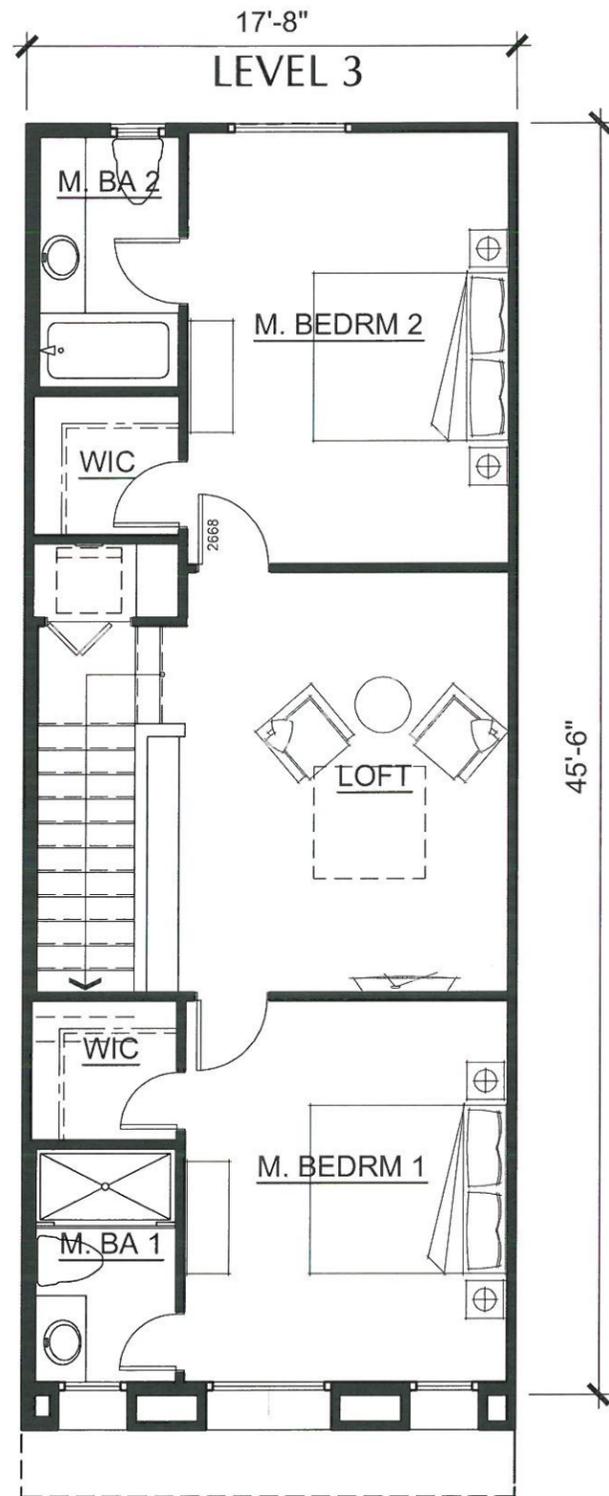
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A.TH.6-4
INTEGRAL
 Communities
 A DEVELOPED REAL ESTATE COMPANY

11.21.13
 33



UNIT 1-ALT.
2 BED, 2.5 BA, TECH - 1,589 SF
 GARAGE: 578 SF
 DECK: 104 SF
 PORCH: 56 SF

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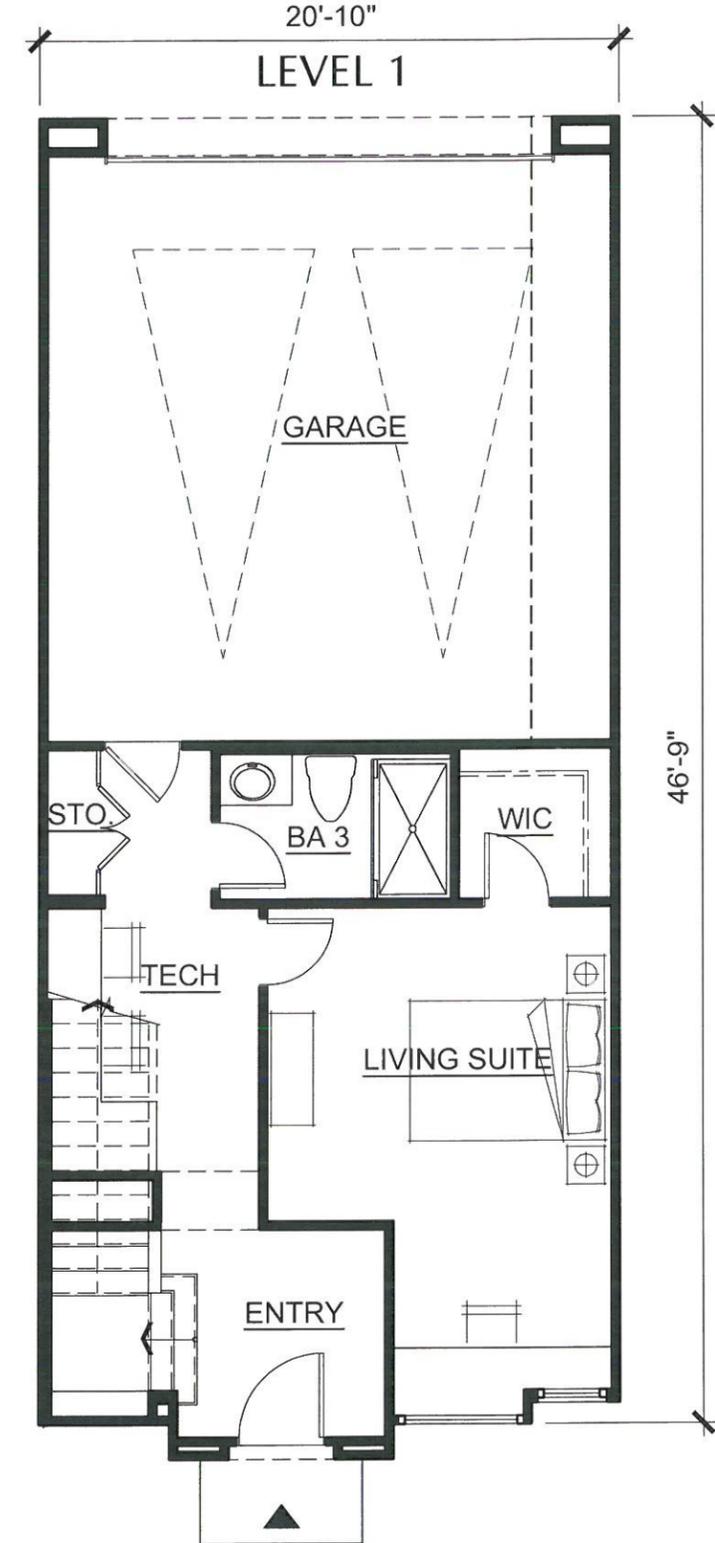
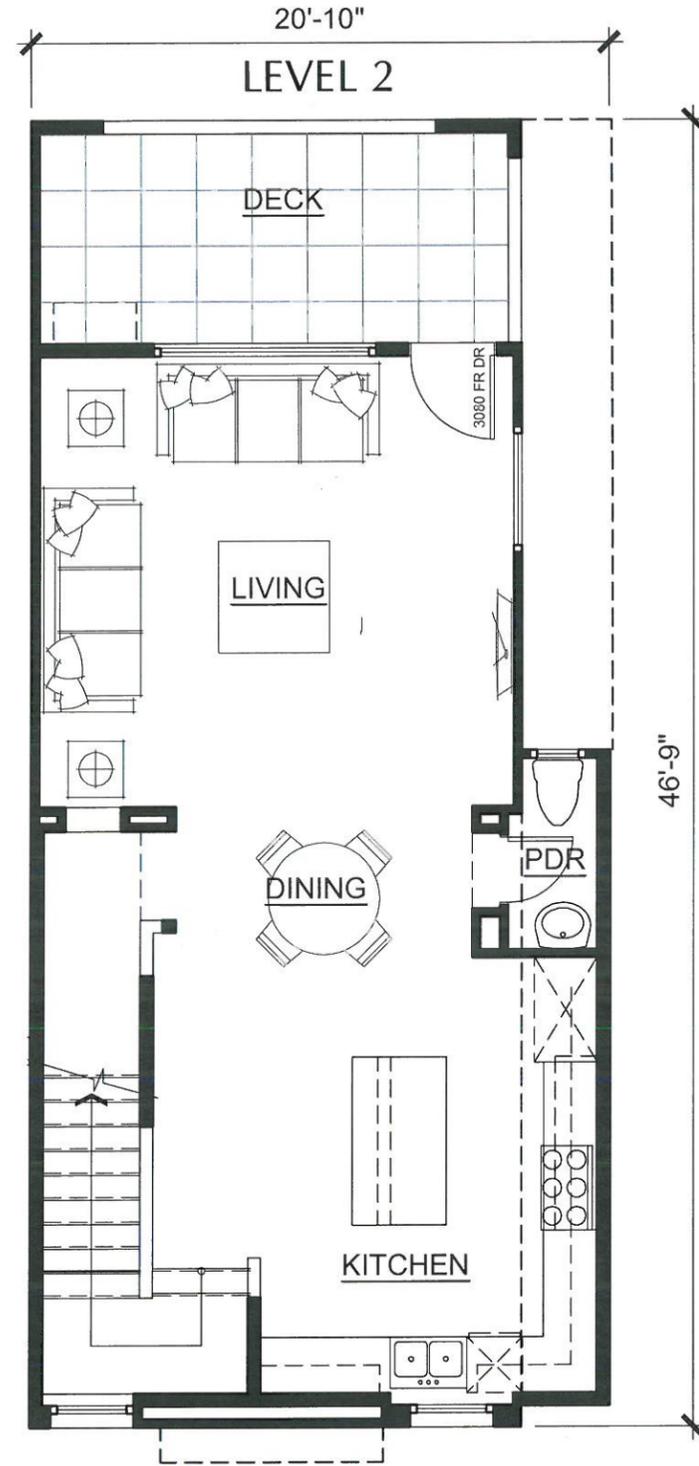
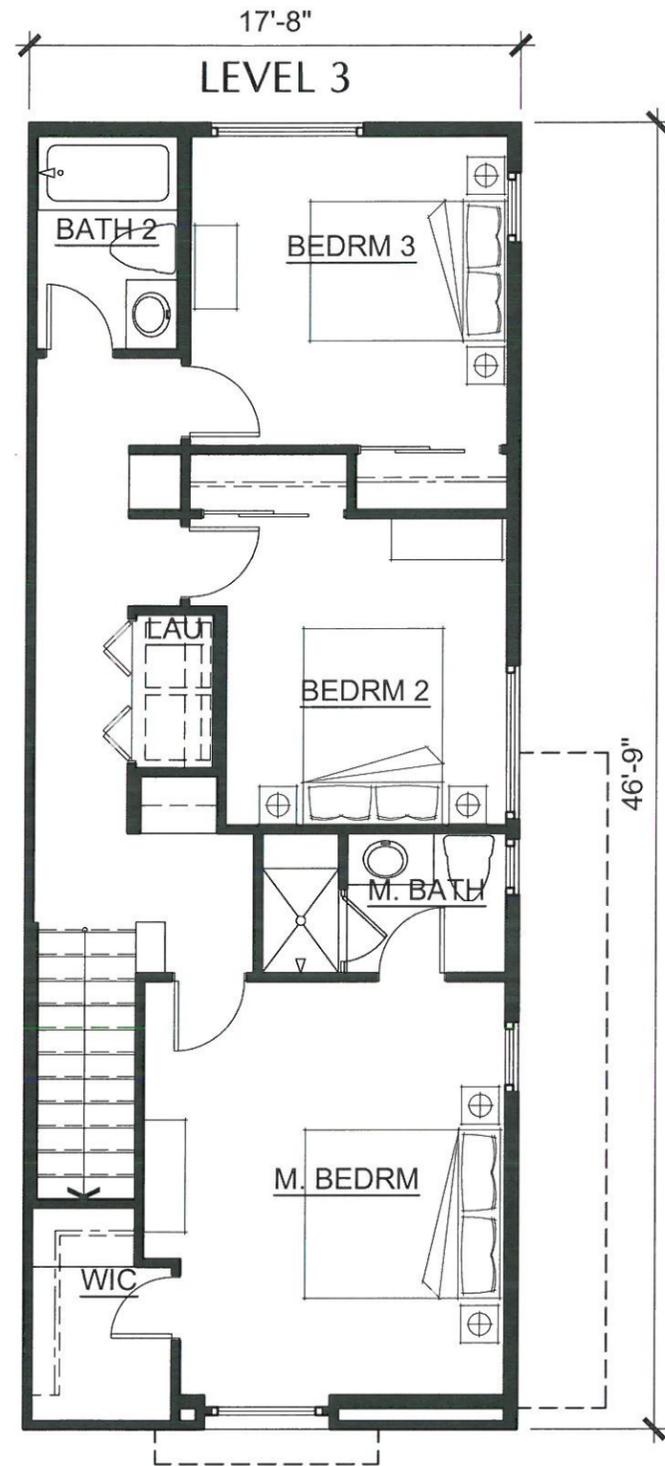
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A.TH.7-1
INTEGRAL
 Communities
A DEVELOPER OF QUALITY HOMES

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 34



UNIT 2-ALT.

3 BED, 3.5 BA, LIVINGSUITE,

TECH - 1,990 SF

GARAGE: 429 SF

DECK: 131 SF

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HAYWARD, CALIFORNIA

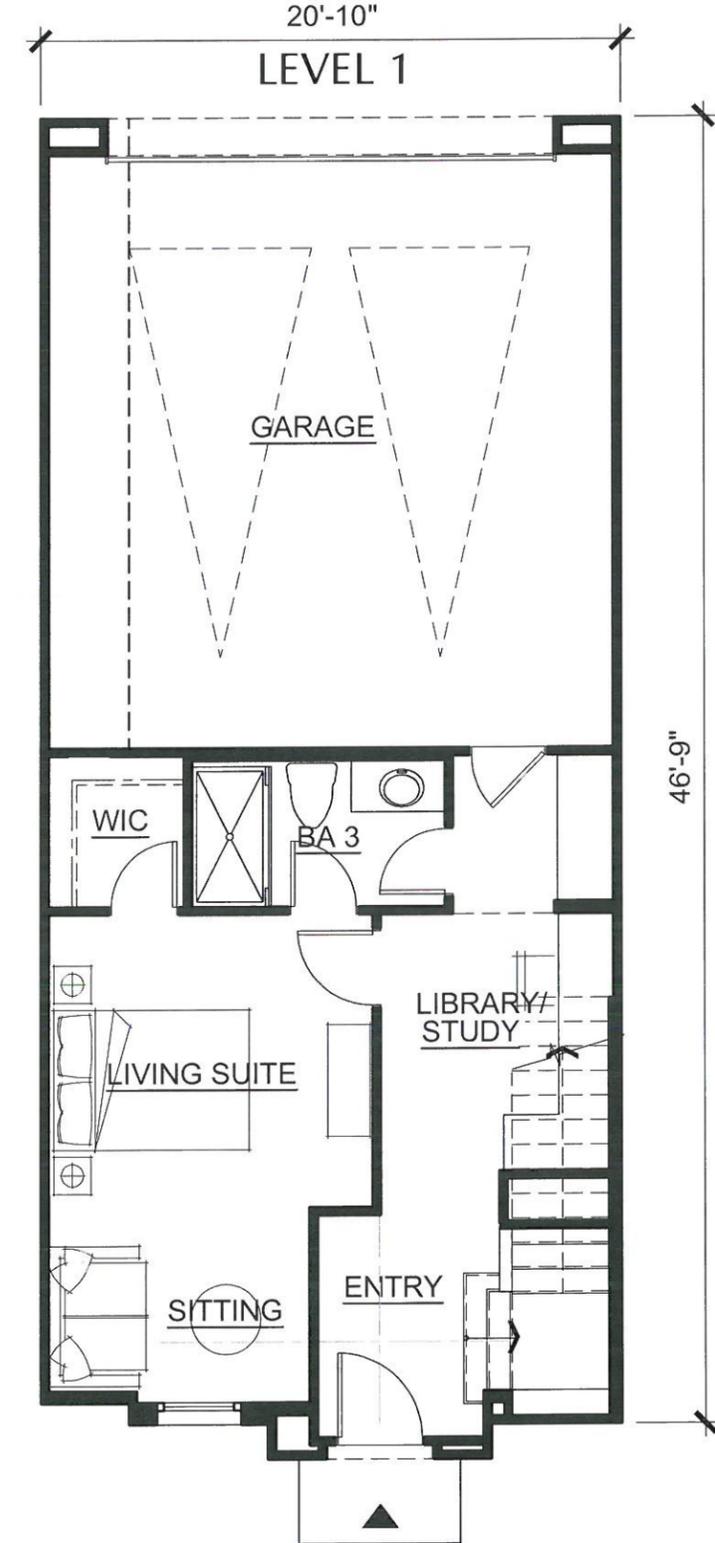
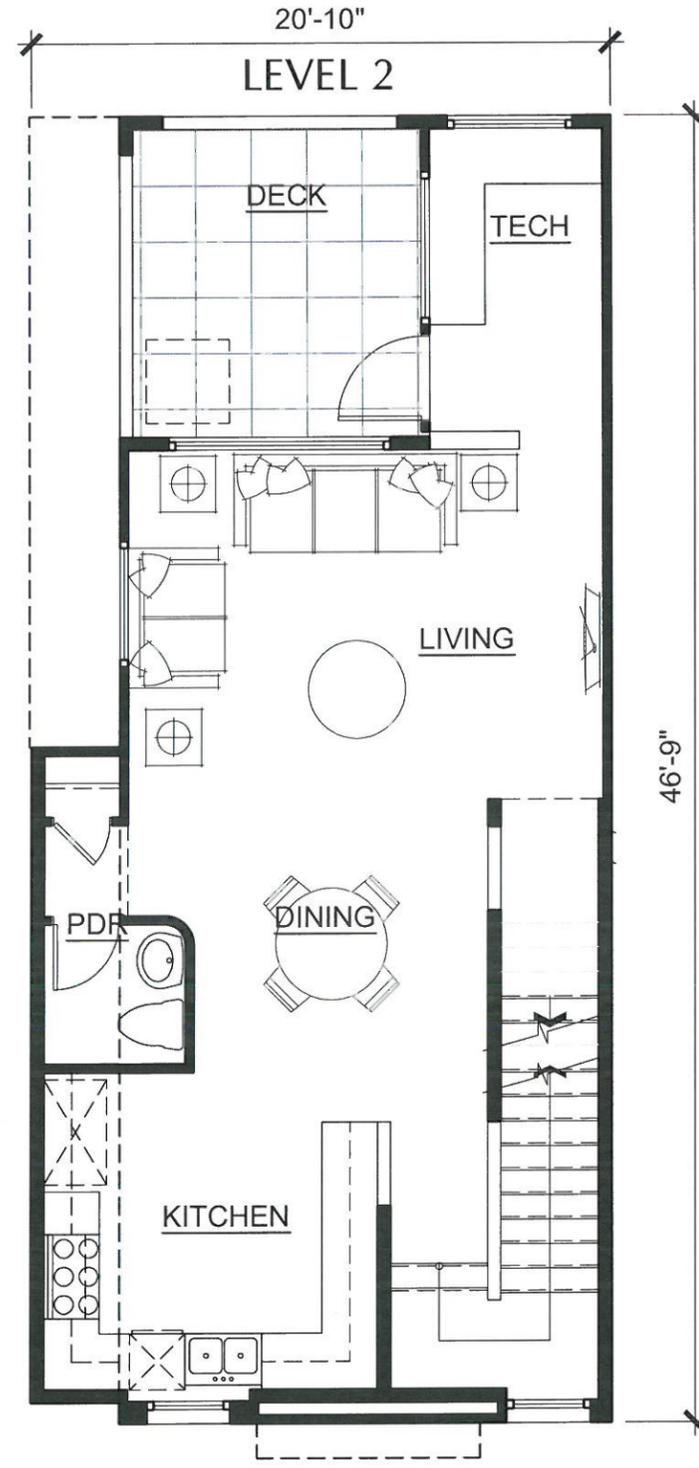
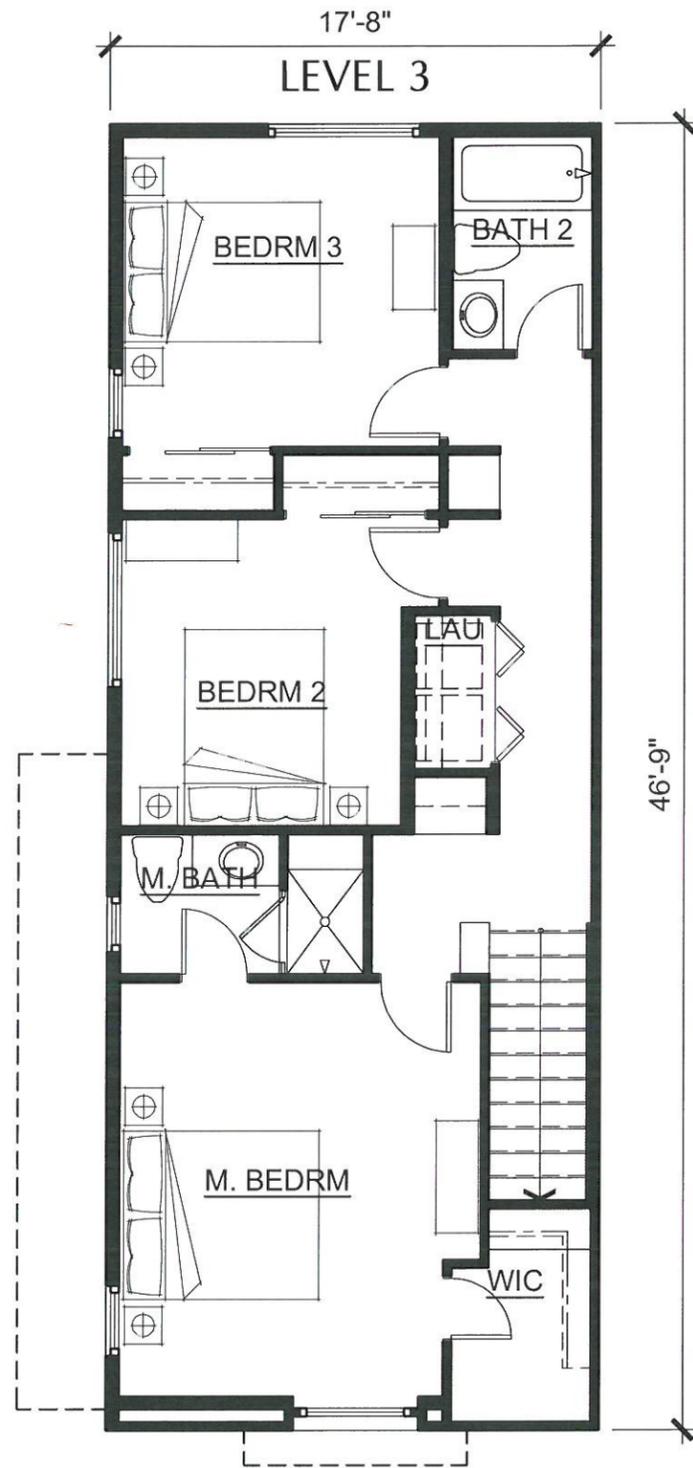
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A.TH.7-2
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Communities
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35



UNIT 3-ALT.
3 BED, DEN, 3.5 BA, LIVINGSUITE,
LIBRARY/STUDY - 2,030 SF

GARAGE 416 SF
 DECK 120 SF

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 HAYWARD, CALIFORNIA

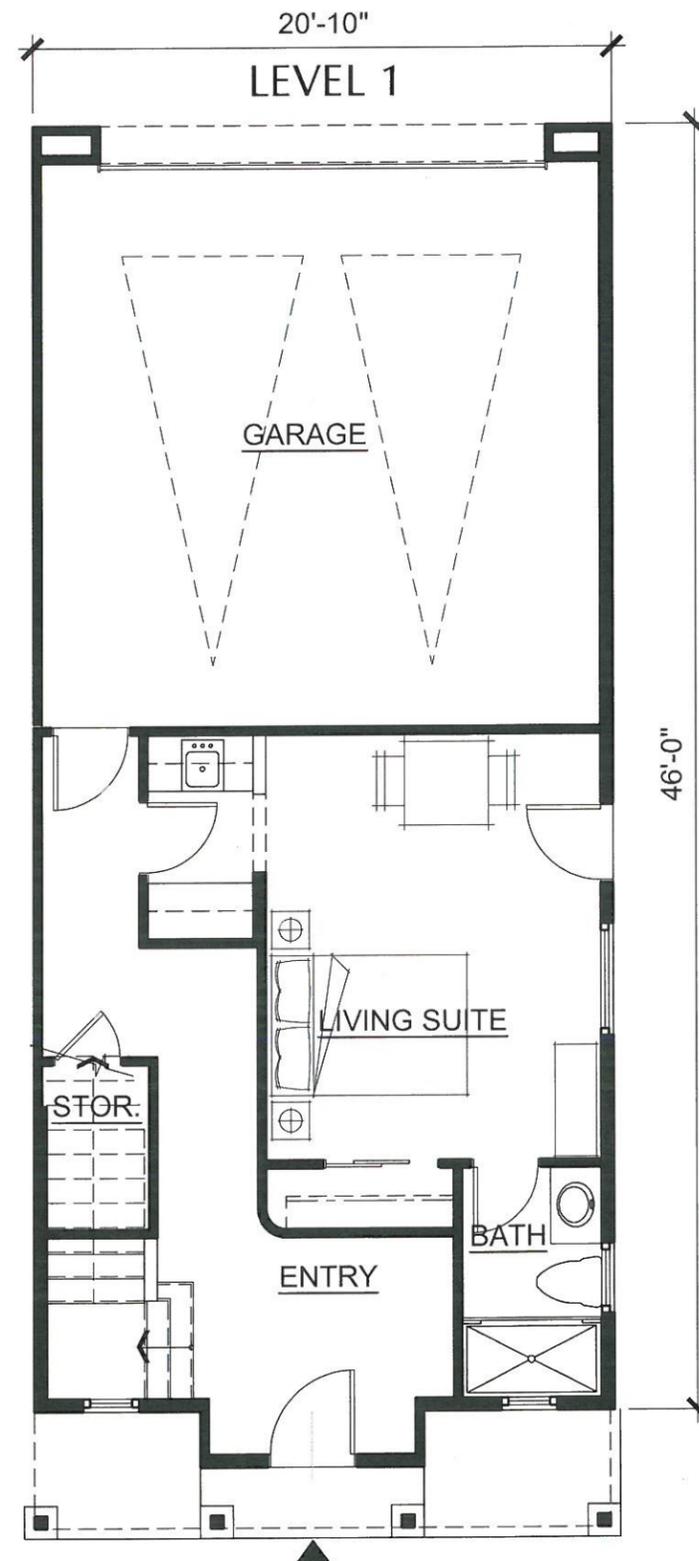
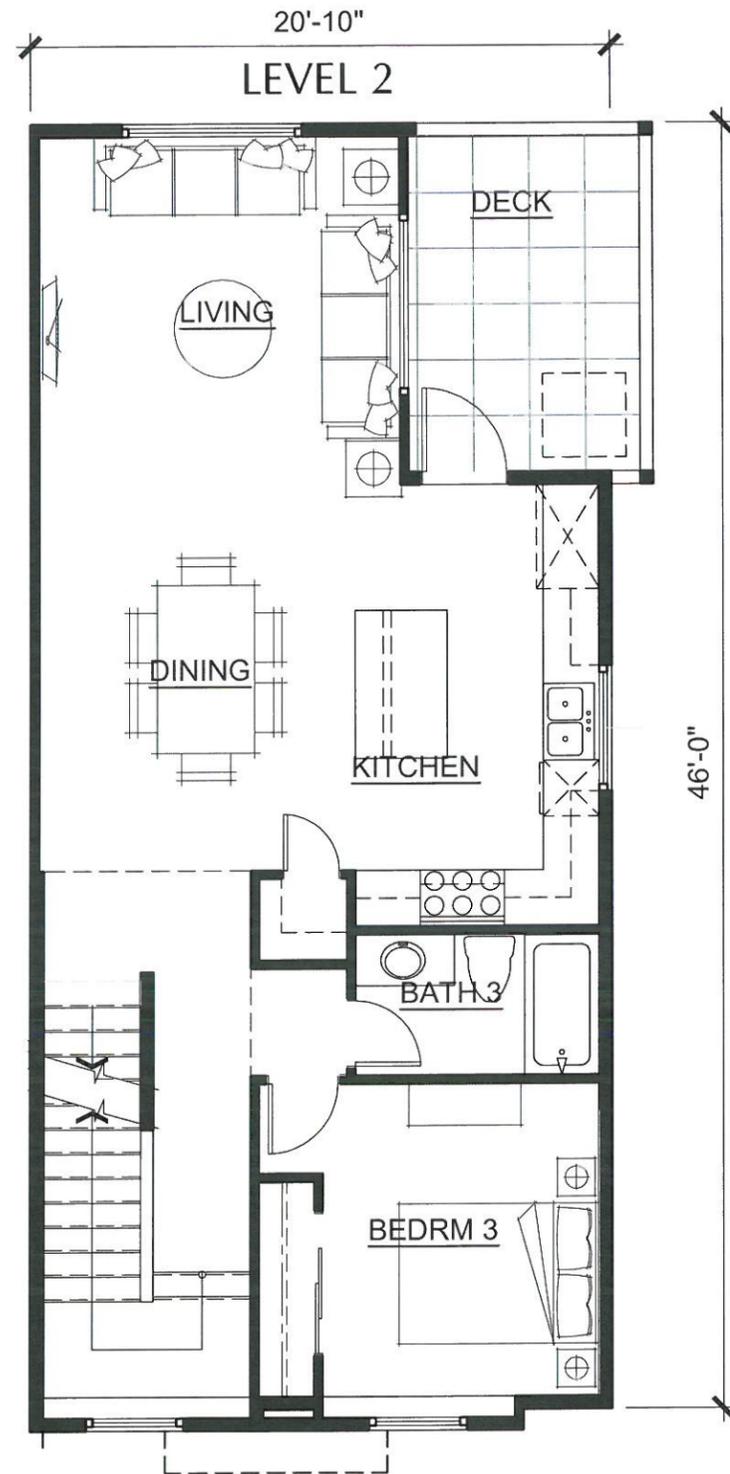
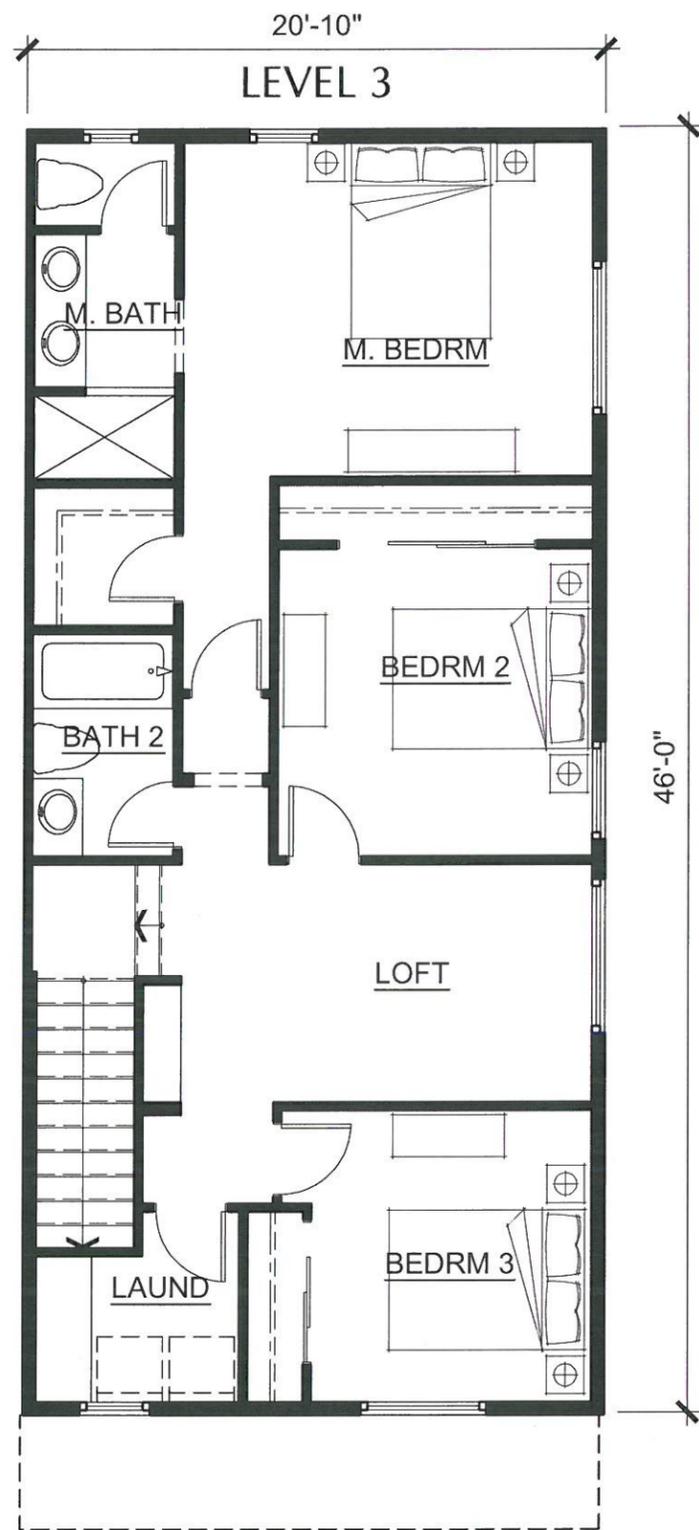
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A.TH.7-3
INTEGRAL
 Communities
A DEVELOPER'S REAL ESTATE COMPANY

11.21.13
 36



UNIT 4-ALT.
4 BED, 4 BA, LIVINGSUITE - 2,295 SF

GARAGE 416 SF
 DECK 105 SF
 PORCH 78 SF

@ **The Boulevard**
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 INTEGRAL COMMUNITIES
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 SUITE 102
 DANVILLE, CA 94526



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"WATER TREATMENT MEADOW" AMENITIES
- BIO-RETENTION BASIN WITH PLANTING

EXISTING GAS STATION

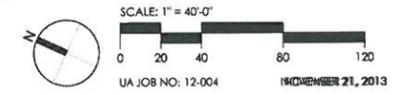
"THE HANGOUT" AMENITIES
- OUTDOOR DINING AND LOUNGE SEATING UNDER OVERHEADS
- COUNTERTOP WITH TWO BARBEQUES
- LARGE TURF PLAY AREA
- BIO-RETENTION BASIN WITH PLANTING
- CHILDREN'S PLAY STRUCTURES



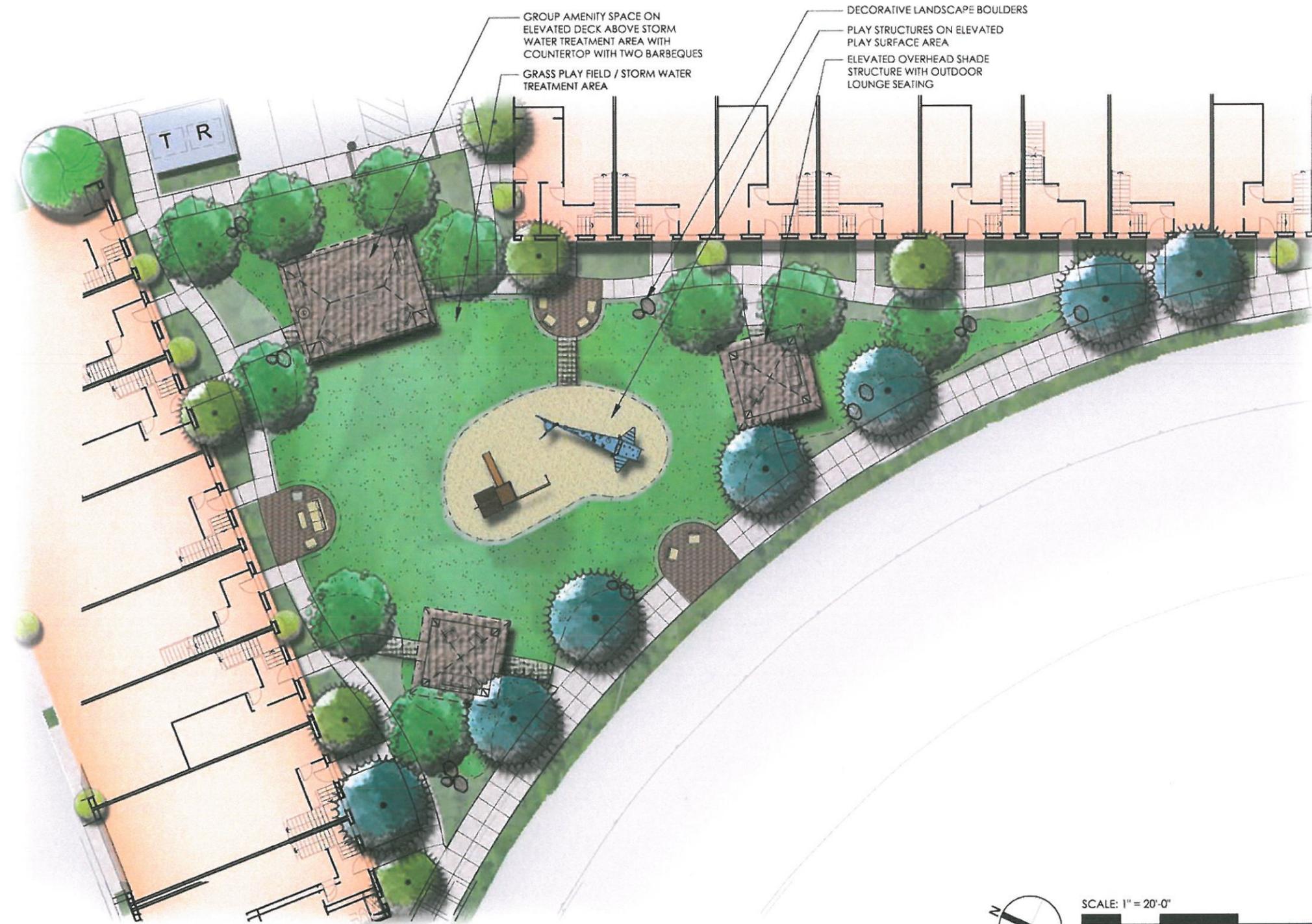
@ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

