



CITY OF HAYWARD
AGENDA REPORT

Meeting Date: 06/14/07

Agenda Item: 2

TO: Planning Commission

FROM: Richard E. Patenaude, AICP, Principal Planner

SUBJECT: **General Plan Amendment Application No. PL-2007-0113, Zone Change Application No. PL-2007-0114, Tentative Tract Map Application No. PL-2007-0112 TTM 7893, and Abandonment of Portions of the Traynor Street and Orchard Avenue Rights-of-Way – Citation Homes Central (Applicant/Owner) - Request to Amend the General Plan Land Use Designation from Mixed Industrial to Medium-Density Residential, to Change the Zone District from Industrial (I) District to Planned Development (PD) District, and to Abandon Public Streets and Subdivide Land Within the Subject 5.68-Acre Site in Order to Develop 80 Homes**

The Project Site is Located Generally at the Intersection of Orchard Avenue and Traynor Street, Westerly of Soto Road

RECOMMENDATION:

Staff recommends that the Planning Commission recommend that the City Council adopt the Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program and approve the General Plan Amendment, Zone Change, Preliminary Development Plan and Tentative Map, and find that the rights-of-way abandonment is consistent with the General Plan, subject to the attached findings and conditions of approval.

BACKGROUND:

The subject property is comprised of 5.68 acres, generally developed with heavy commercial and industrial uses. The subject property is zoned Industrial (I) District, and the existing City General Plan designation is Mixed Industrial.

The site is bordered by Jackson Street along its northerly border and by the Union Pacific railroad tracks along its westerly edge. Both of these transportation corridors produce significant noise levels. However, effects of the noise can be mitigated to a level that is suitable for residential development with the provision of sound walls and the use of appropriate construction methods and materials.

Project Description

The proposal would entail construction of 80 three-story ownership detached homes on lots of approximately 1,472 square feet. The proposed density is 14 units per net acre, which would comply with the proposed General Plan designation of Medium-Density Residential (8.7-17.4 units per net acre). The units would be located along private streets generally running parallel to each other in an east-west direction. The portions of existing streets, Traynor Street and Orchard Avenue, within the project boundaries would be abandoned and acquired from the City. Public access to the project would be from a new westerly terminus of Orchard Avenue; a new westerly terminus of Traynor Street would provide emergency vehicle access only.

Parking is proposed at an overall ratio of slightly more than 3.0 spaces per unit, with all units containing standard two-car garages. All units would have garages fronting onto private "alleys." Homes would have front entrances facing common walkways or paseos that would run between rows of units. The developer would improve the portion of Orchard Avenue, leading to the entrance of the project, with curb and gutter, such that approximately eight parking spaces could be provided along the street's westerly side.

The project provides useable open space in excess of the 28,000 square feet required by the Zoning Ordinance. The project would provide a total of 49,330 square feet of open space with a primary group space of 16,030 square feet and 6,360 square feet in smaller spaces; the project would be credited with 26,940 square feet of open space for its ground-level private courtyards.

The living areas of the homes would range in size from 1,725 to 2,060 square feet. The homes consist of three different plans in three different architectural styles (Cape Cod, Craftsman and Monterey), as described below:

- ◆ Plan "1" (21 units) would consist of 3 stories, 3 bedrooms and 2½ baths. They are the smallest of the units with 1,725 square feet of living area and would have the garage and bonus room on the ground floor, the main living area and kitchen on the second floor, and bedrooms located on the third floor.
- ◆ Plan "2" (28 units) would be a three-story unit of 1,900 square feet with 4 bedrooms and 3½ baths. The garage and one bedroom and bath would be located on the ground floor, the main living area and kitchen would be on the second floor, and three bedrooms and 2 baths would be located on the third floor.
- ◆ Plan "3" (31 units) would be a three-story, 3,060-square-foot unit containing 4 bedrooms and 3½ bathrooms. The garage and one bedroom and bath would be located on the ground floor, the main living area and kitchen would be on the second floor, and three bedrooms and 2 baths would be located on the third floor.

All units are proposed with stucco exteriors, tile roofs and wood porch railings. The Cape Cod style would add wall sections of wood lap siding, decorative window shutters, flat tile roofs, outlookers, decorative louvered vents, and stucco columns. The Craftsman style would add gable end sections of wood lap siding, flat tile roofs, potshelves, and wood posts. The Monterey style would feature profile tile roofs, decorative window shutters, vents and corbels, and wood posts. Staff is recommending, as a condition of approval, architectural enhancements, such as

additional details and greater use of different materials, to provide more interest and variety where the side elevations of the residences face a side street and along rear elevations facing streets where residences are provided garage access. A condition of approval would require that the plans and architectural styles be alternated to prevent a repetitious appearance in the streetscape.

The sewer and water mains would be located within public utility easements in the private streets and would be maintained by the City. The primary storm drain main within the project would be public as it would carry run-off from Traynor Street; new extensions of the storm drain within the project's private streets would be maintained by the homeowners association.

DISCUSSION AND ANALYSIS:

General Plan Amendment (see Attachment B)

The General Plan identifies the community's environmental, social and economic goals, and states the City policies on the location and characteristics of future development. Therefore, when assessing the appropriateness of amending the General Plan, identifying the City's overall goals and policies is a significant consideration, as are the characteristics of the land and its surroundings. Staff's discussion and analysis in the context of these policies follows.

Land Use Designation

The project site is currently designated "Mixed Industrial." This designation is given to properties that contain older industrial uses within the central part of the City, typically along railroad tracks and often surrounded by residential areas as the City has expanded. The General Plan indicates that future uses for these properties must be compatible with adjacent residential and commercial uses. The proposed medium-density residential project would provide a use more compatible with nearby residential and commercial uses and an appropriate transition from the single-family residential developments to the south and the commercial corridor along Jackson Street to the north.

Design

The development will be compatible with surrounding residential land uses in the Jackson Triangle neighborhood and, as conditioned, result in a high-quality project, consistent with City policy that indicates the City should, "Employ sound planning principles to promote a balance of land uses and achieve a vibrant urban development pattern that enhances the character of the city" (Land Use Section, Balance of Land Uses – Policy 1).

Housing

Relevant General Plan policies state that the City should, "Encourage the provision of an adequate supply of housing units in a variety of housing types which accommodate the diverse housing needs of those who live or wish to live in the city" (Housing Section, Expand the Housing Supply, Policy 1); "Encourage the development of ownership housing and assist tenants to become homeowners in order to reach a 70% owner-occupancy rate, within the parameters of Federal and State housing law" (Housing Section, Preserve Owner-Occupied Housing, Policy 3),

and should “Ensure that the City's housing stock contains an adequate number of decent and affordable units for households of all income levels” (Housing Section, Develop Affordable Housing, Policy 4). The development also will further the City's goal to provide ownership housing opportunities, as well as opportunities within the City for moderate-income households.

The City's Inclusionary Housing Ordinance requires that this development provide 15% of the units as affordable to moderate-income households (120% of the area median). A family of four with a household income of less than \$100,600 would qualify as a moderate-income household. For the proposed development, 12 units are required. The required Inclusionary Housing Plan has been submitted and would have to be approved by the Planning Director as part of the Precise Plan.

Parks

The City's park dedication ordinance indicates 5 acres of parkland should be provided for every 1,000 people. Per the City's park dedication ordinance, the development would be required to dedicate 1.23 acres of parkland, pay \$956,240 in park dedication in-lieu fees, or do a combination of both. The proposed project would not entail dedication of any on-site parkland but rather the project proponent would pay the full in-lieu fee amount. However, the project would provide over 1½ times the amount of required useable open space.

The entrance to the project is approximately one-fifth mile from John Muir Elementary School at Orchard Avenue and Soto Road. Although there is no formal agreement between the school district and the Hayward Area Park and Recreation District (HARD) regarding joint use of the property, the school has offered after-school recreation programs and informal use of the open grounds by area residents. HARD is not interested in the development of a park in the location of this project as it would not be of a size, or in a location, that could adequately serve the Jackson Triangle neighborhood. HARD's nearest park facility is the Eden Greenway, slightly more than one-half mile from the project. HARD has used park dedication in-lieu fees for improvements to this facility.

Schools

The project is located within the Park Elementary School, Winton Middle School and Mt. Eden High School attendance areas. The development would be expected to generate approximately 13 elementary students, 5 middle school students and 6 high school students. The School District has undertaken a comprehensive study of their school facilities and needs, and, with declining enrollments, the District is moving toward promoting fewer, larger schools. State law prohibits denial of a housing development based on lack of adequate school facilities. Rather, school impact fees (currently at \$2.62 per square foot) are intended to satisfy the developer's obligation for schools.

Parking

The City's parking standards and practice require that single-family units (detached) provide four parking spaces: two in the garage and two in the driveway. An additional space along the street is also typically expected. Multi-family units (attached) consisting of two or more bedrooms are required to provide 2.1 spaces. The City's parking standards do not distinguish between traditional detached single-family lot subdivisions and small-lot single-family subdivisions, but

rather distinguish between single-family (detached) and multi-family (attached) units. Small-lot single-family developments, such as the one proposed, have not provided parking that is commensurate with standards established for the conventional subdivisions due to increased density and small lot size; nevertheless, they typically contain as much square footage and as many bedrooms as standard single-family dwellings. One difference is that small-lot single-family developments typically have CC&Rs which contain prohibitions against using garages for general storage or some other use that would preclude their use for parking vehicles. Staff is recommending that a similar restriction be incorporated into any approval of the proposed project.

Parking requirements that have been imposed by the City of Hayward on small-lot single-family development within recent years were between 2.25 and 5.3 parking spaces per dwelling. This hybrid "small-lot single-family" project proposes to provide 243 spaces, including 8 spaces along one side of Orchard Avenue at the entrance to the project. The resulting overall average parking ratio is slightly more than 3.0 spaces per unit. Staff would suggest that the proposed ratio is appropriate, based on observed parking for recent developments and would be in keeping with ratios of other similar developments approved in Hayward and in the region.

However, parking areas shown along the westerly and southerly boundaries of the site do not meet minimum requirements for landscaping. These areas are located at the entry to the project, and staff recommends, as a condition of approval, that the landscaping be brought to the minimum requirements to enhance the design and livability of the project. Landscape improvements could result in the loss of about six parking spaces, reducing the parking ratio to slightly less than the desired 3.0 spaces, or 2.96 spaces, per residence. However, as this project meets the requirements of the City's Inclusionary Housing Ordinance, it also meets the State Density Bonus law provisions, which requires the City to permit a reduction in the required number of parking spaces, in this case, to 190 (2 spaces for each 3-bedroom unit, and 2.5 spaces for each 4-bedroom unit).

Traffic

Traffic impacts associated with development in the area were analyzed as part of the Mitigated Negative Declaration. A required traffic study indicated that the proposed project would generate approximately 766 daily vehicle trips, with 60 trips during the weekday AM peak hour and 81 trips during the PM peak hour.

The project was not found to cause a significant impact at any of the eight study intersections. The most-congested intersection, the two-way stop-controlled intersection of Soto Road/Traynor Street, would experience minor additional delays, but the intersection would not meet the criteria to justify a traffic signal. The project has been designed to mitigate traffic impacts by limiting access to Orchard Avenue. The intersection of Orchard Avenue and Soto Road is signalized and provides options for access to Mission Boulevard, Harder Road and Jackson Street. Although the Mission Boulevard/Orchard Avenue intersection already operates at Level of Service (LOS) F (unacceptable to most drivers), the effect of the proposed project would be considered less than significant as only one-tenth of a second of delay would be added. The proposed Route 238 Corridor Improvements are designed to improve the operations at this intersection to LOS D during the morning peak, and to LOS E during the evening peak.

With the addition of project-generated traffic, the queuing along the northbound approach to the Soto Road/Jackson Street intersection would continue to exceed the available storage, and vehicles would continue to spill back into the Soto Road/Traynor Street intersection. The queue in the northbound left-turn lanes would increase from 325 feet to 350 feet per lane, or slightly more than one car length, during both peak hours. The queue in the northbound through-right-turn lane would increase from 550 feet to 575 feet (slightly more than one car length) during the AM peak hour, and from 800 feet to 850 feet (2½ car lengths) during the PM peak hour. The characteristics of the existing roadways do not allow for any physical changes to improve traffic movements, but the impact from this project is considered to be insignificant as the delay at the Soto Road/Jackson Street intersection would increase by only 3.2 seconds.