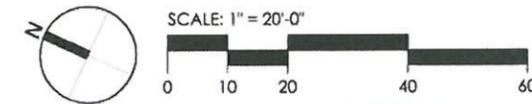
PRELIMINARY LANDSCAPE PLAN



L1

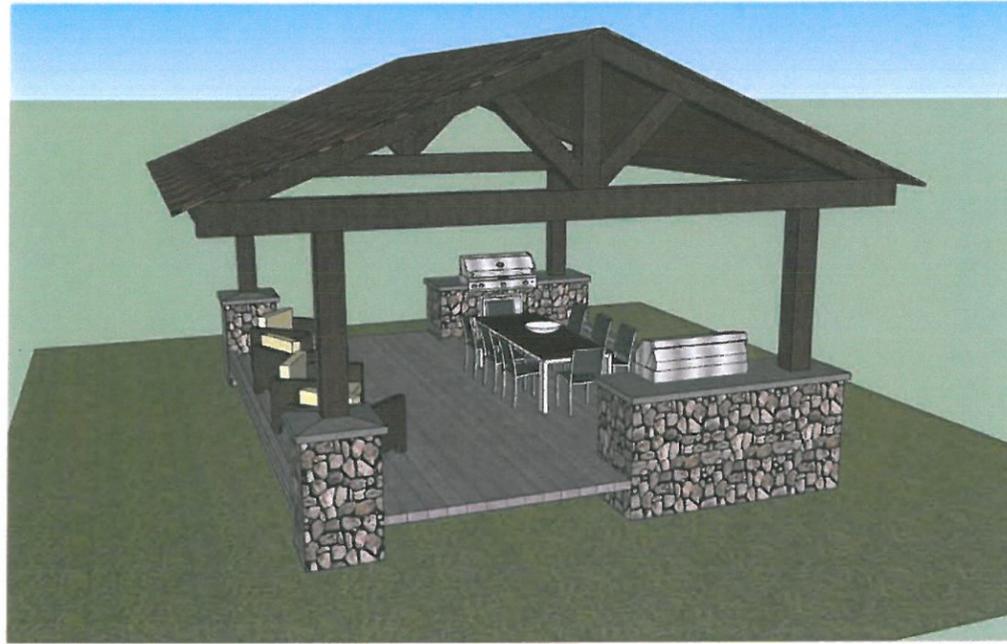


THE HANGOUT

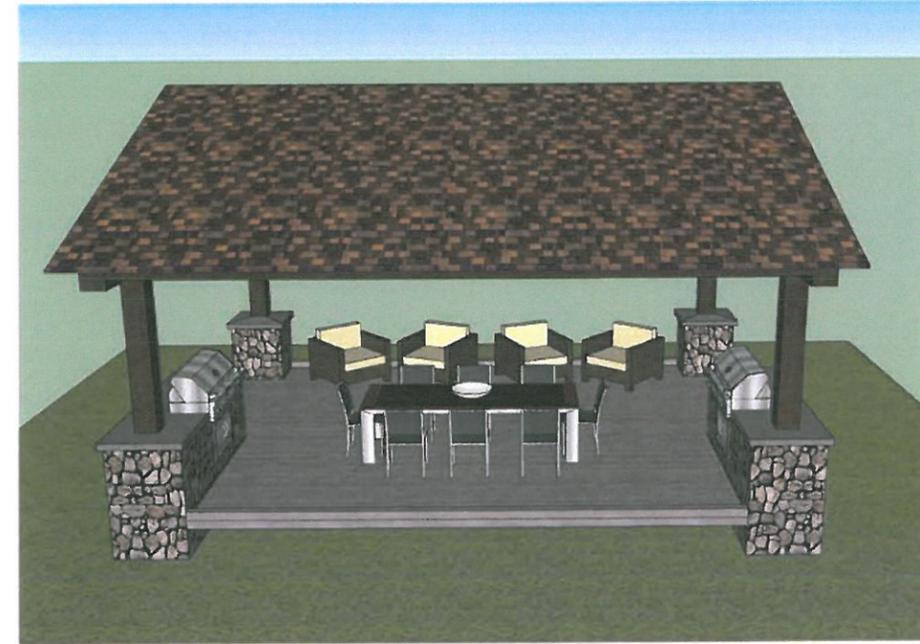




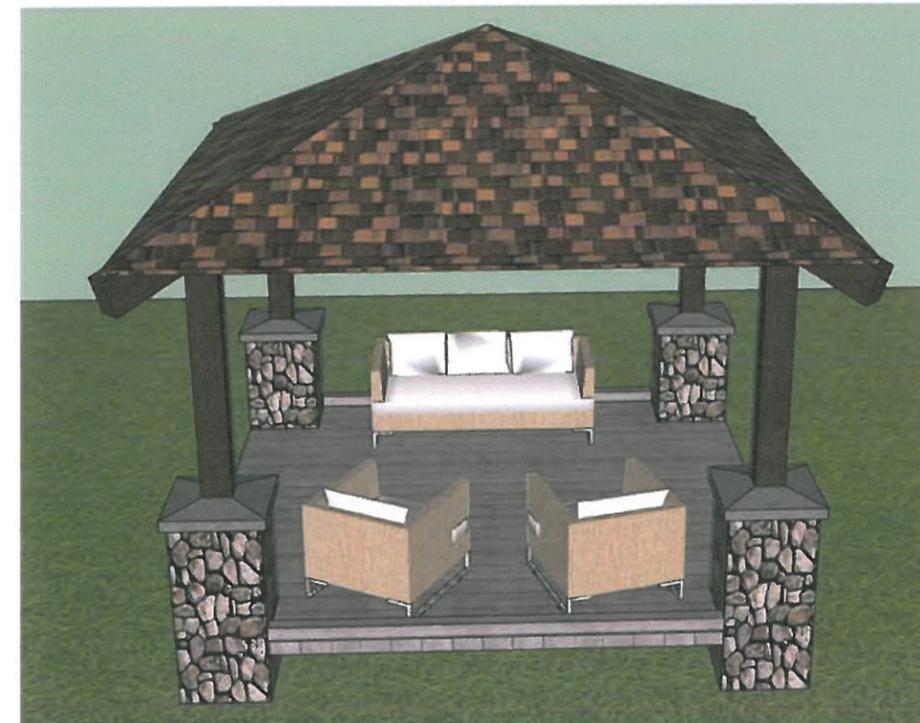
PROPOSED PLAY STRUCTURES FOR MULTIPLE AGE GROUPS

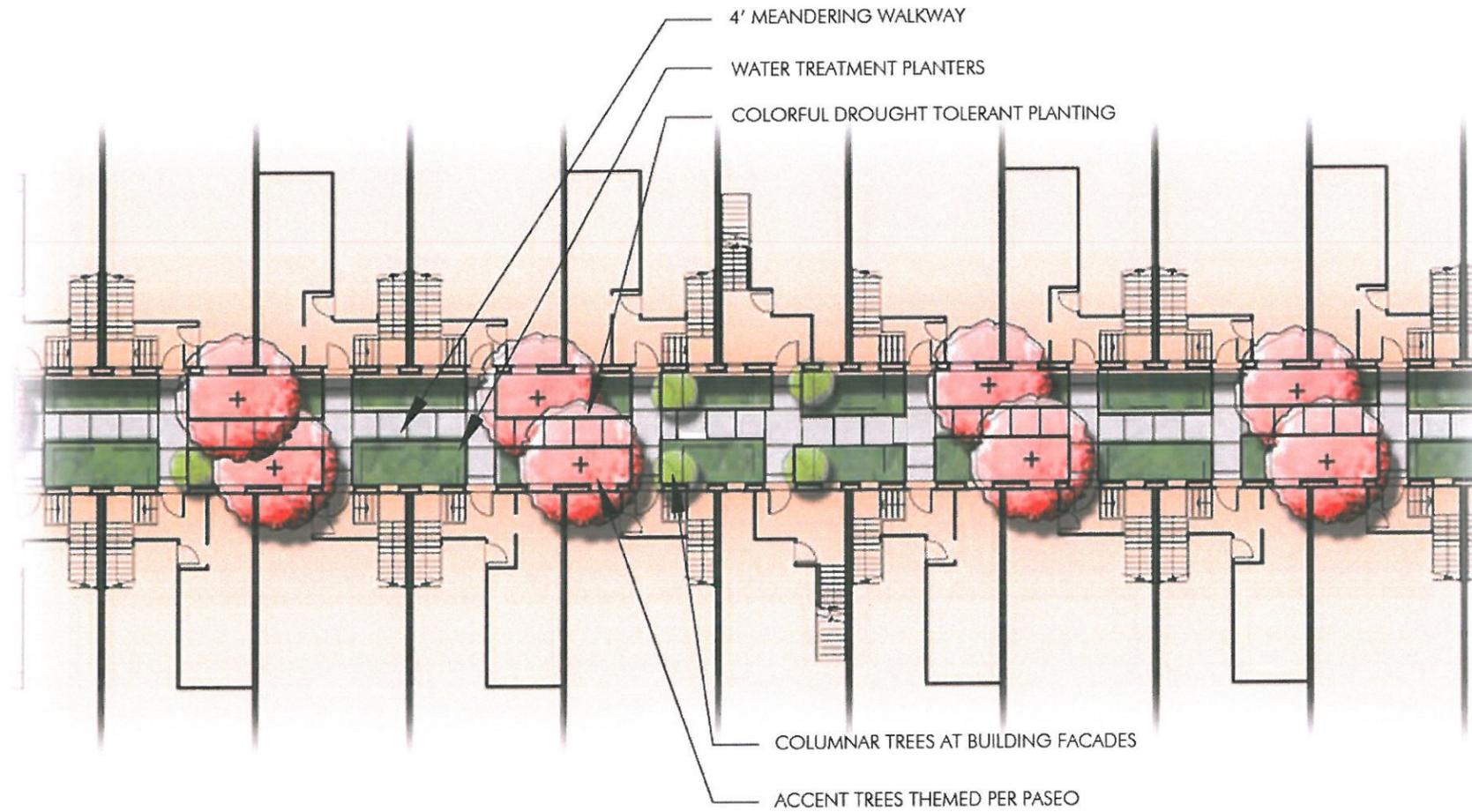


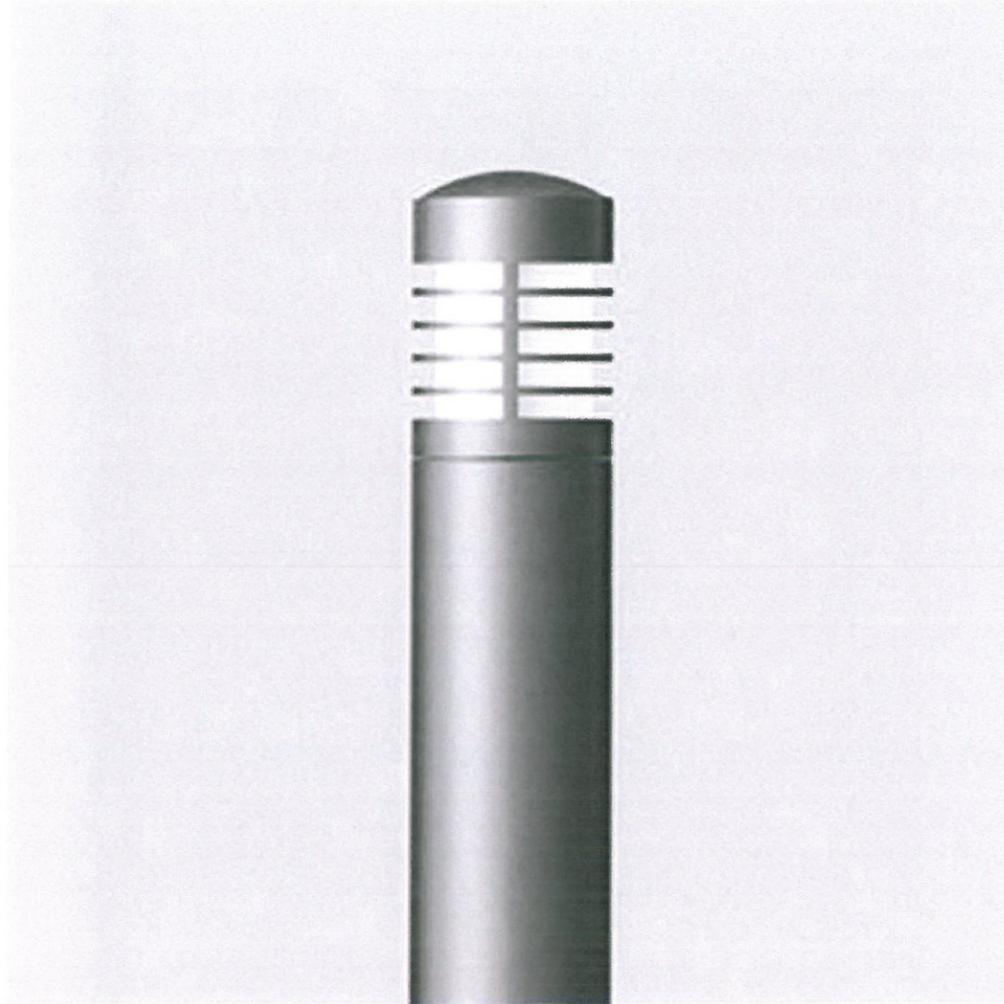
GROUP SHADE STRUCTURE WITH BARBECUES & OUTDOOR DINING



INTIMATE SHADE STRUCTURE WITH OUTDOOR LOUNGE SEATING





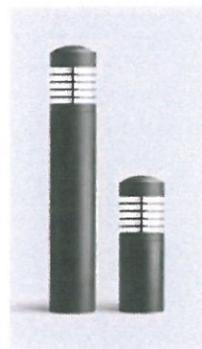
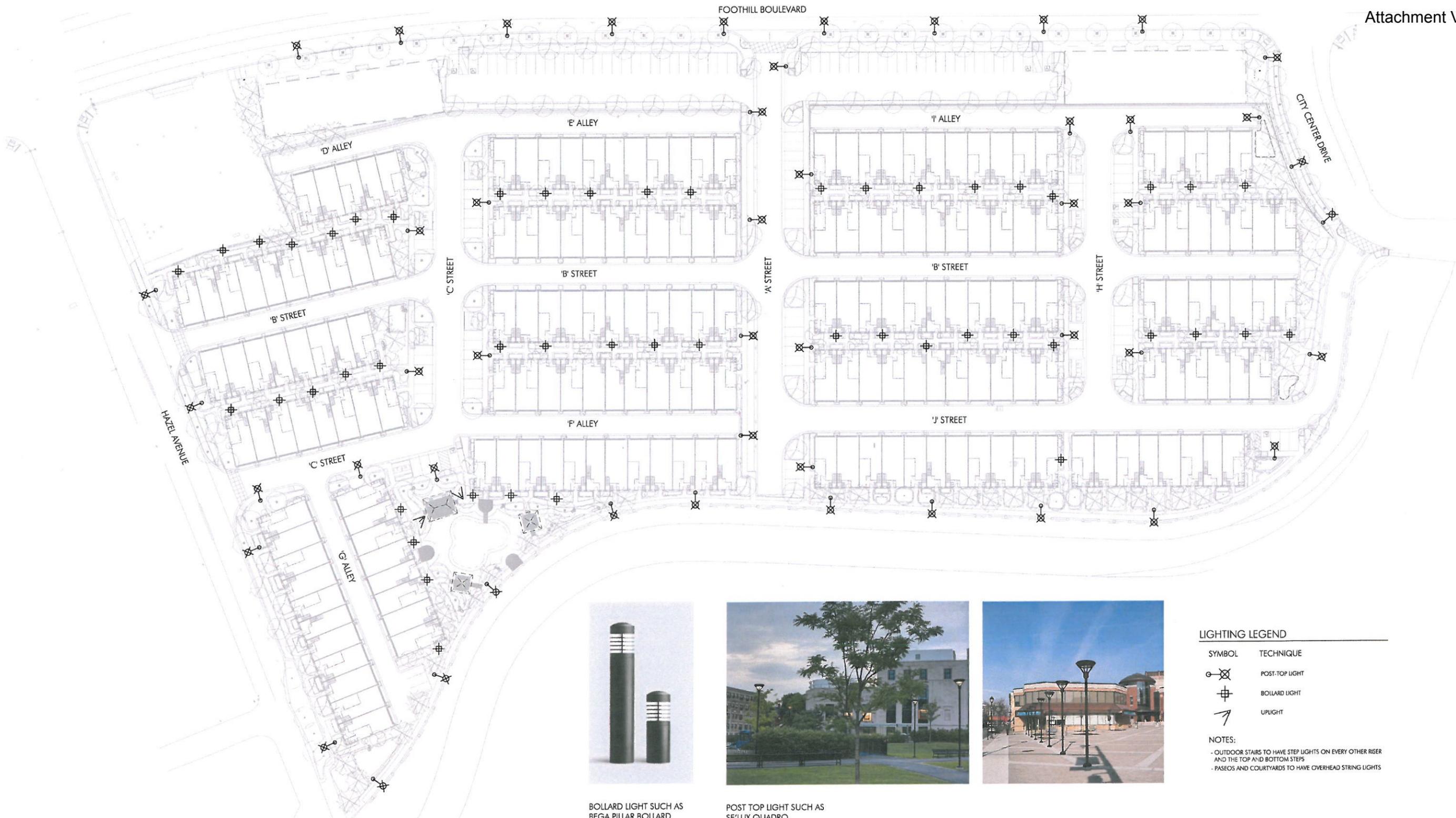


BOLLARD LIGHT SUCH AS BEGA PILLA BOLLARD



POST TOP LIGHT SUCH AS SE'LUX QUADRO





BOLLARD LIGHT SUCH AS BEGA PILLAR BOLLARD



POST TOP LIGHT SUCH AS SE'LUX QUADRO



LIGHTING LEGEND

SYMBOL	TECHNIQUE
	POST-TOP LIGHT
	BOLLARD LIGHT
	UPLIGHT

NOTES:

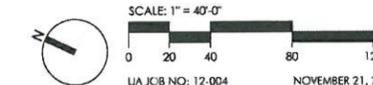
- OUTDOOR STAIRS TO HAVE STEP LIGHTS ON EVERY OTHER RISER AND THE TOP AND BOTTOM STEPS
- PASEOS AND COURTYARDS TO HAVE OVERHEAD STRING LIGHTS



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CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



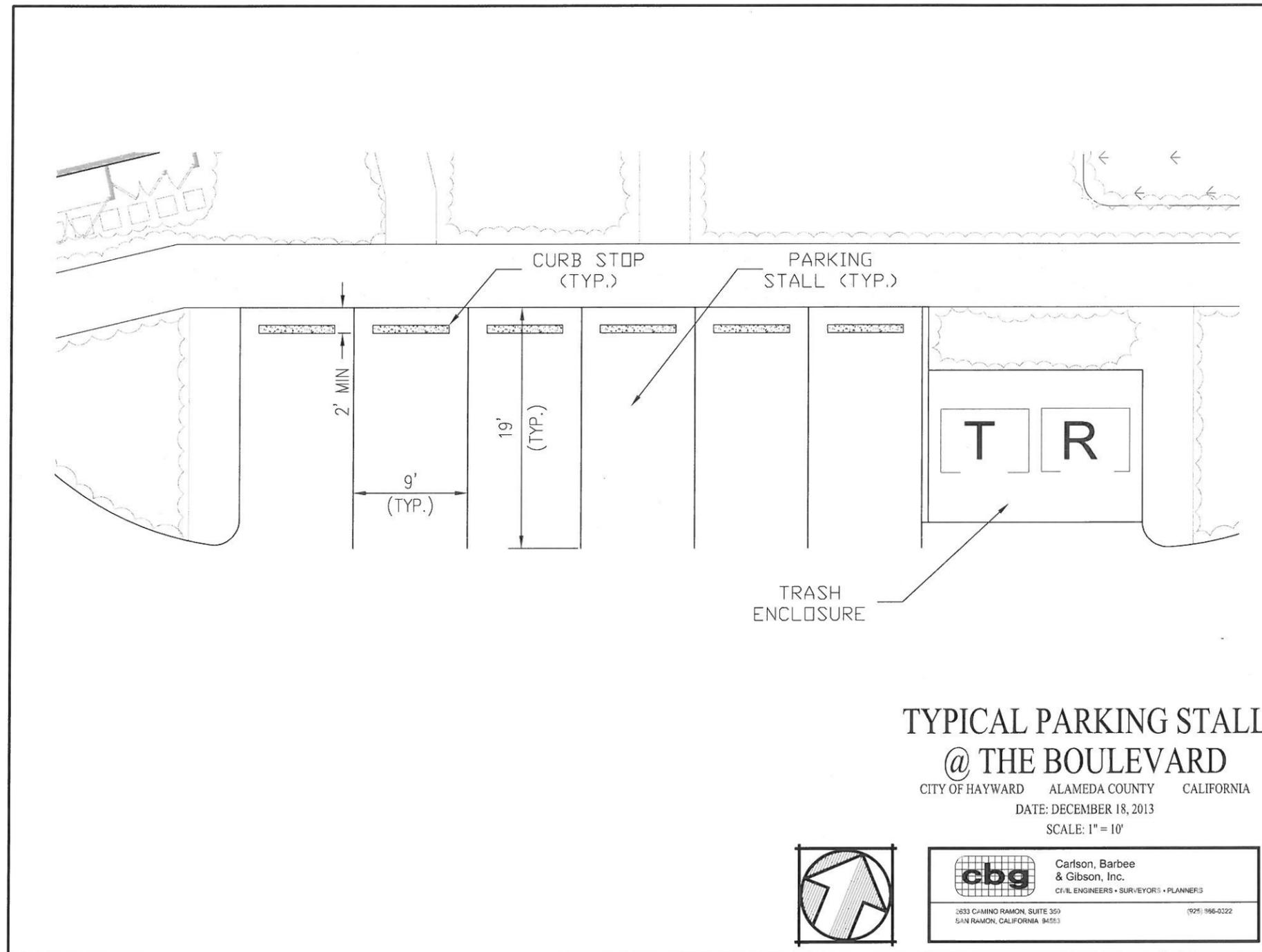
PRELIMINARY LIGHTING PLAN



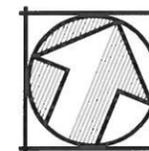
L3



DISTICTIS RIVERS - PARTHENOCISSUS TRICUSPIDATA - FICUS PUMILA
PROPOSED VINE SPECIES FOR SCREENING BLOCK WALL



TYPICAL PARKING STALL
@ THE BOULEVARD
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA
DATE: DECEMBER 18, 2013
SCALE: 1" = 10'



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	2633 CAMINO RAMON, SUITE 359 SAN RAMON, CALIFORNIA 94583

(925) 966-0322

G:\2012\ACAD\EXHIBITS\XB_CURB STOP DETAIL.DWG



VICINITY MAP
NOT TO SCALE

WE, 22301 FOOTHILL HAYWARD, LLC, AGREE TO THE FILING OF SAID MAP AND TO COMPLY WITH THE PROVISIONS OF THE CITY OF HAYWARD SUBDIVISION ORDINANCE AND THE STATE MAP ACT AS THEY APPLY TO THE PROCESSING AND APPROVAL OF SAID MAP.
BY: _____ DATE: _____
AS IT APPLIES TO THE REAL PROPERTY IDENTIFIED AS A.P.N. #428-0026-068-01

WE, MDS REALTY II, LLC, AGREE TO THE FILING OF SAID MAP AND TO COMPLY WITH THE PROVISIONS OF THE CITY OF HAYWARD SUBDIVISION ORDINANCE AND THE STATE MAP ACT AS THEY APPLY TO THE PROCESSING AND APPROVAL OF SAID MAP.
BY: _____ DATE: _____
AS IT APPLIES TO THE REAL PROPERTY IDENTIFIED AS A.P.N. #428-0026-067-03

I, RYAN HANSEN, CERTIFY THAT THIS TENTATIVE MAP WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT IT COMPLIES WITH THE CITY OF HAYWARD SUBDIVISION ORDINANCE AND THE STATE MAP ACT.
BY: _____ DATE: _____
RYAN HANSEN, RCE #80557

THIS TENTATIVE MAP SUBMITTAL HAS BEEN PREPARED IN ACCORDANCE WITH THE DUE DILIGENCE LEVEL REPORT DATED FEBRUARY 10, 2012 AND A SUPPLEMENTAL GEOTECHNICAL RECOMMENDATION DATED MARCH 13, 2013.
BY: _____ DATE: _____
WILLIAM R. STEVENS, RCE #2339

VESTING TENTATIVE TRACT MAP 8129 FOR CONDOMINIUM PURPOSES @ THE BOULEVARD

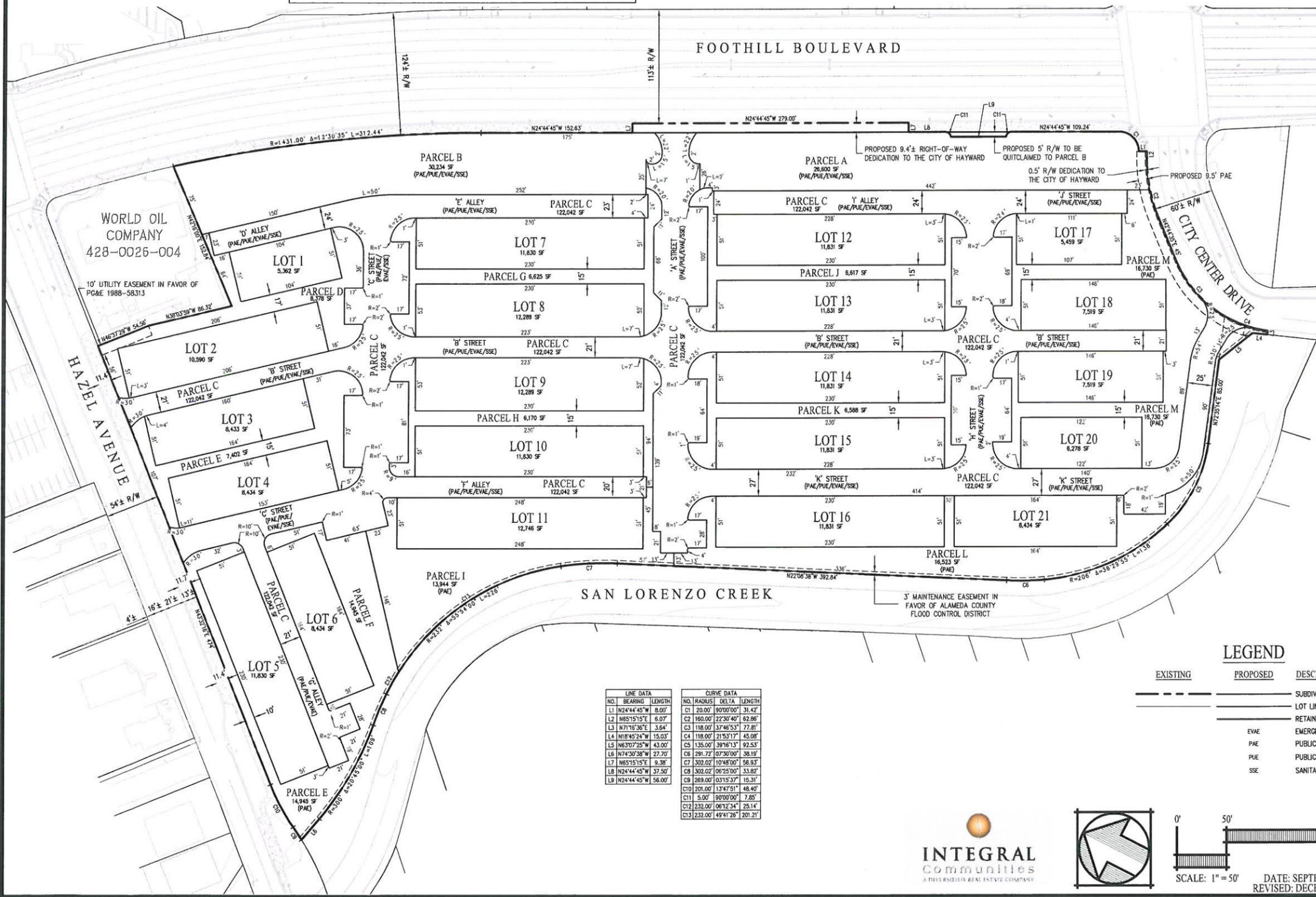
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA
FOR THE CONSTRUCTION OF 194 UNITS AND COMMERCIAL USE

CONTACTS:

- OWNER (PARCEL 1): 22301 FOOTHILL HAYWARD, LLC.
C/O CHAVEZ MANAGEMENT GROUP
1860 EL CAMINO REAL, STE 250
BURLINGAME, CA 94010
ATTN: DR. MARCO CHAVEZ
- OWNER (PARCEL 2): MDS REALTY II, LLC.
C/O KLAFF REALTY, LP
122 SOUTH MICHIGAN AVENUE, STE 1000
CHICAGO, IL 60603
ATTN: LESLIE MARSHAL
- APPLICANT: INTEGRAL COMMUNITIES
500 LA GONDA WAY, STE 102
DANVILLE, CA 94526
ATTN: MARK BUTLER
- CIVIL ENGINEER: CARLSON, BARBEE & GIBSON, INC.
2633 CAMINO RAMON, SUITE 350
SAN RAMON, CA 94583
ATTN: RYAN HANSEN
REGISTRATION #80557
- GEOTECHNICAL ENGINEER: BERLOGAR STEVENS AND ASSOCIATES
587 SUNOL BLVD.
PLEASANTON, CA 94566
ATTN: WILLIAM R. STEVENS
REGISTRATION #2339

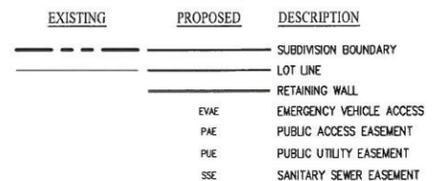
GENERAL NOTES:

- BENCHMARK: CITY OF HAYWARD BENCHMARK - PLATE MONUMENT AT THE CENTERLINE INTERSECTION OF CITY CENTER DRIVE AND FOOTHILL BOULEVARD. EL: 118.45 (NGVD 29)
- BASIS OR BEARINGS: THE BASIS OF BEARINGS FOR THIS SURVEY IS DETERMINED BY FOUND MONUMENTS IN FOOTHILL BOULEVARD AS SHOWN HEREON, THE BEARING BEING N24°44'45"W PER PARCEL MAP 9058 (292 MAPS 77).
- SITE ADDRESS: 22301 FOOTHILL BOULEVARD, HAYWARD, CA.
- A.P.N.: 428-0026-067-03 & 428-0026-068-01
- SITE AREA: GROSS: 11.33± AC (NET 11.27± AC)
- EXISTING/PROPOSED ZONING: CENTRAL CITY - COMMERCIAL CC
- EXISTING LAND USE: VACANT OFFICE BUILDING
- PROPOSED LAND USE: MULTI-FAMILY RESIDENTIAL & COMMERCIAL
- RESIDENTIAL UNITS: TOWNHOMES: 194 UNITS
- DENSITY: TOWNHOMES: 17.2 DU/AC
- STREETS: PRIVATE STREETS ARE TO BE PRIVATELY MAINTAINED. THE MINIMUM LONGITUDINAL SLOPE OF ALL STREETS IS 0.50%.
- STREET TREES AND LIGHTS: STREET TREES AND LIGHTS ARE TO BE PRIVATELY MAINTAINED.
- SEWER: ORO LOMA SERVICES DISTRICT
- STORM DRAIN: CITY OF HAYWARD
- WATER: EAST BAY MUNICIPAL UTILITIES DISTRICT
- GAS & ELECTRIC: PACIFIC GAS & ELECTRIC
- TELEPHONE: TBD
- CABLE TV: TBD
- FLOOD ZONE: LOMA DETERMINATION - 5/5/12, CASE NO: 12-09-1833A
THE SITE IS IN ZONE X - AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOOD PLAIN. COMMUNITY PANEL NO: 06001C 0287 G DATED AUGUST 3, 2009
- EXISTING STRUCTURES: ALL EXISTING BUILDINGS ON-SITE ARE TO BE REMOVED.
- CONTOURS: EXISTING CONTOUR INTERVAL: 2 FOOT
- GRADING: PROPOSED GRADING AS SHOWN IS PRELIMINARY, FINISHED GRADING IS SUBJECT TO FINAL DESIGN.
- HOA'S: A HOMEOWNER ASSOCIATION WILL BE FORMED TO OWN AND MAINTAIN PRIVATE STREETS, DRIVE AISLES, PRIVATE UTILITIES, STORM DRAINAGE FACILITIES AND LANDSCAPE WITHIN PARCELS C THROUGH M AND LOTS 1-21.
- WALLS: ALL WALLS ARE TO BE PRIVATELY MAINTAINED BY THE HOA.
- DIMENSIONS: DIMENSIONS AS SHOWN ARE PRELIMINARY AND SUBJECT TO THE FINAL MAP.
- FINAL MAP: THIS PROJECT MAY BE PHASED. THE SUBDIVIDER RESERVES THE RIGHT TO RECORD MULTIPLE FINAL MAPS ON THE LANDS SHOWN ON THIS VESTING TENTATIVE MAP IN ACCORDANCE WITH ARTICLE A SECTION 66.456.1 OF THE SUBDIVISION ACT.
- CONDOMINIUM MAP: A CONDOMINIUM MAP WILL BE RECORDED FOR LOTS 1 THROUGH 21. THE SUBDIVISION IS A CONDOMINIUM PROJECT AS DEFINED IN SECTION 1350 ET. SEQ. OF THE CIVIL CODE OF THE STATE OF CALIFORNIA AND FILED PURSUANT TO THE SUBDIVISION MAP ACT. THE TOTAL NUMBER OF RESIDENTIAL CONDOMINIUM DWELLING UNITS SHALL BE NO MORE THAN 194 UNITS FOR LOTS 1 THROUGH 21.



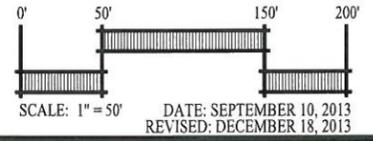
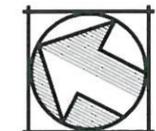
LINE DATA			CURVE DATA			
NO.	BEARING	LENGTH	NO.	RADIUS	DELTA	LENGTH
L1	N24°44'45"W	8.00'	C1	20.00'	90°00'00"	31.42'
L2	N65°15'15"E	6.07'	C2	160.00'	22°30'40"	62.86'
L3	N71°16'30"E	3.64'	C3	118.00'	37°46'53"	77.81'
L4	N18°45'14"W	15.03'	C4	118.00'	21°53'17"	45.89'
L5	N63°07'25"W	43.00'	C5	135.00'	39°18'13"	92.53'
L6	N74°30'30"W	27.70'	C6	281.72'	07°30'00"	38.19'
L7	N65°15'15"E	9.38'	C7	302.02'	10°48'00"	56.83'
L8	N24°44'45"W	37.50'	C8	302.02'	06°25'00"	33.82'
L9	N24°44'45"W	56.00'	C9	289.00'	03°15'37"	16.31'
			C10	201.00'	13°47'51"	48.40'
			C11	5.00'	90°00'00"	7.85'
			C12	232.00'	06°12'34"	26.14'
			C13	232.00'	49°41'26"	20.21'

LEGEND



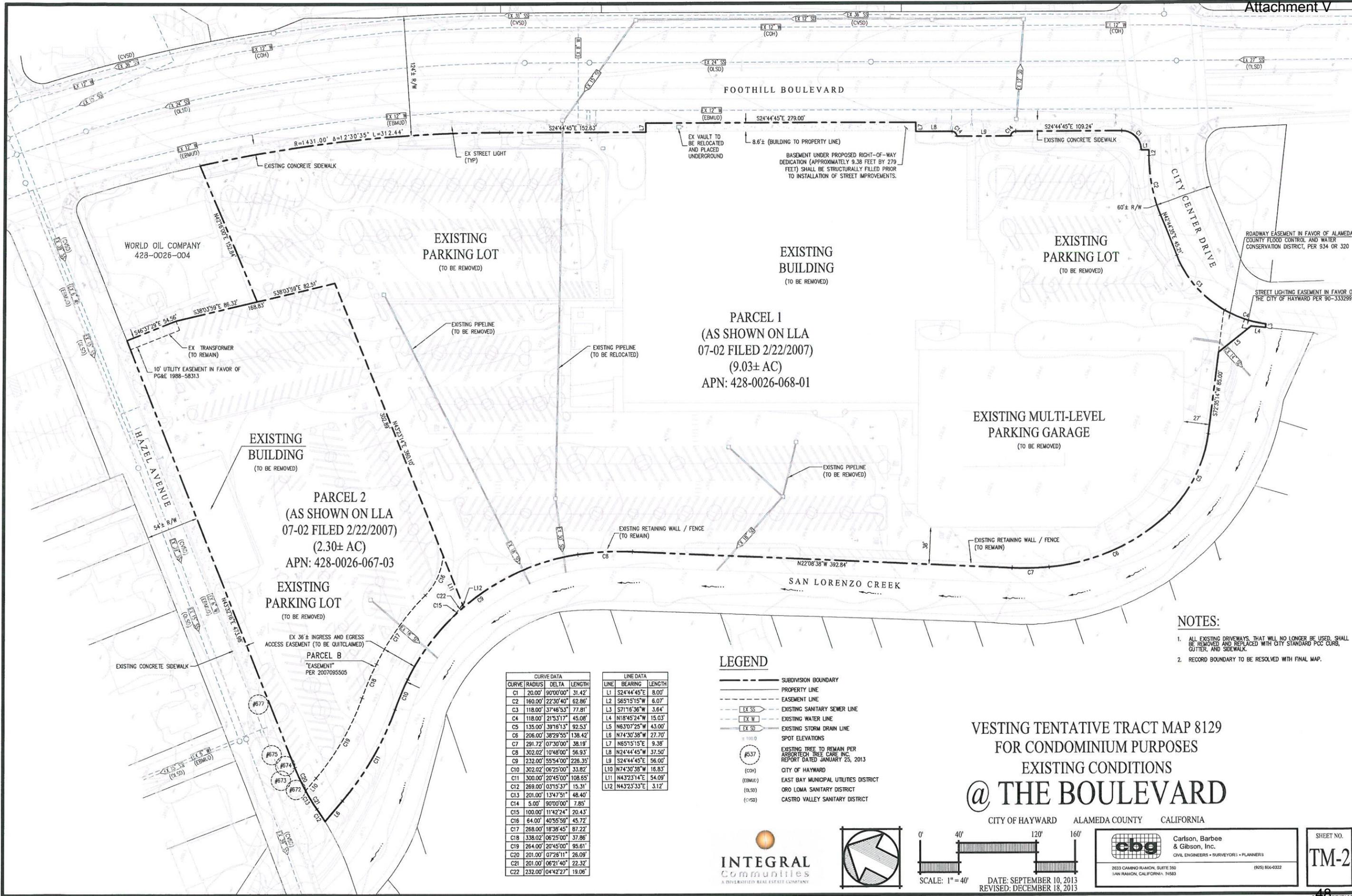
SHEET INDEX

- | | |
|------|--|
| TM-1 | VESTING TENTATIVE TRACT MAP 8129 |
| TM-2 | EXISTING CONDITIONS |
| TM-3 | PRELIMINARY SITE PLAN |
| TM-4 | PRELIMINARY GRADING PLAN |
| TM-5 | PRELIMINARY UTILITY PLAN |
| TM-6 | GRADING AND STREET SECTIONS |
| TM-7 | PRELIMINARY STORMWATER MANAGEMENT PLAN |
| TM-8 | FIRE ACCESS AND SERVICE PLAN |



cbg Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS + SURVEYORS + PLANNERS
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(925) 866-0322
FAX: (925) 866-8675

SHEET NO.
TM-1



PARCEL 1
 (AS SHOWN ON LLA
 07-02 FILED 2/22/2007)
 (9.03± AC)
 APN: 428-0026-068-01

PARCEL 2
 (AS SHOWN ON LLA
 07-02 FILED 2/22/2007)
 (2.30± AC)
 APN: 428-0026-067-03

CURVE DATA				LINE DATA		
CURVE	RADIUS	DELTA	LENGTH	LINE	BEARING	LENGTH
C1	20.00'	90°00'00"	31.42'	L1	S24°44'45"E	8.00'
C2	160.00'	22°30'40"	62.86'	L2	S65°15'15"W	6.07'
C3	118.00'	37°46'53"	77.81'	L3	S71°16'36"W	3.64'
C4	118.00'	21°53'17"	45.08'	L4	N18°45'24"W	15.03'
C5	135.00'	39°16'13"	92.53'	L5	N63°07'25"W	43.00'
C6	206.00'	38°29'55"	138.42'	L6	N74°30'38"W	27.70'
C7	291.72'	07°30'00"	38.19'	L7	N65°15'15"E	9.38'
C8	302.02'	10°48'00"	56.93'	L8	N24°44'45"W	37.50'
C9	232.00'	55°54'00"	226.35'	L9	S24°44'45"E	56.00'
C10	302.02'	06°25'00"	33.82'	L10	N74°30'38"W	16.83'
C11	300.00'	20°45'00"	108.65'	L11	N43°23'14"E	54.09'
C12	269.00'	03°15'37"	15.31'	L12	N43°23'33"E	3.12'
C13	201.00'	13°47'51"	48.40'			
C14	5.00'	90°00'00"	7.85'			
C15	100.00'	11°42'24"	20.43'			
C16	64.00'	40°55'59"	45.72'			
C17	268.00'	18°38'45"	87.22'			
C18	338.02'	06°25'00"	37.86'			
C19	264.00'	20°45'00"	95.61'			
C20	201.00'	07°25'11"	26.09'			
C21	201.00'	06°21'40"	22.32'			
C22	232.00'	04°42'27"	19.06'			

LEGEND

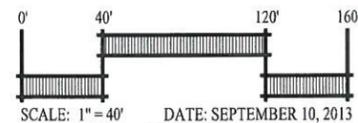
- SUBDIVISION BOUNDARY
- PROPERTY LINE
- EASEMENT LINE
- EX SS --- EXISTING SANITARY SEWER LINE
- EX W --- EXISTING WATER LINE
- EX SD --- EXISTING STORM DRAIN LINE
- SPOT ELEVATIONS
- EXISTING TREE TO REMAIN PER ARBORTECH TREE CARE INC. REPORT DATED JANUARY 25, 2013
- (COH) CITY OF HAYWARD
- (EBMUD) EAST BAY MUNICIPAL UTILITIES DISTRICT
- (OLSD) ORO LOMA SANITARY DISTRICT
- (CVSD) CASTRO VALLEY SANITARY DISTRICT

NOTES:

1. ALL EXISTING DRIVEWAYS, THAT WILL NO LONGER BE USED, SHALL BE REMOVED AND REPLACED WITH CITY STANDARD PCC CURB, GUTTER, AND SIDEWALK.
2. RECORD BOUNDARY TO BE RESOLVED WITH FINAL MAP.

**VESTING TENTATIVE TRACT MAP 8129
 FOR CONDOMINIUM PURPOSES
 EXISTING CONDITIONS
 @ THE BOULEVARD**

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



cbg Carlson, Barbee & Gibson, Inc.
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 SAN RAMON, CALIFORNIA 94583
 (925) 956-6322

SHEET NO.
TM-2

DATE: SEPTEMBER 10, 2013
 REVISED: DECEMBER 18, 2013

COMMERCIAL GARBAGE CAPACITY

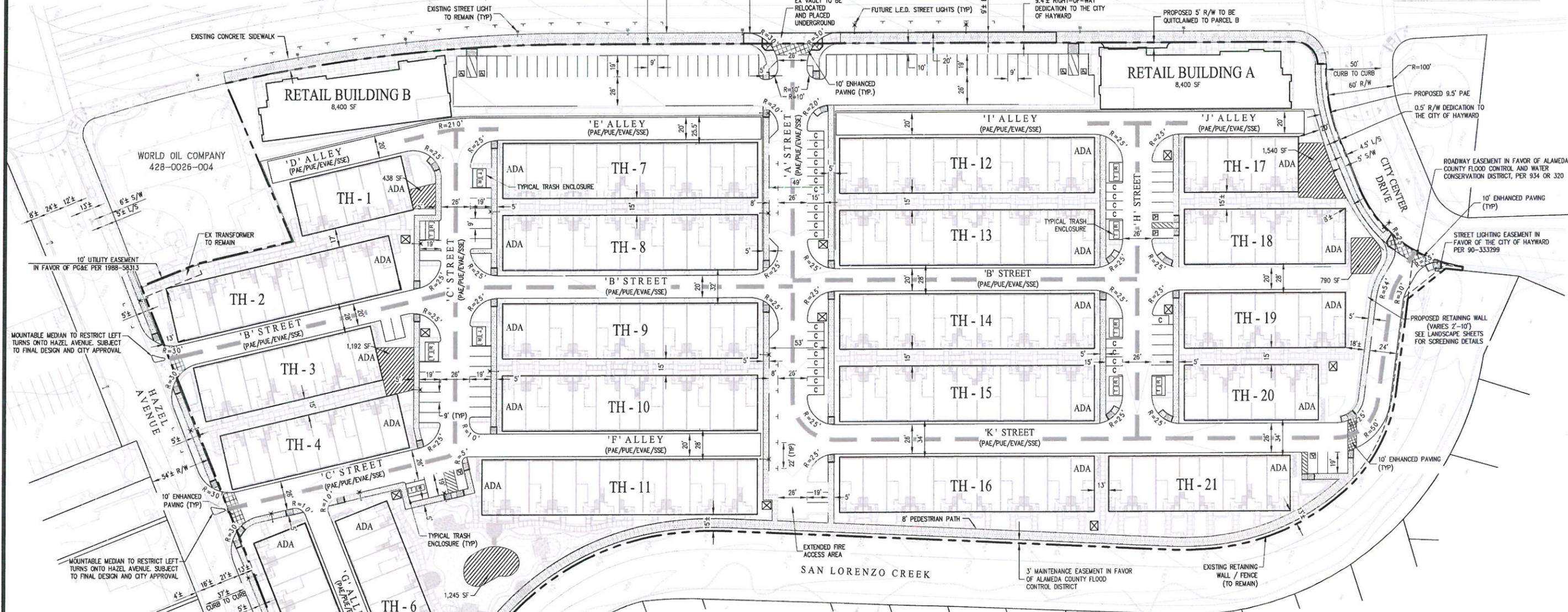
16,800 SF * 0.13 (LBS/SF/MONTH) / 100 (LBS/CUBIC YARD LOOSE) = 21.8 CY

TOWNHOME GARBAGE CAPACITY

194 DU * 1.2 (MOVE IN/OUT FACTOR) * 32 (GALLONS PER UNIT) / 200 = 37 CY

- NOTE:
- PRELIMINARY COMMERCIAL TRASH AREA REQUIRES THREE 4 CY BINS AND 1 RECYCLING BIN PER RETAIL SPACE, TO BE VERIFIED BY TRASH MANAGEMENT CONSULTANT.
 - SEE ARCHITECTURAL SHEETS A3.0 FOR RETAIL TRASH ENCLOSURE DETAILS.

- NOTE:
- REQUIRES TEN 4 CY BINS, AS SHOWN ON THIS SHEET.



LEGEND

- EXISTING PROPERTY BOUNDARY
- - - PROPOSED PROPERTY BOUNDARY
- TRASH COLLECTION ROUTE
- ADA ACCESSIBLE UNIT
- TH-1 BUILDING NUMBER (TOWNHOME)
- TRASH/RECYCLING ENCLOSURE
- CONCRETE SIDEWALK
- GROUP OPEN SPACE
- PARALLEL PARKING SYMBOL
- BIKE RACK (4 STALLS)
- ACCESSIBLE PARKING STALL
- PRELIMINARY STREET LIGHT LOCATION
- COMPACT PARKING STALL
- EVAE EMERGENCY VEHICLE ACCESS EASEMENT
- PAE PUBLIC ACCESS EASEMENT
- PUE PUBLIC UTILITY EASEMENT
- SSE SANITARY SEWER EASEMENT
- S/W SIDEWALK
- L/S LANDSCAPE

TOWNHOME PARKING

REQUIRED PARKING			
PARKING TYPE	UNITS	CITY REQUIREMENT	TOTAL
RESIDENTIAL (PRIVATE GARAGE)	194 UNITS	1.5 SPACES / DU	291 SPACES
ON-SITE GUEST	194 UNITS	0.5 SPACES / DU	97 SPACES
TOTAL REQUIRED PARKING	-	-	388 SPACES
PROVIDED ON-SITE PARKING			
UNIT	UNIT QUANTITY	GARAGE STALLS	TOTAL
UNIT 1	28	2	56 SPACES
UNIT 2	62	3 (1)	186 SPACES
UNIT 3	62	2 (1)	124 SPACES
UNIT 4	42	2 (1)	84 SPACES
GUEST (ON-SITE)	-	-	86 SPACES (1)
TOTAL ON-SITE PARKING	-	-	536 SPACES
PROVIDED OFF-SITE PARKING			
GUEST (HAZEL AVE.)	-	-	7 SPACES
TOTAL PROVIDED PARKING	-	-	539 SPACES

- NOTES:
- GARAGE INCLUDES EXTRA STORAGE SPACE
 - INCLUDES 8 COMPACT SPACES
 - 11 GUEST PARKING STALLS HAVE BEEN REPLACED BY 44 BIKE RACKS (11 LOCATIONS THROUGHOUT THE SITE) PER THE HAYWARD MUNICIPAL CODE SECTION 10-2.406

RETAIL PARKING

REQUIRED/PROVIDED PARKING				
PARKING TYPE	RETAIL SIZE	CITY REQUIREMENT	TOTAL REQUIRED	TOTAL PROVIDED
ON-SITE	16,800 SF	1 SPACE/315 SF	53 SPACES	53 SPACES
OFF-SITE	-	-	-	20 SPACES
TOTAL	-	-	53 SPACES	73 SPACES

- NOTES:
- THIS PRELIMINARY SITE PLAN IS INTENDED TO SHOW BASIC PROJECT FEATURES, STREET SECTIONS AND CRITICAL DIMENSIONS. SEE SHEETS A.1-1 FOR THE ARCHITECTURAL SITE PLAN AND ADDITIONAL BUILDING INFORMATION.
- BUILDING CONSTRUCTION TYPE**
- TOWNHOMES RESIDENTIAL: V-B
- RETAIL: V-B

OPEN SPACE CALCULATION

TOWNHOMES	REQUIRED	UNITS	TOTAL REQUIRED	TOTAL PROVIDED
COMMON OPEN SPACE	100 SF / UNIT	194	19,400 SF	45,770 SF
GROUP OPEN SPACE	30 SF / UNIT	194	5,820 SF	5,885 SF (1)
TOTAL	-	-	19,400 SF	45,770 SF

- NOTES:
- GROUP OPEN SPACE OF 30 SF/UNIT IS INCLUDED IN THE TOTAL REQUIRED 100 SF/UNIT OF COMMON OPEN SPACE.

VESTING TENTATIVE TRACT MAP 8129
FOR CONDOMINIUM PURPOSES
PRELIMINARY SITE PLAN
@ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

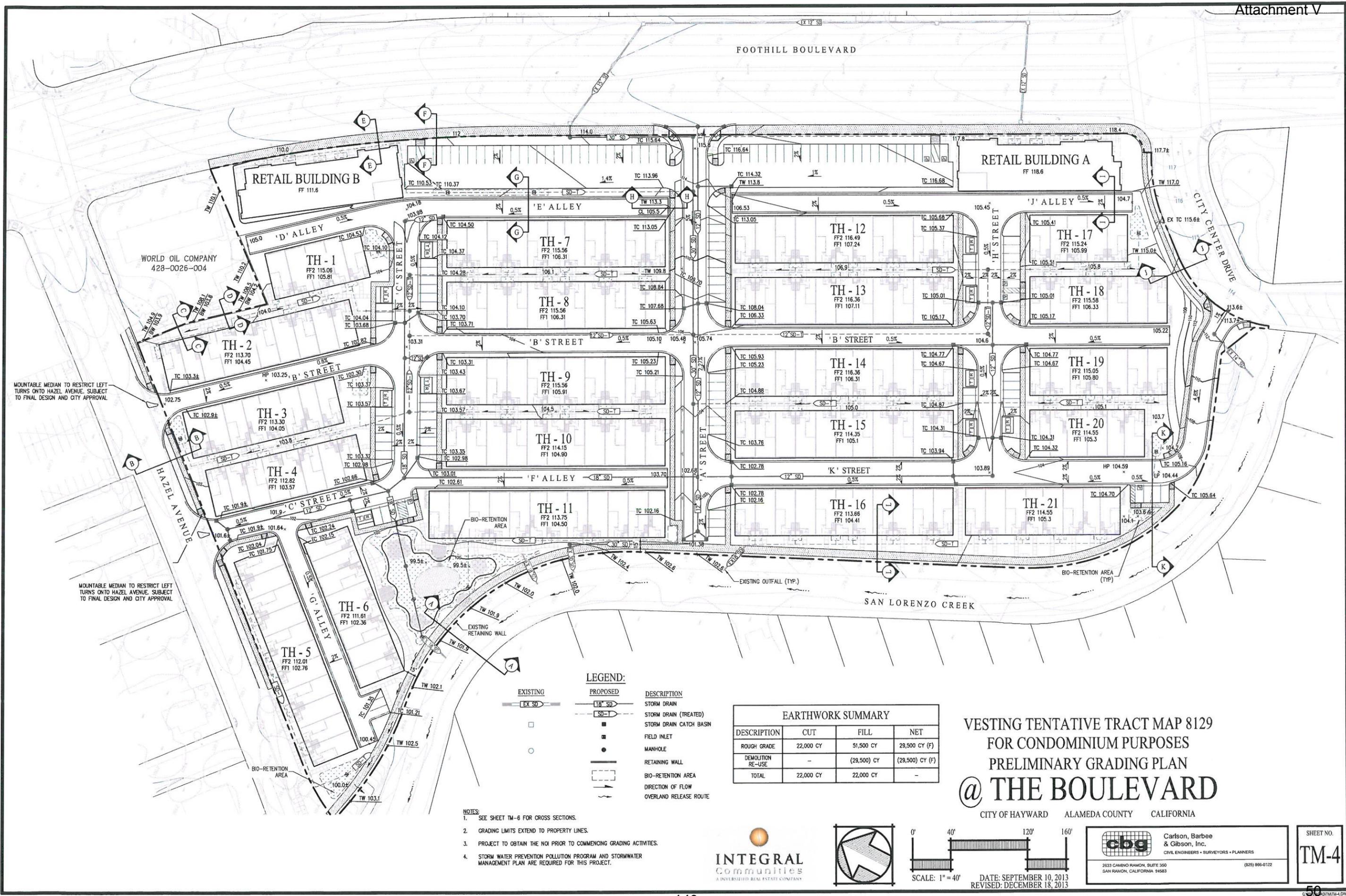


SCALE: 1" = 40'

DATE: SEPTEMBER 10, 2013
 REVISED: JANUARY 14, 2013

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SHEET NO.
TM-3



WORLD OIL COMPANY
428-0026-004

RETAIL BUILDING A
FF 118.6

RETAIL BUILDING B
FF 111.6

FOOTHILL BOULEVARD

CITY CENTER DRIVE

HAZEL AVENUE

SAN LORENZO CREEK

LEGEND:

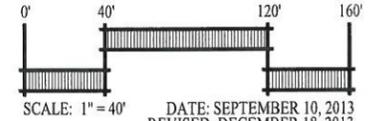
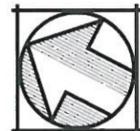
EXISTING	PROPOSED	DESCRIPTION
EX SD	18" SD	STORM DRAIN
	SD-T	STORM DRAIN (TREATED)
	■	STORM DRAIN CATCH BASIN
	●	FIELD INLET
	○	MANHOLE
	—	RETAINING WALL
	---	BIO-RETENTION AREA
	→	DIRECTION OF FLOW
	→	OVERLAND RELEASE ROUTE

DESCRIPTION	CUT	FILL	NET
ROUGH GRADE	22,000 CY	51,500 CY	29,500 CY (F)
DEMOLITION RE-USE	-	(29,500) CY	(29,500) CY (F)
TOTAL	22,000 CY	22,000 CY	-

VESTING TENTATIVE TRACT MAP 8129
FOR CONDOMINIUM PURPOSES
PRELIMINARY GRADING PLAN
@ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

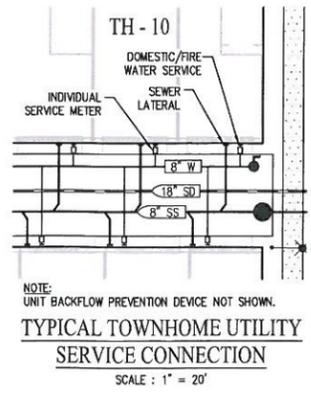
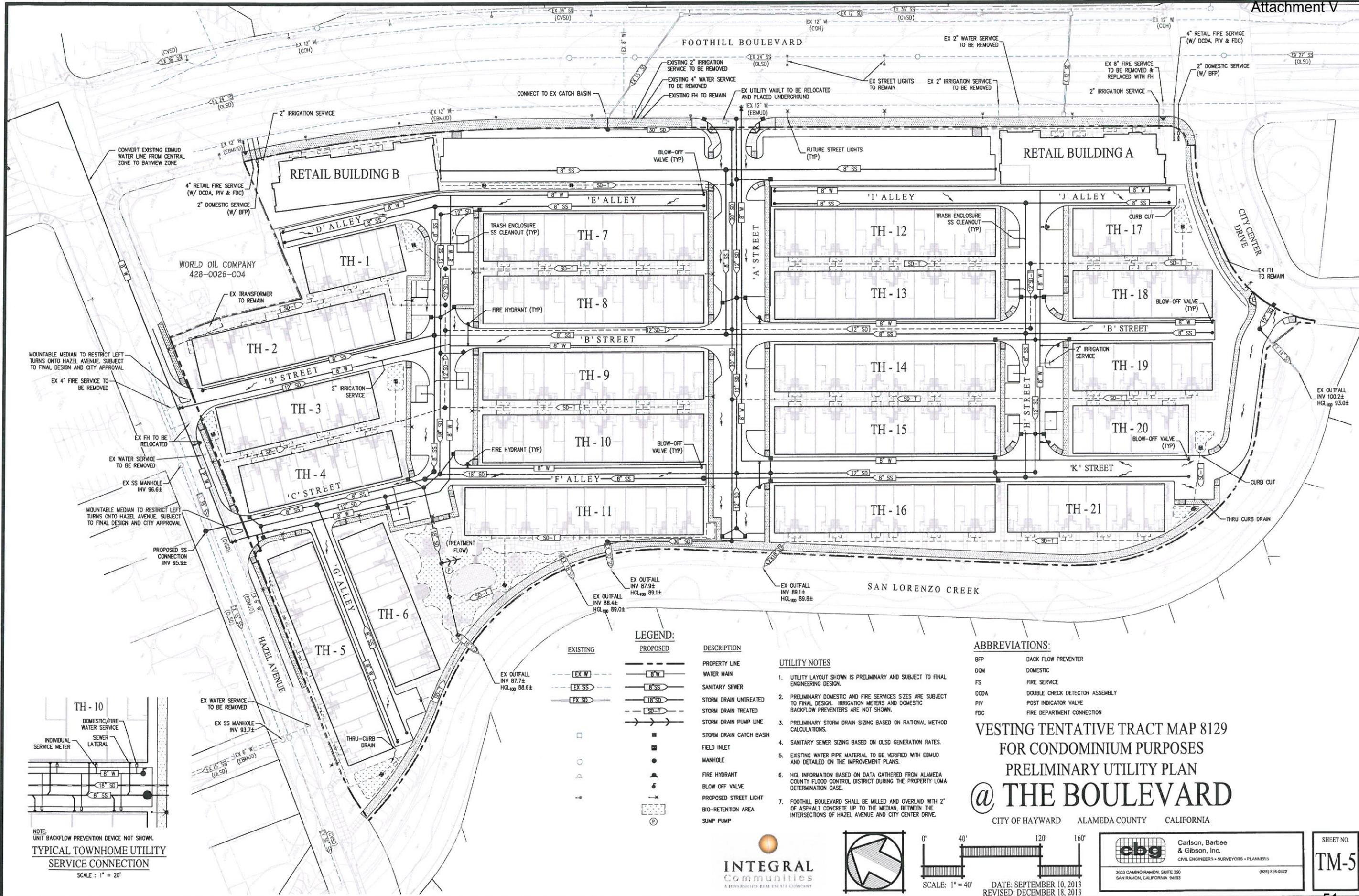
- NOTES:**
- SEE SHEET TM-6 FOR CROSS SECTIONS.
 - GRADING LIMITS EXTEND TO PROPERTY LINES.
 - PROJECT TO OBTAIN THE NOI PRIOR TO COMMENCING GRADING ACTIVITIES.
 - STORM WATER PREVENTION POLLUTION PROGRAM AND STORMWATER MANAGEMENT PLAN ARE REQUIRED FOR THIS PROJECT.



DATE: SEPTEMBER 10, 2013
REVISED: DECEMBER 18, 2013

cbg Carlson, Barbee & Gibson, Inc.
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SAN RAMON, CALIFORNIA 94583
(925) 966-0122

SHEET NO.
TM-4



LEGEND:

EXISTING	PROPOSED	DESCRIPTION
EX W	8" W	PROPERTY LINE
EX SS	8" SS	WATER MAIN
EX SD	18" SD	SANITARY SEWER
	SD-T	STORM DRAIN UNTREATED
	SD-T	STORM DRAIN TREATED
	SD-T	STORM DRAIN PUMP LINE
	SD-T	STORM DRAIN CATCH BASIN
	SD-T	FIELD INLET
	SD-T	MANHOLE
	SD-T	FIRE HYDRANT
	SD-T	BLOW OFF VALVE
	SD-T	PROPOSED STREET LIGHT
	SD-T	BIO-RETENTION AREA
	SD-T	SUMP PUMP

UTILITY NOTES

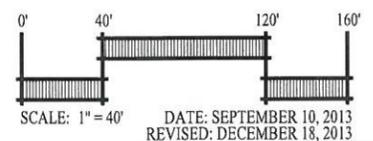
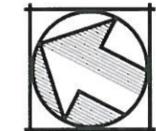
- UTILITY LAYOUT SHOWN IS PRELIMINARY AND SUBJECT TO FINAL ENGINEERING DESIGN.
- PRELIMINARY DOMESTIC AND FIRE SERVICES SIZES ARE SUBJECT TO FINAL DESIGN. IRRIGATION METERS AND DOMESTIC BACKFLOW PREVENTERS ARE NOT SHOWN.
- PRELIMINARY STORM DRAIN SIZING BASED ON RATIONAL METHOD CALCULATIONS.
- SANITARY SEWER SIZING BASED ON OLSG GENERATION RATES.
- EXISTING WATER PIPE MATERIAL TO BE VERIFIED WITH EBMUD AND DETAILED ON THE IMPROVEMENT PLANS.
- HGL INFORMATION BASED ON DATA GATHERED FROM ALAMEDA COUNTY FLOOD CONTROL DISTRICT DURING THE PROPERTY LOMA DETERMINATION CASE.
- FOOTHILL BOULEVARD SHALL BE MILLED AND OVERLAID WITH 2" OF ASPHALT CONCRETE UP TO THE MEDIAN, BETWEEN THE INTERSECTIONS OF HAZEL AVENUE AND CITY CENTER DRIVE.

ABBREVIATIONS:

BFP	BACK FLOW PREVENTER
DOM	DOMESTIC
FS	FIRE SERVICE
DCDA	DOUBLE CHECK DETECTOR ASSEMBLY
PIV	POST INDICATOR VALVE
FDC	FIRE DEPARTMENT CONNECTION

**VESTING TENTATIVE TRACT MAP 8129
FOR CONDOMINIUM PURPOSES
PRELIMINARY UTILITY PLAN
@ THE BOULEVARD**

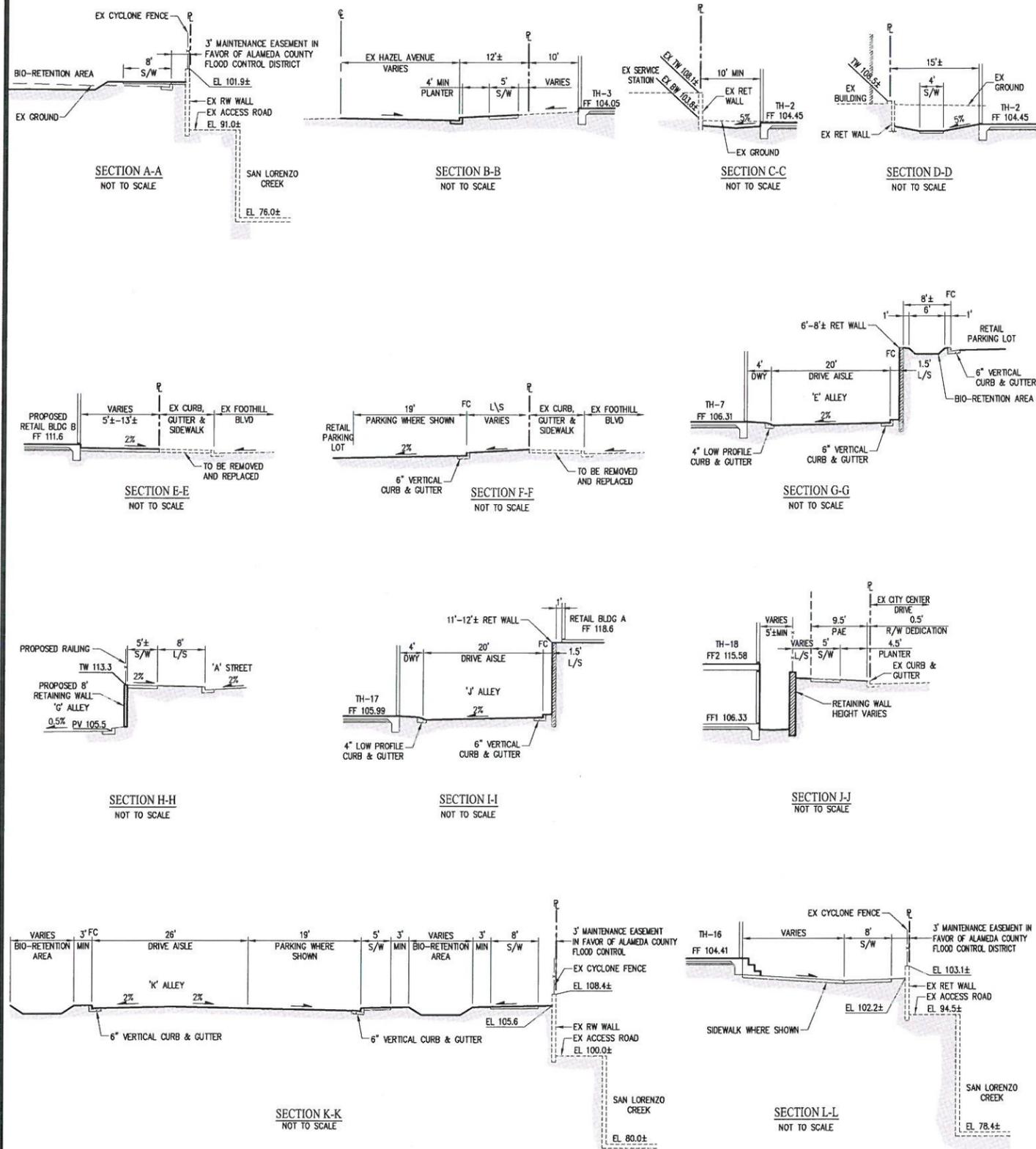
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



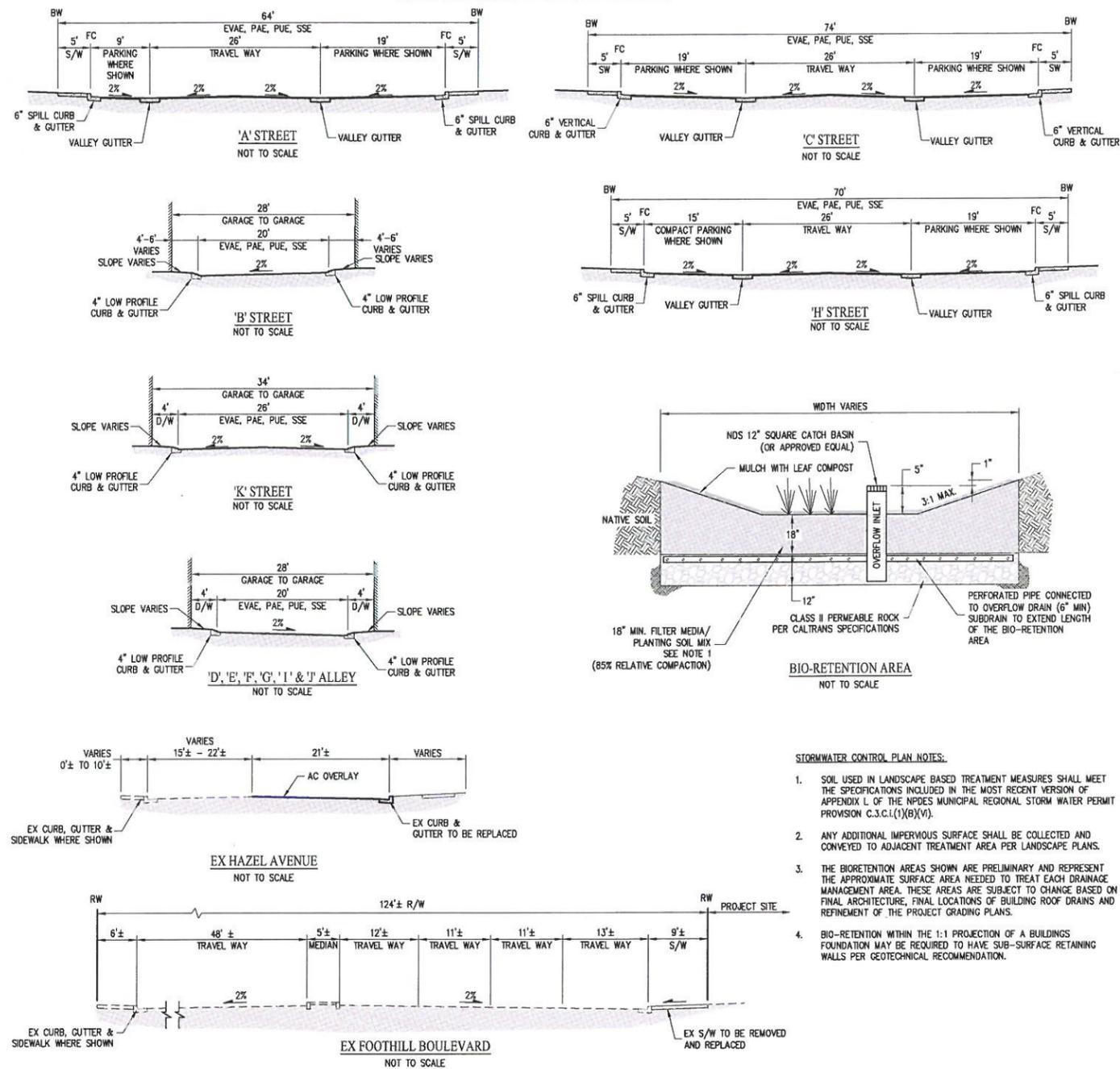
cbg Carlsson, Barbee & Gibson, Inc.
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2633 CAMINO RAMON, SUITE 350
SAN RAMON, CALIFORNIA 94583
(925) 865-9322

SHEET NO.
TM-5

GRADING SECTIONS



STREET SECTIONS



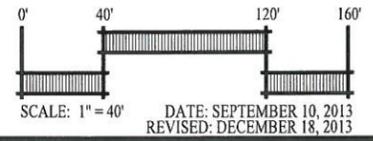
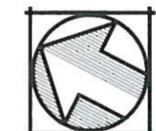
- STORMWATER CONTROL PLAN NOTES:**
1. SOIL USED IN LANDSCAPE BASED TREATMENT MEASURES SHALL MEET THE SPECIFICATIONS INCLUDED IN THE MOST RECENT VERSION OF APPENDIX L OF THE NPDES MUNICIPAL REGIONAL STORM WATER PERMIT PROVISION C.3.C.1.(1)(b)(v).
 2. ANY ADDITIONAL IMPERVIOUS SURFACE SHALL BE COLLECTED AND CONVEYED TO ADJACENT TREATMENT AREA PER LANDSCAPE PLANS.
 3. THE BIORETENTION AREAS SHOWN ARE PRELIMINARY AND REPRESENT THE APPROXIMATE SURFACE AREA NEEDED TO TREAT EACH DRAINAGE MANAGEMENT AREA. THESE AREAS ARE SUBJECT TO CHANGE BASED ON FINAL ARCHITECTURE, FINAL LOCATIONS OF BUILDING ROOF DRAINS AND REFINEMENT OF THE PROJECT GRADING PLANS.
 4. BIO-RETENTION WITHIN THE 1:1 PROJECTION OF A BUILDINGS FOUNDATION MAY BE REQUIRED TO HAVE SUB-SURFACE RETAINING WALLS PER GEOTECHNICAL RECOMMENDATION.

ABBREVIATIONS

- BW BACK OF WALK
- EL ELEVATION
- EVAE EMERGENCY VEHICLE ACCESS EASEMENT
- EX EX
- FC FACE OF CURB
- FF FINISHED FLOOR
- L/S LANDSCAPE
- PL PROPERTY LINE
- PUE PUBLIC UTILITY EASEMENT
- RET RETAINING
- RW RIGHT OF WAY
- S/W SIDEWALK
- TC TOP OF CURB
- SSE SANITARY SEWER EASEMENT
- TW TOP OF WALL

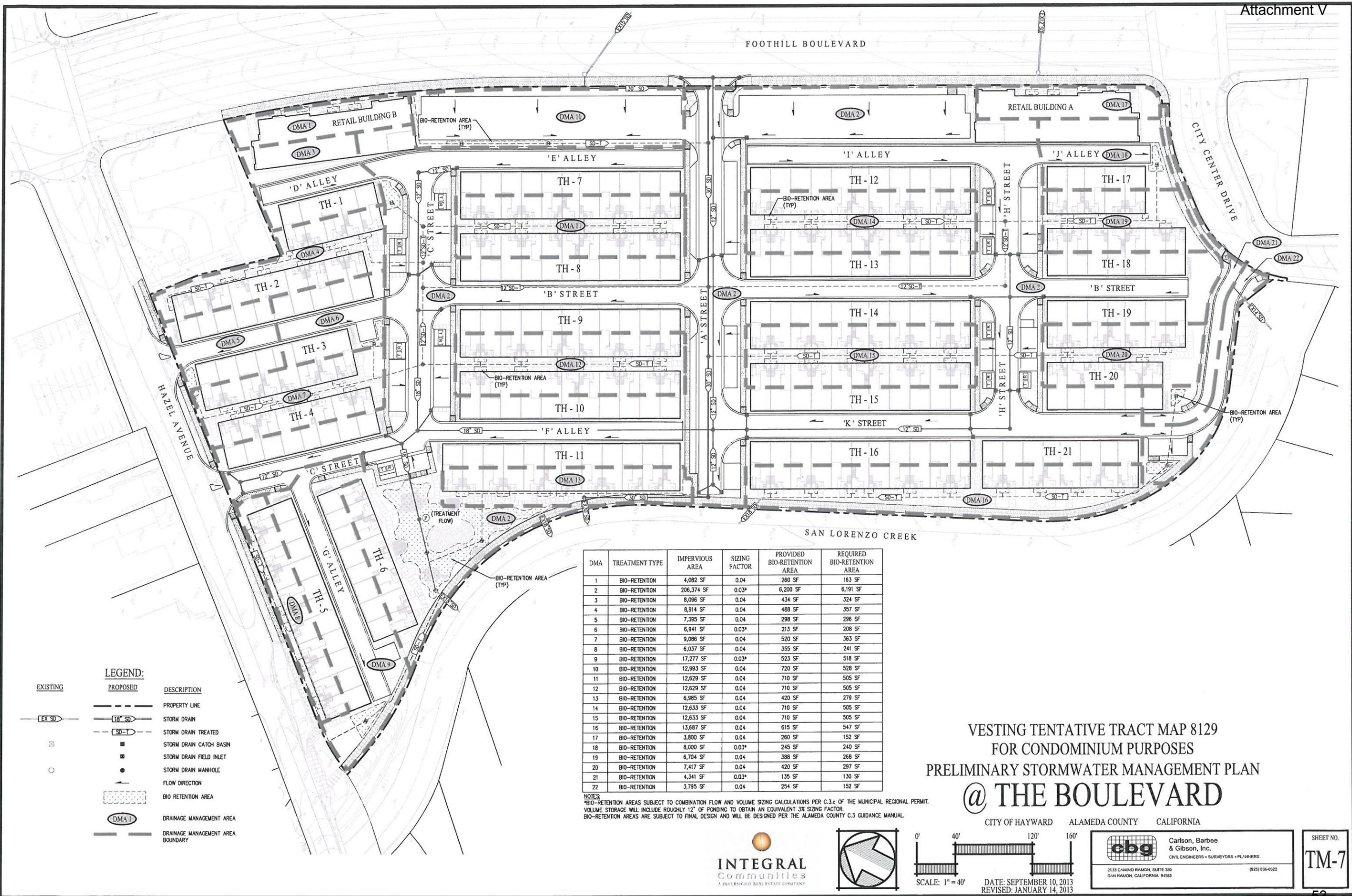
VESTING TENTATIVE TRACT MAP 8129 FOR CONDOMINIUM PURPOSES GRADING AND STREET SECTIONS @ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



cbg Carlson, Barbee & Gibson, Inc.
 CIVIL ENGINEERS • SURVEYORS • PLANNERS
 2633 CAMINO RAMON, SUITE 350
 SAN RAMON, CALIFORNIA 94583 (925) 866-0372

SHEET NO.
TM-6

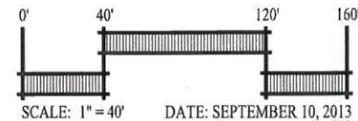
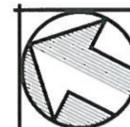


DMA	TREATMENT TYPE	IMPERVIOUS AREA	SIZING FACTOR	PROVIDED BIO-RETENTION AREA	REQUIRED BIO-RETENTION AREA
1	BIO-RETENTION	4,082 SF	0.04	260 SF	163 SF
2	BIO-RETENTION	206,374 SF	0.03*	6,200 SF	6,191 SF
3	BIO-RETENTION	8,096 SF	0.04	434 SF	324 SF
4	BIO-RETENTION	8,914 SF	0.04	488 SF	357 SF
5	BIO-RETENTION	7,395 SF	0.04	298 SF	296 SF
6	BIO-RETENTION	6,941 SF	0.03*	213 SF	208 SF
7	BIO-RETENTION	9,086 SF	0.04	520 SF	363 SF
8	BIO-RETENTION	6,037 SF	0.04	355 SF	241 SF
9	BIO-RETENTION	17,277 SF	0.03*	523 SF	518 SF
10	BIO-RETENTION	12,993 SF	0.04	720 SF	528 SF
11	BIO-RETENTION	12,629 SF	0.04	710 SF	505 SF
12	BIO-RETENTION	12,629 SF	0.04	710 SF	505 SF
13	BIO-RETENTION	6,985 SF	0.04	420 SF	279 SF
14	BIO-RETENTION	12,633 SF	0.04	710 SF	505 SF
15	BIO-RETENTION	12,633 SF	0.04	710 SF	505 SF
16	BIO-RETENTION	13,687 SF	0.04	615 SF	547 SF
17	BIO-RETENTION	3,800 SF	0.04	260 SF	152 SF
18	BIO-RETENTION	8,000 SF	0.03*	245 SF	240 SF
19	BIO-RETENTION	6,704 SF	0.04	388 SF	268 SF
20	BIO-RETENTION	7,417 SF	0.04	420 SF	297 SF
21	BIO-RETENTION	4,341 SF	0.03*	135 SF	130 SF
22	BIO-RETENTION	3,795 SF	0.04	254 SF	152 SF

NOTES:
 *BIO-RETENTION AREAS SUBJECT TO COMBINATION FLOW AND VOLUME SIZING CALCULATIONS PER C.3.c OF THE MUNICIPAL REGIONAL PERMIT.
 VOLUME STORAGE WILL INCLUDE ROUGHLY 12" OF PONDING TO OBTAIN AN EQUIVALENT 3% SIZING FACTOR.
 BIO-RETENTION AREAS ARE SUBJECT TO FINAL DESIGN AND WILL BE DESIGNED PER THE ALAMEDA COUNTY C.3 GUIDANCE MANUAL.