Zone Changes/Preliminary Development Plan

Since the homes proposed would not meet the minimum 20-foot front and rear yard setback standards, would not meet the minimum 5-foot side yard setback standard, would not meet the minimum 2,500 square foot size, which impacts lot coverage, the applicant is proposing a Planned Development (PD) District.

In order for a Planned Development District to be approved, certain findings must be made. Those findings and related discussion follow.

a. The development is in substantial harmony with the surrounding area and conforms to the General Plan and applicable City policies.

The project is consistent with the proposed General Plan designation of Medium Density Residential (8.7 to 17.4 units per net acre) in that the development is proposed at 14 units per net acre. Also, the project will further the City's goal to provide ownership housing opportunities as expressed in the Housing Element of the General Plan and will help further the City's Housing Element policy to provide housing opportunities for all household income levels in that 12 ownership housing units for moderate-income households would be provided in the City as a result of the project. The project would provide a buffer between the commercial uses along Jackson Street to the north and the standard single-family residential developments to the south.

b. Streets and utilities, existing or proposed, are adequate to serve the development.

Existing infrastructure and utility improvements would be adequate to serve the development. Also, the project, as conditioned, would have no significant impacts related to neighborhood traffic.

c. The development creates a residential environment of sustained desirability and stability, that sites proposed for public facilities, such as playgrounds and parks, are adequate to serve the anticipated population and are acceptable to the public authorities having jurisdiction thereon, and the development will have no substantial adverse effect upon surrounding development.

The Jackson Triangle neighborhood is deficient in park space. Future residents of the proposed project would not have access to, nor cause any impact upon, local park facilities because of the project's isolation. However, the project would provide its residents with a combination of both private and public internal recreation space. The open space provided would be nearly double that minimally required by City standards. In addition, the project would contribute nearly \$1 million of park dedication in-lieu fees to improve area park facilities.

- d. **Any latitude or exception(s) to development regulations or policies is adequately offset or compensated for by providing functional facilities or amenities not otherwise required or exceeding other required development standards.**

The reduced minimum lot sizes and yard setbacks are offset by the amount of private open space provided to the residents not realized by attached residential units. The reduced parking ratio is reflective of the small-lot design, is consistent with other similar small-lot developments approved recently in Hayward, and is allowed by State law.

Vesting Tentative Map

The project proponent proposes to subdivide 5.68 acres of land into 80 parcels for detached single-family units and common parcels for parking and landscaping. The parcels will be served by a system of private streets.

The streets will be of a design approved by the City Engineer and their improvements/sections will be constructed to the same standards as a public street. The homeowners' association created for this development would maintain the private streets and common areas, as well as private sanitary sewer and water lines.

Road abandonment

Portions of Traynor Street and Orchard Avenue are proposed to be abandoned to facilitate more flexibility in the design of the project. Per Government Code Section 65402, the Planning Commission must find that such abandonment is in conformity with the adopted General Plan. As shown in Attachment G, portions of the right-of-way should be abandoned, thereby eliminating the need for a public road in the area. Public utility easements will be created to accommodate the sewer and water lines, and the public storm drain, which are located in the road and park areas. Additionally, the portions of the roads to be abandoned do not provide for through-traffic and the design of the project would channel traffic to Soto Road at Orchard Avenue, a signalized intersection, thereby minimizing impacts on traffic.

ENVIRONMENTAL REVIEW (CEQA)

This proposal is defined as a "project" under the parameters set forth in the California Environmental Quality Act (CEQA) Guidelines. Staff has prepared a Mitigated Negative Declaration and Initial Study (see attached), which indicates there could be potentially

significant environmental impacts related to hazards and hazardous materials, and noise. However, as indicated in the Initial Study, staff concludes that those potentially significant environmental impacts will be mitigated to a level of insignificance through the implementation of mitigation measures. Those measures and the timing and implementing/monitoring responsibility for them are indicated in the attached Mitigation Monitoring and Reporting Program.

PUBLIC NOTICE

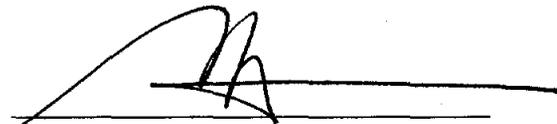
A notice for a March 22, 2007 preliminary meeting for the proposed vesting tentative tract map was mailed to every property owner and occupant within 300 feet of the subject site, as noted on the then-latest assessor's records asking for comments on the project. The Planning Division received favorable comment from the adjacent commercial property owners on Traynor Street.

On May 23, 2007, a Notice of Public Hearing and Availability of a Mitigated Negative Declaration was published in *The Daily Review*, and property owners and occupants within 300 feet of the project boundaries were notified, as were interested parties and appropriate public agencies. At the time of completion of this report, the Planning Division had not received any correspondence related to such notice.

CONCLUSION

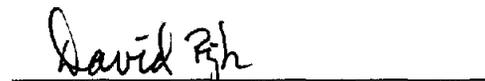
The project would provide public benefit by way of bringing a conversion of a portion of this older industrial area to a residential project compatible with the surrounding neighborhood with insignificant impacts on the environment, with the required mitigation measures. Homes proposed would be of high-quality design and provide much-needed housing in the area, including housing for moderate income households.

Prepared by:



Richard E. Patenaude, AICP
Principal Planner

Recommended by:

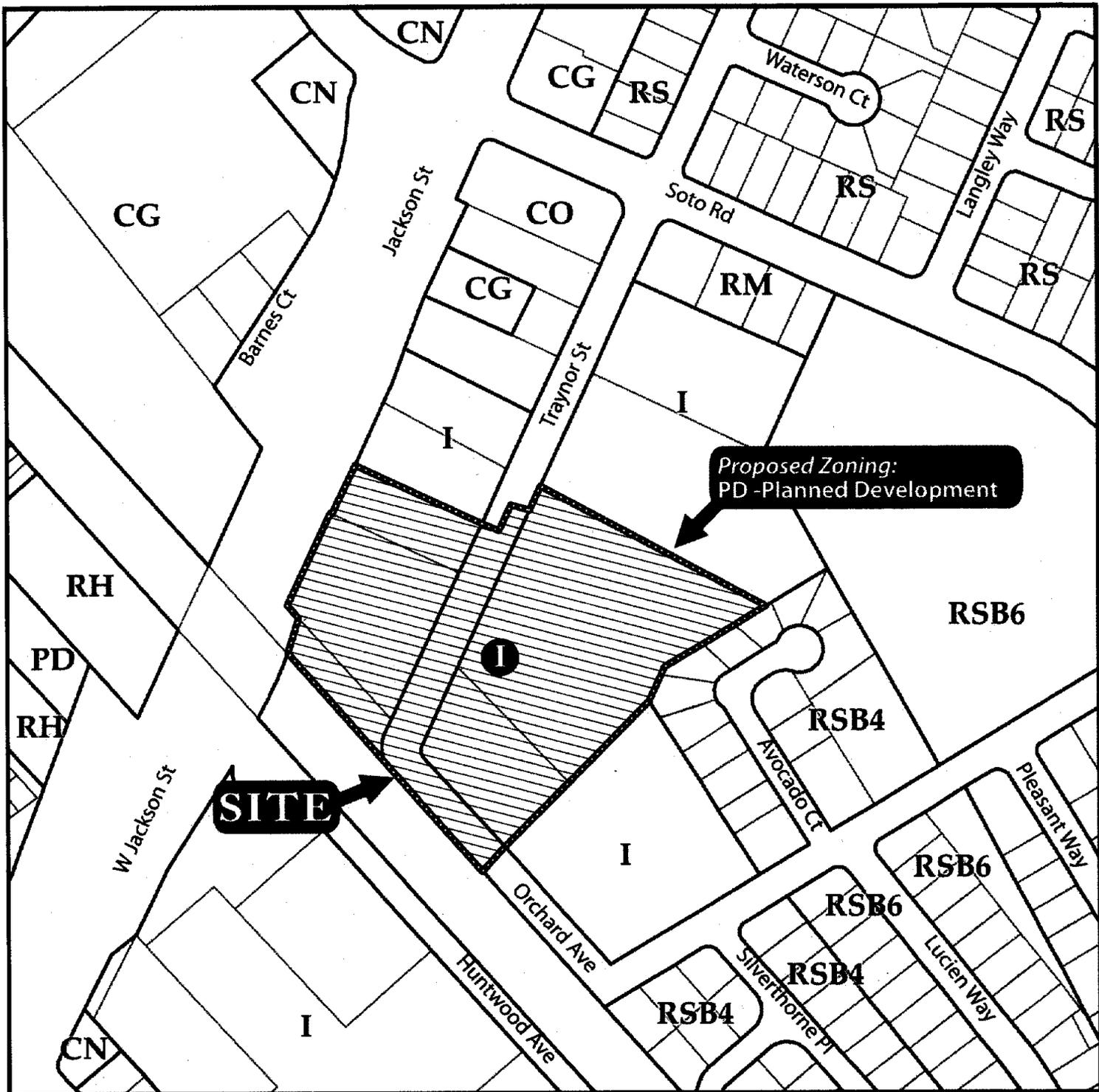


David Rizk, AICP
Planning Manager

Attachments:

- A. Area/Zoning Map
- B. General Plan Land Use Map
- C. Findings for Approval – Zone Change to Planned Development District
- D. Conditions of Approval - Zone Change to Planned Development District
- E. Findings for Approval – Tentative Map (Tract 7893)
- F. Conditions of Approval - Tentative Map (Tract 7893)
- G. Right-of-Way Abandonment
- H. Mitigated Negative Declaration
- I. Initial Study
- J. Mitigation Monitoring and Reporting Program

Plans and Exhibits



Area & Zoning Map

PL-2007-0112 TR 7893

PL-2007-0113 GPA

PL-2007-0114 ZC

Address: Traynor Street &
Orchard Avenue

Applicant: Charles McKeag

Owner: Raymond Nels Nelsen

Zoning Classifications

RESIDENTIAL

- RH** High Density Residential, min lot size 1250 sqft
- RM** Medium Density Residential, min lot size 2500 sqft
- RS** Single Family Residential, min lot size 5000 sqft
- RSB4** Single Family Residential, min lot size 4000 sqft
- RSB6** Single Family Residential, min lot size 6000 sqft

COMMERCIAL

- CG** General Commercial
- CN** Neighborhood Commercial
- CO** Commercial Office

INDUSTRIAL

- I** Industrial

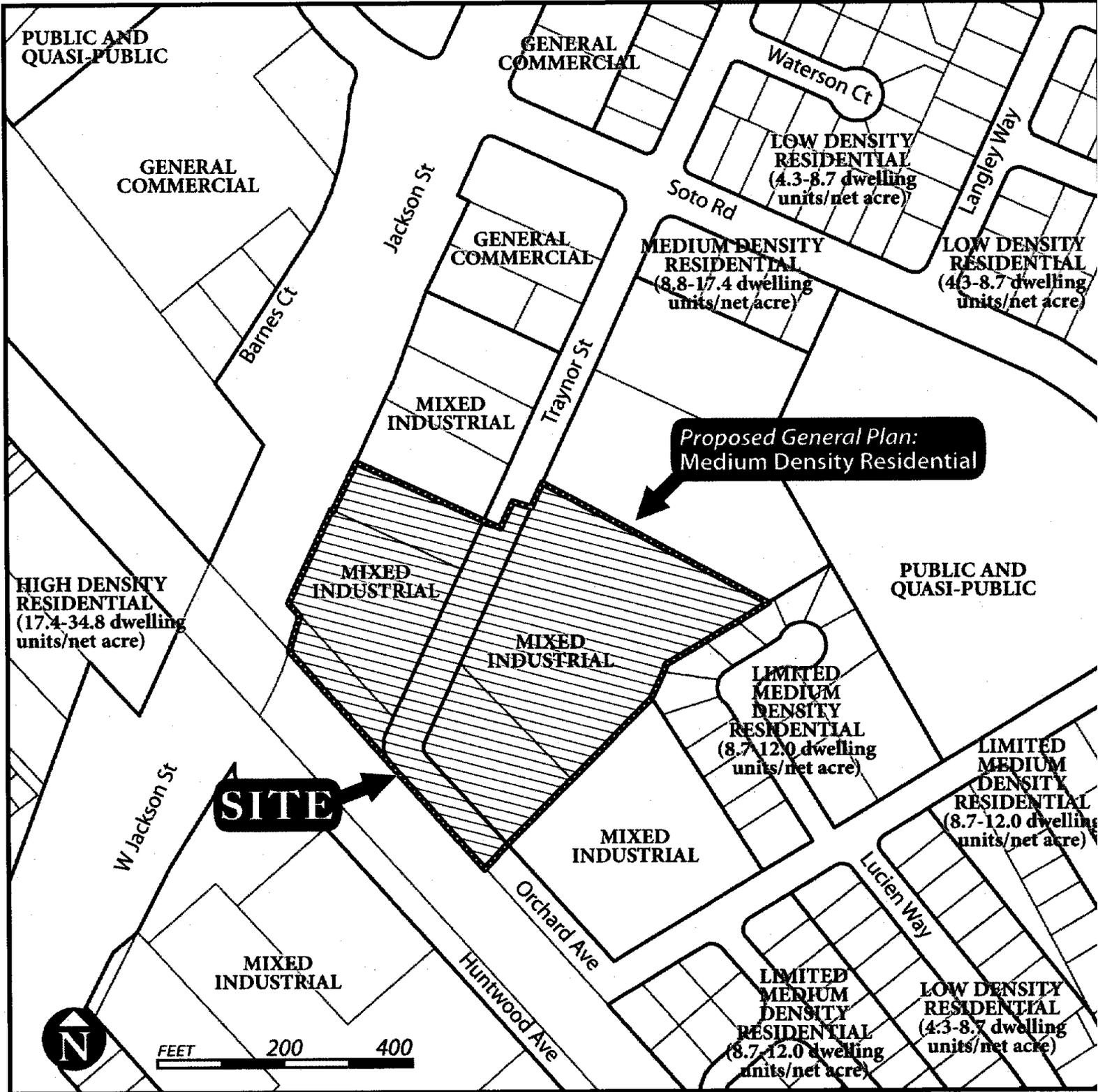
OTHER

- PD** Planned Development

ATTACHMENT A



FEET 200 400



FINDINGS FOR APPROVAL – Citation Homes/Ashwood Place

ZONE CHANGE APPLICATION NO. PL-2007-0114 PD

Findings for Approval – California Environmental Quality Act:

- A. The project will have no significant impact on the environment, cumulative or otherwise, the project reflects the City's independent judgment, and, therefore, a Mitigated Negative Declaration has been prepared.

Findings for Approval – Zone Change to Planned Development District:

- B. The development is in substantial harmony with the surrounding area and conforms to the General Plan, as amended, and applicable City policies in that the project is consistent with the General Plan designation of Medium Density Residential (8.7 to 17.4 units per net acre), since the development is proposed at 14 units per net acre; also, the project will further the City's goal to provide ownership housing opportunities as expressed in the Housing Element of the General Plan and will help further the City's Housing Element policy to provide housing opportunities for all household income levels in that 12 housing units will be provided on-site for moderate income households, and because surrounding development in the City consists of single-family residential development.
- C. Streets and utilities, existing or proposed, are adequate to serve the development. The project would impose minimal impacts on the level of service of street operations.
- D. The development creates a residential environment of sustained desirability and stability since the design and layout of the development involves a variety of single-family homes of high quality design that will not result in significant visual impacts as a result of required landscaping, architecture and colors consistent with the City's Design Guidelines; sites proposed for public facilities, such as playgrounds and other open space areas, are adequate to serve the anticipated population. The project would result in payment of in-lieu park dedication fees and the development will have no substantial adverse effect upon surrounding development in that it would be compatible with existing single-family development and would not generate significant traffic impacts.
- E. Any latitude or exception(s) to development regulations or policies is adequately offset or compensated for by providing functional facilities or amenities not otherwise required or exceeding other required development standards, in that the reduced minimum lot sizes and yard setbacks is offset by the provision of recreational open space to a higher degree than required, and project conditions of approval would require the developer to pay required park in-lieu fees; the reduced parking ratio is reflective of the small-lot design and is consistent with other similar small-lot developments approved recently in Hayward and in the area.

**CONDITIONS OF APPROVAL
ZONE CHANGE APPLICATION NO. PL-2007-0114 PD
ASHWOOD PLACE**

Citation Homes (Applicant)

- 1) Planned Development District (Application No. PL-2007-0114-PD) and Vesting Tentative Tract Map 7893 (Application No. PL-2007-0112 TTM) to accommodate construction of 80 single-family homes shall be developed according to these conditions of approval and in substantial conformance with the preliminary development plan and vesting tentative tract map labeled in the City files as "Exhibit A." Prior to final inspection, all pertinent conditions of approval and all improvements shall be completed to the satisfaction of the Planning Director and Public Works Director.
- 2) The Zone Change for the Planned Development District becomes void two years following the effective date of approval of the Preliminary Development Plan by the City Council, unless before that time, a Precise Development Plan is submitted. A one-year extension for the Preliminary Development Plan, approval of which is not guaranteed, may be granted by the City Council, provided the request for such extension is submitted at least 30 days prior to the expiration of the original approval. A request for a second one-year extension, approval of which is not guaranteed, may also be granted by the City Council, provided the request for such second extension is submitted at least 30 days prior to the expiration of the first extension.
- 3) If a building permit is issued for construction of improvements authorized by the Precise Plan, said approvals shall be void two years after issuance of the building permit, or three years after approval of the application, whichever is later, unless the construction authorized by the building permit has been substantially completed or substantial sums have been expended in reliance upon the zone change approval.
- 4) The approval of this preliminary development plan is tied to the approval of Vesting Tentative Map No. 7893 and the associated conditions of approval. No building permit shall be issued for any structure within this application until the City Council has approved the final map and said map is recorded.
- 5) The permittee shall assume the defense of, and shall pay on behalf of and hold harmless the City, its officers, employees, volunteers and agents from and against any or all loss, liability, expense, claim costs, suits and damages of every kind, nature and description directly or indirectly arising from the performance and action of this permit.

- 6) Any proposals for minor alterations to the proposed site plan and/or design that do not require a variance to the Zoning Ordinance standards require approval by the Planning Director prior to implementation.

- 7) Prior to the sale of any parcel, or prior to the acceptance of site improvements, whichever first occurs, a homeowners' association shall be created to maintain the private street, common area landscaping and open space amenities as depicted on the approved Tentative Map Tract 7893 and CC&R's prepared for the project which shall be reviewed and approved by the Planning Director that shall include the following conditions:
 - a) Each owner shall automatically become a member of the association and shall be subject to a proportionate share of maintenance expenses.
 - b) A reserve fund shall be maintained to cover the costs of replacement and repair of the private street, driveways and common area landscaping.
 - c) The association shall be managed and maintained by a professional property management company.
 - d) Provisions for towing unauthorized and illegally parked vehicles from the site.
 - e) The private street, driveways, common areas and residential front yard landscaped areas shall be maintained in good repair, and free of debris at all times.
 - f) A requirement that the building exteriors and fences shall be maintained free of graffiti. The owner's representative shall inspect the premises on a weekly basis and any graffiti shall be removed within 48 hours of inspection or within 72 hours of notification by the City's Community Preservation Officer.
 - g) The homeowners' association shall maintain the common area irrigation system and maintain the common area landscaping in a healthy, weed-free condition at all times. The homeowner's representative shall inspect the landscaping on a monthly basis and any dead or dying plants (plants that exhibit over 30 percent dieback) shall be replaced within 10 days.
 - h) Landscaping and irrigation shall be maintained in all common areas or the City shall have the right to enter upon the property to maintain the exterior portions of the common area at the expense of the homeowners association per Section 10-3.385 of the Subdivision Ordinance.
 - i) A tree removal permit is required prior to the removal of any tree.
 - j) Trees shall not be severely pruned, topped, or pollarded and any trees that are pruned in this manner shall be replaced with a tree species selected by, and size determined by the Landscape Architect, within the timeframe established by the City and pursuant to the Municipal code.
 - k) A provision that if the homeowners' association fails to maintain the common area or private streets, so that owners, their families, tenants, guests or adjacent owners suffer or will suffer substantial diminution in the enjoyment, use or property value of the project, the City of Hayward shall have the right to enter

upon the project and to commence and complete such work as is necessary to maintain the common areas and private streets, after reasonable notice, and lien the properties for their proportionate share of the costs.

- l) Each resident shall participate in the City's recycling program.
- m) The garage of each unit shall be maintained for off-street parking and shall not be converted to living or storage areas. An automatic garage door opening mechanism shall be provided for all garage doors.
- n) Individual garbage can(s) shall be kept behind fences except upon pick-up day.
- o) The common area parking spaces shall be provided for and maintained as visitors' spaces and shall not be used for recreational vehicles, camper shells, boats or trailers. These spaces shall be clearly marked and monitored by the homeowners association. Parking stalls shall be used only for vehicles in operating condition. The homeowners association shall remove vehicles parked contrary to this provision.
- p) Additions to residential units are prohibited.

Precise Development Plan

- 8) Prior to or in conjunction with submittal of improvement plans and final map(s) for the development, a Precise Development Plan consistent with the approved Preliminary Development Plan shall be submitted for review and approval by the Planning Director and shall include the following information:
 - a. A color and materials board, consistent with that proposed with the preliminary development plan and with the City's Design Guidelines. No changes to colors shall be made after construction unless previously approved by the Planning Director.
 - b. A plan showing distribution of the models and the architectural styles to avoid repetition along any particular block face. No specific model or architectural style shall be repeated on an adjacent lot.
 - c. Plans showing decorative paving for all crosswalks and for the Orchard Avenue entry.
 - d. Plans for the Traynor Street hammerhead reflecting Fire Department requirements.
 - e. Plans for grouped mailboxes, to reflect high-quality, locking mailboxes within covered decorative shelters. Plans shall be submitted showing the locations, design, material, and color of these structures and are to be consistent with the overall project design theme. If grouped mail boxes are not used, a design for attractive, decorative mail box supports shall be provided.

- f. Plans reflecting that all air conditioners and utility connections for air conditioners will be located such that all external equipment is within an area that can be screened and located so as to minimize noise impacts on adjacent properties (e.g., away from non-living areas). Air conditioning systems shall be installed in all residences.
- g. Improvement plans including utility plans, joint trench plans (showing all pull boxes and water, electric and gas meters).
- h. Plans showing how all above-ground utility meters, mechanical equipment and water meters will be enclosed within the buildings or architecturally screened or screened from the streets with minimum 5-gallon shrubs.
- i. A lighting plan, prepared by a qualified illumination engineer, shall be show exterior lighting design. Exterior lighting shall be erected and maintained so that adequate lighting is provided in all common areas. The Planning Director shall approve the design and location of lighting fixtures, which shall reflect the architectural style of the building(s). Exterior lighting shall be shielded and deflected away from neighboring properties and from windows of houses within the project
- j. Plans showing garbage and recycling receptacle storage areas will be adequately screened from public view with landscaping and/or solid screens or be provided within garages, in which case such areas shall be clear of the required parking area for two cars.
- k. Plans showing the design and location of proposed fencing. Fences/walls about private yard areas along public streets and paseos shall be attractive and consist of a mixture of stucco and decorative metal. Fencing shall provide for both privacy and interaction between neighbors.
- l. Architectural drawings shall be revised to reflect the following:
 - i. Side elevations facing public and private streets shall be further enhanced, e.g., with pop-outs, extension of decorative building materials.
 - ii. Where there is a change of building materials, they shall be extended from the front façade around the side(s) of the house.
 - iii. Greater architectural relief shall be provided to the rear elevations to minimize the presence of the garage doors; garage doors shall be inset at least one foot.
 - iv. Provide details of architectural elements, such courtyard fences, and window and door trim.
 - v. Where exterior building materials consist primarily of stucco, incorporate another building material(s) appropriate to the architectural style, such as heavy wooden trim members, tile embellishments, grill work, Bermuda shades, shutters, etc.