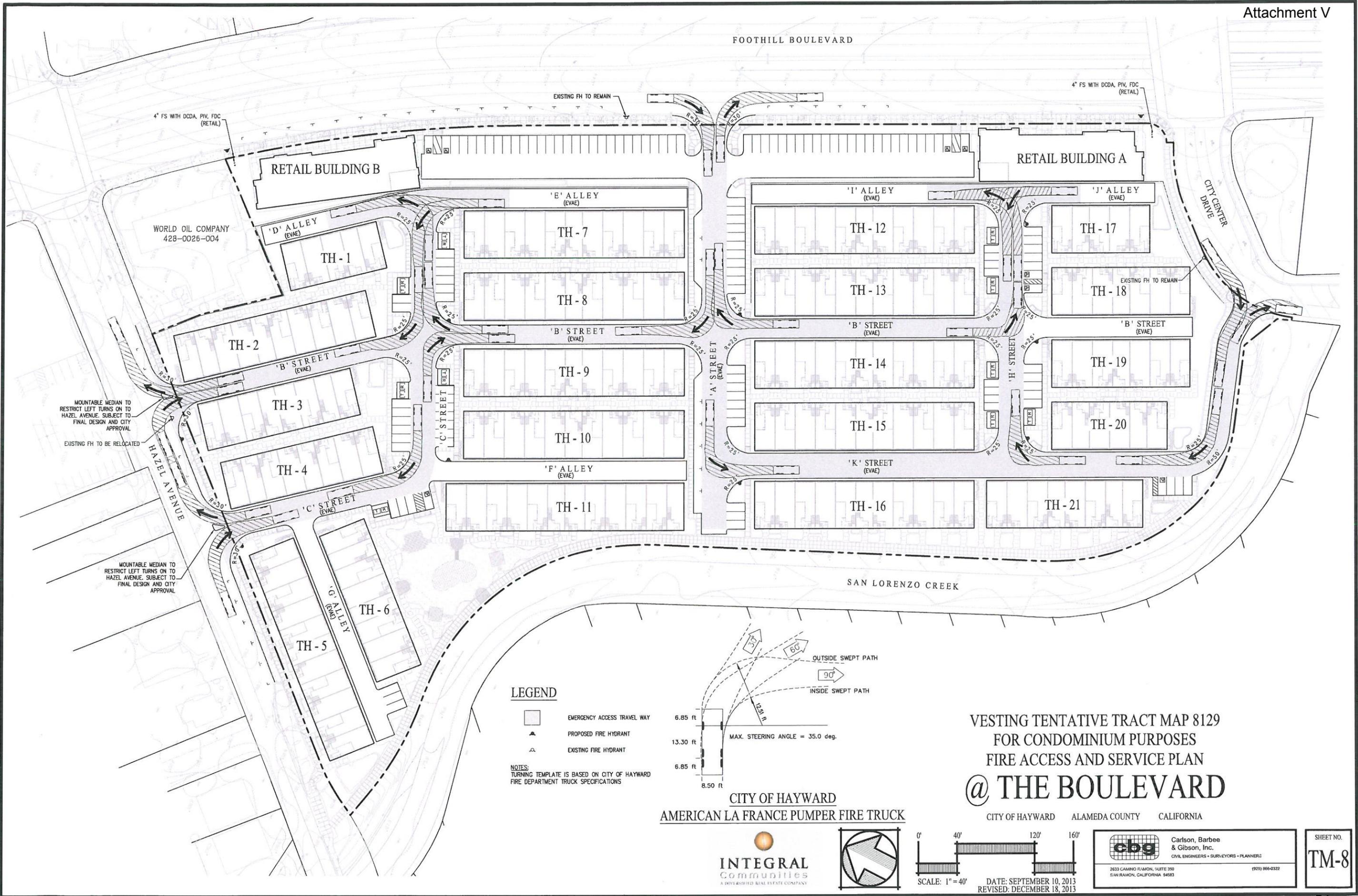
VESTING TENTATIVE TRACT MAP 8129
 FOR CONDOMINIUM PURPOSES
 PRELIMINARY STORMWATER MANAGEMENT PLAN
@ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



cbg Carlson, Barbee & Gibson, Inc.
 CIVIL ENGINEERS • SURVEYORS • PLANNERS
 2033 CAMINO RAMON, SUITE 350
 SAN RAMON, CALIFORNIA 94583
 (925) 956-0322

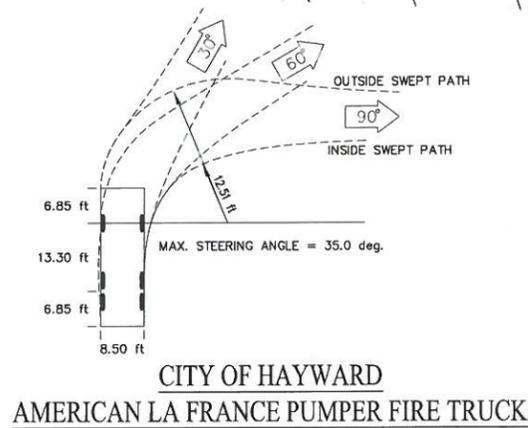
SHEET NO.
TM-7



LEGEND

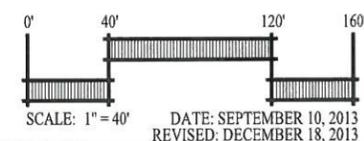
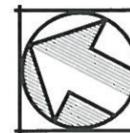
- EMERGENCY ACCESS TRAVEL WAY
- PROPOSED FIRE HYDRANT
- EXISTING FIRE HYDRANT

NOTES:
TURNING TEMPLATE IS BASED ON CITY OF HAYWARD
FIRE DEPARTMENT TRUCK SPECIFICATIONS



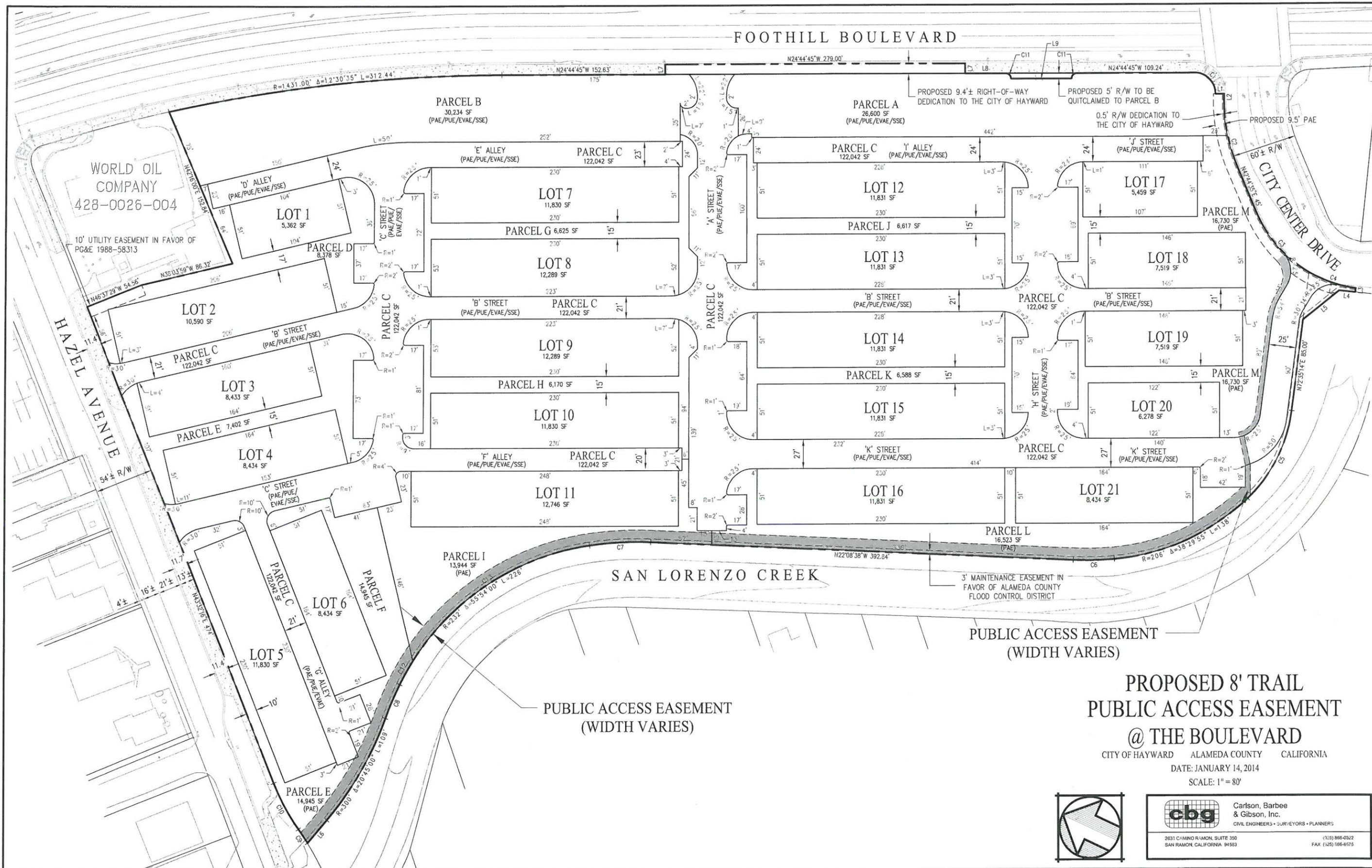
VESTING TENTATIVE TRACT MAP 8129
FOR CONDOMINIUM PURPOSES
FIRE ACCESS AND SERVICE PLAN
@ THE BOULEVARD

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS • SURVEYORS • PLANNERS
2633 CAMINO FLAMON, SUITE 350
SAN RAMON, CALIFORNIA 94583
(925) 866-0322

SHEET NO.
TM-8



**PROPOSED 8' TRAIL
PUBLIC ACCESS EASEMENT
@ THE BOULEVARD**

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA
DATE: JANUARY 14, 2014
SCALE: 1" = 80'



cbg Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS • SURVEYORS • PLANNERS
2633 CAMINO RAMON, SUITE 350 SAN RAMON, CALIFORNIA 94583 (925) 866-0322 FAX (925) 866-6975

**GENERAL PLAN MAP OF THE PROJECT SITE
AND SURROUNDING PROPERTIES**



Legend

CC-ROC	City Center - Retail and Office Commercial
MDR	Medium Density Residential
LOS	Limited Open Space
GC	General Commercial
CHDR	Commercial/High Density Residential
PR	Parks and Recreation
PQP	Public and Quasi-Public
HDR	High Density Residential

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Kathleen Wills

Signature: Kathleen E. Wills

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

~~THE BOULEVARD~~ @

Attachment VII

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Richard Aguilar

Signature: Richard Aguilar

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94591

Phone Day: _____ Eve: 3

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Lacey and Ryan Sutherland

Signature: [Handwritten Signature]

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

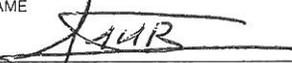
Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

MARIO MARTINEZ
NAME


SIGNATURE

TITLE ORGANIZATION

HAYWARD CA 94541
ADDRESS CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

Prospect Hill/Ha



Attachment VII

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Suzie Cantfield

SIGNATURE *Suzie A Cantfield*

TITLE _____ ORGANIZATION _____

ADDRESS _____
CITY Hayward, STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

Prospect Hill / HALL

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: LESLIE HALL

Signature: Leslie Hall

Title: _____ Organization: _____

Address: _____

City: HAYWARD State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: CONSTANCE Taylor

Signature: C. D. Taylor

Title: _____ Organization: _____

Address: _____

City: Hay. State: Ca. Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

No call backs or E-MAIL

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

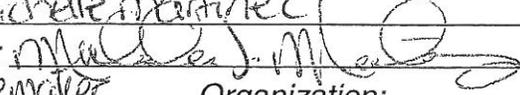
~~THE BOULEVARD~~ @ THE BOULEVARD

Attachment VII

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Michelle Martinez

Signature: 

Title: Homeowner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Sinoda Kelley
NAME

SIGNATURE

TITLE ORGANIZATION

HAYWARD, CA. 94541
ADDRESS
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER It's about time!!

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Callie Germany
NAME

Callie Germany
SIGNATURE

TITLE _____ ORGANIZATION _____

Howard, CA *94549*
ADDRESS
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

Claudia C. Rollins
NAME

Claudia C. Rollins
SIGNATURE

Walker on Foothill Blvd
TITLE ORGANIZATION

Hayward
ADDRESS CITY STATE ZIP
CA 9454

DAYTIME PHONE NUMBER EVENING PHONE NUMBER
Same

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME SANDRA J. RUSH

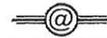
SIGNATURE Sandra J. Rush

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward, Ca. STATE _____ ZIP 94544

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Maria G. LOPEZ

SIGNATURE MA. GUADALUPE LOPEZ

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME GARY + CAROL BOOKOUT

SIGNATURE Gary Robert Carol Bookout

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

Be Sure to have some security

Attachment VII

THE BOULEVARD

retail

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME David and Amy Whaley

SIGNATURE Amy Whaley

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Cindy Towles

SIGNATURE [Handwritten Signature]

TITLE Founder ORGANIZATION Cherryland Transformati

ADDRESS Hayward CITY CA STATE 94541 ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Blanca Cervantes-Madrugal

SIGNATURE 

TITLE Resident of Hayward ORGANIZATION _____

ADDRESS Hayward CA 94544

CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME ELIZABETH MORENO

SIGNATURE 

TITLE _____ ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE CA ZIP 94521

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Abdul Malik

SIGNATURE Abdul Malik

TITLE MA ORGANIZATION AtandT

ADDRESS

CITY Hayward STATE ca ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Sauise Butler
NAME

Sauise Butler
SIGNATURE

Home maker ORGANIZATION
TITLE

ADDRESS

Hayward CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER

EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

MARK DANIEL
NAME

Mark Daniel
SIGNATURE

HAYWARD RESIDENT 11 years
TITLE ORGANIZATION

HAYWARD CA 94546
CITY STATE ZIP

N/A
DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Carol Cambra
NAME

Carol Cambra
SIGNATURE

TITLE ORGANIZATION

ADDRESS

Hayward Ca 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Grace C. Cooper

SIGNATURE Grace Cooper

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Beverly Young Harrison

SIGNATURE Beverly Harrison

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Larry Baugh
 Signature: Larry Baugh
 Title: Re-tired! Organization: _____
 Address: _____
 City: Hayward State: CA Zip: 94541
 Phone Day: _____ Eve: _____
 Email: _____

Please return to:
 @ The Boulevard
 982 B Street
 Hayward CA 94541
 damara@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Debra Vandergrift

Signature: Debra Vandergrift

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Name: Don Lee

Signature: [Handwritten Signature]

Title: Owner Organization: Zippy Shell Bay Area

Address: _____

City: Hayward State: CA Zip: 94545

Phone Day: _____ Eye: _____

Email: _____

Please return to:
 @ The Boulevard
 982 B Street
 Hayward CA 94541
 damaral@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: BRAD W. ELLS
Signature: Brad Wells
Title: VP & CFO Organization: CAL STATE, EAST BAY
Address: _____
City: HAYWARD State: CA Zip: 94542
Phone Day: _____ Eve: _____
Email: _____

LIVES DOWNTOWN
TSUJET LAGUN
TRAW CITY

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: MICHAEL T. MATHON ET
Signature: [Handwritten Signature]
Title: _____ Organization: _____
Address: _____
City: HAYWARD State: CA Zip: 94542
Phone Day: _____ Eve: _____
Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Yvonne Holts

Signature: [Handwritten Signature]

Title: _____ Organization: NO ORGANIZATION

Address: _____

City: Hayward State: CA Zip: 94542

Phone Day: _____ Eve: _____

Email: [Handwritten Email]

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



INTEGRAL
Communities
A DIVERSIFIED REAL ESTATE COMPANY

Yes, I endorse @ the Boulevard! Please add my name to your growing coalition of community supporters.

Signature _____

Please keep me informed.

Name SANDRA C HERRERA

(if applicable) Title CEO Organization/Company _____

Phone(day) _____ (eve) _____ Email _____

Address _____ City Hayward
State CA Zip 94540

Great job on bringing something more upscale to Hayward!

Please call me. I can be of help in the area of waste stream reduction and recycling.

Case compactors.com

Please take a moment to see what we do.

would love to meet with you soon!

Sandra Herrera



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME John Nielsen

SIGNATURE 

TITLE owner ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE Ca ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Rev. & Mrs. John Geach

SIGNATURE Rev & Mrs John Geach

TITLE Doctor of Ministry ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE Ca ZIP 94586

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Charlene Frey

SIGNATURE [Handwritten Signature]

TITLE None ORGANIZATION None

ADDRESS [Redacted]
CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER [Redacted] EVENING PHONE NUMBER [Redacted]

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Benjamin J. Vella

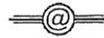
SIGNATURE *Benjamin J. Vella*

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541-5329

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

LAW AVER!!

NAME *Jan Avenue*

SIGNATURE *Please improve Hayward so*

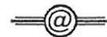
TITLE *that it is no longer "The Ampit*

ORGANIZATION *of the Bay Area". This project*

ADDRESS *is a step in the right direction*

CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

MAZIE B. CHAVEZ
NAME

Mazie B. Chavez
SIGNATURE

Mazie B. Chavez
TITLE ORGANIZATION

RESIDENT since 1952 - since downtown
ADDRESS

is looking better - we need more
CITY STATE ZIP

people there. Please go with project!!!
DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JEANNETTE RODGERS

SIGNATURE J. T. Rodgers

TITLE ORGANIZATION

ADDRESS

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER EVENING PHONE NUMBER



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME DANIEL ZAZUETA

SIGNATURE *Daniel Zazueta*

TITLE Home owner ORGANIZATION _____

ADDRESS _____

CITY Alhambra STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JOSE GORDANO

SIGNATURE 

TITLE _____ ORGANIZATION =

ADDRESS _____

CITY Hayward STATE CA ZIP 94542

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

DIANA K. WALLACE
NAME

Diana K. Wallace
SIGNATURE

TITLE ORGANIZATION

ADDRESS
HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Sheri Edwards

SIGNATURE Sheri Edwards

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Vea Fier

SIGNATURE [Handwritten Signature]

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY / STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME DAVID PELTON

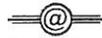
SIGNATURE *David Pelton*

TITLE OWNER ORGANIZATION FOREIGN AUTO SERVICE

ADDRESS

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Doc Miller
NAME

[Handwritten Signature]
SIGNATURE

CEO MCC Enterprises, LTD
TITLE ORGANIZATION

ADDRESS

Hayward CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

WHEN IS SOMEONE GOING TO DO SOMETHING ABOUT "CENTENNIAL HALL" ??

Attachment VII
THE BOULEVARD

PROBES

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

JEAN E. MITCHELL

NAME

Jean E Mitchell

SIGNATURE

RETIRED - TELEPHONE CO. (SERVICE REP.)

TITLE

ORGANIZATION

ADDRESS

HAYWARD

CA

94541-2696

CITY

STATE

ZIP

DAYTIME PHONE NUMBER

EVENING PHONE NUMBER



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Brenda Logsdon

SIGNATURE Brenda Logsdon

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

WILLIAM & ELIZABETH JACKSON
NAME

Elizabeth M. Jackson
SIGNATURE

HOME OWNERS
TITLE ORGANIZATION

ADDRESS
Hayward Ca. 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse ② the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

_____ *Ella Thomas* _____
NAME
_____ *Ella Thomas* _____
SIGNATURE

_____ TITLE _____ ORGANIZATION _____

_____ ADDRESS _____
_____ *Hayward* _____ *CA* _____ *94541* _____
CITY STATE ZIP

_____ DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
 Please keep me informed.

NAME DANIEL H. LARAMIE
SIGNATURE *Daniel H. Laramie*

TITLE _____ ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Leslie Mero
NAME _____

Leslie Mero
SIGNATURE _____

_____ TITLE _____ ORGANIZATION _____

_____ ADDRESS _____

Hayward CITY _____ CA STATE _____ 94541 ZIP _____

_____ DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Aldora A. Gonsalves
NAME

SIGNATURE
Aldora A. Gonsalves
TITLE ORGANIZATION

ADDRESS
Hayward CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

GERALD Williams
NAME

Gerald Williams
SIGNATURE

TITLE _____ ORGANIZATION _____

ADDRESS
 HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Lisa Landis
NAME

Lisa Landis
SIGNATURE

TITLE ORGANIZATION

ADDRESS
 Hayward CA 94546
 CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JEROME CROWNING

SIGNATURE 

TITLE HOME OWNER ORGANIZATION _____

ADDRESS _____

CITY HAAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Carlos McLenna

SIGNATURE [Handwritten signature]

TITLE ORGANIZATION

ADDRESS
CITY Hayward CA STATE ZIP 94541

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Karen Osborne

NAME

Karen Osborne

SIGNATURE

TITLE

ORGANIZATION

ADDRESS

Daywood

CITY

CA

STATE

94541

ZIP

DAYTIME PHONE NUMBER

EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

THOMAS HARTMAN
NAME

Thomas Hartman
SIGNATURE

N/A
TITLE ORGANIZATION

HAYWARD CALIFORNIA 94544-2000
ADDRESS
CITY STATE ZIP

N/A
DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME HOWARD RYDOLPH

SIGNATURE Howard Rydolph

TITLE RETIRED ORGANIZATION _____

ADDRESS HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: EMMA GUZMAN

Signature: Emma Guzman

Title: Homeowner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

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MAR 11 11

@ Attachment VII

THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Andrea White

Signature: [Handwritten Signature]

Title: home owner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



THE BOULEVARD

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Brian Marcus	Hayward CA 94541	
2.	Ahour Daniel	Hayward CA 94541	
3.	Fiona Apim	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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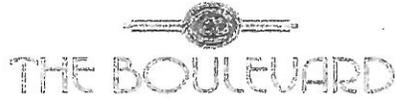
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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name <small>Please Print</small>	Address and EMAIL <small>Please include zip code</small>	Phone <small>Day & Evening</small>
1.	Dena Vuocola	Hayward CA 94541	
2.	Angela Mira	Hayward CA 94541	
3.	Lon Franklin	Hayward CA 94541	
4.	SAMUEL W. ADAMS		
5.	Monika Sobotka	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541



Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Dodie Kramer		
2.	Ewan John-Jones		
3.	Wendy Johnson		LIFE HOUSE RECOVERY HOMES
4.	Dahlia [unclear]	Hayward CA 94541	
5.	Genoveva Flores	94541 Hayward CA	
6.	Carlos Ramirez	94541 Hayward	
7.	Carla Ramirez	9451 Hayward	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541



Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Curtis Soares		
2.	Michaela Parker		
3.	Chris Christenson		
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	David Acosta	22185 Center St Castro Valley	
2.	MARY GRAVES	Hayward, CA 94542	
3.	Edna Norfath	Hayward CA 94544	
4.	Kathy Lagman	Hayward CA 94544	(dont want spam)
5.	Kalette Cole	Hayward CA 94542	
6.	Janette Hernandez	Hayward, CA	
7.	Roberta Alets	HAYWARD CA 94544	
8.	Kyle Watkins	Hayward CA 94541	
9.	Jacqueline Morales	Hayward CA	
10.	Alex Mays	Hayward, CA	
12.	William Paras	Hayward CA	
13.	Demarion ^{English}	94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541



THE BOULEVARD

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Patricia Champoux	Hayward CA 94541	
2.	BERNICE RODRIGUES	Hayward Ca	
3.	Antonette Williams	Hayward Ca 94541	
	LARRY BISHOP	larrybishorea(tor1e)yahoo.com	
5.	Amber Dent	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Jamar C.	Hayward	
2.	JERRY LOVE	Hayward	
3.	Robert Horton	Hayward	
4.	Chandis Sosa	1718 137th St, San Leandro CA 94578	
5.	Gr. Paulo	Hayward CA	
6.	Cuanita Espinoza	Hayward CA 94544	
7.	Victoria Reddy	Hayward CA 94542	
8.	JAMES MANTALVANOS	Hayward	
9.	ROSE ANN HALL	Hayward CA, 94544	
10.	De'Lango Baldwin	Hayward, CA 94541	
12.	Rayan D. CIASSALIA	HAYWARD CA 94545	
13.	Robert Notrangelo	Hayward, CA 94542	

Return to @The Boulevard: 982 B Street, Hayward CA 94541



Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Anthony Piscar	Hayward Ca	
2.	Andre Alexander	Hayward CA 94541	
3.	J Shibuya	Hayward, CA 94541	
4.	Andres Dent	Hayward, CA 94541	
5.	Rodney Marshall	Hayward 94541	
6.	Willie Robles	Hayward, CA 94543	
7.	JOSEPH RODRIGUEZ	HAYWARD, CA	
8.	ROYALD TIFFE		
9.	Cesar Ramos	Hayward, CA	
10.	Barbara Brown	Hayward 94544	
12.	Janet Shano	Hayward CA 94541	
13.	Quiana Ester	Hayward, CA 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Erika Hernandez	Hayward, CA 94541	
2.	Jovann Augustus	Hayward, CA	
3.	Yelena Brown	1135 1st St Apt 19 Oakland CA 94612	(510) 575-5367
4.	Daniel Bo	Hayward, CA 94541	
5.	Saul Rosas	Hayward CA	
6.	Tim Fletcher	Hayward 94542	
7.	Angel Luaces	Hayward CA 94541	
8.	Joann Peppers	Hayward CA 94541	
9.	Clara Ferris	3000 Somerset Ave Castro Valley 94546	510 331 0028
10.	Osni Mendez	2250 Marinette Way San Leandro CA 94579	510 304-4305
12.	Yasmin Sanchez	Hayward CA 94541	
13.	Mandy Garcia	Hayward 94544	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Marilyn Grise

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	WAYNE WEST		
2.	JEFFREY FREID	HAYWARD 94541	
3.	Linda HOOKER	16349 Saratoga St. San Leandro CA 94578	
4.	Felicia Laureano	Hayward, Ca 94541	
5.	Steve Bailey	Hayward, Ca 94541	
6.	Dwayne Wiggins	Hayward, CA 94541	
7.	Jane Malmgren	Hayward CA 94541	
8.	STEVEN DOSS	HAYWARD	
9.	Page Nelson	Hayward CA	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	John Tate	Hayward, CA 94541	
2.	Martin Walter	Hayward, CA	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

**Petition to transform the old Mervyn's site
and endorse @ The Boulevard**

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Alfred Rodriguez	Hayward Ca	
2.	TERRI Conillo	Hayward CA	
3.	JEANIE SIU		
4.	Arthur Foster	Hayward CA 94541	
5.	Jessica Terrazas	Hayward, CA	
6.	Lilcan Cardenas	Hayward CA 94541	
7.	William Demora	1557 165th ave # 15 San Leandro, CA	
8.	Denise Garrett	Hayward 94541	
9.	Matthew Fien	Hayward 94541	
10.	Thomas Nelson	Hayward, CA 94541	
12.	Enrique Alcaraz		
13.	Mareny Hernandez	Hayward C.A. 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	ROBERT OWENS		N/A
2.			
3.			
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

M

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Consuelo Barragan	Hayward CA 94544	
2.	Alicia Alvarez	1652 Mono Ave San Leandro 945	
3.	Abu Karon	Hayward Ca 94544	
4.	Marcia Roberts	Hayward CA 94541	
5.	Shanese Houston	Hayward, CA, 94541	
6.	Rhajimas Holmes	Hayward, CA	
7.	Johanna Pargoulka	Hayward CA 94541	
8.	Cuong Doan	Hayward CA 94542	
9.	M.C. ELIASSEN	Hayward	
10.	ANGELA DAVIS	Hayward CA	
12.	Star Villavicencad	Hayward, CA 94541	
13.	Mike Galvin	Hayward Ca. 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

**PROPONENT'S RESPONSES TO
FINDINGS FOR APPROVAL**

Findings for Approval – California Environmental Quality Act:

1. Pursuant to CEQA Guidelines Section 15220, an Initial Study (“IS”) was prepared for this project with the finding that a Mitigated Negative Declaration (“MND”) was appropriate because all potentially significant impacts could be reduced to a level of insignificance.
2. That the proposed MND was prepared by the City of Hayward as the Lead Agency and was circulated with a twenty (20) day public review period, beginning on September 27, 2013 and ending on October 16, 2013.
3. That the proposed MND was independently reviewed, considered and analyzed by the Planning Commission and reflects the independent judgment of the Planning Commission; that such independent judgment is based on substantial evidence in the record (even though there may be differences between or among the different sources of information and opinions offered in the documents, testimony, public comments and such responses that make up the proposed MND and the administrative record as a whole); that the Planning Commission adopts the proposed MND and its findings and conclusions as its source of environmental information; and that the proposed MND is legally adequate and was completed in compliance with CEQA.
4. That the proposed MND identified all potential significant adverse impacts and feasible mitigation measures that would reduce these impacts to less-than-significant levels, and that all of the applicable mitigation measures identified in the MND and Mitigation Monitoring and Reporting Program will be adopted and implemented. Based on the MND and the whole record before the Planning Commission, there is no substantial evidence that the project will have a significant effect on the environment.
5. That the project complies with CEQA, and that the proposed MND was presented to the Planning Commission, which reviewed and considered the information contained therein prior approving the project. The custodian of the record of proceedings upon which this decision is based is the Development Services Department of the City of Hayward, located at 777 B Street, Hayward, CA 94544.
6. The monitoring and reporting of CEQA mitigation measures in connection with the project will be conducted in accordance with the attached Mitigation Monitoring Program, which is adopted as conditions of approval for the project. Adoption of this program will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in Section 21081.6 of CEQA. All proposed mitigation measures are capable of being fully implemented by the efforts of the project sponsor, City of Hayward or other identified public agencies of responsibility.

Findings for Approval – Conditional Use Permit:

1. The proposed use is desirable for the public convenience or welfare.

As demonstrated by the analysis in the Planning Commission’s staff report, the Project, and specifically, a residential use on the first floor of the Project, is desirable for the public convenience and welfare because the Project will convert a large, vacant commercial building into a vibrant mixed-use community, create economic stimulus, and housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled.

2. The proposed use will not impair the character and integrity of the zoning district and surrounding area.

The Project site is surrounded by residential uses and similarly-zoned properties, and as such, the Project will not impair the character and integrity of the surrounding area. The Project also incorporates a retail element, which is consistent with the mixed-use projects permitted in the Central City – Commercial (“CC-C”) Zone. The IS/MND prepared for the Project also demonstrates that the Project is consistent with the CC-C zoning district and the City’s General Plan, and that no substantial adverse effects would occur on the surrounding area after implementation of the mitigation measures included therein. The Project has been designed to be aesthetically pleasing.

3. The proposed use will not be detrimental to the public health, safety, or general welfare.

The IS/MND prepared for the Project demonstrates that no substantial adverse effects would occur after implementation of mitigation measures included therein, including no significant impacts on public services or hazards. Therefore, the Project’s proposed residential and retail uses will not have a negative effect on the public health, safety, or general welfare. Specifically, a conditional use permit allowing first-floor residential units has no effect on the public health, safety or general welfare. The Project also adds housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled and will be aesthetically pleasing.

4. The proposed use is in harmony with applicable City policies and the intent and purpose of the zoning district involved.

As demonstrated by the analysis in Planning Commission’s staff report and the IS/MND, the Project is in harmony with the intent and purpose of the CC-C zoning district area and conforms to all applicable City policies, such as the Hayward General Plan and the Design Review Guidelines. The Project also fulfills the intent and purpose of the CC-C zone by replacing an underutilized site with a vibrant, pedestrian-friendly mixed use development, and as a result, revitalizing the Central City and creating economic stimulus.

Findings for Approval – Vesting Tentative Tract Map

1. That the proposed map is consistent with applicable general and specific plans as

specified in Section 65451.

The proposed subdivision is, as demonstrated by the Planning Commission staff report and the IS/MND, consistent with the Hayward General Plan. The Project site is designated by the General Plan as “City Commercial – Residential Office Commercial (CC – ROC),” which allows the Project’s proposed uses. No Specific Plan applies to the Project.

2. That the design or improvement of the proposed subdivision is consistent with applicable general and specific plans.

The proposed subdivision, as demonstrated by the Planning Commission staff report, is of a design consistent with the Hayward General Plan. As demonstrated by the IS/MND, the Project will have no significant impacts on aesthetics or land use. The Project is aesthetically pleasing.

3. That the site is physically suitable for the type of development.

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012), which is referenced in the IS/MND, demonstrates that the proposed subdivision would occur on a site suitable for the proposed development. The Project site has already been fully developed, which is strong evidence that the site is suitable for this type of development.

4. That the site is physically suitable for the proposed density of development.

The geotechnical investigation performed by Berlogar, Stevens & Associates (February 10, 2012) demonstrates that the proposed subdivision would occur on a site suitable for the proposed development. Density is not a factor that makes the site suitable or less suitable for development. The Project site has already been fully developed, which is strong evidence that the site is suitable for this type of development.

5. That the design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.

The IS/MND prepared for the Project demonstrates that substantial adverse environmental damage, including to fish or wildlife and their habitat, would not result from the proposed subdivision. Moreover, the Project site has already been fully developed, and as a result, no fish, wildlife or habitats exist on the Project site.

6. That the design of the subdivision or type of improvements is not likely to cause serious public health problems.

Adequate capacity exists to provide sanitary sewer service to the Project site, as analyzed in the IS/MND. There are no other aspects of the Project with the potential to cause serious public health problems. The Project also adds housing inventory near adjacent employment and retail centers to reduce vehicle miles traveled, which reduces impacts on

air quality and greenhouses gases.

7. That the design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision.

There are no existing public easements within the boundary of the proposed subdivision, nor are any easements necessary. The Project site is fully developed and currently consists of a 336,000 square foot office building and parking facilities, and therefore, there is currently no public access through the property.

Findings for Approval – Site Plan

1. The development is compatible with on-site and surrounding structures and uses and is an attractive addition to the City.

The Project site is surrounded by similarly-zoned properties that incorporate residential and retail uses, and as such, the Project is compatible with the surrounding structures and uses. The Project will demolish the all structures that currently exists on-site except for a parking garage. The parking garage will be used to support both the Project’s residential and retail uses. The Project would add housing in a desirable location in the center of the City, including convenient access to job centers and shopping, replace an underutilized site with a vibrant, pedestrian-friendly mixed use development, and is aesthetically pleasing. Indeed, the IS/MND prepared for the Project found that the Project has no significant impacts on aesthetics.

2. The development takes into consideration physical and environmental constraints.

As demonstrated by the analysis in Planning Commission’s staff report and the IS/MND prepared for the Project, no substantial adverse effects on the environment will occur after implementation of mitigation measures included therein. The Project only develops an area that has been previously developed, and utilizes appropriate setbacks and reservation of open space areas.

3. The development complies with the intent of City development policies and regulation.

As demonstrated by the analysis in Planning Commission’s staff report and the IS/MND, the Project complies with the intent and purpose of the CC-C zone and conforms to all applicable City development policies, such as the Hayward General Plan and the Design Review Guidelines. The Project also replaces an underutilized site with a vibrant, pedestrian-friendly mixed use development, and as a result, revitalizing the Central City, fulfilling the intent and purpose of the CC-C zone, creates economic stimulus, and is aesthetically pleasing.

4. The development will be operated in a manner determined to be acceptable and compatible with surrounding development.

The Project site is surrounded by residential uses and similarly-zoned properties, and as

such, will be operated in a manner compatible with surrounding development. The Project also incorporates a retail element, which is consistent with the mixed-use projects permitted in the CC-C Zone. The Project would add housing in a desirable location in the center of the City, including convenient access to shopping. The IS/MND prepared for the Project demonstrates that no substantial adverse effects would occur to surrounding development after implementation of mitigation measures included therein. The Project's addition of housing inventory near adjacent employment and retail centers and replacement of an underutilized site with a vibrant, pedestrian-friendly mixed use development will benefit the surrounding development.

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Kathleen Wills

Signature: Kathleen E. Wills

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Richard Aguilar

Signature: Richard Aguilar

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94591

Phone Day: _____ Eve: 3

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Lacey and Ryan Sutherland

Signature: [Handwritten Signature]

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

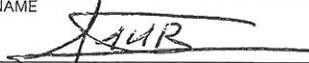


THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

MARIO MARTINEZ
NAME


SIGNATURE

TITLE ORGANIZATION

ADDRESS
HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

Prospect Hill/Ha



Attachment IX

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Curtis Confield

SIGNATURE Curtis A Confield

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward, STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

Prospect Hill / Hall

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: LESLIE HALL

Signature: Leslie Hall

Title: _____ Organization: _____

Address: _____

City: HAYWARD State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: CONSTANCE Taylor

Signature: C. D. Taylor

Title: _____ Organization: _____

Address: _____

City: Hayw. State: Ca. Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

No call backs OR E-MAIL

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

Prospect Hill

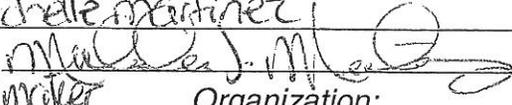
~~THE BOULEVARD~~ @ THE BOULEVARD

Attachment IX

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Name: Michelle Martinez

Signature: 

Title: Homeowner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
 @ The Boulevard
 982 B Street
 Hayward CA 94541
 damara@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

Sinoda Kelley
NAME

SIGNATURE

TITLE ORGANIZATION

HAYWARD, CA. 94541
ADDRESS
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER It's about time!!

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Please add my name to your growing coalition of community supporters.
 Please keep me informed.

Caffie Germany
 NAME _____

Caffie Germany
 SIGNATURE _____

TITLE _____ ORGANIZATION _____

ADDRESS _____
Harvard, CA *94544*
 CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Claudia C. Rollins

SIGNATURE Claudia C. Rollins

TITLE Walker on Foothill Blvd ORGANIZATION

ADDRESS

CITY Hayward STATE CA ZIP 9454

DAYTIME PHONE NUMBER EVENING PHONE NUMBER Same

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME SANDRA J. RUSH

SIGNATURE Sandra J. Rush

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward, Ca. STATE _____ ZIP 94544

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Maria G. LOPEZ

SIGNATURE Ma. GUADALUPE LOPEZ

TITLE ORGANIZATION

ADDRESS

CITY Hayward STATE LA ZIP 94541

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME GARY + CAROL BOOKOUT

SIGNATURE Gary Bookout Carol Bookout

TITLE ORGANIZATION

ADDRESS

CITY HAYWARD STATE CA ZIP 94544

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

Be Sure to ~~have some security~~ Attachment IX

THE BOULEVARD

retail

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME David and Amy Whaley

SIGNATURE Amy Whaley

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Cindy Towles

SIGNATURE *Cindy Towles*

TITLE Founder ORGANIZATION Cherryland Transformation

ADDRESS Hayward STATE CA ZIP 94541

CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Blanca Cervantes-Madrugal

SIGNATURE *[Handwritten Signature]*

TITLE ORGANIZATION Resident of Hayward

ADDRESS CITY STATE ZIP Hayward CA 94544

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME ELIZABETH MORENO

SIGNATURE 

TITLE ORGANIZATION

ADDRESS CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Abdul Malik
NAME

Abdul Malik
SIGNATURE

MA ORGANIZATION AT&T
TITLE

Hayward CA 94541
ADDRESS
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Souise Butler
NAME

Souise Butler
SIGNATURE

Home maker
TITLE ORGANIZATION

ADDRESS
Hayward *CA* *94541*
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

MARK DANIEL
NAME

Mark & Daniel
SIGNATURE

HAYWARD RESIDENT 11 years
TITLE ORGANIZATION

HAYWARD CA 94546
CITY STATE ZIP

n/a
DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

Deborah Gulleyas
NAME

Deborah Gulleyas
SIGNATURE

President
TITLE

Shady Hill Home Owner Assn
ORGANIZATION

Hayward
CITY

CA
STATE

94541
ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Carol Cambra
NAME

Carol Cambra
SIGNATURE

TITLE ORGANIZATION

ADDRESS

Hayward Ca 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Grace C. Cooper

SIGNATURE Grace Cooper

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Beverly Young Harrison

SIGNATURE Beverly Harrison

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Larry Bangs

Signature: Larry Bangs

Title: Retired! Organization: _____

Address: _____

City: Hay State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

Prospect Hill

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Debra Vandergrift

Signature: Debra Vandergrift

Title: _____ Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Don Lee

Signature: *Don Lee*

Title: Owner Organization: Zippy Shell Bay Area

Address: _____

City: Hayward State: CA Zip: 94545

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: BRAD W. ELLS

Signature: Brad Wells

Title: VP & CFO Organization: CAL STATE EAST BAY

Address: _____

City: HAYWARD State: CA Zip: 94542

Phone Day: _____ Eve: _____

Email: _____

LIVES DOWNTOWN
TSUJET LAGUN
TRIAN CITY

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: MICHAEL T. MATHIAS
Signature: [Handwritten Signature]
Title: _____ Organization: _____
Address: _____
City: HAYWARD State: CA Zip: 94542
Phone Day: _____ Eve: _____
Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damaral@integralcommunities.com

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Yocum Holness
Signature: [Handwritten Signature]
Title: _____ Organization: NO ORGANIZATION
Address: _____
City: Hayward State: CA Zip: 94542
Phone Day: _____ Eve: _____
Email: [Handwritten Email]

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



INTEGRAL
Communities
A DIVERSIFIED REAL ESTATE COMPANY

Yes, I endorse @ the Boulevard! Please add my name to your growing coalition of community supporters.

Signature _____

Please keep me informed.

Name

SANDRA C HERRERA

(if applicable) Title CEO Organization/Company _____

Phone(day) _____ (eve) _____ Email _____

Address _____ City Hayward
State CA Zip 94540

Great job on bringing something more upscale to Hayward!

Please call me. I can be of help in the area of waste stream reduction and recycling.

→ Case compactors.com

Please take a moment to see what we do.

would love to meet with you soon!

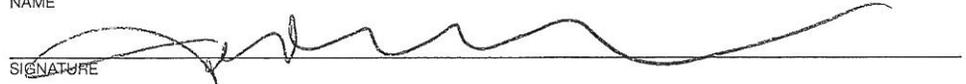
Sandra Herrera



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME John Nielsen

SIGNATURE 

TITLE owner ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE Ca ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Rev. & Mrs. John Geach

SIGNATURE Rev & Mrs John Geach

TITLE Doctor of Ministry ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE Ca ZIP 94506

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME Charlene Frey

SIGNATURE [Handwritten Signature]

TITLE None ORGANIZATION None

ADDRESS Hayward, CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER 510-435-1100 EVENING PHONE NUMBER 510-435-1100

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Charles Harris
 SIGNATURE Charles Harris
 TITLE Home owner ORGANIZATION _____
 ADDRESS _____
 CITY Hayward STATE CA. ZIP 94541
 DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Benjamin J. Vella

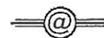
SIGNATURE *Benjamin J. Vella*

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541-5329

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

JAN AVERI
NAME

Jan Averill
SIGNATURE

Please improve Hayward so that it is no longer "The Dump of the Bay Area". This project is a step in the right direction.
TITLE ORGANIZATION ADDRESS CITY STATE ZIP DAYTIME PHONE NUMBER EVENING PHONE NUMBER



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

MAZIE B. CHAVEZ
NAME

Mazie B. Chavez
SIGNATURE

Mazie B. Chavez
TITLE ORGANIZATION

RESIDENT since 1952 - since downtown
ADDRESS

is looking better - we need more
CITY STATE ZIP

people there. Please go with project!!!
DAYTIME PHONE NUMBER EVENING PHONE NUMBER



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JEANNETTE RODGERS

SIGNATURE J. Rodgers

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME DANIEL ZAZUETA

SIGNATURE *Daniel Zazueta*

TITLE Home owner ORGANIZATION _____

ADDRESS _____

CITY Chapel Hill STATE NC ZIP 27514

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JOSE SOLANO

SIGNATURE 

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94542

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME DIANA K. WALLACE

SIGNATURE *Diana K. Wallace*

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Sheri Edwards

SIGNATURE Sheri Edwards

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Veia Fier

SIGNATURE [Handwritten Signature]

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY 1 STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME DAVID PELTON

SIGNATURE *David Pelton*

TITLE OWNER ORGANIZATION FOREIGN AUTO SERVICE

ADDRESS HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER



THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Doc Miller
NAME

Doc Miller
SIGNATURE

CEO MRC Enterprises, LTD
TITLE ORGANIZATION

ADDRESS

Hayward CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

WHEN IS SOMEONE GOING TO DO SOMETHING ABOUT
"CENTENNIAL HALL" ??