- vi. All garages shall provide the minimum required clear space of 19 feet deep by 20 feet wide.
- vii. Window placement and/or type (obscured glass) shall take into consideration the privacy of adjacent homes.

Landscaping

- m. Prior to the approval of the Precise Plan, detailed landscaping and irrigation plans for all common areas shall be prepared by a licensed landscape architect and submitted for review and approval by the City. Landscaping and irrigation plans shall comply with the City's *Water Efficient Landscape Ordinance*. Provide automatic irrigation system that provides 100% coverage to the landscape area
- n. Enhance the project entry and integrate entry paving into the overall landscape design. Extend the landscape entry area by remove parking spaces on both sides of the circular paving if necessary to create a more attractive and stronger project entry.
- o. Paseo walkway design shall be integrated into the overall front yard landscape design.
- p. All areas shall be landscaped except for permitted driveways and walkways. Required landscaped areas shall be planted with water-conserving trees, shrubs, turf grass (less than 50%), groundcovers or a combination thereof. The sole use of bark, decorative paving or decorative rock shall not be allowed in required landscape areas.
- q. Street Trees: One 24" box street tree is required for every 20 – 40 lineal feet of frontage within front yard setback area. Spacing of the trees is dependant on the species of trees. Smaller trees will require closer spacing. Trees shall be planted according to the City Standard Detail SD-122
- r. Screening with evergreen trees: A minimum of one 15-gallon buffer tree shall be planted for every 20 lineal feet along all property perimeters. The easterly perimeter will need evergreen tree planting in addition to evergreen vine planting on 6'-6" high welded wire panel as shown. The southerly perimeter where the bio-swale is proposed must be wide enough to accommodate the swale design, screening tree planting and shrub planting.
- s. All trees planted in turf areas must be provided with a plastic trunk guard.
- t. All paved areas must be a minimum of 5.5 feet from the fence or property line. This will include a minimum 5 foot wide landscape area and a 6 inch concrete curb as all landscape areas must be protected from the driveways and parking areas with a 6 inch concrete curb. The curb at the edge of the drive

must also be located a minimum of 5 feet from the trunk of the existing tree. Narrow the driveway down as necessary to meet these requirements.

- u. Parking areas shall include one 15-gallon tree after every six parking stalls. Trees shall be planted in tree wells or landscape medians or islands located within the parking area. In addition, parking rows shall be capped with a landscaped island with shade trees, shrubs and live groundcovers. All tree wells, islands and medians shall be a minimum of 5' wide measured inside the curbs.
- v. Landscape plans shall include amenities such as, benches, tables, fencing, play equipment and barbecues, for the common open space areas.
- w. Masonry walls, solid building walls, trash enclosures or fences facing a street or driveway shall be continuously buffered with shrubs and vines. All above-ground utilities and mechanical equipment including irrigation backflow preventer shall be screened from the street with shrubs.
- x. Zoning Ordinance Section 10, Article 12: Water Efficient Landscape Ordinance, No. 13, (d): Drip emitters or a bubbler shall be provided to each tree. Bubblers shall not exceed 1.5 gallons per minute per device. Bubblers for trees shall be placed on a separate valve.
- y. A polar blanket and steel caging shall be provided for backflow preventer.
- z. Construction Administration services shall be provided by the project landscape architect. Services to include:
 - Observation of irrigation system before burying pipes;
 - Observation of soil preparation and soil amendments;
 - Observation of plant material upon delivery to the site;
 - Observation of layout and placement of plants material upon delivery to the site;
 - Observation of final acceptance; and
 - Observation of maintenance period commencement.

Inclusionary Housing

- 9) Prior to approval of the Precise Plan, the applicant shall obtain approval of an Inclusionary Housing Plan, assuring that at least 12 units, affordable to at least moderate-income households, will be constructed at a price set forth for such units in accordance with the City's Inclusionary Housing Ordinance. Such units shall be distributed evenly throughout the project subject to approval of the Planning Director.

Environmental Issues

- 10) **Mitigation Measure VII-d:** In order to reduce any potential impact of soil or groundwater contamination, the following conditions are required prior to any grading activities:
- a) The drums stored on the property, along with the contents, shall be disposed off-site at a licensed disposal facility in accordance with applicable federal, state and local laws and regulations;
 - b) The soils affected by petroleum hydrocarbons, in amounts greater than 100 mg/Kg for TPHd and 500 mg/Kg for TPHmo, shall be further assessed to determine vertical and lateral limits of contamination and the affected soil shall be remediated prior to construction of the project to the satisfaction of the Hayward Fire Department; and
 - c) All current tenants and future tenants should conduct site closure activities in accordance with City of Hayward Hazardous Materials Program requirements under the oversight of the Fire Department and/or other federal, state or city agencies as appropriate. The closure activities shall be completed prior to the tenants vacating the property.
- 11) **Mitigation Measure XI-a:** In order to reduce any potential impact from noise, the following conditions are required:
- a) All project residences shall be provided forced-air mechanical ventilation so that windows may be closed at the discretion of the occupants to control noise intrusion; and
 - b) The project plans shall be reviewed during detail design to determine the specific sound transmission class ratings necessary to obtain the prescribed overall A-weighted noise reduction values. A report shall be prepared and submitted to the Building Official, prior to issuance of any building permit, which stipulates the noise control treatments that have been included in the design to demonstrate compliance with the local noise standards.

Landscaping

- 12) Trees shall be preserved in accordance with the Tree Preservation Ordinance. The project sponsor shall submit a tree survey prior to the issuance of a grading permit. The tree survey shall include the location, size, species and condition of each tree based upon ISA standard methods found in the Guide for Plant Appraisal, 8th edition. A separate tree removal permit is required prior to the removal of any trees that is available through the City Landscape Architect. Tree mitigation shall be required for any trees removed above and beyond the required trees based on the approved value in the project arborist report as determined by the City Landscape Architect. All trees to be preserved or removed shall be indicated on the site and landscape plans, and noted with tree protection measures in compliance with City codes. Show dripline of the existing trees to be saved on the plan.

- 13) A comprehensive arborists report by a licensed arborist is required if there are any existing trees on Site. The report shall include health, species, caliper, approximate height, canopy diameter, and value using the latest edition of "Guide for Plant Appraisal" by the International Society of Arboriculture. Provide ISA worksheet per each trees are subjected for valuation. A bond will be required for all trees that are to remain or be relocated. Any trees that are removed or damaged during construction shall be replaced with trees of equal size and equal value.
- 14) Prior to the issuance of a grading or building permit, the developer shall provide a tree preservation bond, surety or deposit, equal in value to the trees to be preserved. The bond, surety or deposit shall be returned when the tract is accepted if the trees are found to be in a healthy, thriving and undamaged condition. The developer shall provide an arborist's report evaluating the condition of the trees. Grading and improvement plans shall include tree preservation and protection measures, as required by the City Landscape Architect. Trees shall be fenced at the drip line throughout the construction period and shall be maintained in a healthy condition throughout the construction period.
- 15) All common area landscaping, irrigation and other required improvements shall be installed prior to acceptance of tract improvements, or occupancy of 80% of the dwelling units, whichever first occurs.
- 16) Landscape improvements shall be installed according to the approved plans and a Certificate of Final Acceptance, and an Irrigation Schedule shall be submitted prior to the issuance of a Certificate of Occupancy.
- 17) Landscaping shall be maintained in a healthy, weed-free condition at all times. The owner's representative shall inspect the landscaping on a monthly basis and any dead or dying plants (plants that exhibit over 30% die-back) shall be replaced within ten days of the inspection. Trees shall not be severely pruned, topped or pollarded. Any trees that are pruned in this manner shall be replaced with a tree species selected by, and size determined by the City Landscape Architect, within the timeframe established by the City and pursuant to Municipal Code.
- 18) Park in-lieu fee is required for each of the unit in the development. Park in-lieu fee shall be applied at the rate in effect at the time a building permit is issued. The fee schedule is updated annually with new fees taking effect on July 1 of each year. The current fee for detached single family dwelling is \$11,953. The fee shall be paid to the City prior to the date of the final inspection or the date of the certificate of occupancy is issued, whichever occurs first.
- 19) A tree removal permit is required prior to the removal of any tree. Replacement trees shall be required for any trees removed, as determined by the City Landscape Architect.

Fire Protection

Access

- 20) The proposed EVA gate at Traynor Street shall be designed with an automatic gate that may be remotely controlled by the HFD Dispatch Center. The gates shall meet City standards for installation and design. The gate shall be equipped with fire key switches.
- 21) All streets within this development shall be dedicated fire lanes. Parking of vehicles will only be allowed within the parking stalls represented on the plans. Red-painted curbing and fire lane signage shall be installed throughout the development.

Water

- 22) Fire hydrants for this development shall be modified steamers with 1 - 4 ½" outlet and 1-2 ½" outlet. The number and location of hydrants is subject to Fire Department review and approval.
- 23) Fire flows for this development shall be a minimum of 1,500 gpm at 20 PSI. Minimum 1-inch water meters will be required with 1-1 ½" water service laterals installed for each lot to support the domestic water and fire sprinkler system water demands.

Fire Protection

- 24) Each SFR shall have an automatic fire sprinkler system installed per NFPA 13-D Standards. The architectural plans will need to have this requirement reflected on the title sheet of the building permit application.

Utilities

- 25) Each residential unit must have an individual water meter and an individual sanitary sewer lateral. The development shall be served by radio-read meters.
- 26) Water meters and services are to be located a minimum of two feet from the top of driveway flares as per City of Hayward Standard Details 213 thru 218. Water meters shall be located a minimum of six feet from sanitary sewer laterals per State Health Code. Proposed locations shall be shown on the Precise Plan and plans for building permit applications. Driveway cuts shall be staked before service laterals are installed.
- 27) All water mains must be looped. Dead end water mains will not be allowed.
- 28) Ductile Iron Pipe is required in all easements, and control valves are required in the streets prior to easements for all water mains.
- 29) The existing 6-inch water mains in Orchard Avenue and Traynor Street must be abandoned at the new points of connection.

- 30) Provide keys/access code/automatic gate opener to utilities for all meters enclosed by a fence/gate as per Hayward Municipal Code 11-2.02.1.
- 31) Only Water distribution personnel, shall perform operation of valves on the Hayward Water System.
- 32) Water & Sewer service shall be available subject to the standard conditions and fees in effect at the time of application.

Other

- 33) A copy of these conditions of approval shall be included on a full-sized sheet(s) in the plan set.
- 34) Show an exterior hose bib for each private yard, patio or porch area on the ground floor.
- 35) No mechanical equipment shall be placed on the roof unless it is completely screened from view by the proposed roof structure. Roof apparatus, such as vents, shall be painted to match the roof color. All roof vents shall be shown on roof plans and elevations. Vent piping shall not extend higher than required by Building Code.

FINDINGS FOR APPROVAL
TENTATIVE TRACT MAP 7893

1. The approval of Tentative Map Tract 7893, as conditioned, will have no significant impact on the environment, cumulative or otherwise. The project is categorically exempt per Section 15332, In-fill Development, per the California Environmental Quality Act (CEQA).
2. The tentative tract map substantially conforms to the State Subdivision Map Act, the City's Subdivision Regulations, the General Plan, and the City of Hayward Zoning Ordinance.
3. The site is physically suitable for the proposed type of development.
4. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
5. The design of the subdivision and the proposed improvements are not likely to cause serious health problems.
6. Existing streets and utilities are adequate to serve the project.
7. None of the findings set forth in Section 64474 of the Subdivision Map Act¹ have been made.