Attachment IX
PROBET #11
THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

JEAN E. MITCHELL

NAME

SIGNATURE

Jean E Mitchell

TITLE

ORGANIZATION

RETIRED - TELEPHONE CO. (SERVICE REP)

ADDRESS

CITY

STATE

ZIP

HAYWARD

CA

94541-2696

DAYTIME PHONE NUMBER

EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Brenda Logsdon

SIGNATURE Brenda Logsdon

TITLE _____ ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

WILLIAM & ELIZABETH JACKSON
NAME

Elizabeth M. Jackson
SIGNATURE

HOME OWNERS
TITLE ORGANIZATION

ADDRESS
Hayward Ca. 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

**YES, I want the old Mervyn's site
transformed and endorse @ the Boulevard!**

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Ella Thomas
SIGNATURE Ella Thomas

TITLE _____ ORGANIZATION _____

ADDRESS _____
CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____



YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME DANIEL H. LARAMIE

SIGNATURE *Daniel H. Laramie*

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Leslie Mero

NAME

Leslie Mero

SIGNATURE

TITLE ORGANIZATION

ADDRESS

Hayward CA 94541

CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Aldora A. Gonsalves

SIGNATURE Aldora A. Gonsalves
TITLE ORGANIZATION

ADDRESS
CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

GERALD Williams
NAME

Gerald Williams
SIGNATURE

TITLE _____ ORGANIZATION _____

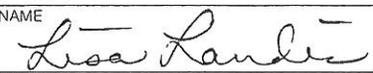
ADDRESS _____

WAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Lisa Landis
NAME

SIGNATURE

TITLE ORGANIZATION

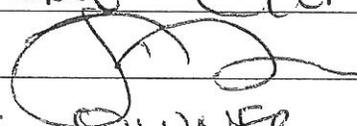
ADDRESS
Hayward CA 94546
CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME JEROME CROWNING

SIGNATURE 

TITLE HOME OWNER ORGANIZATION _____

ADDRESS _____

CITY HAYWARD STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME Carlos McLenna

SIGNATURE [Handwritten Signature]

TITLE _____ ORGANIZATION _____

ADDRESS _____

CITY Hayward STATE CA ZIP 94541

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

**YES, I want the old Mervyn's site
transformed and endorse @ the Boulevard!**

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

Karen Osborne
NAME
Karen Osborne
SIGNATURE

TITLE _____ ORGANIZATION _____

ADDRESS _____
Dayward *CA* *94541*
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

- Please add my name to your growing coalition of community supporters.
- Please keep me informed.

NAME THOMAS HARTMAN

SIGNATURE *Thomas Hartman*

TITLE N/A ORGANIZATION

ADDRESS HAYWARD CALIFORNIA 94544-2000

CITY STATE ZIP

DAYTIME PHONE NUMBER EVENING PHONE NUMBER N/A

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters.

Please keep me informed.

NAME HOWARD RYDOLPH

SIGNATURE Howard Rydolph

TITLE RETIRED ORGANIZATION _____

ADDRESS HAYWARD CA 94541
CITY STATE ZIP

DAYTIME PHONE NUMBER _____ EVENING PHONE NUMBER _____

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: EMMA GUZMAN

Signature: Emma Guzman

Title: Home owner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com

116

11/11

THE BOULEVARD

YES, I want the old Mervyn's site transformed and endorse @ the Boulevard!

Please add my name to your growing coalition of community supporters. Please keep me informed.

Name: Andrea White

Signature: [Handwritten Signature]

Title: home owner Organization: _____

Address: _____

City: Hayward State: CA Zip: 94541

Phone Day: _____ Eve: _____

Email: _____

Please return to:
@ The Boulevard
982 B Street
Hayward CA 94541
damara@integralcommunities.com



Petition to transform the old Mervyn's site and endorse @ The Boulevard

The old Mervyn's site on Foothill Boulevard has been vacant since Mervyn's closed the building in 2008. For many residents, this dark, vacant, empty building contributes to a lack of safety and revitalization in our Downtown and the neighborhood.

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
✓ 1.	Brianne Mannus	Hayward CA 94541	
✓ 2.	Ashur Daniel	Hayward CA 94541	
3.	Fiona Apim	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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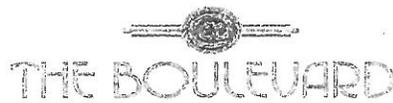
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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Dena Vuocka	Hayward CA 94541	
2.	Angela Mixa	Hayward CA 94541	
3.	Lon Franckin	Hayward CA 94541	
4.	SAMUEL M. ADAMS		
5.	Monika Sobotke	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541



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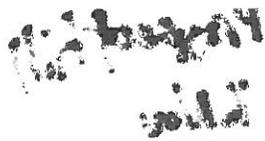
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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Dorlie Kramer		
2.	Ewen John-Jones		
3.	Wendy Johnson		LIFE HOUSE RECOVERY HOMES
4.	Dahlia	Hayward CA 94541	
5.	Genoveva Flores	94541 Hayward CA	
6.	Carlos Ramirez	94541 Hayward	
7.	✓ Carla Ramirez	9451 Hayward	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541



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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Curtis Soares		
2.	Michaela Parker		
3.	Chris Christenson		
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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	David Acosta	22185 Center St Eastvale, CA	
2.	MARY GRAVES	Hayward, CA 94542	
3.	Edna Norlett	Hayward CA 94549	
4.	Kathy Lagman	Hayward CA 94544	(don't want spam)
5.	Kabette Cole	Hayward CA 94542	
6.	Janette Hernandez	Hayward, CA	
7.	Robert Hicks	HAYWARD CA 94541	
8.	Kyle Watkins	Hayward CA 94541	
9.	Jacqueline Morales	Hayward CA	
10.	Alex Mays	Hayward, CA	
12.	William Paras	Hayward CA	
13.	Demarion English	94541	

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Patricia Champoux	Hayward CA 94541	
2.	BERNICE RODRIGUES	Hayward Ca	
3.	Antoinette Williams	Hayward Ca 94541	
4.	LARRY BISHOP	larrybishopca@hotmail.com	
5.	Amer Dent	Hayward CA 94541	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Jamar C.	Hayward	
2.	JERRY LOVE	Hayward	
3.	Robert Horton	Hayward	
4.	Chandis Sosa	1718 137th St, San Leandro CA 94577	
5.	Gi. Paulo	Hayward CA	
6.	Cuanita Espinoza	Hayward CA 94544	
7.	Victoria Beddy	Hayward CA 94542	
8.	JAMES MANTALUAXOS	Hayward	
9.	ROSE ANN HALL	Hayward CA, 94544	
10.	De'Lango Baldwin	Hayward, CA 94541	
12.	Bayan D. CASSALIA	HAYWARD CA 94545	
13.	Roberto Notrangelo	Hayward, CA 94542	

Return to @The Boulevard: 982 B Street, Hayward CA 94541



**Petition to transform the old Mervyn's site
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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Anthony Piscar	Hayward Ca	
2.	Andre Alexander	Hayward CA 94541	
3.	J Shibuya	Hayward, CA 94541	
4.	Andres Dent	Hayward, CA 94541	
5.	Rodney Marshall	Hayward 94541	
6.	WILLIE ROBERTO	Hayward, CA 94543	
7.	JOSEPH RODRIGUEZ	HAYWARD, CA	
8.	ROYAIP TIEFFE		
9.	Cesar Ramos	Hayward, CA	
10.	Barbara ^{Bowles}	Hayward 94544	
12.	Tanet Shano	Hayward CA 94541	
13.	Quiana Ester	Hayward, CA 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Erika Hernandez	Hayward, CA 94541	
2.	Jovann Augustus	Hayward, CA	
3.	Yolanda Brown	2125 Lee St Hayward, CA 94541	(510) 575-5367
4.	Daniel Bo	Hayward, CA 94541	
5.	Saul Rosas	Hayward CA	
6.	Tim Fletcher	Hayward 94542	
7.	Angel Lopez	Hayward CA 94541	
8.	Joann Peppercorn	Hayward CA 94541	
9.	Olivia Dennis	3000 Somerset Ave Castro Valley 94540	510 331 7028
10.	Osni Mendoza	2250 Marinier way San Leandro CA 94579	510 204-4305
12.	Yazmin Sanchez	Hayward CA 94541	
13.	Marilyn Grise	Hayward 94544	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Marilyn Grise

Scanned & Sent
to Nolan 7/2/14



Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name <small>Please Print</small>	Address and EMAIL <small>Please include zip code</small>	Phone <small>Day & Evening</small>
1.	WAYNE WEST		
2.	JEFFREY FREID	HAYWARD 94541	
3.	Linda HOOKER	16349 Saratoga St. San Leandro CA 94578	
4.	Felicia LAUREANO	Hayward, Ca 94541	
5.	Steve Bailey	Hayward, Ca 94541	
6.	Dwayne Wiggins	Hayward, CA 94541	
7.	Jane Malmgren	Hayward CA 94541	
8.	STEVEN DOSS	HAYWARD	
9.	Page Nelson	Hayward CA	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name <small>Please Print</small>	Address and EMAIL <small>Please include zip code</small>	Phone <small>Day & Evening</small>
1.	John Tate	Hayward, CA 94541	
2.	Martin Walter	Hayward, CA	
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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THE BOULEVARD

**Petition to transform the old Mervyn's site
 and endorse @ The Boulevard**

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Alfred Rodriguez	Hayward Ca	
2.	TERRI Lovello	Hayward CA	
3.	JEANIE SIU		
4.	Arthur Foster	Hayward CA 94541	
5.	Jessica Terence ^{Pradas}	Hayward, CA	
6.	Lilcan Cardenas	Hayward CA 94541	
7.	William Demore	1531 165th ave #10 San Leandro, CA	
8.	Denise Barrett	Hayward 94541	
9.	Matthew Fien	Hayward 94541	
10.	Thomas Nelson	Hayward, CA 94541	
12.	Enrique Alcaraz		
13.	Mareny Hernandez	Hayward C.A. 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	ROBERT OWENS		N/A
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Return to @The Boulevard: 982 B Street, Hayward CA 94541

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Petition to transform the old Mervyn's site and endorse @ The Boulevard

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Get on board to improve Downtown Hayward!

We support @ the Boulevard!

	Name Please Print	Address and EMAIL Please include zip code	Phone Day & Evening
1.	Consuelo Barragan	Hayward CA 94544	
2.	Alicia Alvarez	1652 Mono Ave San Leandro 945	
3.	ABU KERRA	Hayward Ca 94544	
4.	MARCIA ROBERTS	Hayward CA 94541	
5.	Shanese Houston	Hayward, CA, 94541	
6.	Rhajiames Holmes	Hayward, CA	
7.	Johanna Pargoulkar	Hayward CA 94541	
8.	CUONG DOAN	Hayward CA 94542	
9.	M.C. ELIASSEN	Hayward	
10.	ANGELA DARRIS	Hayward CA	
12.	Star Villavicencio	Hayward, CA 94541	
13.	Mike Galvin	Hayward Ca. 94541	

Return to @The Boulevard: 982 B Street, Hayward CA 94541

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Councilmembers and Mayor Sweeney:

I am writing you today to explain why I think the City should expedite the approval of the @The Boulevard Project.

I own Alma's Creations, located on A Street. We specialize in bridal clothing and event planning.

My business is just blocks away from the old Mervyn's headquarters. I had grown so used to seeing it, as I drive by, that I had all but forgotten that it existed. That all changed when I heard about @The Boulevard Project: I hadn't given the Mervyn's site any thought in years, but now the prospect of hundreds of new families moving into Downtown Hayward is cause for much excitement.

As you know, Hayward's downtown is much more beautiful than it was just a few years ago. The thing it's missing now is more people.

I have no doubt that attracting 200 neighboring families will be exactly what our City's Downtown needs. I don't think it's a stretch to say that certain young families will be in need of my business's services. The same must be true for many of the other neighboring businesses that could sure use a boost.

Let's give Downtown Hayward some extra liveliness: let's add some bustle to its beauty!

Sincerely,



Alma Baez
Owner
Alma's Creations

Angela Mira

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Mayor and City Council,

I live in Prospect Hill and it has come to my attention that we may soon have some new neighbors in the old Mervyn's space.

I would like to register with you my full support for this project. Using that space for anything is a good idea, but using it to build a community of townhomes with some shops along Foothill seems like the best thing to do. Hope to see it built soon.

Sincerely,

A handwritten signature in dark ink, appearing to read 'A Mira', with a long horizontal flourish extending to the right.

Angela Mira

Alfredo Rodriguez
Owner
Vintage Alley

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Mike Sweeney and Council members,

Recognizing when to say “out with the old and in with the new” is what I specialize in. My chosen profession involves old things. Some things are old and priceless, while others should just be disposed of.

A piece of land can't be disposed of. However, it can be reused in ways that are entirely different. This would be the case for the *new* plans that Integral Communities have for the *old* Mervyn's headquarters. They would salvage the land that it occupies, but change it for the better.

As I've suggested, vintage is only good when it's charming and functional. Leaving the Mervyn's headquarters standing is hardly this. So, in addition to putting it out of its misery, it would be replaced with something that is fresh and new that would bring much-needed change into our downtown's economy.

Sincerely,

A handwritten signature in cursive script that reads "Alfredo Rodriguez".

Alfredo Rodriguez

Amelia Sete

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Mike Sweeney and City Council members,

I live in the Prospect Hill neighborhood, right next to the old Mervyn's headquarters.

I am writing to voice my support for the @The Boulevard project. The Mervyn's building and parking structure are an unfortunate part of our neighborhood, having been vacant all of these years. I've hoped that someone would once again use the space, but so far that hasn't happened.

This is why the project to build homes and some retail space on Foothill Boulevard sounds like a good idea to me. Besides just replacing something that needs to be replaced, it sounds like the project would be a really nice addition to our neighborhood.

As someone who lives right next door to the Mervyn's headquarters, I know better than most how urgent it is to approve this project.

Sincerely,

A handwritten signature in cursive script that reads "Amelia Sete". The signature is written in black ink and is positioned above the printed name.

Amelia Sete

Anthony Waters

Mayor Sweeny and Hayward City Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeny and Hayward City Council Members,

I would like to join many others in registering with you my support for the @ The Boulevard project.

It is high time that something is done in that part of town. Year after year, the old Mervyn's Headquarters grows older and older. It is not aging well.

I think that the @ The Boulevard project addresses two important questions: what should be done with the old Mervyn's headquarters and what is good for Hayward?

The answer to the first question is quite broad: any sort of project would probably be a good one.

The answer to the second question is more specific: Hayward's downtown could really use a boost and additional families would really contribute just this.

Please support the @ The Boulevard project!

Sincerely,

A handwritten signature in black ink, appearing to read 'Anthony Waters', written in a cursive style.



Mayor Mike Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

My Hayward City Council members and Mayor Mike Sweeney,

I am writing you today because I think that it is time that we seriously consider the old Mervyn's headquarters. As time passes, the building looks more and more forlorn, and I have begun to wonder if that is what is best for our community.

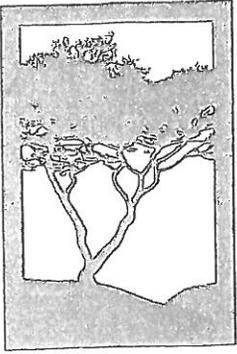
It seems clear that it is unwanted in its current state. While having this space be one that is strictly for retail or commercial use may be what some want, this use of space doesn't seem like one that is currently viable.

Thus, I pledge my support for the @The Boulevard Project. It attractively combines retail and residential uses. Such a plan, independently, sounds like a great addition to Hayward. That this project is going to replace a space that has burdened our community's aesthetic is the cherry on top.

Hayward shouldn't miss this opportunity.

Sincerely,

Arturo "Art" Benin
Benin Legacy Wealth
Management



LEGACY
REAL ESTATE
& ASSOCIATES

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeney and Councilmembers,

We urge you to support the @The Boulevard project. Bringing 200 families into the Downtown will be a great addition to the area and put the old Mervyn's Headquarters site to good use.

We will begin to see the benefits of approving this project the day ground is broken. All of the workers at the site will lead to immediate commerce in the downtown. This additional commerce will eventually be replaced, but further increased as 200 new families move in permanently after the project's completion.

It is hard for us to believe that this site has been vacant for so long. If the @ The Boulevard project is not approved, the old Mervyn's will be left abandoned indefinitely. This would certainly not be in Hayward's best interest.

It is with the best interest of Hayward in mind that we urge your support.

Sincerely,

Bob and Linda Leppert

A tradition of excellence since 1971
www.LegacyRealEstateAssociates.com

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Councilmembers and Mayor Sweeney:

It is with the best interest of Hayward at heart that I write to encourage your support for @The Boulevard.

In times of recession, small businesses suffer a great deal. Like others throughout the country, our Downtown has suffered. This, of course, is not our fault. However, not taking advantage of an opportunity to ameliorate our local economic situation would be.

Nothing would be better for commerce in our downtown than the influx of nearly 200 new families. This is what Integral Communities is offering us. That the space, and other commercial spaces like it, have remained vacant for too long.

The alternative—a balanced mix of commercial and residential space—is ideal for our community: it will beautify it, it will bring in much-needed revenue.

Do right by Hayward and welcome this much-needed expansion of our Downtown community.

Sincerely,

A handwritten signature in black ink, appearing to read 'Brian Blackburn', written in a cursive style.

Brian Blackburn

Brian and Theresa Lotz
Lotz Insurance Agency

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Mike Sweeney and City Council Members,

Our insurance agency is right across the street from the old Mervyn's Headquarters. We have watched it sit vacant for a long time.

Why not reuse it for something that will benefit our town, we've often wondered? When we were told about the @ The Boulevard Project, we realized that the answer was finally here.

It's a great answer, too. Replacing that building will be a real improvement to Foothill Boulevard. Even more importantly, the additional families that this project will bring into our neighborhood will be fantastic for business.

We look forward to seeing the @ The Boulevard Project approved.

Sincerely,



Brian and Theresa Lotz

Brittany Manus

Hayward City Council
777 B Street
Hayward, CA 94541

Dear City Council,

I live on Prospect Street, just blocks away from the former Mervyn's Headquarters. I would like to see something done with this site that enhances our neighborhood.

The aspect that really attracts me to the @ The Boulevard project is the plan to build a small park and a walking path along the San Lorenzo Creek. Recreational space is always a worthwhile addition to any neighborhood.

My enthusiasm for this project, though, is not limited to the one component that I mentioned. A new vibrant community of homes sounds like a wonderful use of this vast space.

Thank you for considering approving this project. It will be a welcome addition to our neighborhood.

Sincerely,

A handwritten signature in cursive script that reads "Brittany Manus".

Brittany Manus

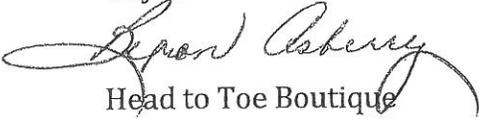
Dear Hayward City Council,

I am writing in support of the project on Foothill Boulevard to build new townhomes and retail space Downtown. This project is necessary because the streets are just not busy anymore. I am hoping this project will bring business back into Downtown Hayward. Many businesses have already left the area because they couldn't make it here. Something needs to change.

This project is also a positive stop because it will beautify the area. I do plan to keep an eye on the types of retail that go into the space, and the construction process --- I hope you will do that too so that there is not any negative impact on Downtown businesses. Integral's track record says they listen to community input and act on it when possible, so these things should be fine.

Thank you for your time. I hope you approve this project. We need the boost.

Byron Asberry
Buyer



Head to Toe Boutique

Carla Ramirez

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Mayor Sweeney and Hayward City Council:

Prospect Hill is a nice place to live.

There is room for improvement, namely at the old Mervyn's site. I think it's time that something is done about it.

This is why I am writing today: I want to inform you of my support for the @The Boulevard project. It is a well-conceived solution for our neighborhood's single largest problem.

Sincerely,

A handwritten signature in black ink that reads "Carla Ramirez". The signature is written in a cursive style with a large, looping initial "C" and a long, sweeping tail that extends to the right.

Carla Ramirez

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeney and Councilmembers,

I am writing to express my support for the proposed project on Foothill Boulevard at the site of the old Mervyn's Headquarters. A mixed-use retail and residential project would be the best use of this enormous abandoned space, and this is exactly what is proposed in the @ The Boulevard plans.

Our community has suffered long enough from the economic downturn. This project will benefit local construction workers, and will subsequently bring in 200 families whose patronage of downtown businesses can only contribute to an economic upswing. Anything that brings in new business is a good thing!

Reusing this space will serve to beautify our community and add to Downtown Hayward's vibrancy. It is truly a shame that the old Mervyn's headquarters has been abandoned for so long. Revitalizing this space will keep Hayward moving in the right direction.

I cannot emphasize enough how important for our community it is that this project be given the green light.

Sincerely,

A handwritten signature in cursive script that reads "Cece Turner". The signature is written in black ink and is positioned below the word "Sincerely,".

Cece Turner - Dream Girls Hair Salon

Connie Butler

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear City Council and Mayor Sweeney,

I am one of many Prospect Hill residents who support the @The Boulevard project. It is a project that makes sense for our community:

Let's start with the obvious: the Mervyn's site has been vacant for many years. No one has shown interest in reusing it until now. If we don't take advantage of the opportunity that the @The Boulevard Project presents us with, how many more years will we wait until another group of people comes along with a solid plan to reuse the Mervyn's site?

The other reason that permitting the @The Boulevard Project makes sense is that it is actually a really good plan in its own right. Outside of the fact that it is simply a good idea to reuse the Mervyn's site, this plan to build homes, a small retail space on Foothill Boulevard and to develop a walking path along the San Leandro Creek sounds quite nice.

Sincerely,

A handwritten signature in black ink, appearing to read 'C. Butler', with a large, stylized flourish at the end.

Connie Butler



Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Councilmembers and Mayor Sweeney:

I see a bright future for Downtown Hayward. It's really not so hard to imagine with all of the recent improvements. All it's lacking, now, is people, but I see a solution looming before us: the @The Boulevard Project.

Aside from the extra families it would bring to Downtown Hayward, I see this project being beneficial to Hayward in two other ways. I also think this project would be a source of much needed money for our city. Finally, I see revitalizing the space that the Mervyn's occupies as a sort of 'next step' in the already-started process of renewing our downtown's vibrancy.

Keep up the good work you've already done with Downtown Hayward by supporting the @The Boulevard Project!

Sincerely,

A handwritten signature in black ink that reads "Daisey Lynam". The signature is written in a cursive, flowing style.

Daisey Lynam

Owner

Hayward City Council and Mayor Michael Sweeney

777 B Street
Hayward, CA 94541

Hayward City Council Members and Mayor Sweeney:

I've been doing business in Hayward for 44 years. I've seen good times but I also remember some of Hayward's harder times. Unlike some, though, I'd argue that our town is on an upswing these days. I like what was done with Downtown Hayward, and I'd like for this to be a trend. Hayward is a beautiful and vibrant area, but I can't think of any reason to halt progress and say, "this is good enough."

I've seen the drawings the people at Integral Communities have come up with for their @ The Boulevard plans, and I think they're quite beautiful. This new neighborhood would be a perfect expansion of Hayward's revitalized downtown.

It goes without saying that this project would be beneficial to local business. This, in turn, would lead to more revenue for the city. Wisely spent, as I'm certain it would be, this additional money could be used for even more improvements like the ones we've already seen! This inertia is what I'd call an upswing; let's not prevent it.

Sincerely,

A handwritten signature in black ink, consisting of several overlapping, sweeping strokes that form the name 'Darrell Davidson'.

Darrell Davidson

Owner

ELC Eden Loan Company

Mayor Sweeny and Hayward City Councilmembers
City of Hayward
777 B Street
Hayward, California 94541

Dear Mayor Sweeny and Hayward City Councilmembers,

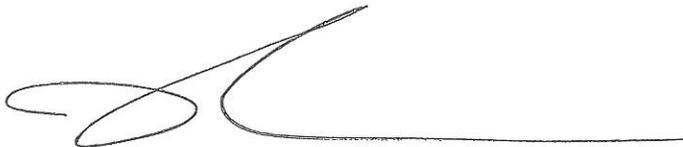
I am writing to voice my support for the downtown development project proposed at the site of the former Mervyn's on Foothill Boulevard. The large, vacant site is not doing the City any favors, as the abandoned, poorly lit only complicates continued efforts to revitalize and reinvigorate our downtown community.

We all want to keep Hayward moving in the right direction, and by utilizing the space on Foothill to expand retail and commercial presence while simultaneously improving the safety of the surrounding community, everyone wins.

Attracting more families to Hayward provides businesses with more foot traffic, and revitalizes our neighborhoods. The location of the development provides a pleasant balance between the beauty of Hayward's scenic landscape, and the convenience of our bustling downtown environment. Improving access to businesses, increasing lighting in streets and along the creek, and providing more opportunities for businesses to locate or expand will all contribute to an aggregate improvement in our quality of life.

I believe that this project would be a worthy use of the proposed site, and urge you to join myself and the community in supporting this effort.

Sincerely,

A handwritten signature in black ink, appearing to read 'Darren Guillaume'. The signature is fluid and cursive, with a long horizontal stroke extending to the right.

Darren Guillaume

Doc's Wine Shop



10/29/13

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor and Members of the Council,

I would like to take a moment to voice my support for the proposed development project at the site of the former Mervyn's headquarters on Foothill Boulevard. As a downtown business owner, I can tell you that this property has had a negative impact on not just my own business, but many of the businesses that are within proximity of the vacant lot. Utilizing this space to provide additional residential housing as well as more retail space would be a vast upgrade that would benefit all Hayward residents as well as the downtown business community.

Doesn't a lively and energetic downtown filled with families and vibrant businesses sound more appealing than the dark and destitute backdrop of an abandoned building. The poorly lit parking garage and walkways around the property further contribute to an unsafe climate that drives businesses and customers away from our downtown area.

But things don't have to be this way. This project would help our city by attracting families to enjoy our shops and schools and businesses that will contribute to our community for years to come, revitalizing and reinvigorating Hayward.

@The Boulevard is the best use of the property where Mervyn's was formerly headquartered, and represents a significant step forward for our city. For these reasons, I implore you to support this project.

Sincerely,

Dan McEwen
Area Manager
Selix Formalwear

Mayor Sweeny and Hayward City Councilmembers
City of Hayward
777 B Street
Hayward, California 94541

Dear Mayor Sweeny and Hayward City Councilmembers,

I am writing to voice my support for the downtown development project proposed at the site of the former Mervyn's on Foothill Boulevard. The large, vacant site is not doing the City any favors, as the abandoned, poorly lit structure is an unsafe and only complicates continued efforts to revitalize and reinvigorate our downtown community.

The design of the project is well done. I hope you will make sure the development provides sufficient parking on its grounds, so that the parking does not spill over onto City streets and take away any existing parking for downtown shoppers.

Attracting more families to Hayward, as this project will do, provides businesses with more foot traffic, and revitalizes our neighborhoods. The location of the development provides a pleasant balance between the beauty of Hayward's scenic landscape, and the convenience of our bustling downtown environment. Improving access to businesses, increasing lighting in streets and along the creek, and providing more opportunities for businesses to locate or expand will all contribute to an aggregate improvement in our quality of life.

I believe that this project would be a worthy use of the proposed site, and urge you to join myself and the community in supporting this effort.

Sincerely,

A handwritten signature in black ink, appearing to read 'D Higgins', with a long horizontal flourish extending to the right.

Dann Higgins

Higgins Jewelry Center

David Guard

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Mayor Sweeney and City Council,

I near the old Mervyn's Headquarters. I am writing you to let you know that I think the @ The Boulevard Project is one that is worthy of your approval.

I have watched the Mervyn's Headquarters fall apart year after year, and think that it is time that it be put to good use.

This is why the @ The Boulevard Project must be approved. It will reuse the space and provide Hayward with some much-needed revenue.

Sincerely,

A handwritten signature in black ink, appearing to read "D. Guard", written in a cursive style.

Kathleen & David Pelton

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

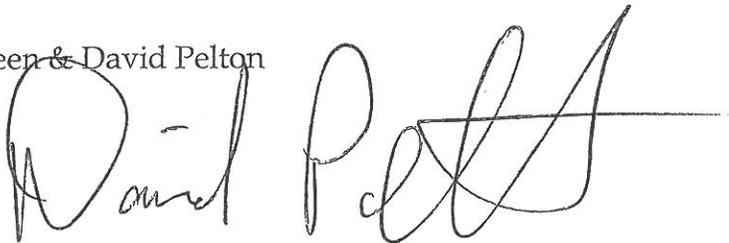
Being both Prospect Hill residents and the owners of a local business, we are quite concerned about the @ The Boulevard project; we're in favor of it.

It is frustrating living here and knowing that a project that will be so good for our town is taking so long to be approved. There are two main reasons that we voice our frustrations today: we think that this project will both beautify our neighborhood and it will help Hayward's downtown area.

There is no way to overstate what an improvement replacing the Mervyn's lot with a brand new mixed-use space will be to the aesthetic of our neighborhood. The influx of 200 new families will be a great benefit to Downtown's commerce.

Let's not hesitate any longer in the approval of the @ The Boulevard project.

Kathleen & David Pelton

A handwritten signature in black ink, appearing to read "David Pelton". The signature is written in a cursive, flowing style with a long horizontal line extending to the right.

Diego Esquivel

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Hayward City Council

I am a homeowner who lives in Prospect Hill, less than a block away from the old Mervyn's Headquarters.

I speak for myself and many of my neighbors when I say that we support the @ The Boulevard Project. I believe that it will enhance our neighborhood. The Mervyn's site is a real problem for us. Replacing it with a community of homes would make my neighborhood safer and more beautiful. I believe, also, that it will boost my property value.

When making your decision about the @ The Boulevard Project, please do keep in mind that many of the residents who neighbor the Mervyn's site—those who will be most affected by any sort of project that happens there—are strong supporters of the @ The Boulevard Project.

Sincerely,

Diego Esquivel

A handwritten signature in black ink, appearing to read "Diego Esquivel", written in a cursive style.

The Cobblers

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor and City Council,

I understand you will be considering soon a new project by Integral Communities in Downtown Hayward on Foothill Boulevard. I am writing to pass along my enthusiastic support on the project.

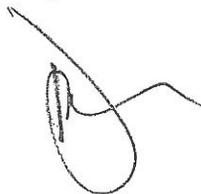
The idea of this new activity in the area is very exciting. I have been in business for 40 years, and am looking forward to the new retail establishments, because they will bring more customers into the area.

In addition, the homes will bring in more foot traffic and customers to my store and the rest of Downtown. This is all around good news for Hayward, and I hope you approve the project.

Thank you,

Dino Grasseschi

Owner



10/25/13

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

There are two parts to revitalizing Hayward's Downtown economy. In beautifying the area, we have accomplished one: we have made it a desirable space for people to go.

Now we need to increase the number of people who will take advantage of this. This is the second part of revitalizing Hayward: more people living in the immediate area will lead to more commerce.

This second step will solidify Hayward's future success. We have come quite far, and we should encourage more liveliness in our commercial areas of town. This will enrich our businesses and their success will encourage others to come back to Hayward.

Sincerely,

Dong Lee

J.R. Bakery

A handwritten signature in black ink, appearing to read 'Dong Lee', written over a horizontal line.

Ernesto Martin

Mayor Sweeny and Hayward City Councilmembers
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeny and Hayward City Councilmembers,

Living in Prospect Hill is convenient. It is located near BART, a Safeway, and Downtown Hayward.

It has its drawbacks too, though. The most notable probably is that across the street there is an unkept parking lot and an enormous abandoned building and parking structure.

I think that the plan called "@ The Boulevard" would kill two birds with one stone. It would make my neighborhood more ideal, by putting the vacant property across the street to good use. It would also give 194 new families the same convenience that I have enjoyed while living in this area.

Sincerely,

A handwritten signature in black ink, appearing to be 'EM', with a long horizontal line extending to the right.

Ernesto Martin

Felipe Rios

Hayward City Council
777 B St.
Hayward, CA 94541

Hayward City Council Members:

I live in Prospect Hill—extremely close to the old Mervyn's Headquarters parking lot. I and many of my neighbors feel that embracing the @The Boulevard Project is one of the best ways to improve our neighborhood.

This site is a nuisance for us. It is ugly, unlit and unsafe. The proposed project would be just the opposite: attractive, well-lit and safe.

In short, the @The Boulevard Project is a good idea, and I urge you to approve it.

Sincerely,

A handwritten signature in black ink that reads "Felipe Rios". The signature is written in a cursive style with a large, prominent loop at the top of the letter 'P'.

Felipe Rios



ON TIME SIGNS

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

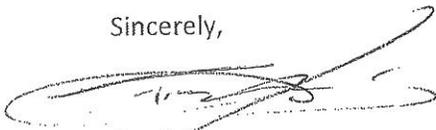
I would like weigh in with my opinion on the @The Boulevard project.

The common wisdom for city planners in the 1960s was to keep things separate. They would designate certain areas for commercial use and other areas for residential development. The result was urban sprawl where people had to drive everywhere because there was too much distance between where they lived and where they worked and shopped.

Fast-forward to today and the wisdom has changed drastically. You don't need to be an environmentalist to believe that reducing driving time is always a good thing. Living just 'next door' to where you shop is considered an enormous benefit. Not having to drive far to get to work or to shop is time-saving and money-saving. Living within walking distance of a BART Station for those who need to travel to other parts of the Bay Area is a huge bonus.

As a local business owner, I see the convenience of this project being one that would benefit me personally. Besides the extra business that would probably be brought in by an additional 200 families, I think that one of the townhomes would be something I would be interested in purchasing. It would bring me much closer to my own business and to Downtown Hayward. I am sure that there are others who, like me, would be interested in living within walking distance of everything that Downtown Hayward has to offer.

Sincerely,



Frank Lin
On Time Signs



Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

Downtown Hayward has come a long way in the last decade. I'd like to suggest that we take the next step and improve the surrounding locale. Specifically, I would like to see the Mervyn's site transformed into something that would be good for our entire community.

This is why I believe it's important to approve the @The Boulevard community. A small commercial space overlooking Foothill Boulevard combined with townhomes and a walking path along the stream sounds like a comprehensively thought out plan for revitalizing yet another section of Hayward.

Replacing the old Mervyn's Headquarters with an attractive mixed-use space seems like reason alone to approve this project. When you consider the benefit this project will be for the downtown businesses, it is clear that this is the right thing to do. I hope you will approve the project.

Sincerely,

A handwritten signature in black ink, appearing to read 'Fred Palmer'.

Fred Palmer
Union Bank

Giselle Martin

Mayor Sweeny and Hayward City Councilmembers
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeney and Council members,

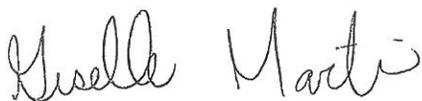
I live in Prospect Hill, on Hazel Avenue across the street from the old Mervyn's parking lot.

Most people don't get to choose new neighbors, but here I am asking you to grant me that rare privilege. Let's get rid of that old building and replace it with a better neighbor. I don't think anyone will be sad to see this neighbor go!

I have seen the drawings for the @ The Boulevard Project and I liked what I saw. I think that nearly *anything* would be an improvement, but this project looks to be about as good as anyone could hope for.

I will be very grateful if you expedite the approval of this project. It's something our neighborhood is sorely in need of.

Sincerely,

A handwritten signature in cursive script that reads "Giselle Martin".

Giselle Martin

Heidi White

Mayor Michael Sweeney and City Council Members
City of Hayward
777 B Street
Hayward, CA 94541

City Council and Mayor Sweeney,

I write you today to urge your support of the @The Boulevard Project. I live in the Prospect Hill neighborhood, so I am made keenly aware of the Mervyn's site's vacancy on a daily basis. It is an undeniable truth that this place, in its current state, is not good for our neighborhood.

But this does not have to be the case! Given the right attention, the Mervyn's site could be transformed into an asset for our neighborhood. This is what I think the @The Boulevard Project could be for us: an invaluable asset. It would replace a problem with something that would actually improve our neighborhood and be beneficial for everyone in our area.

What is not to like?

Sincerely,

A handwritten signature in black ink, appearing to read 'Heidi White', with a long horizontal stroke extending to the right.

Heidi White

Ignacio Trejo
KO1 Fitness

Mayor Mike Sweeney and Members of the Hayward City Council
777 B Street
Hayward, CA 94541

To Mayor Mike Sweeney and Councilmembers:

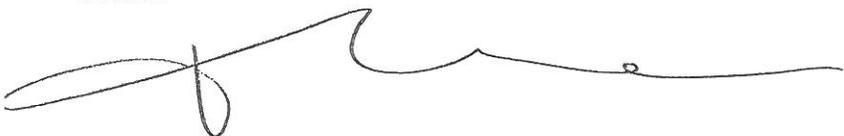
I am writing today to express my support of the @The Boulevard plans.
I think it is right for our community.

Hayward is ready for expansion. The downtown area is looking good,
but it needs more customers. Businesses, like mine, stand to benefit
immensely from the new families that will be coming to Hayward and
living nearby its commercial district.

Usher in a new era of Hayward prosperity and beauty by supporting
this project.

Sincerely,

Ignacio Trejo
Owner

A handwritten signature in black ink, appearing to read 'Ignacio Trejo', with a long horizontal flourish extending to the right.

Mayor Mike Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Mike Sweeney and Councilmembers,

I was recently made aware of the plan by Integral Communities to revitalize our downtown area through a mixed-use development project on the site of the vacant Mervyn's building situated on Foothill Boulevard, and I personally can think of no better way to promote patronage of our local businesses than by helping this design become a reality. Integral has been proactive in engaging with the community and refining the details of their project based on the input of residents, as well as local business owners, and has come up with a plan that is beneficial for all of Hayward.

As a business owner, I want to see my business succeed. But as a Hayward resident, I am also concerned with seeing the quality of life in my City improved. Expanding our community with more residential housing will provide more shoppers to frequent local businesses, while at the same time improving the safety and atmosphere of our downtown community.

If Integral has the resources to make their project, @The Boulevard, a reality, then I ask the city to lend this development its support. The current lot will not stand idle forever, and the opportunity to utilize the location is before us now. What reason do we have to wait?

Best Regards,

Jack Perez
Studio One Barber Shop

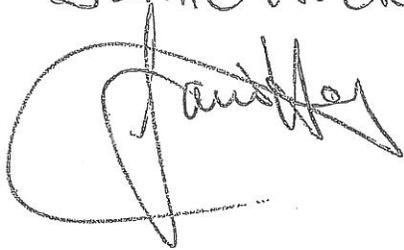
A handwritten signature in black ink, appearing to read 'Jack Perez', written in a cursive style.

Jamie Hocker

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear Mayor and City Council,
I live almost next door to the old Mervyn's site. I believe that the @ The Boulevard Project would be a vast improvement over the current situation we have over there.
I think that it is urgent that you approve the @ The Boulevard Project right away. The sooner construction starts, the sooner our neighborhood will be safer and more beautiful.

Sincerely,

Jamie Hocker


Jennifer Brown

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

I would like to urge you to support the @ The Boulevard Project.

It comes at a time when Hayward business could use an extra boost. This sort of project would be just that. Put quite simply, new families means new customers that would be going to shop in our downtown area.

Obviously, too, this project would beautify a part of Hayward that is somewhat behind the times. The rest of the downtown area is looking quite nice and this project would bring the area up to that new standard.

I look forward to seeing this project happen.

Sincerely,

A handwritten signature in cursive script that reads "Jennifer Brown". The signature is written in black ink and is positioned to the right of the word "Sincerely,".

Casablanca Bridal

CasablancaBayArea.com

Mayor and Council

City of Hayward

777 B Street

Hayward, California 94541

Honorable Mayor and City Council,

I am looking forward to new residents in the neighborhood that the @ the Boulevard project will bring. This project will help to increase the foot traffic Downtown, which is great for our businesses.

There are too many homeless individuals wandering in the Downtown area. I want us to be able to provide services they need, but they aren't going to find it here. I think this large vacant space with the dark creek area and abandoned garage draws them here. Customers feel uncomfortable with this, and don't like to shop here sometimes because of it.

We all want to keep Hayward moving in the right direction. By utilizing the space on Foothill to expand retail and commercial presence while simultaneously improving the safety of the surrounding community, maybe we can generate a small amount of public funds to provide services for these individuals. Then everyone wins. I hope you will approve this project. It will be good for Downtown.

Sincerely,



Jimmy Grogg

Joseph Degmman

Hayward City Council
777 B Street
Hayward, CA 94541

Dear City Council,

I live near the old Mervyn's site.

The @The Boulevard project for the Mervyn's Headquarters is one of the best ideas I've heard about for Hayward.

I think that it will be a significantly benefit to our community. Addressing the fate of the Mervyn's site is something that our community has put off for too long. Now that some developers have come up with a viable solution, we have no choice but to consider their proposal.

It's a good one. It makes no sense to keep the Mervyn's site as a 100% commercial space. The alternative is some sort of residential use, with some commerce along Foothill Boulevard. This is exactly what they plan to do.

Approving the @The Boulevard project is a sensible thing to do.

Sincerely,

A handwritten signature in black ink, appearing to be 'JD', with a long horizontal line extending to the right.