¹ The findings of Section 64474 set forth the grounds for denial of a tentative map which are as follows:

- (a) That the proposed map is not consistent with applicable general and specific plans as specified in Section 65451.
- (b) That the design or improvement of the proposed subdivision is not consistent with applicable general and specific plans.
- (c) That the site is not physically suitable for the type of development.
- (d) That the site is not physically suitable for the proposed density of development.
- (e) That the design of the subdivision or the proposed improvements are likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
- (f) That the design of the subdivision or type of improvements is likely to cause serious public health problems.
- (g) That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of, property with the proposed subdivision.

**CONDITIONS OF APPROVAL
TENTATIVE TRACT MAP 7893
Traynor Street and Orchard Avenue**

Unless otherwise stated, all necessary easements shall be dedicated, and all improvements shall be designed and installed at no cost to the City of Hayward.

All improvements shall be designed and constructed in accordance with the City of Hayward Municipal Code – Chapter 10, Article 3, and Standard Specifications and Details – unless otherwise indicated hereinafter.

The applicant/developer's engineer shall perform all design work unless otherwise indicated.

IMPROVEMENTS

Improvement plans shall be submitted to the City Engineer for review and approval. Subject plans shall, in addition to the standard improvements, incorporate the following special design requirements:

STREETS

Public Street – Orchard Avenue

1. Approximately 230 feet of new curb, gutter, sidewalk and tie-in paving shall be installed along the southerly side of Orchard Avenue west of the project entrance. The design shall include all necessary traffic controls to accommodate street parking along the south curb-line. The design shall meet the approval of the Fire Chief and the City Engineer. Any failed pavement within the Orchard Avenue segment described above, shall be removed and replaced per the City Engineer.
2. Traffic control sign(s) shall be installed within the Orchard Avenue right-of-way to alert motorists that the public street terminates at a private street entrance. The types of signs and their locations shall be approved by the City Engineer.

Public Street – Traynor Street

3. Traynor Street terminus shall be a hammerhead turnaround configuration to accommodate pedestrian, including necessary handicap ramp(s), and emergency vehicle access (EVA). The design shall include an automatic gate that can be remotely controlled by the Hayward Fire Department Dispatch Center and shall be equipped with fire key switches. The design shall meet the approval of the City Engineer and the Fire Chief.
4. The Traynor Street terminus design shall include a gated pedestrian access way. The design shall meet the approval of the City Engineer.

5. Traffic control sign(s) shall be installed within the Traynor Street right-of-way to alert motorists that the public street terminates. The types of signs and their locations shall be approved by the City Engineer.

Private Streets

6. The minimum private street width shall be at least 22 feet with a 21-foot wide travel way. Wider streets are necessary to accommodate on-street parking or adjacent parking bays. The street designs shall be approved by the City Engineer.
7. On-street parking shall only be allowed within the designated areas along the private street and within the proposed parking bays. Where no parking is allowed, the curbs shall be painted red and "No Parking" signs shall be installed along the sides of the private street. The locations of signs and red curbs shall be approved by the City Engineer and the Fire Chief.
8. Decorative pavement e.g. interlocking pavers or stamped colored concrete, or bands of decorative paving, etc. shall be installed at the project entrance and at all interior private street intersections within the subdivision. The Planning Director shall approve the material, color and design and the City Engineer shall approve the pavement section for the decorative paving.
9. The onsite streetlights and pedestrian lighting shall have a decorative design approved by the Planning Director. The locations of the lights shall be shown on the improvement plans and shall be approved by the City Engineer.
10. Upon any necessary repairs to the public facilities under the on-site decorative paved areas, the City shall not be responsible for the replacement cost of the decorative paving. The replacement cost shall be borne by the homeowners association established to maintain the common areas within the subdivision boundary.
11. The CC&R's shall include provisions for maintenance of the driveway, decorative pavement, onsite streetlights and pedestrian lighting.

Storm Drainage

12. The 24-inch storm drain main that collects drainage from Traynor Street and connects to the 30-inch storm drain main located within Orchard Avenue shall be a publicly owned and maintained by the City of Hayward. All other on-site storm drainage systems shall be privately owned and maintained by the homeowners association.
13. The storm drain facilities shall be designed to comply with the Alameda Countywide Clean Water Program including the established provision C.3. Any natural or mechanical storm drain filtration systems shall be designed as part of the private storm drain systems and shall not be located on the public storm drain main. The filtering devices shall be privately owned and maintained by the homeowners association.

14. The location and design of the public and private storm drain systems shall meet the approval of the City Engineer.
15. Prior to the issuance of a grading permit and/or the beginning of any construction activity on-site, the Developer's Engineer shall complete the Development Building Application Form Information: 1) Impervious Material Form and 2) Operation and Maintenance Information Form.
16. The Hydrology and Hydraulics Criteria Summary, Alameda County Flood Control and Water Conservation District, latest edition shall be used to determine storm drainage runoff. A detailed grading and drainage plan with supporting calculations and a completed Drainage Review Checklist shall be approved by the Alameda County Flood Control and the City Engineer. Development of this site is not to augment runoff to the District flood control facility downstream. The hydrology study shall substantiate that there will be no net increase in the quantity of runoff from the site versus the flow rate derived from the original design of downstream facilities. If there is augmented runoff, off-site and/or on-site mitigation measures will be necessary.
17. A Storm Water Pollution Prevention Plan (SWPPP) shall be submitted with a design to reduce discharge of pollutants and sediments into the downstream storm drain system. The plan shall meet the approval of the City Engineer.
18. The developer shall provide a copy of the Notice of Intent filed with the State Water Resources Control Board, prior to the issuance of a grading permit for the project site.
19. The project plans shall include storm water pollution prevention and control measures for the operation and maintenance of the project during and after construction for review and approval of the City Engineer. The storm drain design shall comply with post-construction stormwater requirements to provide treatment of the stormwater according to the NPDES permit's numeric criteria. The storm drain design shall comply with the C.3 established thresholds and shall incorporate measures to minimize pollutants to the maximum extent practicable (MEP).
20. The project plan shall identify Best Management Practices (BMPs) appropriate to the uses conducted onsite to effectively prevent the entry of pollutants into storm water runoff. Options include utilization of a grassy swale, use of unit pavers for all paved areas or installation of an inline treatment system.

The project should be designed to direct runoff to the landscaped yards and common space, prior to entry into the storm drain system. Unit pavers should also be used for impervious areas such as the driveway, parking areas and fire truck turn around. If these methods are utilized, the inline treatment system may not be required.

21. The project plan measures shall also include erosion control measures to prevent soil, dirt, debris and contaminated materials from entering the storm drain system, in accordance with the regulations outlined in the ABAG Erosion and Sediment Control Handbook.

22. The applicant/developer is responsible for ensuring that all contractors are aware of all storm water quality measures and implement such measures. Failure to comply with the approved construction BMPs will result in the issuance of correction notices, citations or a project stop order.
23. The project shall not block runoff from, or augment runoff to, adjacent properties. The drainage area map developed for the hydrology design shall clearly indicate all areas tributary to the project area. The developer is required to mitigate augmented runoffs with off-site and/or on-site improvements.
24. No surface runoff is allowed to flow over the sidewalks and /or driveways. Area drains shall be installed behind the sidewalk area to collect all runoff from the project site.
25. All storm drain inlets must be labeled "No Dumping - Drains to Bay" using City approved methods.

Sanitary Sewer System

26. The portion of the existing sanitary sewer main, laterals and appurtenances within Traynor Street and Orchard Avenue and within the boundaries of the subdivision shall be removed per the specifications of the City Engineer.
27. The proposed sanitary sewer main shall be a public main designed in accordance with the City of Hayward Standard Details. The design and location shall meet the approval of the City Engineer.
28. Each residential unit shall have a separate sanitary sewer lateral.

Water System

29. The portion of the existing water main, fire hydrants, meters and appurtenances within Traynor Street and Orchard Avenue and within the boundaries of the subdivision shall be abandoned and/or removed per the specifications of the City Engineer. The design shall allow for any dead-end water mains.
30. Water service is available subject to standard conditions and fees in effect at the time of application.
31. The proposed water main system shall be a looped system. Water mains located within the proposed dead-end streets shall be looped together via easements created between these streets. Ductile iron pipe shall be used for the mains within easements that are outside of the private street right-of-way. The location and design of the looped water system and necessary easements shall meet the approval of the City Engineer.
32. Each residential unit shall have an individual radio read water meter.

33. The fire service line for the fire hydrant shall have a double check detector assembly with trim meter.
34. All existing water services to be abandoned shall be abandoned by turning off the corporation stop and cutting the existing line. This work shall be done under the direct supervision of the City Inspector.

Fire Protection

35. Fire Department requirements shall be as follows:
 - a. The private streets shall be dedicated as fire lanes and shall be posted as a fire lane and no parking of vehicles shall be allowed on either side except where parking abuts the private streets and driveways as shown on the approved tentative map. Red curbing will be required for the entire length of the driveway. Signs shall be posted to allow towing of illegally parked vehicles to ensure adequate fire truck access.
 - b. The private streets and driveways shall be designed and engineered to withstand 50,000 lbs. GVW of fire apparatus including the areas where pavers or decorative concrete is being used.
 - c. The emergency vehicle access that conforms to the California Fire Code and Hayward Fire Department Standards shall be designed to the satisfaction of the Fire Chief and the City Engineer.
 - d. Addressing of the buildings shall be in agreement with the Fire Department. All buildings shall have a minimum 4 inch self-illuminated address installed on the front of the building so as to be visible from the street.
 - e. The fire hydrant system for this development shall be a lopped system design that meets the approval of the City Engineer.
 - f. New fire hydrants shall be installed along the private streets within the development. The type of fire hydrant shall be a modified steamer with one 4½ inch outlet and one 2½ inch outlet capable of flowing 1500 GPM at 20 PSI for a 2-hour duration. The design and layout of the hydrants shall meet the approval of the Fire Department prior to construction.
 - g. Crash post may be required around fire hydrants if they are installed in a location that may be susceptible to vehicular impact. Blue reflective markers shall be installed on the street pavement adjacent to the fire hydrant location. The Fire Chief will determine where crash post are needed.

- h. Provide a one-inch minimum water meter size with a 1½ inch water service lateral serving shall be required to serve each lot. These requirements will allow for adequate water flow to each lot to support the domestic water and fire sprinkler system demands.
- i. Blue reflective pavement markers shall be installed at fire hydrant locations.
- j. If fire hydrants are located so as to be subjected to vehicle impact, crash posts shall be installed around the fire hydrant(s).
- k. Fire hydrants for the development shall be operational and in-service prior to the start of any combustible construction and /or storage of combustible construction materials.
- l. Each single-family dwelling shall have an automatic fire sprinkler system installed in conformance with NFPA13-D (Modified) Standards. This requirement includes the installation of fire sprinklers within all living space areas, garages, under attached decking and balconies built with combustible construction materials, within crawl space areas, foyers and porches, and any other area of the structure deemed hazardous by the Fire Chief.
- m. Exterior (local) alarm bell(s) shall be installed on each fire sprinkler riser on each single-family dwelling.
- n. Interior audible devices shall be installed within each single-family residence and shall be capable of activating upon any fire sprinkler system water flow activity.
- o. Interior single station residential smoke detectors shall be installed within each single-family residence. Smoke detectors shall be installed per the California Building Code and shall be hard-wired electric with a battery back-up.
- p. Phase I and II hazardous materials site assessment documents must be submitted to the Hayward Fire Department's Hazardous Materials Coordinator. The reports shall be approved prior to the issuance of a building permit.

Utilities

- 36. All service to dwellings shall be an "underground service" designed and installed in accordance with the Pacific Gas and Electric Company, AT&T (phone) Company and local cable company regulations. All facilities necessary to provide service to the dwellings, including transformers and switchgear, shall also be undergrounded.
- 37. The joint trench location and design shall meet the approval of the City Engineer.
- 38. All utilities, including water mains, located underneath decorative paving shall be encased in steel sleeves.

39. All surface-mounted hardware (fire hydrants, electroliers, etc.) along the private streets shall be located outside of the sidewalk within the Public Utility Easement in accordance with the requirements of the City Engineer or, where applicable, the Hayward Fire Chief.
40. All utilities shall be designed in accordance with the requirements of the City of Hayward and applicable public agency standards.

Dedications, Easements and Encroachment Permits

41. The final map shall reflect all easements needed to accommodate the public portions of the storm drain, sanitary sewer and water systems that are outside of the private driveway. The easements shall be a minimum of 12 feet wide. The private streets shall be designated as a Public Utility Easement (PUE), Storm Drain Easement (SDE), Sanitary Sewer Easement (SSE) Water Easement (WE) and Emergency Vehicle Access Easement (EVAE).
42. Prior to the approval of the final map, all documents that need to be recorded with the final map shall be approved by the City Engineer and any unpaid invoices or other outstanding charges accrued to the City for the processing of the subdivision application shall be paid.
43. An encroachment permit shall be obtained by the Developer/Contractor prior to commencement of any work within the right-of-way of any public street.

Subdivision Agreement

44. The developer shall execute a subdivision agreement and post bonds with the City that shall secure the construction of the proposed improvements per Section 10-3.332, Security for Installation of Improvements, of the Municipal Code. Insurance shall be provided per the terms of the subdivision agreement.

PRIOR TO CONSTRUCTION WITH COMBUSTIBLE MATERIALS

45. Required water system improvements shall be completed and operational prior to the start of combustible construction.

DURING CONSTRUCTION

46. The following control measures for construction noise, grading and construction activities shall be adhered to, unless otherwise approved by the Planning Director or City Engineer:
 - a. Grading and construction activities shall be limited to the hours 7:00 AM to 6:00 PM on weekdays; there shall be no grading or construction activities on the weekend or national holidays;
 - b. Access to the construction site shall be limited to Traynor Street unless otherwise approved by the City Engineer.

- c. Grading and construction equipment shall be properly muffled;
- d. Unnecessary idling of grading and construction equipment is prohibited;
- e. Stationary noise-generating construction equipment, such as compressors, shall be located as far as practical from occupied residential housing units;
- f. Applicant/developer shall designate a "noise disturbance coordinator" who will be responsible for responding to any local complaints about construction noise;
- g. The developer shall participate in the City's recycling program during construction.
- h. Daily clean up of trash and debris shall occur along Traynor Street and Orchard Avenue;
- i. The site shall be watered twice daily during site grading and earth removal work, or at other times as may be needed to control dust emissions;
- j. All grading and earth removal work shall follow remediation plan requirements, if soil contamination is found to exist on the site;
- k. Pave, apply water three times daily, or apply (non-toxic) soil stabilizers on all unpaved access roads, parking areas and staging areas at construction sites;
- l. Sweep daily (with water sweepers) all paved access roads, parking areas and staging areas at construction sites;
- m. Apply (non-toxic) soil stabilizers or hydroseed to inactive construction areas (previously graded areas inactive for 10-days or more);
- n. Enclose, cover, water twice daily or apply (non-toxic) soil binders to exposed stockpiles (dirt, sand, etc.);
- o. Gather all construction debris on a regular basis and place them in a dumpster or other container that is emptied or removed on a weekly basis. When appropriate, use tarps on the ground to collect fallen debris or splatters that could contribute to storm water pollution;
- p. Remove all dirt, gravel, rubbish, refuse and green waste from the sidewalk, street pavement, and storm drain system adjoining the project site. During wet weather, avoid driving vehicles off paved areas and other outdoor work;
- q. Broom sweep the sidewalk and public street pavement adjoining the project site on a daily basis. Caked on mud or dirt shall be scraped from these areas before sweeping;

- r. No site grading shall occur during the rainy season, between October 15 and April 15, unless approved erosion control measures are in place;
 - s. Install filter materials (such as sandbags, filter fabric, etc.) at the storm drain inlet nearest the downstream side of the project site prior to: 1) start of the rainy season; 2) site dewatering activities; or 3) street washing activities; and 4) saw cutting asphalt or concrete, or in order to retain any debris or dirt flowing into the City storm drain system. Filter materials shall be maintained and/or replaced as necessary to ensure effectiveness and prevent street flooding. Dispose of filter particles in the trash;
 - t. Create a contained and covered area on the site for the storage of bags of cement, paints, flammables, oils, fertilizers, pesticides or any other materials used on the project site that have the potential for being discharged to the storm drain system through being windblown or in the event of a material spill;
 - u. Never clean machinery, tools, brushes, etc., or rinse containers into a street, gutter, storm drain or stream. See "*Building Maintenance/Remodeling*" flyer for more information;
 - v. Ensure that concrete/gunite supply trucks or concrete/plasters finishing operations do not discharge washwater into street gutters or drains; and
 - w. The applicant/developer shall immediately report any soil or water contamination noticed during construction to the City Fire Department Hazardous Materials Division, the Alameda County Department of Health and the Regional Water Quality Control Board.
47. A representative of the soils engineer shall be on the site during grading operations and shall perform such testing as deemed necessary by the City Engineer. The representative of the soils engineer shall observe grading operations with recommended corrective measures given to the contractor and the City Engineer.
48. The minimum soils sampling and testing frequency shall conform to Chapter 8 of the Caltrans Construction Manual. The subdivider shall require the soils engineer to daily submit all testing and sampling and reports to the City Engineer.

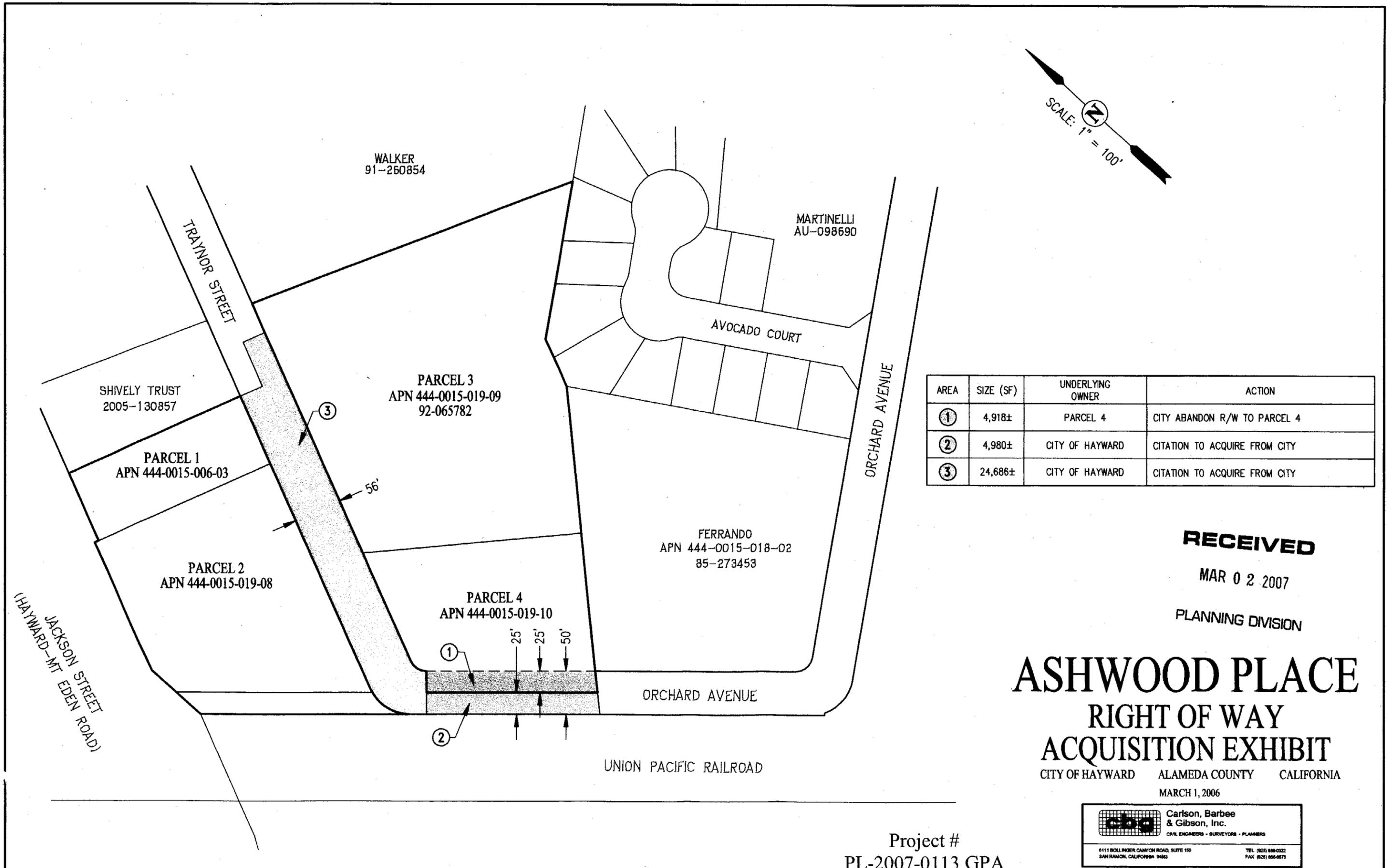
PRIOR TO CONNECTION OF UTILITIES AND ISSUANCE OF CERTIFICATES OF OCCUPANCY

49. The applicant/developer shall pay the following fees;
- a. Supplemental Building Construction and Improvement Tax;
 - b. School Tax; and

- c. Park Dedication in-lieu fees for each unit. The amount of the fee shall be in accordance with the fee schedule in effect at the time the vesting tentative map was accepted as complete.
 - d. Water Facilities Fee and Sewer Connection Fee for each dwelling unit at the rate in effect when the utility service permit for the dwelling unit is issued.
50. A reduced pressure backflow preventer shall be installed behind the water meter per City of Hayward Standard Detail 202.
51. Prior to granting occupancy, water services shall be installed by City crews at the developer's expense. The application for water services shall be presented to the City Inspector.
52. Prior to the City setting the water meters, the subdivider shall provide the Water Department with certified costs covering the installation of the public water mains and appurtenances.
53. All common area landscaping, irrigation and other required improvements shall be installed according to the approved plans.

PRIOR TO CITY APPROVAL OF THE TRACT IMPROVEMENTS AS BEING COMPLETED

54. All tract improvements, including the complete installation of all improvements relative to streets, fencing, sanitary sewer, storm drainage, water system, underground utilities, etc., shall be completed and attested to by the City Engineer before approval of occupancy of any unit. Where facilities of other agencies are involved, such installation shall be verified as having been completed and accepted by those agencies.
55. All common area landscaping, irrigation and other required improvements shall be installed prior to acceptance of tract improvements, or occupancy of 80 percent of the dwelling units, whichever first occurs.
56. The improvements associated with the Pacific Gas and Electric Company, AT&T (phone) company and local cable company shall be installed to the satisfaction of the respective companies.
57. The subdivider shall submit an "as built" plan indicating the following:
- a. All the underground facilities, sanitary sewer mains and laterals, water services (including meter locations), Pacific Gas and Electric, AT&T (phone) facilities, local cable company, etc; and;
 - b. All the site improvements, except landscaping species, buildings and appurtenant structures.



AREA	SIZE (SF)	UNDERLYING OWNER	ACTION
①	4,918±	PARCEL 4	CITY ABANDON R/W TO PARCEL 4
②	4,980±	CITY OF HAYWARD	CITATION TO ACQUIRE FROM CITY
③	24,686±	CITY OF HAYWARD	CITATION TO ACQUIRE FROM CITY

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MAR 02 2007

PLANNING DIVISION

ASHWOOD PLACE RIGHT OF WAY ACQUISITION EXHIBIT

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

MARCH 1, 2006

Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS - SURVEYORS - PLANNERS
6111 BOLLINGER CANYON ROAD, SUITE 150 SAN RAMON, CALIFORNIA 94583
TEL: (925) 866-0322 FAX: (925) 866-4575

Project #
PL-2007-0113 GPA
PL-2007-0114 ZC



**CITY OF HAYWARD
MITIGATED NEGATIVE DECLARATION**

Notice is hereby given that the City of Hayward finds that could not have a significant effect on the environment as prescribed by the California Environmental Quality Act of 1970, as amended will occur for the following proposed project:

I. PROJECT DESCRIPTION:

PL-2007-0112 TR, -0113 GPA & -0114 ZC - Request to Amend the General Plan Land Use Designation from Industrial Corridor (IC) to Medium-Density Residential (MDR), to Change the Zone District from Industrial (I) to Planned Development (PD), and to Subdivide 4 Parcels to accommodate 80 Residential Units and Common Area

The property is located at the intersection of Orchard Avenue and Traynor Street, westerly of Soto Road.