Jose Medina

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

City Council members and Mayor Sweeney,

I am writing you to urge you to support the @The Boulevard Project. I live in the Prospect Hill area and believe that this project would be an excellent addition to our neighborhood.

The old Mervyn's site has been vacant for far too many years. It's time we do something with it. I look forward to the day, soon, where I can walk the new path along the San Lorenzo Creek to the little park over by Hazel Avenue.

Sincerely,

A handwritten signature in cursive script, appearing to read "Jose Medina".

Jose Medina

Judith Ferrera Hutchinson

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Dear City Council and Mayor,

I am writing you as a concerned resident of Prospect Hill. I am worried about the extended vacancy of the old Mervyn's. It has been unused for far too long and its state of disrepair has become blatant.

The time to act is now. I urge you to support the @The Boulevard effort. It will be a worthwhile fix for the Mervyn's problem and a valuable addition to Hayward.

Sincerely,

A handwritten signature in black ink that reads "Judith Ferrera Hutchinson". The signature is written in a cursive, flowing style.

Judith Ferrera Hutchinson

Mayor Sweeney and City Council
777 B Street
Hayward, CA 94541

Dear Mayor Sweeney and Council,

I am writing you today with regards to the @The Boulevard project. Using the site of the old Mervyn's to bring in new residents to the area and boost business activity downtown is a win-win situation for the city and its residents.

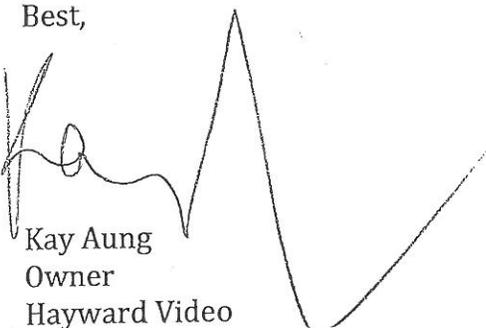
Instead of preserving the unused Mervyn's site, we should take this opportunity to instead encourage new businesses and families to locate in Hayward. We have a chance to grow our local community and revitalize our city by expanding residential housing and improving safety in and around the area.

Improving safety and cleaning up the area adjacent to the San Lorenzo Creek will go a long way in uplifting the current status of Hayward's downtown. Providing more retail space will further promote local businesses and encourage residents to spend their money in Hayward, as opposed to traveling out of town.

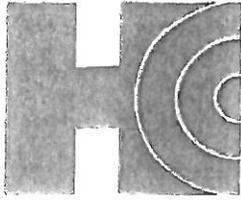
In fact, the proximity of the proposed development to the Hayward BART station only provides further reason to support the project. Locating residential properties next to public transportation, in addition to making businesses easily accessible to those who utilize BART's services is a great way to make use of the lot.

It is with the warmest regards that I ask you to give this proposal your support. The community thanks you.

Best,



Kay Aung
Owner
Hayward Video



HAYWARD
CHAMBER of
COMMERCE

October 10, 2013

Hayward Planning Commission
City of Hayward
777 B Street
Hayward, Calif, 94541

Commissioners,

On behalf of the Hayward Chamber of Commerce I would like to express full support for the proposed redevelopment of the former Mervyn's Headquarters at 22301 Foothill Blvd. by Integral Communities. Additional households from this new modern development will have a significant positive impact on downtown.

The chamber has worked closely with Integral Communities since early 2012. During that time Integral has hosted numerous informational meetings at their local office in downtown Hayward and I was personally responsible for the attendance of more than two dozen downtown business persons. We have yet to find a downtown business owner that does not support the project, including those along B Street as well as on Foothill Boulevard.

I have been told by downtown bank managers, restaurant owners, managers of retail stores, our historical society, our theatre complex manager, and countless other business owners and executives how this project will contribute to the rejuvenation of economic development downtown. At the Oct. 2 meeting of the Downtown Business Improvement Area Advisory Board, we heard from Marco Li Mandri, president of New City America, that "you rebuild downtown areas with residential." We heartily agree.

During 18 months of planning, Integral Communities has made several important changes to the site plan that are good for downtown Hayward and the city's economic development as a whole. The chamber's Government Relations Council and its Board of Directors are in support of the project, as well.

We urge the members of the Planning Commission to join us in supporting this positive development for the City of Hayward, its businesses, and residents.

Sincerely

Kim Huggett
President & CEO

Hayward City Council Members and Mayor Michael Sweeney
777 B Street
Hayward, CA 94541

Hayward City Council members and Mayor Sweeney,

The @The Boulevard Project only recently came to my attention. I was pleasantly surprised to hear that after all this time the old Mervyn's will once again be a vibrant part of our community, albeit an entirely different one.

When I found out that it was not yet approved, I decided to find out more about the project. It is all very positive. The only concern I have heard is about the added traffic that additional families would bring to the area. While vacant the Mervyn's hasn't caused any traffic in recent years, it certainly attracted plenty of traffic when it was still in operation. Leaving it vacant will keep that traffic out of Hayward. My point is that *any* use of this space will bring in a certain amount of traffic.

The difference, though, is that residential use of the space will bring in the *right* kind of traffic. Nearly 200 will move into an area that is within walking distance of our beautiful new downtown. This would be a dream come true for local business!

Sincerely,

A handwritten signature in cursive script, appearing to read "Laura Vargas".

Laura Vargas
Impressions Hair Design

Hayward City Council
777 B Street
Hayward, California
94541

To my Hayward City Council:

I want to urge you all to support the “@ the Boulevard” project on the corner of Foothill Boulevard. This project is a positive change for the abandoned Mervyn’s there.

That area feels unsafe at times. Activity in the area, with new residents, a park, and lighting, will make it safer. New residents in Hayward will bring some new life back into downtown

The trend for a vacant Downtown needs to stop. This project is one step towards a better downtown. It is one step towards a better Hayward. Help downtown Hayward make that first step.

Sincerely,

A handwritten signature in black ink, appearing to read "Lauren Mendoza". The signature is fluid and cursive, with a long horizontal flourish extending to the right.

Lauren Mendoza

Lourdes Vasquez

City of Hayward
777 B Street
Hayward, California 94541

Dear Mayor and City Council:

I live in Prospect Hill, near the old Mervyn's Headquarters, and I think it's time that something is done to improve the space.

This is why I'm excited about the @The Boulevard Project. I know that such a vast space could be used for just about anything, but I think that a mostly residential project, with a little shopping, would be a nice addition to our sleepy neighborhood. The drawings I've seen have me convinced that this is the best use of this space, but it is also undoubtedly better than leaving this space vacant for years to come.

Please grant the people who have planned the @The Boulevard Project permission to break ground soon. It is important for my neighborhood to have the Mervyn's Headquarters dealt with once and for all.

Sincerely,

Lourdes Vasquez

A handwritten signature in black ink, appearing to read "Lourdes Vasquez". The signature is fluid and cursive, with a large loop at the end of the last name.

Mayor Mike Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Mike Sweeney and Councilmembers,

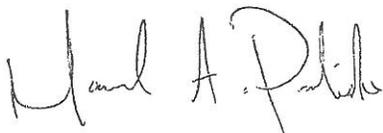
I am writing to encourage your support for @the Boulevard. Using the vacant lot left by Mervyn's to bring more residents and businesses to Hayward will only benefit our city and community.

I believe it is in the best interest of the city to make use of the site. Bringing more residents and businesses provides the city with a larger tax base while also offering more local options for current and future Hayward residents. I feel it is important to promote and support our local businesses, and this project does just that.

There is also no question that our downtown needs cleaning up. The current state of the area surrounding the creek is depressing. This development will provide much needed lighting improvements in addition to the expansion of businesses that will improve the safety and accessibility of our downtown.

I hope that you will join me in supporting this project in the best interest of Hayward residents and businesses.

Sincerely,

A handwritten signature in black ink that reads "Manuel A. Pulido". The signature is written in a cursive style with a large, stylized initial "M".

Manuel Pulido
Owner/Barber

Mayor Michael Sweeney and Members of the City Council
City of Hayward
777 B Street
Hayward, CA 94541

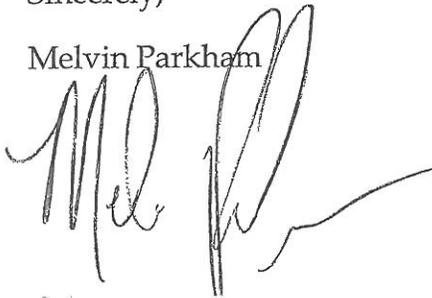
City Council members and Mayor Sweeney,

I live in Prospect Hill and I would like to voice my support for the @The Boulevard Project. This is something our neighborhood could really benefit from.

The Mervyn's buildings have been left to fall apart for far too many years. The project will certainly fix that, but it will also bring something new and beneficial to our town. I think that the time is right to bring some new families into Hayward, and the time is certainly right to do something with the old Mervyn's buildings.

Sincerely,

Melvin Parkham

A handwritten signature in black ink, appearing to read 'Melvin Parkham', written over the printed name.

Michael Francisco

Mayor Michael Sweeney and City Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Michael Sweeney and City Council:

I am writing to show my strong support for the project to change the old Mervyn's building on Foothill Boulevard. I have been in Hayward for a couple years, and that area has been empty since the day I moved to Hayward.

This empty building isn't doing anything for our community. Doing something with the area is a positive change. In this particular area, this project adds to the Downtown area and would be good for the businesses in Downtown. It's also great having the homes so close to BART.

Please make the decision to help out this part of community.

Sincerely,

A handwritten signature in black ink that reads "Michael Francisco". The signature is written in a cursive, flowing style.

Michael Francisco

Monica Lucero

October 21, 2013

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Councilmembers and Mayor Sweeney:

As a 40-year Hayward resident, I am writing to relay my strong support for the @ the Boulevard project on Foothill Boulevard. I grew up just down the road in Castro Valley and remember going to Capwell's and Compton's as a child. After I moved to Hayward in 1971, it had become Mervyn's headquarters.

This large vacant outdated building isn't doing our community any good. I think the mixed-use project before you is perfect for the site. Hayward needs additional residential developments. In this particular location, they complement the Downtown nicely and would be good for the businesses just across the street. It's also great having the homes so close to BART.

I have seen Hayward change considerably since my childhood. I am certain that this upcoming change is one for the best.

Sincerely,



Monica Lucero

Monika Sobotka

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Mayor Sweeney and City Council,

My family lives in Prospect Hill, on Prospect Court, fairly close to the old Mervyn's building.

I believe that the time is right to reuse the old Mervyn's site for something that contributes to our community, rather than detracting from it. I am convinced that building homes is the right thing to do for our neighborhood. Hayward doesn't really need any more large commercial spaces, and leaving the space unused for any longer is not a very good option.

Please support the @The Boulevard Project; it's what is right for Hayward.

Sincerely,

A handwritten signature in black ink, appearing to read 'Monika Sobotka', with a stylized flourish at the end.

Monika Sobotka

Paolo Viale

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Mayor Sweeney and City Council,

I live near the old Mervyn's Headquarters. I am writing you to let you know that I think the @ The Boulevard Project is one that is worthy of your approval.

I have watched the Mervyn's Headquarters fall apart year after year, and think that it is time that it be put to good use.

This is why the @ The Boulevard Project must be approved. It will reuse the space and provide Hayward with some much-needed revenue.

Sincerely,

A handwritten signature in black ink, appearing to read 'P. Viale', written over the word 'Sincerely,'.

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor and Council,

I am sure that you are by now familiar with the development project planned for the property where the former Mervyn's headquarters building currently stands. I would like to ask that the city embrace this plan to help bring new life, new business, and new opportunity to the people of Hayward. It is the right thing to do.

The old Mervyn's building does little in the way of providing anything of value to the city or its residents. Why not use that space to expand housing for 200 families who are anxious to call Hayward home? Or offer businesses the chance to locate in a newly enlivened downtown?

Our city has so much more to offer than relics of businesses that have fallen by the wayside. The merit of allowing this structure to remain in the presence of a viable and superior alternative is questionable, and even the most nostalgic of us has no doubt accepted that the time to move on has come and gone. We have the opportunity to improve our city. Let's seize it.

With the best interest of Hayward residents and businesses in mind, I encourage you to support @The Boulevard.

Most Sincerely,

A handwritten signature in cursive script that reads "Penni Sanders". The signature is written in black ink and is positioned below the "Most Sincerely," text.

Penni Sanders

Nu Revelations Christian Bookstore

Mayor Michael Sweeney and Members of the Council

City of Hayward

777 B Street

Hayward, California 94541

Dear Mayor Sweeney and Councilmembers,

I am writing to urge your support of the mixed-use project on Foothill Boulevard at the old Mervyn's site. A quick drive through Downtown Hayward shows more vacancies than is ideal for a thriving Downtown. It's not just this site, but many small main-and-pop businesses that haven't been able to make ends meet. It is so important that we support these small businesses with all of the tools at our disposal.

Your approval of this project will give Hayward just the kind of boost we could use right now. The multi-year vacancy of the old Mervyn's Headquarters is a drag on Downtown. This project will beautify and enhance Foothill Boulevard, and be a more appropriate gateway to Downtown.

Bringing 200 new professionals and families to the area will enliven the neighborhood, and provide a new source of shoppers and diners for Downtown businesses. Needless to say, the fees the City will receive from such a development are not insignificant! Those millions of dollars will go far in these times to support the City's needs as we recover from the recent downturn.

I look forward to your approval of @ The Boulevard, and new life for Hayward's Downtown.

Sincerely,


Richard Weinstein, *max AGOR*

*RLW LLC (adjacent property
on Foothill)*

Hayward City Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Members of City Council,

I fully support the @the Boulevard development planned by Integral Communities at the site of the former Mervyn's headquarters. I initially had questions regarding this project, but after reading more about it, I fully support it. The developers have done a nice job engaging with the community.

As one of the oldest businesses operating downtown, I have seen many shops come and go. Sadly, the rate at which good comes into Hayward is not as favorable as it once was, and you need look no further than the many "For Sale"/"For Lease" signs littering shop windows throughout the city for proof. Activity in downtown Hayward just isn't what it used to be.

We have seen the negative impact empty parcels can have on our businesses and homes. Shuttered stores lead to diminished foot traffic, which affects all businesses in the area. Now, we have a chance to revitalize and improve Hayward by growing our community with families and businesses that will contribute to our city for generations to come.

I fully endorse the development @the Boulevard, and I hope that you will as well.

Best Regards,

Rod Vargas
Jewelry Designer / Owner
Avalon Jewelers

A handwritten signature in black ink, appearing to read "Rod Vargas". The signature is fluid and cursive, with a large, prominent loop at the beginning.

Mayor Sweeney and City Council Members
City of Hayward
777 B Street
Hayward, California 94541

Dear Mayor Sweeney and Council,

I am writing you today to ask for your help. As a long-time Hayward resident, Tennyson high graduate and current Chabot College student, I am concerned about the present condition of our City. For many, a City's appeal is measured by the accessibility and vibrancy of its downtown. Sadly, Hayward's downtown is coming up short, and a big reason for that is the abandoned Mervyn's headquarters on Foothill, which has contributed nothing of value to our community for over 5 years.

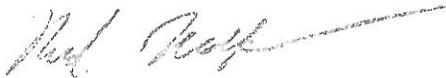
The @The Boulevard development project finally provides our City with an opportunity to do something about this issue, and I am writing to you today to voice my support for the project.

The current state of the property is depressing and unproductive. By allowing this project to progress, the City can display its willingness to invest in the future of its residents by cleaning up and improving the condition of our downtown cityscape, making it a more appealing place to live and do business.

Increasing residential housing, expanding retail space and improving the landscape of our downtown area will go a long way in beautifying and uplifting our City. Improved lighting and accessibility will have a positive impact on the safety of Hayward residents, providing a more welcoming environment in which new – and old – residents alike can frequent local businesses, eat at local restaurants and enjoy the local scenery.

I encourage you to join myself, and dozens of other Hayward residents, in supporting this development.

Sincerely,



Roel Peralta

Roel Peralta

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Dear Mayor Sweeney and Councilmembers,

I am writing to express my support for *Integral Communities* and their plans for the long-vacant Mervyn's site.

Having met this team, I know them to be committed to Hayward's best interests. They have developed their project in concert with our community's input.

This site is more than just a blemish on the way to Downtown Hayward. As a business owner, I see its development as a potential source of more commerce in our city. An additional 200 families could only serve to benefit local business. This would be a great boost for us as we recover from the economic downturn.

Sincerely,

Russell Foote

A handwritten signature in cursive script, appearing to read "Russell Foote". The signature is written in black ink and is positioned below the typed name.

Russell Foote Photography

Sanny Quach

Mike Sweeney and Hayward City Council
777 B Street
Hayward, CA 94541

Mayor Sweeney and City Council Members,

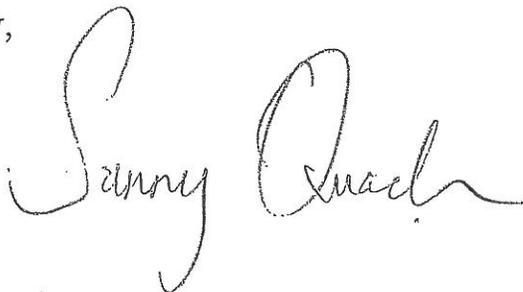
I am writing to signal my interest in seeing the @ The Boulevard Project quickly approved.

This is something that would benefit Hayward immensely.

The project builds on the theme of improving the downtown area's appearance. The space may not be in the main part of downtown Hayward, but most people who are on their way there inevitably drive by it. The @ The Boulevard project would be much more aesthetically pleasing for those on their way to the downtown part of town, than the existing Mervyn's buildings.

Keep up the good work in improving the way Hayward looks!

Sincerely,

A handwritten signature in cursive script that reads "Sanny Quach". The signature is written in black ink and is positioned below the "Sincerely," text.

Mayor Michael Sweeney and Members of the Council
City of Hayward
777 B Street
Hayward, CA 94541

Councilmembers and Mayor Sweeney,

I write you today to eagerly affirm my support for your approval of the plans that Integral Communities has put forth.

Their plan has been tailored to our community's needs. The Mervyn's site has been abandoned for far too long. Anyone who can successfully make use of this space ought to be encouraged because the community will benefit. It will be a great boost for the entire Downtown.

This project will provide businesses with more customers, customers with more businesses, and all of downtown with a more safe and inviting environment in which to shop and operate.

It is without reservation that I endorse the @The Boulevard plans. I urge you to do the same.

Sincerely,

A handwritten signature in black ink, appearing to read "Syed Karim". The signature is written in a cursive style with a horizontal line underneath the name.

Syed Karim
Quick Printing Center

Damon Golubics

From: David Rizk
Sent: Tuesday, January 21, 2014 4:51 PM
To: Pat Siefers; Damon Golubics
Cc: Maureen Conneely
Subject: FW: Boulevard Project petition

FYI

-----Original Message-----

From: Fran David
Sent: Tuesday, January 21, 2014 4:33 PM
To: David Rizk
Subject: FW: Boulevard Project petition

FYI

Fran David
ICMA-CM
City Manager
City of Hayward
510.583.4300

-----Original Message-----

From: Per Bothner
Sent: Tuesday, January 21, 2014 4:30 PM
To: List-Mayor-Council
Subject: Boulevard Project petition

I signed the petition in favor of the Boulevard development at the old Mervyn's cite. I hereby recind my support.

The petition-collector gave the impression than would be substantially more retail than what I later saw in plans. The tiny amount of retail is totally unacceptable, and I favor the General Plan: The ground floor must be primarily commercial or retail; re-using the existing building for offices or stores is also a good use.

--

--Per Bothner

Hayward CA 94541



DATE: January 30, 2014
TO: Planning Commission
FROM: Pat Siefers, Planning Manager
SUBJECT: Capitol Corridor Train Service

This item is an informational item for the Planning Commission.

The Capitol trains and their connecting bus service provide transportation to/from Sacramento, the Northern Sacramento Valley, the Gold Country, the East Bay, San Jose and Santa Cruz, Monterey, and Salinas. Current East Bay stops for the Capitol train are: Oakland, the Oakland Coliseum and Hayward. The Capitol train frequencies will increase to half-hourly service in the peak hour and eventually CCJPA hopes to operate 16 round daily trips between Oakland and San Jose.

In November, the City became aware that a *Draft Vision Plan* for the Capitol Corridor Joint Powers Agency (CCJPA) proposes to skip the Hayward Amtrak stop and run express service between Oakland and San Jose. This appears to be occurring out of an abundance of desire to serve the new 49ers Stadium via the Santa Clara train stop. The train is proposed to switch tracks to operate on the Mulford Subdivision, which is the track that the Coast Starlight train currently uses. Currently the Capitol trains operate on the Niles Subdivision south of Oakland. At their November 20, 2013 meeting, the CCJPA Board considered and approved in concept retaining professional services to determine the extent and cost of future rail track improvements needed for passenger service to run as well as to prepare the Environmental Impact Statement/Environmental Impact Report for the capital improvements needed. It is noted that numerous Federal permits will be required for making substantial improvements or increasing the capacity of the Mulford Subdivision. Attachment I shows the three rail alignments, BART stations and major highway and arterial routes as well as sea water rise expectations that would logically figure into a study of Capitol service track options.

NOVEMBER 20, 2013 CAPITOL CORRIDOR JOINT POWERS AGENCY MEETING

On November 20, 2013, staff carried a letter from the Mayor of Hayward (Attachment II) to the CCJPA Board members and staff. Hayward staff was joined by Union City staff and staff from State Senator Corbett's office and Assemblyman Bill Quirk's office in opposing any movement of the Capitol service away from Hayward. We also proposed that the future study and environmental document include the option of the existing line as currently operated (Niles Subdivision) and the Oakland Subdivision that runs parallel to the BART tracks. Further, we testified that any decisions about the Capitol Corridor should await the Transit Study and Goods Movement Study being

performed this year.

Mayor Sweeney notes in his letter (Attachment II) that using the Oakland Subdivision track would permit each BART Station along the corridor (San Leandro, Bayfair, Hayward, South Hayward and Union City) to receive a skip-stop service using their existing BART stations. To do this, an across-the-platform transfer facility would be built at the rear of each BART station. This would be very similar to the Millbrae BART Station across the platform transfer to the Caltrain service. At the CCJPA meeting, staff suggested that the Oakland Subdivision Alternative, along with the current Niles Subdivision, be studied, as well as the Mulford line. This is in the spirit of the National Environmental Policy Act (NEPA), which requires that all feasible alternatives be evaluated and compared.

The *Draft Vision Plan* (Attachment III) was handed out to the CCJPA Board at their prior meeting and was not publicly available until just before the November 20 meeting. Following the approval of the item considering issuing/awarding the professional services contract, the Executive Director announced a meeting with the Union Pacific Railroad the following day to discuss using the Mulford line instead of the Niles Subdivision track for the Capitol trains. The Mulford line is out in the wetlands; will be underwater under rising sea levels forecast by the US Geological Survey; and will cost increasing amounts to maintain and operate on as structures will be required to carry the track in the long run. The Mulford line is also the line that carries the most freight south from the Port of Oakland. Routing the Capitol trains to the Mulford line places any future Hayward station out literally “in the weeds” and far from BART, AC Transit and our developing Priority Development Areas (PDAs).

CURRENT HAYWARD CAPITOL TRAIN SERVICE AND RIDERSHIP

Hayward accounts for approximately 100 passengers boarding or de-boarding the Capitol each day and this is up 20 percent over the 2012 fiscal year. Ridership at the Hayward Station is ranked between that of the Fremont Station (90 passengers per day) and the Oakland Coliseum Stop (128 passengers per day). Attachment IV shows ridership by stop. The City is planning to perform a shuttle study to determine what additional shuttle services are needed for the Amtrak Station and major employers in Hayward.

Hayward currently receives seven weekday train trips in each direction and similar service on weekends. Times for the trains are convenient for business trips, commute trips as well as pleasure trips. A connection is made in Martinez between Capitol trains and the San Joaquin trains to Los Angeles via the Central Valley. Hayward has been a Capitol train service stop since roughly 1995. The Hayward Amtrak Station was strategically located to serve the densely populated transit oriented community (the Cannery) and was jointly funded by the City of Hayward and the State of California. The Hayward Amtrak Station provides the only mid-Alameda County train stop.

FUTURE TRAIN SERVICE TO HAYWARD

Moving the Capitol train service to the Mulford track would result in Hayward receiving no Capitol train service. In addition, should a station be built, it would be far away from both our downtown, our BART Stations, AC Transit service, and our Priority Development Areas (PDAs). It is noted that over 1,000 new median income housing units have been built in the Cannery area of the PDA. Also, it appears that future funding for transportation improvements will increasingly focus on

projects within PDAs. The Mulford alignment is nowhere near any PDA or any connecting transportation provided by BART or AC Transit.

Going forward, train service will expand. Currently there is an effort to create the “Northern California Blended Rail System” which would be a combination of commuter trains Amtrak service and incrementally higher speed state and regionally funded trains serving key points in Northern California. This “Northern California Blended System” would use funding from federal funds, state high speed rail bonds and grants as well as potentially Cap and Trade funds. Eventually one of the tracks will become the passenger track because the freight railroads have little tolerance for capacity constraints on their main freight lines. This is a train we do not want Hayward to miss, as the system (and track) invested in will likely become the permanent passenger line. The Altamont Commuter Express (ACE) is leading the efforts to create a “blended system” of train service in the Central Valley and to/from the Silicon Valley. ACE provides service to/from Santa Clara and San Jose, Livermore, Tracy and Stockton. The Caltrain system on the San Francisco Peninsula will be electrifying their system and strategically adding passing tracks to accommodate the higher speed intercity trains planned to operate between Los Angeles and San Francisco.

It is unclear who is developing the overall blended service plan or how one has input. While the state continues to prepare the State Rail Plan, both Caltrans Division of Rail and Amtrak West seem to be taking less of a lead position in planning future rail service. Typically in the planning process, local agencies, regional transportation authorities, transit operators and the public are consulted prior to preparing plans. This has not been the case with the Vision Planning process initiated by the CCJPA. Consequently, staff has contacted the Northern California Director of the High Speed Rail Authority to advise him of the City’s objection to losing our Hayward train service. Staff will also be meeting with the Executive Director and Planning Manager for the Capitol service in early February. We continue to work in partnership with the City of Union City and others.

Recently we met with a major Hayward employer who advised us that approximately 40 percent of his employees are living in the Central Valley. It may be constructive for the City to engage with the ACE system to study future rail services that may be useful for commuters destined for Hayward.

Attachment I shows all three Union Pacific rail lines and expected sea water rise. Each is described below.

The Niles Subdivision—This rail line is the current track used by the Capitol. Over the past three decades, about \$20 Million has been expended in public funds to make track improvements, add second track/sidings, install grade crossing protection and make other capital improvements to this rail line. This cost does not include the cost of new structures to grade separate the railroad from city streets. This is the current track servicing the Capitol train and the Hayward Amtrak Station. From time to time, usually in conjunction with increasing passenger rail service, the capacity of the tracks is determined and track rights and passenger train “time slots” are negotiated with the private railroad. Typically this involves a physical inspection, a ridership study and a traffic and capacity simulation to assess what additional capital facilities are needed by the UPRR before more passenger service may be operated.

The Oakland Subdivision—This track runs adjacent to the BART tracks. With across the platform transfer facilities created at the rear elevation of each station at San Leandro, Bayfair, Hayward and South Hayward, a skip stop service could be operated to serve each community and make direct connections with both BART and AC Transit. Likely, because the BART overhead structures already exist, the cost of grade separations under this alternative would be lower than the other two alternatives.

The Mulford/Coast Subdivision—This track is located in the marshlands adjacent to the San Francisco Bay and currently carries both freight trains and the Amtrak Coast Starlight trains. The track and marshlands are sinking as can be seen from the Coast Starlight train—the abandoned community of Drawbridge continues to sink into the bay marshlands.

PRIOR STUDIES—THE DUMBARTON RAIL CORRIDOR

Several years ago, a study was commissioned for the Dumbarton Rail Bridge which is owned in fee title by the Caltrain Joint Powers Agency. The study reviewed options for rebuilding the rail bridge and operating it as a “Fourth Crossing” of the Bay. A robust service plan was envisioned with Caltrain service running to the East Bay, ACE service running to the San Francisco Peninsula in addition to the Capitol train service and the national Coast Starlight train service running north-south between the Central Coast, San Jose and Oakland. The study included the extension of the Dumbarton Rail service north to Hayward and moving the Capitol service to the Oakland Subdivision (which could share the Hayward BART Station with an across-the-platform transfer). The Dumbarton Rail project is currently unfunded.

NEXT STEPS

Staff will continue to reach out to the CCJPA staff, ACTC staff and will keep the Planning Commission, City Manager and City Council informed of the status of Capitol train service and our Hayward stop. In addition, we will continue to work with our state elected officials.

Prepared by:

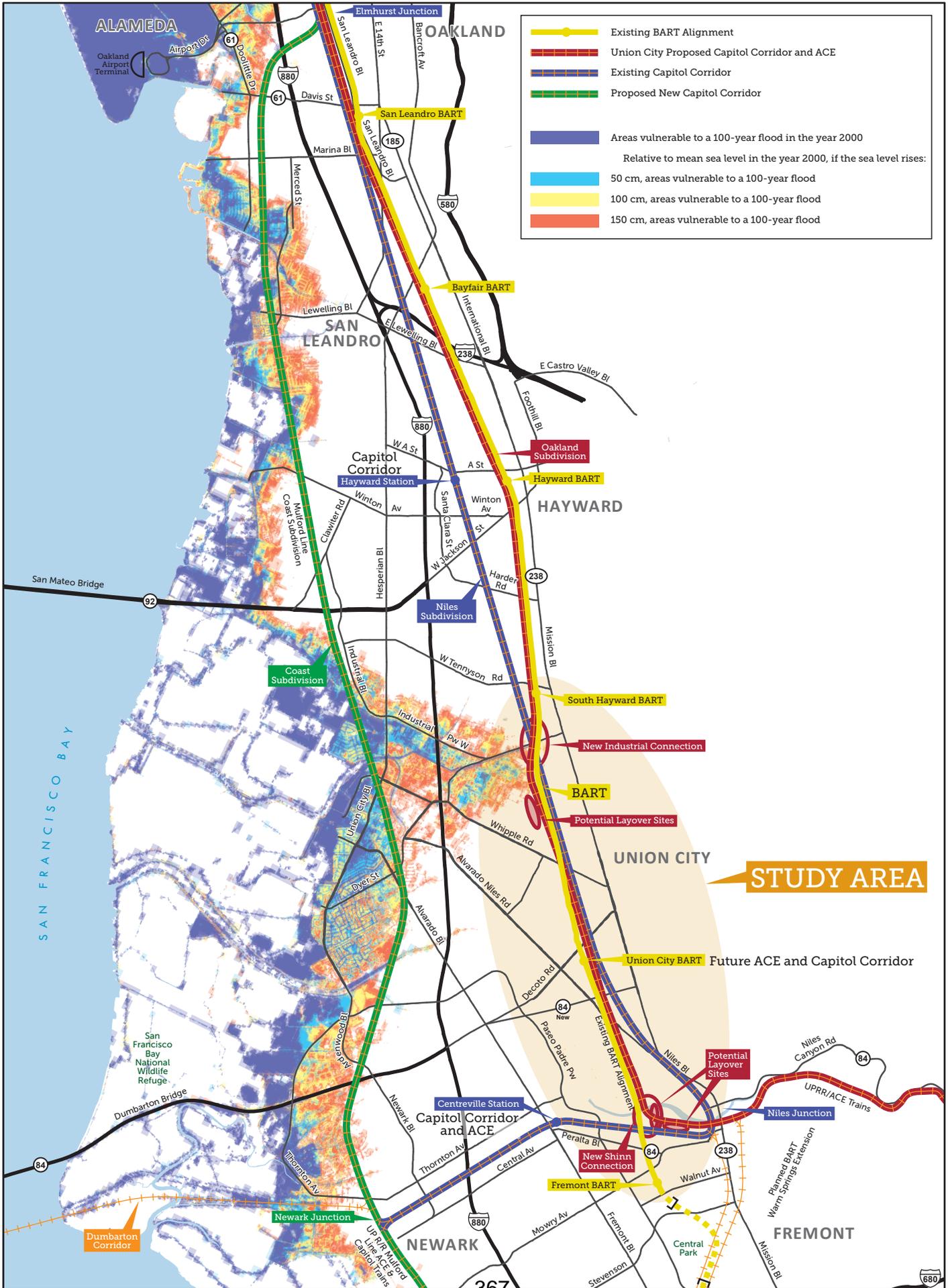
Pat Siefers
Planning Manager

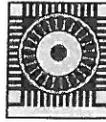
Attachments:

Attachment I	Map of Alignments under Sea Water Rise Forecasts
Attachment II	November 19, 2013 Letter to James Spering, Chairman of the <u>Capitol</u> Corridor Joint Powers Authority from Mayor Sweeney
Attachment III	<u>Capitol</u> Draft Vision Plan
Attachment IV	Current <u>Capitol</u> ridership by Stop
Attachment V	Members of Policy Boards



Potential Inundation due to Rising Sea Levels





CITY OF
HAYWARD
 HEART OF THE BAY

November 19, 2013

James Spring, Chairman
 Capitol Corridor Joint Powers Authority
 300 Lakeside Drive, 14th Floor East
 Oakland, CA 94612

Dear Chairman Spring:

Since roughly 1995, Hayward has been an intermediate stop for the Capitol Corridor service between Oakland and San Jose. The Capitol Corridor trains and their connecting bus service (Amtrak Thruway) connect the Gold Country, Sacramento, Northern Sacramento Valley, the East Bay, San Francisco, San Jose, and Santa Cruz/Monterey/Salinas. (See Attachment I) Currently there are seven weekday trips westbound and seven weekday trips eastbound that stop in Hayward. Similar service is provided on weekends and holidays. Hayward currently receives a combination of train and Amtrak thruway bus service. Our existing station was developed cooperatively between Amtrak, Caltrans, and the City.

The line currently stops in Hayward and runs through the adjacent cities of San Leandro and Union City on the Niles Subdivision of the Union Pacific Railroad (UPRR). The UPRR requires and has required that numerous track improvements, grade crossing improvements, sidings, bridge improvements, and signal improvements be made in return for "trackage rights" to run a passenger train on their freight line. Over the past nineteen years, public funds have been spent on the Niles Subdivision for all of the above mentioned improvements as well as local funds for station facilities, parking lots, connecting bus service, and other necessities of providing a good service to the public.

We learned only three weeks ago, quite by accident, that our stop on the Capitol Corridor train is being considered for elimination (See Attachment II), and that a new "Vision Plan" (See Attachment III) calling for an express, non-stop service to Santa Clara Great America and Santa Clara University and San Jose with no stop anywhere in the middle of the East Bay. The purpose of this change is stated to be a reduction in travel times. However, we believe that a better alternative would be a skip-stop service to each of the three cities (Union City, Hayward, and San Leandro) using the Oakland Subdivision line that runs immediately next to BART and is easily served with new across-the-platform transfers at each BART station.

A skip-stop pattern works like this: Trip one would stop adjacent to the Union City BART, and then express to Santa Clara/San Jose. Trip Two would express to Hayward, stop and then express to Santa Clara/San Jose. Trip Three would express to San Leandro and then express to Santa Clara/San Jose, and so on. This is a far more productive and serviceable change than operating the Capitol trains on the Coast Subdivision, which is out in the marshes; is a track that appears to be sinking; will likely be under water in the future due to sea water rise; and will not



serve the population and employment centers of the East Bay.

The current Amtrak Station is located away from the BART station a short distance, has very few amenities and was partially financed by the City of Hayward. File records indicate that the total City contribution to the station was in excess of \$1 Million. Daily train boarding's are around fifty passengers per day, which is respectable among other stations on the Capitol Corridor route. In fact, in a discussion of ridership on the Capitol train it was noted that ridership in 2013 over 2012 has decreased at several stations; Hayward was not among those stations. (See Attachment IV) The City maintains the station and the adjacent area including the parking lot.

September, 2013 CCJPA Action with which City Disagrees: The CCJPA distributed a report at its September meeting entitled the "Vision Plan: Phase 1". The report proposes to move the Capitol off of the Niles Subdivision to the Coast line alignment (in marshlands, far from the center of Hayward, San Leandro, and Union City). While the report says that the CCJPA would work with the City to move the station, a station out in marshlands is in conflict with regional and City policies on transit-oriented development. Phases 2 (environmental) and 3 (preliminary engineering) of the study will be considered at the CCJPA at their November 20, 2013 meeting and consist of Environmental documentation and Preliminary Engineering to move the train service to the marshland Coast Starlight alignment. It is noted that the City of Hayward was not consulted on this change nor did we learn of it from the CCJPA or Amtrak. It is also noted that the decisions regarding the "Vision Plan" and service changes are being made by an "Ad Hoc subcommittee" comprised of no Alameda County representatives. All of the cities in the central and southern East Bay were similarly surprised to learn of the planned changes.

Since the Capitol is funded in part federally; uses a federal contractor, Amtrak; and the repair of tracks through the marshland as proposed would require federal environmental permits; the NEPA process is required. Scoping of a project, (i.e., its "purpose and need") is required to be done at the earliest time it is considered and prior to an agency committing to an action and prior to any federal action. A federal action can be surmised to have occurred in the funding of this work. Therefore, the NEPA process should already be underway with all associated notification and public participation; yet the process has not yet been initiated. In addition, several CEQA requirements apply to the contemplated project

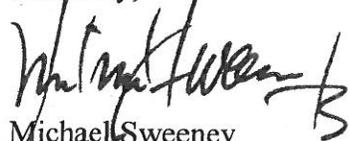
One Bay Area Plan and Priority Development Areas: Hayward has worked diligently to meet the regional objectives set by the MTC and ABAG to plan and construct transit-oriented developments within one mile of the Hayward Amtrak Station. This has been consistent with City goals supporting location of housing near transit stations to reduce vehicle trips and decrease greenhouse gas emissions; and to develop the PDAs that have been approved by Council, and which will be regionally connected for receiving transportation funding. The City of Hayward has succeeded in permitting and building nearly 1,000 affordable and market-rate housing units within a mile of the Hayward Amtrak station within the last three years. Similarly, Union City and San Leandro have developed their Priority Development Areas (PDAs) near mainline railroad tracks. Relocation of the Capitol Corridor service would inhibit development of rational and green commuting patterns and potential economic development for Hayward, and be counter to regional and local policies.

Past Hayward Plans for Train Service: Several years ago, the City of Hayward contracted for a study of the potential ridership of the Capitol Corridor Trains if the service were to be moved to the tracks that run adjacent to the BART tracks, with a multimodal station in downtown Hayward adjacent to the BART station (existing now). The study was done in conjunction with plans to extend the Dumbarton Rail service north to Hayward. The study concluded that ridership would increase and that capital improvements along the railroad track route would be required. One of these improvements, the Shinn Connection in Fremont, was opposed by the Fremont City Council since it would move trains too close to newly constructed residential areas. As the Dumbarton Rail Service is not currently a funded project, the future plans to move the Capitol Corridor trains to the tracks adjacent to our BART Station are uncertain, making the Hayward Amtrak Station and continued stops of the Capitol Corridor train even more critical.

Conclusion: It appears that plans have been made without adequate public or agency input and without a full airing of possible alternatives. Staff members from our neighboring jurisdictions agree. Staff fears that moving the track for the Capitol trains will also mean that future high speed service, ACE extensions, and Caltrain extensions would also bypass the key central East Bay communities. This action appears to be driven by an abundance of desire to service the Great America Station (new 49ers Stadium) as well as a second Santa Clara University station and the Diridon San Jose station with express service between Oakland and San Jose. While this will be great for those two origin-destination pairs, it “misses the boat” in terms of intermediate travel between Oakland and San Jose and is not truly a growth option that equitably serves our city or its new transit-oriented developments.

It would appear that a logical alternative would be to keep the current Hayward station stop and continue to operate on the Niles Subdivision. Another feasible alternative would be a skip-stop pattern on the existing track or on the track that runs adjacent to BART (the “Oakland Subdivision” track). This would allow full market penetration of possible new riders and would also serve the new Priority Development Areas surrounding the BART/Capitol Corridor current alignment. This skip-stop train service accompanied by convenient and guaranteed bus connections connecting non-served stations on each trip with Oakland and San Jose would result in much larger ridership in the future than a service that stops nowhere in the mid-East Bay. A skip-stop service would serve the entire East Bay rather than just two cities at either end.