Citation Homes proposes the development of a new residential community consisting of 80, 3-story, detached single-family residential structures on 5.68 acres. The homes would range in size from 1725-2050 square feet of living area with 2-car garages.

Each residence would be located on an individual lot. The project would contain a common park and various smaller open space areas to accommodate passive and active recreational activities.

Circulation would be accommodated by a network of private streets. Guest parking spaces would be located along the private streets. The primary access to the site would be from the westerly end of Orchard Avenue. The westerly end of Traynor Street would be gated and available for access by emergency vehicles.

II. FINDING PROJECT WILL NOT SIGNIFICANTLY AFFECT ENVIRONMENT:

The proposed project, as mitigated, could not have a significant effect on the environment.

FINDINGS SUPPORTING DECLARATION:

1. The proposed project has been reviewed according to the standards and requirements of the California Environmental Quality Act (CEQA) and an Initial Study/Environmental Evaluation Checklist has been prepared for the proposed project. The Initial Study has determined that the proposed project, with the recommended mitigation measures, could not result in significant effects on the environment.
2. The project will not adversely affect any scenic resources. The project is a redevelopment of a property that has been developed with various commercial/industrial uses.

3. The project will not have an adverse effect on agricultural land since the property is urban land previously developed for various commercial/industrial uses.
4. The project will not result in significant impacts related to changes in air quality. The project was previously developed for urban uses, and the proposed residential project does not conflict with the Bay Area Clean Air Plan or the City General Plan policies regarding air quality.
5. The project will not result in significant impacts to biological resources such as wildlife and wetlands since the site is urban land not located in a wetland and the site is fully developed.
6. The project will not result in significant impacts to known cultural resources including historical resources, archaeological resources, paleontological resources, unique topography or disturb human remains.
7. The project site is not located within a "State of California Earthquake Fault Zone," however, construction will be required to comply with the Uniform Building Code standards to minimize seismic risk due to ground shaking and liquefaction.
8. The project will not lead to the exposure of people to hazardous materials. Mitigation measures require that, prior to development, storage drums must be removed from the property, soils affected by petroleum hydrocarbons must be remediated and site closure activities must occur per city, state and federal regulations.
9. The project will meet all water quality standards. Drainage improvements, if any will be made to accommodate storm water runoff.
10. The project is consistent with the policies of the City General Policies Plan, as amended, the City of Hayward Design Guidelines and the Zoning Ordinance.
11. The project could not result in a significant impact to mineral resources since the site is urban land without the potential for the extraction of mineral resources.
12. With mitigation, the project will not result in significant impacts related to noise. The project will not result in a significant change to ambient noise levels and the noise levels within open space areas will be consistent with City requirements. To protect interior living spaces from inappropriate noise levels, all residences shall be provided with forced-air mechanical ventilation so that windows may be closed at the discretion of the occupants to control noise intrusion, and project plans shall be reviewed at the time of building permit application by the Building Official to ensure that the appropriate sound transmission class ratings are used the construction of the individual residences.
13. The project will not result in a significant impact to public services.
14. The project will not result in a significant impact to recreation facilities. The project will provide on-site recreational areas.

15. The project will not result in significant impacts to traffic or result in changes to traffic patterns or emergency vehicle access.

16. The project will not result in a significant impact to utilities and service systems.

III. PERSON WHO PREPARED INITIAL STUDY:

Richard E. Patenaude, AICP, Principal Planner
Dated: May 23, 2007

I. COPY OF ENVIRONMENTAL CHECKLIST IS ATTACHED

For additional information, please contact the City of Hayward, Planning Division, 777 B Street, Hayward, CA 94541-5007, telephone (510) 583-4200

DISTRIBUTION/POSTING

- Provide copies to all organizations and individuals requesting it in writing.
- Provide a copy to the Alameda County Clerk's Office.
- Reference in all public hearing notices to be distributed 20 days in advance of initial public hearing and/or published once in Daily Review 20 days prior to hearing.
- Project file.
- Post immediately upon receipt at the City Clerk's Office, the Main City Hall bulletin board, and in all City library branches, and do not remove until the date after the public hearing.



**DEPARTMENT OF COMMUNITY AND ECONOMIC DEVELOPMENT
Planning Division**

INITIAL STUDY CHECKLIST FORM

Project title: PL-2007-0112 TR, -0113 GPA & -0114 ZC - Request to Amend the General Plan Land Use Designation from Industrial Corridor (IC) to Medium-Density Residential (MDR), to Change the Zone District from Industrial (I) to Planned Development (PD), and to Subdivide 4 Parcels to accommodate 80 Residential Units and Common Area

Lead agency name and address: City of Hayward, 777 "B" Street, Hayward, CA 94541-5007

Contact persons and phone numbers: Richard E. Patenaude, AICP, Principal Planner, (510) 583-4213

Project location: The property is located at the intersection of Orchard Avenue and Traynor Street, westerly of Soto Road

Project sponsor's name and address:
Citation Homes Central
Charles G. McKeag, Vice President
404 Saratoga Ave., Ste. 400
Santa Clara, CA 95050

General Plan: Industrial Corridor

Zoning: Industrial (I)

Description of project: Citation Homes proposes the development of a new residential community consisting of 80, 3-story, detached single-family residential structures on 5.68 acres. The homes would range in size from 1725-2050 square feet of living area with standard 2-car garages.

Each residence would be located on an individual lot. The project would contain a common park and various smaller open space areas to accommodate passive and active recreational activities.

Circulation would be accommodated by a network of private streets. Guest parking spaces would be located along the private streets. The primary access to the site would be from the westerly end of Orchard Avenue. The westerly end of Traynor Street would be gated and available for access by emergency vehicles.

**Surrounding land
uses and setting:**

To the north, the project is bordered by Jackson Street (State Highway 92), a major thoroughfare providing access to Downtown and the Hayward-San Mateo Bridge; the City of Hayward Corporation Yard and various small service commercial uses are located across Jackson Street. To the south, the project is bordered by single-family residential development and a 2-acre parcel leased by SBC and used as a parking facility for its field employees. To the west, the property is bordered by the Union Pacific Railroad trackway; a number of industrial properties are located across the tracks. To the east (along Traynor Street), is a combination of commercial and industrial uses, including a self-storage facility and a multi-tenant commercial structure adjacent to the project.

**Other public agencies
whose approval is
required:**

None

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology /Soils |
| <input checked="" type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality | <input type="checkbox"/> Land Use / Planning |
| <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Population / Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

DETERMINATION: (To be completed by the Lead Agency)
On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signature

Richard E. Patenaude, AICP, Principal Planner

May 23, 2007
Date

City of Hayward

ENVIRONMENTAL ISSUES:

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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I. AESTHETICS -- Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Have a substantial adverse effect on a scenic vista?

<i>Comment: The project would not have a substantial adverse effect on a scenic vista. The site is relatively flat and surrounding structures are low-rise, up to two stories. The project would not obstruct the view of the hills to the east from existing or future development.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

<i>Comment: The project will not damage any scenic resources. The surrounding area is developed with a number of small mixed commercial/industrial entities.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Substantially degrade the existing visual character or quality of the site and its surroundings?

<i>Comment: The project will not degrade the existing visual character and quality of the site and its surroundings. The redevelopment of this site with contemporary residential structures would upgrade the character of the site and its surroundings.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?

<i>Comment: The project will not create a new source of light or glare that will adversely affect day or nighttime views in the area. Light would be contained on site and colors and materials would not create glare.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

II. AGRICULTURE RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Comment: *The site was planted in orchards until the 1950s, when it was developed as a lumber processing mill. The site has since been developed by a number of small mixed commercial/industrial entities.*

b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: *The project is not located in an agricultural district or an area used for agricultural purposes.*

c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: *The project area does not contain agricultural uses or farmland; See II a) and b).*

III. AIR QUALITY -- Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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a) Conflict with or obstruct implementation of the applicable air quality plan?				
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Comment: *The project will not conflict with the Bay Area 2000 Clean Air Plan or the City of Hayward General Plan policies relating to air quality.*

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
<p>b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?</p> <p>Comment: <i>The Bay Area air basin currently exceeds both federal and state standards for ozone and state standards for particulate matter <10 microns in diameter (PM10). The project is of a relatively small scale and is not expected to generate enough vehicle trips to make a significant contribution to the existing air quality violation.</i></p> <p><i>Air pollutants, especially suspended particulates, would be generated intermittently during the construction period. This could be potentially significant impact. However, the developer is required to ensure that unpaved construction areas are sprinkled with water as necessary to reduce dust generation, construction equipment is maintained and operated in such a way as to minimize exhaust emissions, and if construction activity is postponed, graded or vacant land will be immediately revegetated.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?</p> <p>Comment: <i>The project would not result in a considerable net increase of any criteria pollutant.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d) Expose sensitive receptors to substantial pollutant concentrations?</p> <p>Comment: <i>The project would not expose sensitive receptors to substantial pollutant concentrations.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e) Create objectionable odors affecting a substantial number of people?</p> <p>Comment: <i>The project would not create objectionable odors affecting a substantial number of people.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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IV. **BIOLOGICAL RESOURCES** -- Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| <p>a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?</p> <p><i>Comment:</i> <i>The property is currently used for mixed industrial/heavy commercial uses. There is no evidence of any candidate, sensitive, or special status species.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?</p> <p><i>Comment:</i> <i>The site contains no riparian or sensitive habitat.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?</p> <p><i>Comment:</i> <i>The site contains no wetlands.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?</p> <p><i>Comment:</i> <i>The site does not contain habitat used by migratory fish or wildlife nor is it a migratory wildlife corridor.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?</p> <p><i>Comment:</i> <i>The project would be in conformance with the General Plan, as amended, and all other policies and ordinances.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional or state habitat conservation plan?</p> <p><i>Comment:</i> <i>There are no habitat conservation plans affecting the property.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

V. **CULTURAL RESOURCES** -- Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| <p>a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?</p> <p><i>Comment:</i> <i>No known historical resources exist on-site.</i></p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Comment: No known archaeological resources exist in on-site. Soil investigations for hazardous materials have not encountered any such resources. If previously unknown resources are encountered during future grading activities, the developer and the City of Hayward will take appropriate measures to analyze and recover such resources.

c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: No known paleontological resources exist on-site. Soil investigations for hazardous materials have not encountered any such resources.

d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Comment: No known human burials are located on-site. Soil investigations for hazardous materials have not encountered any such remains. If previously unknown resources are encountered during future grading activities, the developer and the City of Hayward will take appropriate measures to analyze and recover such resources.

VI. GEOLOGY AND SOILS -- Would the project:

a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Comment: The project is not located within the Hayward Fault Zone.

ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Comment: The site is not located within a "State of California Earthquake Fault Zone" but would experience significant ground shaking during a seismic event. New construction would be required to comply with the Uniform Building Code Standards to minimize seismic risk due to ground shaking. Ground shaking can be expected at the site during a moderate to severe earthquake, which is common to virtually all development in the general region. This impact is considered less than significant.

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
iii) Seismic-related ground failure, including liquefaction? <i>Comment: The site is located within a high liquefaction hazard area. New construction would be required to comply with the Uniform Building Code Standards to minimize seismic risk due to liquefaction.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv) Landslides? <i>Comment: The project is not located within an area subject to landslides.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil? <i>Comment: The site is fully developed and the project would not result in substantial soil erosion or the loss of topsoil. Development of the site with associated drainage improvements would reduce such potential.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? <i>Comment: The project would not result in such hazards.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? <i>Comment: The project would be designed to avoid such risks.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater? <i>Comment: The project would be connected to the City of Hayward sewer system.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

VII. HAZARDS AND HAZARDOUS MATERIALS - Would the project:

a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? <i>Comment: The residential project would not create a significant hazard to the public or the environment; hazardous materials would not be associated with the project.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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	<i>Potentially Significant Impact</i>	<i>Potentially Significant Unless Mitigation Incorporation</i>	<i>Less Than Significant Impact</i>	<i>No Impact</i>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Comment: See VII a)</i>				
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Comment: See VII a).</i>				
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

***Comment:** No obvious evidence of underground storage tanks (USTs) was found as a result of a geophysical survey of the site.*

There are relatively low concentrations of metals and pesticides at the site. The concentrations of metals and pesticides in soils do not warrant additional investigation and do not limit future use of the property.