Sincerely,



Michael Sweeney
 Mayor

- Attachment I: Map of Current and Proposed Amtrak Service
- Attachment II: CCJPA Staff Report for November 20, 2013 Meeting
- Attachment III: Phase 1 “Vision Plan” by Nelson-Nygaard for CCJPA
- Attachment IV: Current Capitol Ridership by Stop
- Attachment V: Members of Policy Boards

November 19, 2014
Capitol Corridor – Amtrak Services
Page 4 of 4

Cc: Senator Ellen Corbett
Assembly Member Bob Wieckowski
Congressman Eric Swalwell
James Spering, Chairman, Capitol Corridor Joint Powers Agency
Jay Commer, General Manager, Amtrak West Services
Jonathan Hutchinson, Senior Director Corridor Development Policy & Development West
Bill Bronte, Director, Caltrans Division of Rail
Andres Boutros, Executive Director, California Transportation Commission
Steve Heminger, Executive Director, Metropolitan Transportation Commission
Art Dao, Executive Director, Alameda County Transportation Commission

CAPITOL CORRIDOR VISION

It may take decades for the vision described in this document to take shape, but some day, the Capitol Corridor will serve as the spine of a rapid transit system spanning Northern California. If the region is to remain a significant economic engine and healthy place to live, major investments in the Capitol Corridor will not only be justified – eventually, they will become necessary.

From our early 21st-century perspective, the physical and financial obstacles may appear insurmountable; the need may appear unclear; the vision may seem like a dream. But now is the time to start planning and outlining the progressive steps. The Bay Area and Sacramento Valley are growing together, and only the Capitol Corridor will be able to offer fast, clean, reliable and safe travel across the emerging mega-region. Imagine Sacramento to Oakland in an hour; imagine Oakland to San Jose in 30 minutes.

It doesn't just stop with Capitol Corridor either. In 2012, California embarked on an effort of blending its passenger train services together with a progression toward high-speed passenger rail service. Capitol Corridor itself may have to yield some of its identity to become blended with passenger rail operations while at the same time the Board who oversees the Capitol Corridor and its customers will remain focused on ways to better utilize the Capitol Corridor route today and into the future.

How do we get there? It will be combination of considering policy and keeping to plans built on the vision included in this document. The Capitol Corridor's short and medium term plans outline a process of maximizing prior investments and are certainly a roadmap for the next two decades that is compatible with the State's long-term passenger rail objectives. Beyond that, the political and financial climate is uncertain. However, the Long-Term Capital and Service Plan at the core of this document identifies the major steps required to transform the Capitol Corridor into a truly 21st-century railway, including:

- Incremental improvements to speed
- Major new infrastructure to further improve speed, reliability, access and connectivity and protect against sea-level rise
- New service designed to improve speed and cost-effectiveness

Unlike the Capitol Corridor's last Vision Plan, last updated in 2005, this document looks past the next two decades, 50 years or more into the future. It outlines challenges and policy decisions related to that vision. It respects the needs along the route while fitting into the larger California passenger rail network. It seeks to offer a true *vision* – not just a list of improvements, but a long-term, overarching goal to work toward. But sometimes executing a vision can depend on actions taken early. With this perspective, this document organizes a vision for tomorrow that begins today.

THE CAPITOL CORRIDOR, THEN AND NOW

There is no debate about the success of the Capitol Corridor Intercity Passenger Rail (IPR) service and the role it plays today and must play tomorrow as Northern California grows and the environment changes around us. Capitol Corridor service, along with the rest of the State's IPR services, has grown from a ballot measure for bond funding in the early 1990's to what was intended when those votes were cast – a robust IPR system for California that sets the standard for the nation. Now Capitol Corridor service is part of the DNA of Northern California and it will be for years to come.

In 2009, the San Francisco Bay Area's Metropolitan Transportation Commission (MTC) and the Sacramento Area Council of Governments (SACOG) and other associated agencies released a foundational document which recognized the emerging Northern California Megaregion (http://www.mtc.ca.gov/planning/smart_growth/I-80/I-80_Land_Use_Tranport_Study.pdf). Capitol Corridor service was the only existing public transportation linkage in the mega-region. Mobility is crucial as an economic engine and as a megaregion relying upon the already congested I-80/I-680 corridor to expand its lanes as a panacea to future pressures, such as highway capacity growth is not viable or acceptable to the communities along those highway corridors. But neither is settling for the travel conditions experienced today that are only expected to grow worse over time. Therefore, moving forward, the future for Northern California's megaregional mobility is tied to the growth prospects of Capitol Corridor IPR service as at least one transportation alternative for the megaregion. This Vision Plan Update looks at the achievable, captured in the short-term vision (0-10 years), the medium term (10-20 years), but also looks out long-term (20+ years), further out than in prior vision plan documents to present a comprehensive step-by-step maturation and transformation of the service to fulfill future economic needs and environmental conditions.

Completion of the short and medium term vision is contrasted very sharply with the long-term prospects for the Capitol Corridor service. The short and medium term projects are all built off the core service levels established when the State purchased the service frequency rights between Sacramento and Oakland from the then owner of the railroad, the Southern Pacific Railroad, by implementing the California Pacific project. That project was begun in 1996 and was completed with the modification of the Martinez Station in 2001. That project's frequency guarantee, which the UPRR bought when they purchased the Southern Pacific Railroad, is the core service that allowed the State IPR services to run with up to twenty (20) round trips on the Capitol Corridor route. Today, the Capitol Corridor and San Joaquin (which partially shares the alignment with Capitol Corridor) IPR services use all twenty (20) of those round trips. The Capitol Corridor uses fifteen (15) of those trips today and based on the Sacramento to Oakland core service level, this is the service level foundation for service expansions east of Sacramento and south of Oakland are built – all of which are included in the short and medium term portion of the Vision Plan Update. Once all the short and medium term expansions are completed, Capitol Corridor service will be built out as based on the core service levels begun in 1996.

Long-term service expansion plans are oriented around transforming Capitol Corridor service for the mobility needs of Californians twenty or more years from now. The next steps for Capitol Corridor after all the short and medium term expansion is completed are on a scale that would radically transform the service. In doing so, the curve of possibilities and costs would also rise to realize the long-term transformations. Even though these long-term plans seem far in the future, there are real pressures which will force CCJPA and policy makers to begin considering this transformation starting right now. As mega-regional pressures rise (e.g., population, highway

congestion, economic growth) by planning now for this future the Capitol Corridor can be politically and financially prepared to become more frequent and faster service. Sea-level rise, a situation already negatively affecting Capitol Corridor today, will also force adaptive change before long. Fortunately, adapting to the affects of sea level rise can align with the transformation to a faster and more frequent service if the planning groundwork, the policies, and the relationships to shape a Capitol Corridor future are initiated now. The detailed descriptions of the short, medium, and long term vision for Capitol Corridor service are described below after a description of the Capitol Corridor service, its management, and its history.

Context

The Capitol Corridor service is managed by the Capitol Corridor Joint Powers Authority (CCJPA) but under contract to Amtrak for operations. For most of the 171 miles of the present day route, the Intercity Passenger Rail (IPR) service operates on tracks owned by the Union Pacific Railroad (UPRR) with just the southern 2.5 miles on tracks owned by Caltrain. The rolling stock is owned by the State of California, administered through the Caltrans Division of Rail with some additional rolling stock added to the fleet leased through Amtrak. The CCJPA is governed by a Board of Directors comprised of 16 elected officials from six member agencies along the 170-mile Capitol Corridor rail route (see Figure 1-1):

- Placer County Transportation Planning Agency (PCTPA)
- Solano Transportation Authority (STA)
- Yolo County Transportation District (YCTD)
- Sacramento Regional Transit District (Sac RT)
- San Francisco Bay Area Rapid Transit District (BART)
- Santa Clara Valley Transportation Authority (VTA)

Ex-officio members of the CCJPA include the Metropolitan Transportation Commission (MTC) and the Sacramento Area Council of Governments (SACOG), the Metropolitan Planning Organizations (MPOs) along the route.

As administrator for the Capitol Corridor, the CCJPA's responsibilities include overseeing day-to-day train and motorcoach scheduling and operations; reinvesting operating efficiencies into service enhancements; overseeing Amtrak's deployment and maintenance of rolling stock for the Capitol Corridor and San Joaquin trains; and interfacing with Amtrak and the UPRR on dispatching, engineering, and other railroad-related issues.

Presently, the Capitol Corridor serves 17 stations along the 170-mile rail corridor connecting Placer, Sacramento, Yolo, Solano, Contra Costa, Alameda, San Francisco (via motorcoach), and Santa Clara counties. The train service parallels the I-80/I-680 highway corridor between Sacramento and Oakland, and I-880 between Oakland and San Jose.

INSERT MAP OF CCJPA SERVICE AREA

To supplement train service, the Capitol Corridor provides dedicated motorcoach bus connections to San Francisco and communities south of San Jose and east of Sacramento. In addition, the CCJPA works with its partners and local transit agencies to offer expanded options for transit connections throughout the corridor. Currently, the train service connects with the BART system at the Richmond and Oakland Coliseum stations; Caltrain service (Gilroy – San Jose – San Francisco) at San Jose Diridon station; the Altamont Commuter Express service (Stockton – Livermore – San Jose) at the Fremont/Centerville, Great America/Santa Clara, and San Jose

Diridon stations; San Joaquin intercity trains at the Oakland Jack London, Emeryville, Richmond, Martinez, and Sacramento stations; VTA light rail at Great America and San Jose Diridon stations; and Sac RT light rail at Sacramento station. Together with these local transit systems, the Capitol Corridor covers the second-largest urban service area in the Western United States.

The CCJPA offers several programs to enhance transit connectivity. BART tickets are sold at a 20% discount onboard the Capitol Corridor trains to facilitate transfers to BART at the Richmond and Oakland Coliseum stations. The Transit Transfer Program allows Capitol Corridor passengers to transfer free of charge to participating local transit services, including AC Transit, Sac RT, Rio Vista, E-Tran (Elk Grove), YoloBus, Unitrans, County Connection (Martinez), Santa Clara VTA, Suisun-Fairfield Transit, Benicia Transit, and WestCAT. The CCJPA reimburses the transit agencies for each transfer collected as part of our operating expenses. CCJPA also partners with Santa Cruz Metro and Monterey-Salinas Transit, sharing operating costs for the benefit of both agencies and their riders.

History of Service Growth

On December 12, 1991, the State of California Department of Transportation (Caltrans) and the National Railroad Passenger Corporation (Amtrak®) initiated the Capitol Corridor intercity train service with six daily trains between San Jose and Sacramento. In 1996, legislation was enacted to establish the Capitol Corridor Joint Powers Authority (CCJPA), a partnership among six local transportation agencies to share in the administration and management of the Capitol Corridor intercity train service. In July 1998, an Interagency Transfer Agreement (ITA) transferred the operation of the Capitol Corridor service to the CCJPA for an initial three-year term. The CCJPA now operates and manages the Capitol Corridor service through an operating agreement with Amtrak. In July 2001, the ITA was extended for another three-year term through June 2004. In September 2003, legislation was enacted that eliminated the sunset date in the ITA and established the current, permanent governance structure for the CCJPA.

In response to growing demand, the CCJPA expanded service in October 2002, January 2003, and April 2003 to achieve a schedule of 24 weekday trains between Sacramento and Oakland, using the same State budget allocated for 18 daily trains. In August 2006, with another flat budget allocation, the CCJPA increased service to 32 weekday (22 weekend day) trains between Sacramento and Oakland, and 14 daily trains between Oakland and San Jose. This expansion was made possible with the completion of Phase 1 of the Oakland-to-San Jose track improvements and the Yolo Causeway second main track (completed in February 2004). Together, these projects contributed to a 10-minute reduction in travel time between Sacramento and Oakland, in addition to more frequent service.

The August 2006 service expansion, which initiated hourly service between Sacramento and Oakland, represented a major milestone in the CCJPA's management of the Capitol Corridor. This service expansion was in place until the revised Sacramento Station was opened in August 2012. This station revision added greater station track capacity that permitted a service optimization restructuring process for Capitol Corridor. Accordingly, Capitol Corridor reduced service levels to 30 daily trains or fifteen round trips thus allowing the sister IPR service, the San Joaquins, to utilize the right for two daily trains for its service expansion plans. Moving forward regardless of service expansion plans included in this Vision Plan Update, one of the most cost-effective capital improvements is increasing seating capacity by adding more rail cars to the existing scheduled

trains. This is the only practical means of increasing ridership during the increasingly congested peak hours.

The benefits of these service expansions and corresponding track capacity improvements and train equipment acquisitions have enabled the Capitol Corridor to increase market share and sustain significant growth in ridership and revenues. The Capitol Corridor remains the third busiest route in the Amtrak national system.

Prior Vision Plan Updates

The initial CCJPA Vision Plan was developed in 2002. At the time, CCJPA had just initiated the construction process which would lead towards the 2006 service expansion between Oakland and San Jose and also double tracking of the Yolo Causeway, for more service reliability. At the time, it was important for the CCJPA Board to express what was next for the Capitol Corridor service. Then, as the Oakland to San Jose and Yolo Causeway second main track construction process was winding down and amid a changing State budgetary landscape, the CCJPA updated its Vision Plan in April 2005. This 2005 document identified short- and long-term goals to guide the operating and capital development plans of the Capitol Corridor for the next five to 20 years. Unfortunately, the State's budgetary situation remained unchanged and very little of the vision expressed in the 2005 Vision Plan Update was realized.

Blending California's Passenger Rail Services

In 2012 the State of California began taking significant strides to truly integrate passenger rail services across the State. There were ample lessons from other countries and regions around the world to learn from. In 2012, how high-speed rail services grow, how IPR and commuter rail services and local transit services feed and distribute travelers, began to take shape in California. This process was evolving just as CCJPA was developing earlier versions of this Vision Plan Update. Recognizing the importance of both the Vision Plan Update but also the blending of California IPR and high-speed rail services, the CCJPA paused the development of this document until the blended rail picture began to take on more shape.

The crux of passenger rail services for the State of California is development of the high-speed train system but all of this development takes place in regions and with existing passenger rail systems, most notably the three IPR services in the State (Capitol Corridor, San Joaquins, and Pacific Surfliner). High-speed rail for California will create a connecting spine of high-speed passenger rail service connecting Southern and Northern California with existing IPR services, primarily the San Joaquin, being the most parallel system to the initial high-speed rail operating segment. There are two working groups (Northern California and Southern California) formed made of existing passenger rail operators working with the California High Speed Rail Authority (CHSRA) and representatives of the Federal Railroad Administration (FRA) who are directly involved due to the degree of interest in passenger rail mobility at the White House. These groups get together to work on a variety of planning efforts to create a blended passenger rail service so that service objectives leading to high-speed rail and integration with high-speed rail are met by a variety of funding imposed deadlines.

SHOW MAP OF NOR CAL BLENDED SYSTEMS

Blending passenger rail operations for Northern California means utilizing rolling stock, planning service and operations, funding, and implementing particular capital projects to grow passenger rail use in California, incrementally in steps that support the various stages of passenger rail

growth in California. Geographically, Capitol Corridor service sits on the northern end of the feeder/distribution system for Northern California blended passenger rail. With the San Joaquins being the major initial feeder/distributor to the first phase of high-speed rail, Capitol Corridor will first “blend” with the San Joaquin service at Martinez and later, when high-speed rail gets to San Jose and even later, Sacramento, there will be a more direct feeder/distributor relationship of IPR service along the Capitol Corridor route to high-speed rail service.

Blending services is requiring extensive analysis, modeling, planning, agreement drafting, and debate all behind the scenes, but in front of the future customer, the goal is for blended service to look seamless and better integrated with overall travel around California regardless of the route terminology rail operators use today (e.g., Capitol Corridor). From ticketing, customer service, website information, and on down to lost and found, it is crucial that the customer who wants to travel is not presented barriers to doing so because of institutional artifacts that exist today. This represents a major change to the way customers relate to passenger rail in California. This will also require transforming how the term ‘Capitol Corridor’ or ‘San Joaquins’ is used in that context with the public. How this will look and how it will be structured is an ongoing matter of development with the blended rail partnership but it doesn’t erase the fact that a vision plan for the Capitol Corridor route still requires leadership, governance, advocacy, and professional management for the vision to be realized for this service area.

Vision Plan Update Organization

This document is organized to cover the short and medium term vision plan – projects built off the core system to be completed in the next twenty years, and then the long-term vision plan. The best analogy might be our education system. Today, we are about to graduate elementary school. The short-term vision is junior high, and the medium-term vision is high school. Of course, like all high school students, once they graduate, they are free enter the work force having accomplished society’s prescribed educational goals. Like a high school graduate, the Capitol Corridor could stop at that point (after medium-term projects are completed) having expanded from the core system started in 1996 with nearly that same core frequency across the 171-mile route. Or, the Capitol Corridor could expand upon that excellent foundation, deepen their skills, become more specialized, refined, and effective, and complete college and then head out into the world albeit with a more expensive college degree in hand. The long-term vision is what Capitol Corridor wants to be after it graduates college. Like going through the educational system, each passage through to the next level requires success at the level before.

This document organizational effort is, however, a convenient way to categorize and discuss projects when, in practice, efforts may be focused on a mixture of short, medium, and long term objectives depending on circumstances. There are projects planned in conjunction with the Northern California Blended rail services that are the first elements of the short-term vision and if funding is assembled, these projects will be completed in the ten-year cycle of the short-term vision but there will also be policies that need to be considered in the short-term and on into the medium term to set up particular objectives in the long-term. And lastly, the improvements included in this Vision Plan update are not just about service frequencies or extensions – there are station facilities and on-train or train service amenities planned that are also discussed.

Extensions, expansions, sea-level rise, and route realignment will be important terms used throughout the document. The term “extension” will refer to adding new route portions of Capitol Corridor service to the existing service whereas an “expansion” will mean an increase in service level for routes or portions of the route that are in service today. An underlying factor in project

delivery will be the effect of future “sea level rise” and what might be done to adapt the track conditions or alignment to those projected changes. As well, in both the short, medium, and long-term, there will be opportunities explore “route realignment” - to improve overall travel time by generally serving the same market albeit shifted onto a more operationally preferable track alignment.

SHORT-TERM CAPITAL & SERVICE PLAN

Overcoming a Stagnant Decade of Capital Investment

Capitol Corridor’s prior vision plan outlined, in 2005, a short-term capital investment strategy that was poised to build upon the then recent capital improvements made to expand service frequency between Oakland and San Jose. Unfortunately, realizing even the short-term vision first expressed in 2002 remains a challenge just over a decade later primarily due to lower or non-existing capital funding sources. In the intervening years since 2002 the formulas that previously provided steady Intercity Passenger Rail (IPR) investment through the every two-year State Transportation Improvement Program (STIP) were altered resulting in, by some measures, ten-fold reductions of capital funding. Despite several State Bond measures passed by California voters over this period, which partially prepared for shorter-term investment, sufficient capital has not been committed to realize corridor investments. Passenger rail in California is not alone in this; vastly minimized transportation infrastructure investment on the State and national level has been highlighted as one of the factors contributing toward higher unemployment levels than ideal, and raised questions about whether future mobility needs will be able to be met. But looked at another way, the attention to the dearth of transportation investment is spurring new discussions about how to reverse this situation.

Despite the setbacks caused and aided by California’s budgetary woes, the economic recession of 2008, it appears the future of capital funding support is looking better than it has in nearly a decade for IPR services. More than just hoping that some capital funding will magically appear, CCJPA staff is at the forefront of advocating and shaping funding programs for IPR services. A sustainable source of capital is the goal because that will provide CCJPA the ability to build and shape a meaningful capital program that will accomplish vision expressed in this document.

At the State level, key legislation was passed which created a “Cap and Trade” program to help offset carbon emissions. “Cap and Trade” is under policy and implementation development at this time but it is clear that California’s IPR services are viewed as important carbon-friendly transportation alternatives which are being poised as eligible for dollars that this program will generate. As well, a variety of alternatives other than being indexed to declining gas tax revenue, are being explored and California’s IPR services and are being included in the conversations needed for legislation that, in the near future, might restore and steady the capital funding climate in California for IPR services.

At the federal level, the 2009 passage of the Passenger Rail Infrastructure and Investment Act (PRIIA) is soon set to expire. This program jump-started the nation’s focus on a high-speed and intercity passenger rail but in the later years of the program, the Congress did not appropriate funds to support the program. In 2012, the Congress adopted a two-year transportation bill but its upcoming expiration date and the work to find a successor to PRIIA, there are ongoing discussions to include intercity and high-speed rail into a separate passenger rail focused bill that would establish a steady source of merit-based project funding. So, like what is going on in California, at the federal level, there is a focus on the role of passenger rail in the nation’s

transportation future all of which aligns for timely implementation of the elements of this Vision Plan Update

Short-Term Vision (0 – 10 Years)

The short-term vision is the immediate, near-term list of service expansion projects Capitol Corridor staff has been pursuing for some time (since 2005!) but has not been implemented due to lack of sufficient funding. They are thought of as projects that would be completed or at least in construction ten years from present. Capitol Corridor service was last significantly changed on August 28, 2006 with the introduction of three additional round trips (from four to seven) between Oakland and San Jose. These investments provided an immediate ridership and revenue boost to the Capitol Corridor service and solidified a viable transportation market in the East Bay with just enough frequency to show the market could grow further. There are other markets for growth as well. Focusing on the northern end of the service, the 2005 Vision Plan also concentrated on increasing service to and from Placer County stations. Despite various attempts and close calls, the service goals from the 2005 Vision Plan remain unfulfilled serving Placer County with additional service. Another market that is emerging is south of San Jose with service to and from Salinas. Each of these short-term markets are targeted for service frequency expansion or extension.

Additional frequencies and new markets are not the only element of CCJPA's short term vision. Additional service amenities while on the train and at stations are also on the horizon.

Oakland to San Jose Service Frequency Expansion: Phase Two

Carrying over from the 2005 Vision Plan, this Vision Plan Update retains the goals of building upon the first phase of service frequency between Oakland and San Jose. The eventual plan is to achieve sixteen (16) round trips but this will have to be realized incrementally in more modest jumps from seven (7) to eleven (11) round trips which are included in the short-term vision (the third phase from eleven (11) to sixteen (16) round-trips is in the medium-term vision section).

With any frequency expansions, improvements to rail infrastructure have to be installed that both preserve existing and future growth patterns of freight and passenger rail services operating or planned to be operating in the service area. Naturally, since 2005, there has been some evolution of the driving factors for freight and passenger rail growth. As such, the mixture of projects that will allow for that phase-two service frequency expansion have changed slightly over time. The CCJPA will continue to work with the host railroads (UPRR and Caltrain) to implement the particular blend of track infrastructure projects that provide that track capacity to allow service frequency improvements.

Of note, the CCJPA is exploring the potential to route the Capitol Corridor service south of the Oakland Coliseum station on to a different railroad alignment which may offer travel time savings and permit a better service operations pattern than the existing alignment (i.e., moving from the Niles Subdivision to the Mulford Subdivision). Both of these factors will result in an increase in ridership and revenues based on the ridership models that have, to this point, been excellent predictors of service alterations. If such a shift is pursued, the CCJPA will work with the existing communities with stations (Hayward and Fremont – two of the lower utilized stations in the system) along the existing route to identify and build a replacement station in association with the track infrastructure projects along the newer alignment.

NEED A MAP SHOWING THE ALTERNATIVE ALIGNMENTS

Oakland to San Jose Frequency Expansion Table – Phase One and Two:

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Phase One	Completed	4 to 7 round-trips	N/A	N/A
Phase Two	Planned	7 to 11 round-trips	\$50.8 Million	\$250 Million

Placer County Service Frequency Expansion

Placer County stations have been served by one west-bound morning train and an late afternoon/early evening train east-bound ever since the Capitol Corridor service was started in 1996. The alternative to the Capitol Corridor, Highway I-80, has grown more congested over time and this trend is expected to continue. Increasing Capitol Corridor frequency is an alternative transportation mode and can relieve highway congestion pressure. In the intervening years since the 2005 Vision Update, the CCJPA with the UPRR came close to utilizing a mixture of State and UPRR rail funding until, late in the process, the UPRR had to make a difficult financial decision to pull their commitment to invest elsewhere in their railroad network. This investment would have resulted in one additional round trip between Auburn and Sacramento thus doubling the service provided. The benefits provided by this project remain a potential but lack of funding is a crucial hurdle to overcome but one that CCJPA and UPRR stay apprised of in the anticipation that one more round trip train to/from Auburn can become a reality in the short-term.

One outgrowth of the 2005 Vision Plan was a goal to focus on increasing service between Roseville and Sacramento and to that end, the CCJPA utilized some of the STIP funding appropriated in the intervening time to initiate an initial design and environmental documentation process to expand service from the current one daily round trip to ten round trips. This important work will set the foundation for moving directly into the construction phase should sufficient or phased construction funding become available in the coming years.

The breadth of the project expanding service between Sacramento and Roseville is expected to take place in several funding phases. In the short-term a portion of the overall project might be constructed, such as a relocated station and associated station and station access track infrastructure. As a result of the phasing, a lower number of round trips would initially be expected (e.g., five round trips). The second phase of investments would be required to achieve a ten round trip service and funding availability would likely put this out more than ten years in the future.

Placer County Service Expansion

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Auburn Expansion	Planned	1 to 2 round-trips	\$0	\$50 Million
Roseville Expansion	Planned	1 (2) to 5 round-trips	\$18.8 Million	\$125 Million

Monterey County Service Extension

Highway congestion between San Jose and Salinas along U.S. Highway 101 is a common frustration for travelers. The Transportation Agency for Monterey County (TAMC) approached both Caltrain and CCJPA management to explore which extension of service would best meet the transportation needs along the corridor. Capitol Corridor’s equipment and service pattern best fit the desired operating needs. An expansion of service to/from Salinas will only be possible once Phase Two service expansion between Oakland and San Jose is implemented. Two additional trainsets would be required to support this service extension. Only then will there be the operating schematics necessary to extend service to/from Salinas and intermediate stations. Additional capital costs will be a phased upgrade to the stations along the route and any track infrastructure upgrades associated with the UPRR owned track. Initially service is planned for launch with two daily round trips. The eventual short-term operating goal is to operate up to six round trips between Salinas and San Jose.

Monterey County Service Extension

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Salinas Extension	Planned	2 round-trips	\$45 million	\$175 Million

Other Service Extension Options Considered

Over the last fifteen years, Capitol Corridor has explored a possible service expansion east of Auburn and into Reno, Nevada. Explorations were largely driven by the natural linkage of the Bay Area to the summer and winter recreational opportunities in the communities in and around Lake Tahoe. Freight rail use of the rail corridor has actually increased since use of this corridor was explored and dismissed as already being too crowded with freight rail to successfully see extended Capitol Corridor service. Not unless there were significant changes, such as a successful Reno-Tahoe Winter Olympics bid, would sufficient political and funding attention be available to warrant this as a viable short-term service option. Therefore, it is not being considered as a viable service extension.

Short-Term Service Amenities

Service expansions are not the only aspect where Capitol Corridor service will be improved. Not only will the train journey improve, but also will amenities at the station. Since the 2005 Vision Plan Update, two notable technology amenities were introduced. These were the advent of e-Ticketing and the introduction of free passenger Wi-Fi. Both projects were in the works when the 2005 Vision Plan Update was being developed. Technology changes quite rapidly but even at this point it is feasible to include some of the amenities headed to the Capitol Corridor service. As things change, CCJPA will be diligent in keeping passenger rail travel modern and attractive whatever personal use or information technologies become established.

On the technology front and building upon the Wi-Fi network communications infrastructure, a comprehensive on-board public information system is planned (OBIS). Like with Wi-Fi, the Capitol Corridor and the Caltrans Division of Rail will work with Amtrak to establish a national vendor to provide the OBIS service. OBIS will feature a mixture of automated video and audio communications to update travelers on a real-time basis about their train journey, station

arrivals, and any alerts. There will also be options for using video screen space (either into distinct windows or on a temporal rotation) for displaying advertisements, service promotions, and upcoming service alerts. Each car in the fleet will be retrofitted to include these upgrades but doing so will take some time. These OBIS modifications may start to be seen on some of the rolling stock in mid-2014 at the earliest.

The Wi-Fi system installed in 2011 and upgraded in early 2013 will continue to the basis of improvement, including exploration of providing digital media content (movies, television, and games) via the Wi-Fi network. Digital media rights and delivery systems are, like technology, evolving quickly and at some point a viable business model for delivering digital media to train customers seems possible and likely. CCJPA will remain actively engaged with Amtrak and digital media content providers on this front moving forward.

Improvements at stations (and to some extent on trains) will include the introduction of several bicycle associated amenities. It is no secret that bicycle use on the Capitol Corridor as a mode of access is growing and the existing rolling stock as configured is not always able to accommodate that demand. Starting in late 2012 and continuing through 2013, Caltrans led a modification process to the cab cars to retrofit the lower level of the 8300-series cars to nearly double the available bicycle parking. This improvement will bode well for a while for Capitol Corridor service until predicted bicycle use demand begins to even challenge that modification. At that point, modifying additional lower levels in cars may warrant similar modification. The other way to offset demand is to increase the supply of bicycle infrastructure at stations. The CCJPA Board authorized the Bicycle Access Plan which included introduction of secure e-Locker facilities and folding bicycle rental services. Both of these amenities will allow more people who currently take a bicycle to the train to utilize facilities at stations where they feel their bicycle will remain secure or they may just opt to use one of the folding bicycle rentals that will be available at select stations. Another amenity that will help will be the growth of bicycle sharing in the communities that decide to install bicycle sharing. Whether it is bicycle sharing or the e-Locker and folding bicycle rentals, it is the marriage of technology used to access these systems that holds great promise as an amenity to enhance Capitol Corridor service. These station amenities will start to be installed in late 2014 and continue to expand as demand warrants.

Medium-Term Vision (10 – 20 Years)

The medium-term vision contains most of the ‘next phase’ expansions that build upon the short-term vision service to ultimately complete the build-out of the Capitol Corridor route from the core service established in 1996.

Oakland to San Jose Service Frequency Expansion: Phase Three

Building upon the Oakland to San Jose Phase Two service expansion, Phase Three represents and expansion of service to extend the core service levels between Oakland and Sacramento of fifteen round trips all the way to and from San Jose. The exact mixture of track capacity projects to permit additional service expansion is speculative at this time but a likely project to permit this expansion would be double or triple tracking the Alviso Wetlands area of the route. Right now this is just a single-track section of railroad. Construction and suitable design, including being mindful of future sea levels and the surrounding wetland environment, will make service expansion in this area very challenging. A significant environmental documentation and mitigation process can be anticipated along with a significant design and engineering effort.

Oakland to San Jose Frequency Expansion Table – Phase Three:

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Phase Three	Planned	11 to 15 round-trips	\$0	\$210 Million

Placer County Service Frequency Expansion

As mentioned in the short term, expansion of Capitol Corridor’s Placer County service will be phased with the later parts of the service expansion occurring in the medium-term, the 10-20 year time horizon. A station relocation and construction of a third mainline track with various track crossovers will require additional planning and acquisition of funding that will likely shift the final phase to the medium term. This second project phase, envisioning the relocated station and station track access completed in phase one, will likely involve the longest portion of the linear third track option and a new American River bridge crossing for rail. The environmental document that will disclose impacts this project is underway now so the exact linear alignment and bridge elements are speculative at this time.

Placer County Service Expansion

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Roseville Expansion	Planned	5 to 10 round-trips	\$0 Million	\$125 Million

Monterey County Service Expansion

TAMC has identified an eventual operating goal of up to six round trips between Salinas and San Jose. Presumptively in the short-term, a service extension beginning with two round trips will be initiated thereby making additional round trips in the medium-term a service expansion. These two initial round trips are conditioned upon completing Oakland to San Jose Phase Two service expansion and it can be expected that a similar expansion, perhaps associated with the medium term Oakland to San Jose service expansion to fifteen round trips might be required to bump up service frequency to six round trips. A complete operational analysis considering additional rolling stock needs will be required as will operational modeling to identify the needed capital improvements that will create the track capacity for the additional round trips.

Monterey County Service Extension

Phase	Status	Frequency Gain	Funding Secured	Funding Need Estimate
Salinas Extension	Planned	From 2 to 6 round-trips	\$0 million	\$200 Million

Medium-Term Service Amenities

Considering service amenities more than ten years into the future is rather speculative especially given the pace of technology innovation. It can be expected that the nature of how rail passengers work and entertain themselves may continue to evolve. Providing Wi-Fi or the latest evolution of that may change as may the ways the Wi-Fi system is delivering connectivity to the train and also

to the passengers. CCJPA will have to be prepared to refresh this technology to maintain this valued amenity.

The way people are ticketed for train travel may also evolve in this period with an eye toward making ticket use more versatile or customized to better match both customer and revenue objectives. One concept is to create a customer selected (via the internet or an application on a smart phone) sliding scale multi-ride ticket that can be set for the number of rides and the duration over which those rides can be used. As an example, customers could opt for a long period of use for 15 rides (e.g., 75 days) or a short period of use (e.g., 20 days) with the longer use period costing more. Conversely, over a thirty-day period, a customer could hypothetically select four trips, or twenty trips, or whatever they felt they would use. These sorts of decisions could be customized by the customer so they match their needs.

Other amenities such as food service, bicycle storage at stations and on trains, and customer communications (website, customized service alerts) will likely grow as the service evolves and as ridership changes over time. Over the medium-term, a maturation of the blended IPR and even California High Speed Rail system are likely and this factor will require directed attention to support those service relationships especially as relates to service amenities.

LONG-TERM CAPITAL & SERVICE PLAN

Long-term capital and service planning takes Capitol Corridor as we know it today, the short term vision, and really turning loose the potential of passenger rail serving the corridor and the larger market. It is about answering the question of what could the Capitol Corridor become. But also, with respect to environmental change coming to the corridor in the form of sea level rise, it is also about adapting to that change in a way that enhances the potential of Capitol Corridor service. The analogy was made before – the long-term vision plan is like deciding what Capitol Corridor wants to be after it goes through college whereas getting through the short and medium term plans was akin to finishing high school. College can be expensive but it is also transformative.

The Emerging Megaregion and the Capitol Corridor's Place In It

Today, nearly 9.6 million people live in the 15 counties that make up the San Francisco Bay Area and Sacramento region. By 2040, the state expects that total to grow by more than two million. Regional planners have projected even greater growth: the Bay Area's Metropolitan Transportation Commission, or MTC, anticipates another two million residents in the Bay Area alone by 2040, as well as another 1.1 million jobs, while the Sacramento Area Council of Governments (SACOG) expects close to 800,000 more residents in its region by 2035, and roughly 350,000 more jobs.

It should come as no surprise to anyone who rides the Capitol Corridor or who drives Interstate 80 (already one of the longest stretches of continuous six-lane freeway in the world) between the Bay Area and Sacramento that Northern California has been identified by the Regional Plan Association of New York as one of ten emerging "megaregions" in the United States. Communities across Northern California have been linked economically since the Gold Rush; the rise of Pacific Rim trade and the digital economy have only strengthened those ties.

Maintaining mobility throughout this megaregion will be central to its success in an increasingly competitive global economy. At the same time, Northern California's famous quality of life will, more than ever, depend on our ability to avoid congestion and preserve the environment.

The Capitol Corridor is, literally, *well-positioned* to serve as the centerpiece of a regional rapid transit system. It connects downtown Sacramento to central Oakland and San Jose, and it connects to San Francisco by way of transfers to the Bay Area Rapid Transit system, BART. In addition to BART, it is connected to the Sacramento and San Jose-area light rail systems (RT and VTA), to Silicon Valley commuter rail (Caltrain) and, in the future, it will be connected to California High-Speed Rail in San Jose and Sacramento.

For the Capitol Corridor to function as part of a seamless system, it must be fully integrated. Fortunately, efficient transit networks don't require a single operator, or even the same sort of vehicle. Rather, they simply require application of a few simple principles:

- *Coordinated connections.* Many Northern Californians have experienced “timed transfers” at BART’s MacArthur and 19th Street Oakland stations. Trains arrive and depart simultaneously; passengers also need only cross a platform. Any transit transfer can be made similarly convenient, regardless of vehicle – indeed, cross-platform transfers can be made between BART and Caltrain in Millbrae. Use of smart cards such as the Bay Area’s Clipper can also eliminate the need to purchase separate tickets or passes.
- *“Clockface” scheduling.* Timed transfers or “pulses” at transit hubs are made even easier for users when trains or buses depart at regular, easy-to-remember intervals: on the hour, every half-hour, or at 15 or 45 minutes after the hour.
- *Frequency.* More frequent service offers travelers more options to choose from, can reduce “door-to-door” travel times, and is often the difference between a trip that is viable or attractive by transit and one that isn’t. Frequency also increases capacity.
- *Service types tailored to diverse markets.* Different users have different needs. Rather than rely on a “one-size-fits-all” model of all-stop local service, many transit agencies also provide limited-stop and express service. When Caltrain began offering “Baby Bullet” express service in 2004, ridership rose 10 percent in a matter of months. Express and limited service has also allowed Caltrain to better match supply and demand, as busier stations see more trains.

These principles have been successfully applied around the world, most famously in central European countries (where U-Bahn subway and S-Bahn commuter rail networks complement one another) and in Japan. But the most important principle of all is speed. Faster service is not only more attractive to customers, but allows service providers to improve frequencies at no cost, attracting even more passengers in a virtuous cycle. In transit, time really is money.

Getting Up to Speed

Many of the tools available to reduce travel times are familiar to Capitol Corridor staff, Board members and riders. Indeed, when a second track was added to the Yolo Causeway in 2004, removing a bottleneck, travel times were cut by 10 minutes.

Still, a one-way trip from Sacramento to San Jose currently takes, at best, 3 hours and 8 minutes. It’s not the trains, at least not so much: Capitol Corridor locomotives have a top speed of 110 mph. But safety-related factors both physical and regulatory slow them, everything from stops and curves to track configuration, conflicts with freight trains, at-grade road crossings, signal systems, bridges that lift for marine vessels, and a lack of automatic safety controls. Legally, Capitol Corridor trains are limited to 79 mph, and their average speed is only about half that, 42 mph. So reducing travel times will require both increased top speeds as well as fewer (and lesser) slow-downs. Following are descriptions of various tools to contribute to reducing travel times.

How they are used, phased-in, and combined is discussed after they are introduced and incorporated based on a segment-by-segment description.

Positive Train Control and Speed Increases

The promise of higher overall speeds is through Positive Train Control, or PTC, which is in the process of being installed on Capitol Corridor train equipment right now and is soon to be installed by the respective host railroads. Once installed, PTC, which is a next-generation automated “fail-safe” system, it has to be proven to be a reliable system with neither failures of on-train or trackside PTC equipment. The specter of PTC unreliability concerns operation, not safety. When all trains in a network are linked under PTC, an unreliable piece of on-train PTC hardware 40 miles ahead means all trains on the same network must stop, for safety reasons, until the issue is fixed. This sort of cascading effect is of real concern but one that is being worked on by Federal Rail Administration staff and the freight and passenger rail community. Once that is solved, there is no question that the promise of higher speeds is built into the way PTC can, theoretically, operate. Right now in Michigan, Amtrak trains have been allowed to reach top speeds of 110 mph following implementation of PTC but this system is not as heavily integrated with freight rail services where an equipment failure could lead to system-wide delays. CCJPA is not involved at the national level with PTC deployment but will be keenly focused on reaping the benefits of improved safety and, eventually, higher overall operating speeds (110 mph).

It is nonetheless possible to achieve much greater speeds using a strategy of incremental upgrades. The Federal Railroad Administration has defined passenger trains with top speeds of between 90 and 110 mph as “emerging high-speed rail,” and will allow trains to reach 110 mph *without* “sealing” at-grade crossings, or making them impenetrable to cars and trucks using four-quadrant gates and other safety measures. Complete grade separation of intersections is not required until trains exceed 125 mph.

Tilting Rolling Stock

The Capitol Corridor’s current trains physically cannot go faster than 110 mph, and they must slow down dramatically around tight curves like those along the bayshore between Martinez and Richmond. Amtrak’s *Cascade* service between Seattle and Portland uses Talgo tilting trains allowing higher speeds through curves, and America’s fastest trains, on the Amtrak *Acela* between Boston and Washington, D.C., similarly rely on tilting technology. Tilting technology is an option but there is already a heavy and continuing investment in today’s non-tilting rolling stock so the viability of this option to improve overall operating speeds is not likely.

Express Train Service

One way to improve speeds, of course, is to make fewer stops, and the express and limited service option alluded to above would do so. Caltrain is a good local example of the benefits of express and limited service used in the peak travel hours, but it, of course, is operated as a commuter train service. Capitol Corridor station spacings, utilization, and frequency make express and limited service options a challenge. Capitol Corridor’s core service area does not have sufficient frequency to make skipping one or more station stops very practical since much of the ridership accumulates by precisely making all the stops made today.

Electrification

Electrification, in some ways similar to tilting trains, would involve new rail equipment, but also major track infrastructure change. Electric locomotives or electric multiple unit (EMU) railcars powered by overhead catenary provide a range of benefits, including cleaner, quieter operation, faster acceleration and deceleration, higher top speeds and reduced operating costs. Additionally, a switch to electric power would insulate the Capitol Corridor against rising fuel prices.

Under current Federal Railroad Administration (FRA) regulations, lightweight EMUs cannot share tracks with heavier equipment, and even if these regulations were amended, UPRR would have to agree to shared operation. A future concept of electrification would have to include the concept of constructing separate passenger-only tracks within the existing right-of-way.

Caltrain is planning to electrify its system by decade's end, and the Altamont Corridor Rail Project will eventually result in electrification of the Altamont Commuter Express (ACE) alignment, allowing 150 mph service. Electrification costs can vary widely: Caltrain's electrification project, including PTC and 112 new railcars, is projected to cost roughly \$30 million per mile. Operating costs, however, are projected to be reduced by over 40 percent.

Straight Lines and Super-Elevation

With the exception of geography, which defines where people live, where the stations are, and the track that was installed respecting the physical confines and historical reasons for its alignment, the quickest option is a do-over on the route – this time with straight track segments between stations which will allow for the fastest travel times possible. Only considering the entirety of the route does this seem ridiculous – there can be sections where it can actually pay off, but only to a degree. And if the track can't be straight, there are sometimes modifications to “tilt” the track to the characteristics of the curve so that it can be taken at higher speed, provided the host railroad can agree to the degree of the tilt. The many curves between Richmond and Martinez stations are an obvious location for straightening or “tilting” the track but how to actually straighten something when straightening segments may run into a hillside or a body of water is another matter. Nonetheless, straight track and/or tilting track is a way to marginally increase operating speed. Another future option would be to identify new route alignments and construct track which is optimized to perform at as high a speed as possible. Straight lines are a way of avoiding geographical limitations and tilted tracks are, if allowed, both tools to decrease travel time, provided there is the funding to create them, and in the case of super-elevation, maintain them.

Travel Time Reduction Benefits

If reduced travel time is the goal, any one or a combination of the above options can achieve it. Certainly the ridership and therefore models used for passenger rail travel are very largely influenced by travel time but conversely, achieving wholesale travel time reduction might prove to be a very large funding, political, and environmental challenge. Faster service unquestionably boosts ridership and revenue but there are a number of other side benefits if done right to further boost ridership.

Clockface Scheduling

Faster service would also make it easier to implement another of the recommended principles, clockface scheduling. Trains that took 90 minutes to travel from Sacramento to Oakland and 45 minutes to travel from Oakland to San Jose could depart those major stops on clockface

schedules; trains that took 60 minutes to reach Oakland and another 30 to reach San Jose would be even better.

The former would require average speeds close to 60 mph, while the latter would require average speeds of around 90 mph. Average speeds of 60 mph for express service would require speed improvements via PTC implementation, improvements to grade crossings, additional track segments to reduce conflicts with freight and potentially tilting trains. To achieve average speeds of 90 mph, however, major new infrastructure would be necessary. Major investments would also be needed to improve connectivity and reliability and reduce the Capitol Corridor's exposure to rising sea levels.

Long-Term Vision with Sea Level Rise

Capitol Corridor has a "love/hate" relationship with the San Francisco Bay along the present alignment. The proximity to marshland, tidal waters, and the coast make travel a picturesque experience for many travelers. Yet that same proximity to these beautiful but wet landscapes is going to increasingly be the cause of delays or outright cessation of service for unacceptable period of time. Higher groundwater and high tide already impact rail infrastructure along the route and this will only grow over time because sea levels are steadily rising and along with that comes a higher groundwater table, higher tides, and storms that have a higher reach especially with higher tides. The myriad of rising sea level effects will, as will be shown, shape the long-term vision of Capitol Corridor.

How Sea Level Rise Will Impact Capitol Corridor

Adaptation to this change is already requiring directed capital funding to deal with the impacts of sea level rise. To a large extent, especially with respect to slow orders, CCJPA already pays for ongoing capitalized maintenance in areas where track is subject to repeated subsidence due to high ground water. A higher percentage of CCJPA's capitalized maintenance budget is used in marshy areas, such as the Suisun Marsh, than elsewhere along the route.

Mitigating the effects of track bed subsidence is just the beginning. A far more disruptive event than just gradually higher sea levels or groundwater will be higher high tides coupled with a storm surge. Such "perfect storm" intrusions will pack enough power to shut down the route, perhaps for extensive distances, until it is repaired and restored to safe operations. Damage done this way will increase over time. Initially, these type of intrusions will be considered isolated incidents, but then over time the frequency will become too commonplace, expensive, and disruptive and require some longer-term adaptation response. The Capitol Corridor route earns the distinction of having the most linear exposure to effects of sea level rise of any transit service in the Bay Area. Of course, the issue extends to the UPRR freight trains as well but from a public transit operator perspective, CCJPA is in unfamiliar and unexplored territory. Responding these eventual effects must be incorporated into the long-term vision.

Now that vulnerability has been established and the rail asset has been determined to not be naturally resilient (e.g., a flooded park will dry out and not suffer much structural damage whereas scoured rail ballast and track does retain safe function), an adaptation response will eventually be warranted. CCJPA and UPRR will always be linked in these efforts but depending on location, there will be a dearth or an abundance of partners who will be interested in adaptation response since their own assets are also vulnerable. Partners will be sparse when the rail is isolated, such as in a marsh area or hugging a coastline. By contrast, when the route enters

a more urbanized setting, there are many other partners such as waste-water facilities, highways, senior centers, housing developments, etc. In that urban setting, sometimes the rail is closest to the water but there are other affected assets adjacent to the rail line so a sense of partnership might be formed because how adaptation proceeds for the rail may affect its neighbors. A hypothetical example of this situation is raising the track bed can act like a levee protecting other inland properties until it potentially fails one day, or waters rise enough along a water channel to come around the rail bed barrier. In another situation, the rail is mixed in among other assets that are closer to the water and those properties and assets suffer first with the rail not far behind. In this situation there may be other entities who are more directly motivated to act but want to coordinate adaptation solutions with the other affected entities. Comprehending the future challenge and response now especially at the scale involved is a daunting task yet, being prudent long-term managers of the Capitol Corridor service, these things must be imbedded into the long-term vision.

Shaping Policy and Planning Responses to Sea Level Rise

The short-term vision projects are not equipped to address the effects of sea level rise and, for the most part, if they are built in vulnerable areas, there are usually other urban partners who will also have to consider their response at some future time. In that sense, CCJPA can make an investment decision to spend capital dollars today to realize the benefits of the investment in the short and medium term, and in the future, when an adaptation response is required, work with others to support any number of a variety of adaptation responses to maintain that investment long-term. In other words, short-term investment is still worthwhile because there will be so many other asset managers (owners of property) that will be in the same predicament.

But in other cases, making some investment decisions will commit Capitol Corridor, like a branch on a tree, to a particular set of future adaptation responses. These are worthwhile thinking about as they arise as opposed to letting happenstance decide things. Soon CCJPA anticipates that Hercules will ask CCJPA consider a new station stop in an area that is unquestionably vulnerable to rising sea levels. While such a station could be built to last a while given the effects of sea level rise, the track infrastructure getting to and from the station would also, seemingly, be committed towards a particular adaptation response just because a station was put in place. Tolerance for such a public investment may require more analysis but, perhaps, a thirty-year amortization of the station benefits before wholesale track upgrades are required is acceptable but perhaps ten years is not. Regardless, at the time extensive adaptation investment is warranted, the entity on the hook to ensure that station is still served will likely not be the city where the station is located – it will be the Capitol Corridor and UPRR. Regardless, balancing this type of decision in the near term is a policy decision for policy makers that would be best informed by sound analysis, but by also considering other adaptation options especially with combined with other long term objectives.

Looked at another way, adapting to rising sea level can prompt a coordinated response with other long-term Capitol Corridor objectives such as speed and frequency enhancements. This type of thinking sets the stage for a long-term win-win for Capitol Corridor service. These combined responses, looking at speed and frequency enhancements that are blended with a sea level rise adaptation response are incorporated and discussed in the following section.

Long-Term Vision for Capitol Corridor Segments

Following are descriptions of the major long-term capital needs in each primary segment of the Capitol Corridor. These capital needs combine travel time reduction methods with sea-level rise adaptations where practical for operational benefit. The capital needs identified assume track infrastructure from implementing short and medium term vision plan projects are completed.

Auburn to Benicia

This 90-mile-plus segment from the Sierra foothills across metropolitan Sacramento and the Central Valley will require additional capacity in order to accommodate growth in freight traffic from the Port of Oakland, and to reduce conflicts between that traffic and Capitol Corridor trains.

The 2007 Bay Area Regional Rail Plan led by MTC recommended a continuous third track; alternately, it might be possible to apply international best practice by “working backwards” from optimal schedules to more targeted strategic investments (to the extent that this is possible given interaction with UPRR). For instance, a series of sidings could be installed at strategic points, leading toward a third main track or even sidings or parts of a fourth mainline track. The Yolo Causeway is a significant section of elevated track where new support structures would need to be added to realize additional mainline track or long sidings. These sorts of investment can lead to greater frequency which can support skip-stop or express services that will drive overall travel time down. If clock-face “pulses” at major hubs are to be achieved, a high degree of reliability will be essential. Fortunately, west of Sacramento much of the right-of-way is straight and level, there are relatively few grade crossings, and trains are already able to maintain top speeds over the long distances between stations. Utilization of PTC at 110 mph will be vital to achieve higher speeds. Especially as rolling stock is replaced, locomotives that can achieve 125 mph should be included to achieve even higher speeds, coupled with the attendant grade crossing alterations.

Electrification would only be a possibility if Capitol Corridor trains were built on their own tracks in the existing right-of-way. This would be a very expensive proposition and so it would be one of the furthest out options to pursue. Super elevation of tracks, another tactic, would not likely yield many benefits due to the already largely straight track sections between Sacramento and Benicia.

To address the impacts of sea-level rise, the tracks in the Suisun Marsh would require significantly wider track support beds to stave off the gradual process of subsidence, especially if more tracks are added. The wider beds will allow the placement of higher track elevations. As tracks are added, there is a significant risk of such the overall track infrastructure (the beds) going outside the existing UPRR right-of-way. Tidal water function would have to be addressed as a pre-requisite of widening and effectively creating barriers due to wider track beds.

Additional stations have also been proposed in this segment. New stations should only be added where supportive land use policies are in place to generate additional ridership, and local partners must understand that express and limited-stop trains may bypass their stations.

Finally, extension of the Capitol Corridor to the east, toward Truckee and potentially Reno, has been a part of previous plans. Any such extension should be subjected to careful analysis of costs and benefits, as the market for travel between the Lake Tahoe area, Sacramento and the Bay Area is largely recreational and limited. If a future Olympic games were to come to the Lake Tahoe area, that event might be the appropriate driver for such analysis.

In progressive order, the following long-term capital investments are suggested broken down by smaller segments:

Segment	Order	Improvement
Auburn to Roseville	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Upgrade crossings with PTC to achieve 125 mph where possible
Roseville to Sacramento	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Assist with 4 th mainline track and universal crossovers to gain additional peak period capacity. Seek an additional 5 round trips (10 additional daily trains) to increase frequency per Sacramento to Benicia segment.
	#3	Upgrade crossings with PTC to achieve 125 mph where possible
Sacramento to Benicia	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Add strategic sidings or 3 rd mainline track to gain frequencies which will allow additional trips during peak morning/evening hours; in Solano Marsh elevate track to maintain function against rising sea levels. Seek an additional 8 round trips (or 16 daily trains) for half-hour headways during peak periods (23 round trips or 46 daily trains).
	#3	Initiate express/skip stop service using new frequencies to create faster travel time for peak period trains.
	#4	Upgrade crossings with PTC to achieve 125 mph where possible

Benicia to Richmond

The long-term prospect for Benicia to Richmond is the segment the most in need of significant future modification and it unlocks the best chance for speed increases because it is presently the most speed restricted. It has already been stated that sea level rise along the existing Martinez to Richmond alignment used today will be subject to ever increasing disruption due to rising sea levels (exacerbated during storm and high tide events). Plus, this alignment with its many curves is the slowest but most frequently travelled portion of the corridor today. As well, this segment contains the Benicia rail drawbridge, which is raised on a regular basis causing train delays for shipping traffic. All of these factors must be overcome or faster and more frequent trains become meaningless. This area is the most important long-term fix for Capitol Corridor service and has the added benefit of being able to be developed and eventually constructed while operations are maintained on the corridor in use today.

The key investment that must be made is in a new high-level crossing of the Carquinez Strait. The existing Benicia drawbridge was built in 1930 and provides just 70 feet of clearance at high tide. Trains are delayed whenever the bridge must be raised, and a collision damaging the span could have severe impacts, potentially disrupting service for a period of years. The approaches, and not so much the new bridge, are the primary challenge to replacing the existing drawbridge with a new high-level crossing. With UPRR subject to the same impacts of sea level rise, it would be likely that the approaches to such a new bridge would be at a grade acceptable to freight rail, a grade requiring much more room for rise to the new bridge than for passenger rail alone. On the Benicia side of the Carquinez Strait, there is more physical space to raise the grade. On the Martinez side, this is not as feasible with the geography and built environment. It may become

necessary to also relocate the Martinez station (the current Martinez station is quite vulnerable to sea level rise as well) to a higher location, on a different track alignment than today. Once at this level, the issue of avoiding sea level rise and a significantly faster travel time to Richmond can, in concept, be realized by keeping the new route high and explore several available options (new track, additional sidings, and tunnel construction) to accommodate increased and faster train traffic.

The cost to complete the projects in this segment could run into the billions of dollars but the benefits could be substantial, not only to Capitol Corridor and even the blended passenger rail service, but also to freight rail service. There is simply no other way to deal with existing tracks that are just a few feet above the existing waterline, subject to accelerated erosion and closure and even if tracks were raised in place and protected somehow from future storm and tidal intrusion, there would still be the issue of the slow, curvy alignment. Combine these elements, and a higher, straighter, and more direct alignment appears to be the best option to significantly change the long-term course for Capitol Corridor service.

Such an improvement must be done considering the accommodation of additional peak-hour trains on the north and south of this segment. A total of 23 daily round trips with additional half-hour headway service in the peak periods will align this segment along the core of the Capitol Corridor route.

Analysis of the alternatives that might exist in this segment for straighter track options outside of the effects of sea level rise is well beyond the scope of this document. However, a number of options can be said to exist:

- One, use of parts of the single-track Burlington Northern Santa Fe (BNSF) right-of-way between Martinez and North Richmond, with a variety of upgrades to ensure all parties have sufficient track capacity.
- Another option, deviating from the existing alignment at Martinez, would require substantial tunneling through the Crockett Hills. However, much of the alignment could parallel Highway 4 and Interstate 80, and this option would benefit Amtrak San Joaquin trains, which currently share the UPRR/Capitol Corridor right-of-way west and south of Martinez to Oakland.
- A third option, a new alignment paralleling Interstate 80 between Fairfield and Richmond, was studied as part of the Regional Rail Plan, and ultimately was not recommended due to its high cost. However, it was found to have strong ridership potential, in part because it would serve the City of Vallejo, with its more than 100,000 residents. An I-80 alignment might have to bypass the existing Richmond Station and BART connection; however, MTC's Transit Sustainability Project proposed a new BART station at I-80, a concept now under study by BART.
- A fourth option would be similar to the above, but would use the existing UPRR right-of-way paralleling Highways 12 and 29 between Suisun City and Vallejo.

Segment	Order	Improvement
Benicia to Richmond	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Build replacement high-level bridge and replace via a new high level crossing; relocate the Martinez station to the alignment; utilize new, faster track alignment (depending on which alternative is deemed optimal). Add strategic sidings or 3 rd mainline track to gain

		frequencies which will allow additional trips during peak morning/evening hours. Seek an additional 8 round trips (or 16 daily trains) for half-hour headways during peak periods (23 round trips or 46 daily trains).
	#3	Use route improvements to implement a clock-face schedule
	#4	Upgrade crossings with PTC to achieve 125 mph where possible, and revise the clock-face schedule

Richmond to Oakland

UPRR and the Port of Oakland are already planning to “four-track” this busy segment, providing additional capacity for cargo as well as passenger service. The 2007 Regional Rail Plan recommended “overlay” service between Hercules and Oakland or Union City. Such service might be operated using diesel multiple unit (DMU) trainsets like those that will be used on the Sonoma Marin Area Rail Transit (SMART) line in the North Bay.

For such service to be effective, it would be essential that connections to BART be improved. Transfers can currently be made between Capitol Corridor and BART trains at Richmond and at Oakland Coliseum, providing access to San Francisco for Capitol Corridor riders. However, access could be greatly improved by a connection between the Capitol Corridor and BART in West Oakland. The Regional Rail Plan recommended a “people mover” connection from the existing West Oakland BART Station to a new Capitol Corridor station nearby. BART is now studying an alternative, a new intermodal station where BART’s tracks pass directly over those of the Capitol Corridor, on Port of Oakland property next to Interstate 880. Travel times on BART from this site to Downtown San Francisco would be approximately six minutes, and travel times to Emeryville on the Capitol Corridor would be roughly equivalent, providing access to that increasingly important destination for BART riders. If clock-face schedules were introduced, this would be an ideal location for Capitol Corridor trains to arrive and depart on the hour, half-hour and/or quarter-hour.

A third major project in this segment would eliminate the existing street-running segment along Embarcadero in Oakland’s Jack London Square area. As this former warehouse district has evolved into a mixed-use urban neighborhood featuring a regional retail destination, traffic and pedestrian volumes have increased. Unfortunately, tunneling would likely be impossible due to engineering issues (the alignment is adjacent to the Oakland Estuary, and two auto tunnels, the Posey and Webster Tubes, are at a relatively shallow depth). The only hope for elimination of this segment might be a future reconstruction of Interstate 880 including new right-of-way for rail. The freeway is just a few blocks to the north, and a station located near I-880 and Broadway could provide improved access to the core of Downtown Oakland.

Other than the spot adjustments to deal with the effects of sea level rise, there are no other alignment options which suggest re-routing the Capitol Corridor to allow for faster travel times. The additional frequency would align with the corridor sections to the north and south which would add half-hour travel in the peak periods for a total of 23 daily round trips.

Segment	Order	Improvement
Richmond to Oakland	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Implement track raising and protection strategies for isolated areas of

		tracks impacted by sea level rise. Add strategic sidings, 3 rd or 4 th mainline track to gain frequencies which will allow additional trips during peak morning/evening hours. Seek an additional 8 round trips (or 16 daily trains) for half-hour headways during peak periods (23 round trips or 46 daily trains).
	#3	Use route improvements to implement a clock-face schedule
	#4	Upgrade crossings with PTC to achieve 125 mph where possible, and revise the clock-face schedule

Oakland to San Jose

The short-term project between Oakland and San Jose will lay the foundation for the long-term prospects for Capitol Corridor service. A shift in the short-term from the Niles Subdivision to the Mulford Subdivision will represent a key first step in reducing travel time. Presuming that to have been made in the short-term, the remaining long term aspects of Capitol Corridor service in this corridor will mostly require various location-specific adjustments due to the effects of sea level rise. There are specific vulnerable areas which will require coordination with adjacent land uses and owners but there are no issues which would suggest a new alignment should be considered and no realistic speed options to pursue in a joint improvement. The usual speed enhancements available through next evolution of PTC implementation. The additional frequency sought would boost frequency to half-hour headways in the peak travel times for a total of 23 daily round trips.

Segment	Order	Improvement
Oakland to San Jose	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Implement track raising and protection strategies for isolated areas of tracks impacted by sea level rise. Add strategic sidings, 3 rd or 4 th mainline track to gain frequencies which will allow additional trips during peak morning/evening hours. Seek an additional 8 round trips (or 16 daily trains) for half-hour headways during peak periods (23 round trips or 46 daily trains).
	#3	Use route improvements to implement a clock-face schedule
	#4	Upgrade crossings with PTC to achieve 125 mph where possible, and revise the clock-face schedule

San Jose to Salinas

There are no new alignments suggested for this route which would suggest higher travel times might be available. Except for the marsh areas near Watsonville, there are no sea-level rise issues which could be combined into an alignment and sea-level rise adjustment. The frequency goal for this corridor will be to achieve an additional 4 daily round trips to supplement 6 round trips already identified in the short-term vision for a total of 10 daily round trips.

Segment	Order	Improvement
Oakland to San Jose	#1	Utilize PTC to achieve 110 mph where possible (application limited due to geography)
	#2	Implement track raising and protection strategies for isolated areas of

		tracks impacted by sea level rise. Add strategic sidings, 3 rd mainline track to gain frequencies which will allow additional trips during peak morning/evening hours. Seek an additional 4 round trips (or 8 daily trains) for half-hour headways during peak periods (20 round trips or 40 daily trains).
	#3	Use route improvements to implement a clock-face schedule
	#4	Upgrade crossings with PTC to achieve 125 mph where possible, and revise the clock-face schedule

Policy, Advocacy, and Vision Plan Implementation

Three primary themes carry this Vision Plan Update and imply policy, advocacy, and implementation responsibilities that include:

- Supporting the evolving economic and social linkages of the northern California mega-region
- Integrating Capitol Corridor into the blended passenger rail operations of California
- Directing long-term service frequency change that respects a changing environment (with sea level rise) while speeding up the service and improving intermodal connectivity

Policy leadership is required for four critical aspects of the vision plan. These areas where more analysis supporting policy formation, sometimes with implications for governance, are as follows:

- Blended IPR service
- Service extensions
- Sea-level rise
- Long-term service route modifications and upgrades, including advocacy leadership

Implementation in the short, medium, and certainly in the long-term will require appropriate analysis. CCJPA is familiar with these efforts which include ridership and revenue analysis, but also rail network modeling. Where CCJPA is in uncharted waters is with sea level rise. Adapting to sea level rise itself will require an unprecedented and step-wise series of studies and analysis. As indicated previously, the opportunity to couple a sea-level adaptation response with possible track realignment and decreased travel time is a potential, however the pairing of these things will require much more analysis. Policy decisions that may be considered in the short term might have large implications on long-term Capitol Corridor prospects so it is imperative that an effort to analyze and conduct feasibility studies for the variety of long-term options for the Capitol Corridor is commenced in the near term. This information will help policy makers of today handoff a solid Capitol Corridor service to policy makers twenty or more years from now.

Advocacy is necessary policy maker role. As the most recent rather capital funding stagnant decade indicates, without strong advocacy the Capitol Corridor service will not grow to meet the transportation demands of future generations. The megaregional perspective and generational changes all point to a transportation system that needs to better marry land-use with non-auto mobility options. Capitol Corridor is in an enviable position today for these emerging changes thanks to leadership advocacy from the early years of Capitol Corridor service growth. But that won't carry Capitol Corridor service forward to answer future mobility needs and expectations.

The steps to transform Capitol Corridor service expressed in this Vision Plan Update will require steady but also renewed advocacy leadership to catalyze the planned transformation.

Monthly Station Ridership Activity
Capitol Corridor Train Service
FFY 2013 - 2013 Cumulative FFY Year-to-Date: September 2013

Station	FFY 2013 - 2013						% Change from last FFY		Weighted Boardings	Weighted Alightings	Weighted Total
	Boardings	Alightings	Total	Boardings/ day	Alightings/ day	Total/ day	Boardings	Alightings			
ARN	1859	1926	3785	61.97	64.20	126.17	69.8%	73.7%	61.97	64.20	126.17
RLN	962	992	1954	32.07	33.07	65.13	-26.4%	-23.7%	32.07	33.07	65.13
RSV	1509	1556	3065	50.30	51.87	102.17	-19.7%	-17.5%	50.30	51.87	102.17
SAC	33916	32982	66898	1130.53	1099.40	2229.93	-3.8%	-3.0%	101.85	99.05	200.89
DAV	15892	15802	31694	529.73	526.73	1056.47	1.5%	1.4%	47.72	47.45	95.18
SUI	8035	8071	16106	267.83	269.03	536.87	8.0%	6.8%	24.13	24.24	48.37
MTZ	12444	13055	25499	414.80	435.17	849.97	3.6%	4.3%	37.37	39.20	76.57
RIC	8740	9072	17812	291.33	302.40	593.73	-1.7%	-1.0%	26.25	27.24	53.49
BKY	6062	6032	12094	202.07	201.07	403.13	-4.1%	-2.7%	18.20	18.11	36.32
EMY	13658	13677	27335	455.27	455.90	911.17	1.5%	-0.4%	41.02	41.07	82.09
OKJ	11819	12099	23918	393.97	403.30	797.27	2.2%	-0.2%	35.49	36.33	71.83
OAC	1804	2023	3827	60.13	67.43	127.57	32.2%	26.7%	9.25	10.37	19.63
HAY	1460	1561	3021	48.67	52.03	100.70	20.4%	25.8%	10.57	11.30	21.87
FMT	1346	1373	2719	44.87	45.77	90.63	-15.9%	-16.1%	9.74	9.94	19.68
GAC	5033	5011	10044	167.77	167.03	334.80	6.0%	7.6%	36.43	36.27	72.70
SCC	322	341	663	10.73	11.37	22.10	n/a	n/a	#Div/0!	#Div/0!	#Div/0!
SJC	7931	7219	15150	264.37	240.63	505.00	14.4%	12.2%	57.41	52.25	109.66
FFY Total or Average	132792	132792	265584	4426.40	4426.40	8852.80					

Members of Policy Boards: Capitol Corridor

Capitol Corridor Joint Powers Agency “Ad Hoc Committee” for Vision Plan and Service Planning¹

Director Cohn, Sacramento Regional Transit
Director Mallett, BART Board of Directors
Director Raburn, BART
Director Rose Herrera, Santa Clara Valley Transportation Authority
Director Rohan, Placer County Transportation Planning Agency

Capitol Corridor Joint Powers Agency Board of Directors

James Spering, Solano County Transportation Authority, Chairman
Jim Holmes, Placer County Transportation Planning Authority
Keith Nesbitt, Placer County Transportation Planning Authority
Dan Wolk, Yolo County Transportation District
Thomas Blalock, BART Board of Directors
James Fang, BART Board of Directors
Tom Radulovich, BART Board of Directors
Zachary Mallett, BART Board of Directors
Gail Murray, BART Board of Directors
Robert Raburn, BART Board of Directors
Jack Batchelor, Jr., Solano County Transportation Authority
Oscar Villegas, Yolo County County Transportation District
Steve Hanson, Sacramento Regional Transit
Xavier Campos, Santa Clara Valley Transportation Authority
Steve Miller, Sacramento Regional Transit (Alternate for Director Cohn)
Susan Rohan, Placer County Transportation Planning Authority(Alternate for Director Nesbitt)
Lucas Frerichs, Yolo County Transportation District (Alternate for Director Villegas)

¹ Per Item VI.5, September 18, 2013 CCJPA Agenda



**MINUTES OF THE REGULAR MEETING OF THE
CITY OF HAYWARD PLANNING COMMISSION
Council Chambers
Thursday, December 5, 2013, 7:00 p.m.
777 B Street, Hayward, CA94541**

MEETING

A regular meeting of the Hayward Planning Commission was called to order at 7:00 p.m. by Chair Faria.

ROLL CALL

Present: COMMISSIONERS: Loché, Trivedi, McDermott, Lamnin, Lavelle
CHAIRPERSON: Faria
Absent: COMMISSIONERS: Márquez
CHAIRPERSON: None

Commissioner Trivedi led in the Pledge of Allegiance.

Staff Members Present: Ajello, Conneely, Lt. Martinez, Madhukansh-Singh, Siefers

General Public Present: 8

PUBLIC COMMENTS

None

PUBLIC HEARING

1. Conditional Use Permit - Request for approval of a Conditional Use Permit (PL-2012-0414 CUP) to operate a distillery with an ancillary tasting room, and a Conditional Use Permit (PL-2013-0489 CUP) for a separately licensed and operated retail establishment that will sell products produced by the distillery at 22549 Foothill Boulevard in the Central City Commercial (CC-C) Zoning Sub-district. The Proposed Project is Categorically Exempt from Environmental Review in Accordance with the California Environmental Quality Act (CEQA) Guidelines, Section 15303(c), New Construction or Conversion of Small Structures – Geoff Harries, Buffalo Bill’s Property Group (Applicant/Owner)

Associate Planner Ajello provided a synopsis of the staff report. She noted that recent changes to the state law would permit an ancillary tasting room to the distillery, where the applicant could charge patrons of the establishment a tasting fee. Associate Planner Ajello emphasized that the operator of Russell City Spirits retail store will have to purchase the finished products of Russell City Distillery directly from the wholesaler, and that the applicant could not sell products straight from the production room of the distillery.

Associate Planner Ajello confirmed for Commissioner Lamnin that the applicant would be responsible for lighting around the premises of the establishment which will help ensure that patrons of the business have ample lighting to safely enter and exit the establishment and noted that the City would be responsible for the maintenance of Municipal Parking Lot No. 2.

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Commissioner Lamnin pointed out to staff that Condition of Approval No. 42, Utilities, contained language that described the proposed project as a restaurant and bar. Associate Planner Ajello responded that references to a restaurant and bar component were erroneously included in the report and should be removed.

Commissioner Lavelle asked staff if the applicant would need to obtain City approval for crafting a spirit which was not included in the list of liquors included in the project plans. Associate Planner Ajello indicated that no further approval from the City would be required if the establishment sought to distill a spirit other than what was provided in the project plans; however, she stated that the applicant would need to be properly licensed through the California Department of Alcohol Beverage Control (ABC).

Commissioner Lavelle recommended that staff review the language in the Conditions of Approval referring to how much time the applicant had to pull building permits and commence operations after the approval of the Conditional Use Permit, and also how much time the applicant had to file for an extension. She commented that the current language was confusing and that the conditions of approval be written to grant the applicant flexibility in the event that the applicant face challenges in commencing business.

In response to Commissioner Lavelle's question regarding Condition of Approval No. 12, Associate Planner Ajello said that the lease agreement between the applicant and the lessee of the retail store would have to be submitted to the Planning Manager and to the Hayward Police Department. She underscored that ABC had strict regulations that there be an independent operator of the retail store.

Associate Planner Ajello noted for Commissioner Lavelle that if the applicant wanted to increase the production capacity of the distillery, he would be permitted to do so as long as the hours of operation of the distillery were not modified and approval had been obtained from the Development Services Director. She shared that the applicant had provided staff with the figure of 7,000 cases as the production capacity of the distillery.

Commissioner Lavelle asked staff if there was an age requirement for individuals visiting the tasting room. Assistant City Attorney Conneely responded that staff was unaware if there was a State law addressing age restrictions for distilleries; however, she mentioned that the Planning Commission could add a condition of approval setting an age limit for the establishment.

Associate Planner Ajello noted for Commissioner Lavelle that the applicant would have to check with the State to see if it would be permissible for the proposed establishment to provide food items such as crackers, to patrons in the tasting room. She shared that State law prohibits the distillery from operating a distillery that is attached to a restaurant.

In response to Commissioner Trivedi's question, Associate Planner Ajello indicated that it was an ABC requirement that distilled spirits be sold in bottles or containers that are 750 ml in size or greater.

Commissioner McDermott recommended that staff look into whether there was an age restriction for entering the tasting room at Doc's Wine Shop.



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Associate Planner Ajello clarified for Commissioner McDermott that the Foothill Boulevard entrance into the establishment would not be accessible to the public and she further noted that customers would be able to utilize the entrance from the Municipal parking lot.

Commissioner McDermott expressed concern about the self-closing fire doors that the establishment would have and asked staff if there would be another way for individuals to evacuate the building in the event of a fire. Associate Planner Ajello stated that the applicant had worked extensively with the Hayward Fire Department (HFD) in meeting fire safety requirements and indicated that she would confirm with the HFD.

In response to Commissioner McDermott's question on what types of chemicals would be located inside the establishment Associate Planner Ajello commented that the applicant will submit a chemical inventory list which will be reviewed by the HFD.

Associate Planner Ajello stated that the five year moratorium on trenching commenced after the completion of construction on Foothill Boulevard.

In response to Commissioner Loché's question, Associate Planner Ajello said that the recent amendments to the City's alcohol regulations now permit a distillery in the downtown with a CUP. She noted that the change in the State law enabled distilleries to charge a nominal fee for services offered in ancillary tasting rooms.

Commissioner Loché expressed that individuals tasting the distilled spirits be required to present their identification card in the tasting room in order to ensure that they are of legal drinking age.

In response to Chair Faria's question, Associate Planner Ajello stated that regular inspections of the distillery and/or the retail was not required by the CUP; however, she noted that the Health Department might conduct inspections since the product being produced was a consumable good. She confirmed for Commissioner Loché that two letters of support for the proposed establishment were received by staff.

Chair Faria opened the public hearing at 7:35 p.m.

Mr. Geoff Harries, applicant of the proposed distillery, indicated that distilled spirits contained within a 750 ml bottle would cost no more than \$25 per bottle. He shared that the distillery may produce spirits contained in bottles smaller than 750 ml; however, these products would not be sold at Russell City Spirits.

Mr. Harries stated that he always had an interest in producing distilled spirits; however, federal and state laws previously were too stringent for craft distilleries and he shared that recent changes in the law led him to pursue operating the business. He stated that the idea to distill in Hayward came to fruition due to his love for botanical sciences and the craft of distilling. Mr. Harries noted that Hayward has great resources, primarily the quality of its water, and that this is one reason why his

distillery will be able to produce a great product locally. He commented that there was an upcoming trend in the Bay Area supporting distilled spirits crafted locally.

Mr. Harries noted for Commissioner Trivedi that he was comfortable with the language in the CUP stating that the distillery could produce up to 7,000 cases of distilled spirits annually. He added that if needed, additional cases of distilled spirits could be produced at a manufacturing site located elsewhere, which was the same practice that Buffalo Bills Brewery does at the moment. Commissioner Trivedi encouraged that if the applicant is interested in expanding the distillery in the future by crafting spirits at a different facility, then the applicant consider a manufacturing site within Hayward.

In response to Commissioner Trivedi's question, Mr. Harries said that in order to serve products of his distillery at his restaurant, Buffalo Bills Brewery, spirits manufactured by the distillery would have to be sold to the wholesaler, and then this would have to be shipped back to the restaurant. Mr. Harries shared that he is interested in crafting rested products and described that among the spirits distilled would be an agave product and white whiskey. He pointed out that one of the reasons his establishment's products will not be as aged is due to the space limitation. Mr. Harries emphasized that in order to have continued patronage at his establishment there will be a huge variety of spirits produced at his distillery and that there would be continued innovation in crafting these products.

Mr. Harries shared that the staff at the distillery would consist of a sales team and a logistics team, a master and assistant distiller, and a liaison with the consumer that will work in the tasting room. He commented that under current laws, even though he will be able to have consumers taste the distilled spirits, he is unable to sell those products at his distillery. He described that instead, the process is that products manufactured at the distillery have to be sold and shipped to an independent wholesaler. After which, the wholesaler that Mr. Harries works with will re-sell the products back to the Russell City Spirits retail store. Mr. Harries pointed out that because he is unable to sell the products at the distillery, the current legal limitations result in a loss of 40% sales profit.

Commissioner Trivedi asked the applicant to describe what the appearance of the distillery would be like since the Planning Commission did not have too many details available to them on the floor plans and layout of the establishment. Mr. Harries shared that the still would be situated at the front of the building and would be 17 feet tall. He added the appearance of the distillery will be minimalist and will consist of black and white décor; it will be architecturally very clean and will have an industrial look that is consistent with the industrial Russell City theme.

In response to Commissioner Trivedi's question about the security system at the distillery, Mr. Harries stated that the premises would have 24 hour surveillance and that the facility would be equipped with four cameras. He added that the public would only be permitted inside the distillery during guided tours.

In response to Commissioner McDermott's question, Mr. Harries responded that the distillery's competitors could be perceived as other businesses engaged in entertainment, citing the movie theater as an example. He pointed out that a majority of the crafted spirits produced at the distillery will be shipped out and sold at other retail stores as well, thus the distillery's competition would be the other types of alcohols sold on the shelves of these stores.



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Commissioner Lamnin expressed appreciation to the applicant for his commitment to the local community and she commented that Buffalo Bills Brewery is an anchor business in the downtown area.

In response to Commissioner Lamnin's concerns about the security system at the distillery, Mr. Harries responded that in his twenty years of ownership of Buffalo Bills Brewery, he has learned that success in fighting crime and ensuring safety at his business was accomplished primarily by being vigilant.

Mr. Harries responded to Commissioner Lamnin's question by stating that he has not yet selected the entity that will operate the retail store located within the distillery because he was waiting to acquire the City's approval of the CUP. Mr. Harries expressed his gratitude to staff for including in the conditions of approval that Russell City Spirits be limited to only selling Russell City Distillery products.

Commissioner Lavelle shared that of the 32 craft distilleries in California, there are six located within the Bay Area. She asked the applicant if the establishment would be interested in providing food items such as bread and crackers in the tasting room. Mr. Harries indicated that he favored having such food items in the tasting room for palate cleansing as this would enhance the tasting experience; however, he would have to look into whether this would be legally permissible.

Chair Faria asked the applicant what his projected timeline was for opening the facility. Mr. Harries responded that upon approval of the CUP, his plan was to move along quickly in opening up the business.

Mr. Kim Huggett, President of the Hayward Chamber of Commerce, supported the proposed establishment underscoring that it would enhance and rejuvenate downtown Hayward as a destination location. He shared that the crafting of distilled spirits was an industry founded in California and he pointed out that the proposed distillery would add a stylish storefront to a building that currently sat vacant.

Chair Faria closed the public hearing at 8:08 p.m.

Commissioner Lamnin asked staff if the fire service line had already been installed considering the current moratorium on trenching on Foothill Boulevard. Associate Planner Ajello responded that it has not yet been installed and that the Public Works staff was looking into alternative ways in which the fire service line could be installed. Ms. Ajello further noted that the installation of the fire service line would not cause a delay in moving forward with the proposed establishment. Commissioner Lamnin made a motion to approve staff's recommendation with the following amendments: that Condition of Approval No. 12 be modified to include that the proposed lease agreement be submitted to the Planning Manager and that all references to a restaurant in the CUP be removed; and direction to staff to revise the "request for a one-year extension" submission date

from “15 days prior to December, 5, 2014” to “15 days prior to December 5, 2016” and directed staff to review the proposed project’s security plan.

Commissioner McDermott seconded the motion.

Commissioner Trivedi supported the proposed project stating that the distillery will help revive the downtown area as it offers another form of entertainment to the community. He pointed out that the business will create jobs in the City and that the establishment will feature another product made in Hayward.

Chair Faria supported the establishment and thanked the applicant for opening another business in Hayward, for his involvement in the local community and for making downtown Hayward a better place.

AYES:	Commissioners Loché, Trivedi, McDermott, Lamnin, Lavelle Chair Faria
NOES:	None
ABSENT:	Commissioner Márquez
ABSTAINED:	None

COMMISSION REPORTS

4. Oral Report on Planning and Zoning Matters

Planning Manager Siefers reported that the Libitzky Property Companies and Integral Communities Project are on the agenda for the December 19, 2013 Planning Commission meeting. She noted that it was staff’s hope that both developers provide plan sets that are complete and reflect what the Planning Commission has requested. Ms. Siefers shared that there would be three final subdivision maps going to the Hayward City Council at an upcoming meeting in December. She added that staff has received a subdivision map for Village E of the Stonebrae development. Ms. Siefers noted that staff is searching for a new Development Services Review Engineer to take the position of Mr. John Nguyen who will be retiring and noted that Assistant Planner Michael Christenson is a new employee in the Planning Division.

5. Commissioners’ Announcements, Referrals

Chair Faria expressed sorrow at the passing away of Mr. Nelson Mandela and recognized his work and the significant impact that Mr. Mandela had made on the world. She also mentioned that clothing and shoe collection boxes were springing up in various locations in the City and commented that individuals go through these boxes and as a result, leave items strewn throughout parking lots. Chair Faria asked staff if there was a way to regulate where the collection boxes are located. Planning Manager Siefers responded that she will check with the Code Enforcement Division and will also look into the Zoning Ordinance for what the placement requirements are of the collection boxes.

Commissioner McDermott requested that in 2014, staff take into consideration that Planning Commission meetings do not coincide on the same night as City sponsored celebrations such as



**MINUTES OF THE REGULAR MEETING OF THE
CITY OF HAYWARD PLANNING COMMISSION
Council Chambers
Thursday, December 5, 2013, 7:00 p.m.
777 B Street, Hayward, CA94541**

Light Up The Season, which Planning Commissioners were unable to attend due to the scheduling conflict with the present Planning Commission meeting.

Commissioner Trivedi commented that the Planning Commission Agenda Packets published on the City's website were easily accessible to him and he preferred that the Agenda Packets be published on the website soon after being mailed out to the Planning Commissioners.

Commissioner Lamnin asked staff if Planning Commissioners could be sent an email alert when the Planning Commission Agenda Packets are posted on the website.

APPROVAL OF MINUTES

6. The minutes of October 24, 2013 were unanimously approved with Commissioner Márquez absent.
7. The minutes of November 7, 2013 were unanimously approved with one correction and Commissioner Márquez absent.

ADJOURNMENT

Commissioner Loché shared a quote of Mr. Nelson Mandela and stated that even though Mr. Mandela has passed away, he hoped that we, as a society, could continue to draw inspiration from the life of Mr. Mandela.

Chair Faria adjourned the meeting in memory of Mr. Nelson Mandela at 8:26 p.m.

APPROVED:

Dianne McDermott, Secretary
Planning Commission

ATTEST:

Avinta Madhukansh-Singh, Senior Secretary
Office of the City Clerk