Shallow soil in four localized areas of the site has been affected with levels of petroleum hydrocarbons that are greater than screening levels developed by the Regional Water Quality Control Board for soil in a residential setting. The affected soil is associated with drum storage areas or rinsing of fuel tanks. It does not appear that groundwater has been affected. It is possible that other areas of localized affected soils are present and may be encountered during redevelopment. All affected soils would be properly assessed and managed.

Low levels of VOCs were detected in the groundwater samples collected from the site. The VOCs appear to be from off-site sources as no VOCs were detected in any of the soil samples. It does not appear that additional assessment or remediation of affected groundwater is warranted.

***Impacts:** There is a potential that future residents could be impacted by the presence petroleum hydrocarbons in the soil.*

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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Mitigation Measures: In order to reduce any potential impact of soil or groundwater contamination, the following measures will be required:

- The drums stored on the property, along with the contents, shall be disposed off-site at a licensed disposal facility in accordance with applicable federal, state and local laws and regulations;
- The soils affected by petroleum hydrocarbons, in amounts greater than 100 mg/Kg for TPHd and 500 mg/Kg for TPHmo, should be further assessed to determine vertical and lateral limits of contamination and the affected soil should be remediated prior to construction of the project; and
- All current tenants and future tenants should conduct site closure activities in accordance with City of Hayward Hazardous Materials Program requirements under the oversight of the Fire Department and/or other federal, state or city agencies as appropriate. The closure activities shall be completed prior to the tenants vacating the property.

Implementation of this mitigation measure will reduce hazardous materials impacts to a level of insignificance.

Monitoring: Condition of Approval

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Comment: The project is not located within an airport zone.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Comment: See VII e.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Comment: The project will not interfere with any known emergency response plan or emergency evacuation plan. The Hayward Fire Department serves the area. Emergency response times will be maintained.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Comment: The project is not located in an area of wildlands and is not adjacent to wildland fires.

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
VIII. HYDROLOGY AND WATER QUALITY -- Would the project:				
a) Violate any water quality standards or waste discharge requirements? <i>Comment: The project will meet all water quality standards and waste discharge requirements.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? <i>Comment: The site will be served with water by the City of Hayward and would not impact groundwater supplies. Therefore, water quality standards will not be violated and groundwater supplies will not be depleted</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? <i>Comment: The site is fully developed and the redevelopment to another use would not alter the existing drainage pattern.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? <i>Comment: See VIII c.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? <i>Comment: The site is fully developed and the redevelopment to another use would not increase runoff water that exceed capacities.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality? <i>Comment: See VIII a.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? <i>Comment: According to FEMA Flood Insurance Rate Maps (panel # 065033-0011E, dated Feb. 9, 2000); this site is located within Zone C and not within the 100-year flood hazard area.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows? <i>Comment: See VIII g.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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| i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?
<i>Comment: The site is not within the 100-year flood zone, is not near any levees and is not located downstream of a dam.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| j) Inundation by seiche, tsunami, or mudflow?
<i>Comment: The project is not in a location that would allow these phenomena to affect the site.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

IX. LAND USE AND PLANNING - Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Physically divide an established community?
<i>Comment: The project will not physically divide an established community. The project is at the end of streets that serve a variety of land uses; the property backs onto a railroad.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?
<i>Comment: As amended, the project would not conflict with any applicable land use plan, policy or regulation as conditioned or amended.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Conflict with any applicable habitat conservation plan or natural community conservation plan?
<i>Comment: See IV f.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

X. MINERAL RESOURCES – Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
<i>Comment: The project will not result in a significant impact to mineral resources since the subject site is located in an urbanized area that does not contain mineral resources that could be feasibly removed.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?
<i>Comment: See X a)</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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XI. NOISE - Would the project result in:

- a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Comment: *The primary noise sources at the site are vehicular traffic along West Jackson Street and trains on the adjacent Union Pacific Railroad. Intermittent noises are also generated by the existing uses on the site, occasional truck traffic along Traynor Street and Orchard Avenue, and occasional aircraft.*

Two long-term noise measurements were conducted at representative locations to complete the noise monitoring survey. The first measurement (LT-1) was made approximately 90 feet from the UPRR track and about 10 feet from the edge of the cut section for Jackson Street (about 40 horizontal feet from the edge of the curb). The measurement location was about 25 feet below the grade of the site. This location documented noise levels generated during train operations, as well as noise generated by vehicular traffic along West Jackson Street. A second long-term noise measurement (LT-2) was conducted at the same setback from the railroad track, but at a distance of about 90 feet from the edge of the cut section for West Jackson Street. The location was well-shielded from traffic noise along West Jackson Street by the cut section. Train movements dominated the noise environment here, with ambient background noise levels generated by traffic on West Jackson Street. The Ldn noise level over the course of the measurement period ranged from 72 to 74 dBA at LT-1 and 65-71 dBA at LT-2.

Approximately 126 train movements took place over a 5-day period. The number of train passages per day ranged from about 20 to 30, with about 72% of the trains occurring during daytime hours, 13% during evening hours, and 15% during nighttime hours. Through trains do not sound their horns at this location. Trains stopped on the siding do sound their horns when they begin to accelerate after stopping. Instantaneous maximum noise levels generated by train passages typically ranged from 80 to 90 dBA, with a few trains generating noise levels as high as 99 dBA Lmax. Instantaneous maximum noise levels in the range of 94 to 99 dBA, which occurred 7 times over the 5-day measurement period, are most likely associated with the occasional horn soundings.

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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The primary industrial noise sources were truck movements on the project site and along Traynor Street, construction activities to the south across the UPRR tracks, and occasional manufacturing equipment operations on the project site. The majority of these noise sources are temporary in nature or would be removed with the development of the project. It is assumed that the majority of the truck movements along Traynor Street are also associated with the current site usage, as trucks accessing the industrial sites to the north would not need to pass by the site. No significant noise-generating activities were identified on the adjacent properties to the north. Storage facilities do not typically generate substantial noise and the truss company does not appear to be in operation at this time. The only notable noise source generated by the commercial use to the east would be parking lot noise, which would not make a significant contribution to overall noise levels and would be removed if the site were to be developed with residences.

Impacts: The future weekday/Saturday Ldn noise levels at the height of an observer at ground level are:

- 76 dBA Ldn at the northwest corner of the site, which is exposed to West Jackson Street and is located about 15 feet above the roadway;
- 72 to 74 dBA Ldn at the southwest corner of the site, which is exposed to West Jackson Street and the railroad and is located about 25 feet above the roadway;
- 66 to 71 dBA Ldn at the southeast corner of the site depending on railroad activity; and
- Less than 60 dBA Ldn in well-shielded areas within the interior of the site.

The maximum acceptable exterior noise level for this residential development is 65 dBA Ldn. This goal is applied where outdoor use is a major consideration and in area considered to be "useable" open space.

Park: The park is well shielded from traffic noise and the only significant source of noise affecting the park would be railroad trains.

With the partial shielding provided by project homes, the park would be exposed to a future Ldn of 57 to 62 dBA. The project proposes an 8-foot wall between the Ashwood Place driveway and the railroad tracks. This wall would not be required to mitigate noise, but would provide about 1 dBA of noise reduction to the park.

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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Private Outdoor Areas: Traffic-generated noise levels would range from 71 to 76 dBA Ldn at ground level patios fronting West Jackson Street. In addition, front porches in the southeastern portion of the site would be exposed to train-generated noise levels of 66 to 71 dBA Ldn. The Ldn, excluding nighttime train passages, would be 61 to 63 dBA Ldn. The project proposes an 8-foot high wall along the southern and western boundaries of the site, between the Ashwood Place driveway and the railroad tracks, wrapping around along the top of the cut section adjacent to West Jackson Street. This barrier would provide about 7 dBA of noise reduction from traffic noise levels in the northwestern portion of the site, and about 1 dBA of noise attenuation from railroad noise levels. The resultant dBA levels, with the barrier, for the private outdoor areas, would be at or below the maximum acceptable level of 65 dBA with the exception of those at the northwestern portion of the site, which would experience levels of 66 dBA. The outdoor areas experiencing levels above 65 dBA would not be considered useable open space, and impact of their exclusion from the calculations for required useable open space would be negligible. City regulations do not require private outdoor open space.

Interior Noise Levels: The City specifies a maximum acceptable interior noise level of 45 dBA Ldn in new housing units. In addition, maximum instantaneous noise levels generated by the railroad are limited to 50 dBA in bedrooms during nighttime hours, 55 dBA in bedrooms during daytime hours and 55 dBA in other rooms during all hours of the day. Standard California residential construction typically provides about 15 dBA of noise reduction with windows partially open and 20 to 25 dBA of reduction with windows closed. Interior noise levels can typically be maintained below 45 dBA Ldn, 50 dBA for instantaneous noise levels, with incorporation of forced air mechanical; ventilation systems.

Mitigation Measures: In order to reduce any potential impact from noise, the following measures will be required:

- All project residences shall be provided forced-air mechanical ventilation so that windows may be closed at the discretion of the occupants to control noise intrusion; and
- The project plans shall be reviewed during detail design to determine the specific sound transmission class ratings necessary to obtain the prescribed overall A-weighted noise reduction values. A report shall be prepared and submitted to the Building Official, prior to issuance of any building permit, which stipulates the noise control treatments that have been included in the design to demonstrate compliance with the local noise standards.

Implementation of these mitigation measures will reduce hazardous materials impacts to a level of insignificance.

Monitoring: Condition of Approval

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
b) Exposure of persons to or generation of excessive ground borne vibration or ground borne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Comment: The UPRR line, located westerly of the project site, consists of one track that accommodates train movements in both directions and is elevated about 2 feet above the site elevation. Passenger and freight trains travel on this track and train speeds are typically high as they pass the site.</p> <p>Vibration levels measured on the site are representative of vibration levels that would enter the building foundation at ground level. Vibration monitoring was conducted during the daytime at a distance of 90 feet from the railroad tracks. Six train movements, including two freight trains and four Amtrak trains, were measured during this time period. Train speeds were measured to range from 51 to 81 mph. Groundborne vibration levels ranged from 64 to 74 VdB at the setback of the proposed nearest residences.</p> <p>Measured groundborne vibration levels were below the recommended FTA guidelines for infrequent events of 80 VdB and for occasional events of 75 VdB. Vibration levels would be considered acceptable at the site, although train vibration would occasionally be perceptible to occupants of the nearest residences. No vibration control measures would be needed.</p>				
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Comment: The project would not create a permanent increase in ambient noise levels.</p>				
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Comment: During the construction period, the project would cause a temporary increase in the ambient noise level.</p> <p>Mitigation Measure: Construction vehicles shall be required to use Traynor Street to access the project site.</p> <p>Implementation of these mitigation measures will reduce noise impacts to a level of insignificance.</p> <p>Monitoring: Condition of Approval</p>				
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Comment: Not applicable.</p>				
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Comment: Not applicable.</p>				

<i>Potentially Significant Impact</i>	<i>Potentially Significant Unless Mitigation Incorporation</i>	<i>Less Than Significant Impact</i>	<i>No Impact</i>
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XII. POPULATION AND HOUSING -- Would the project:

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|---|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?
<i>Comment: The project would redevelop an in-fill parcel in conformance with General Plan policy using existing infrastructure.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?
<i>Comment: The project replaces industrial uses; no housing will be removed.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?
<i>Comment: See XII b.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

XIII. PUBLIC SERVICES

Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Fire protection?
<i>Comment: The proposed project would have no effect upon, or result in only a minimal need for new or altered government services in fire and police protection, schools, maintenance of public facilities, including roads, and in other government services.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Police protection?
<i>Comment: See XIII a.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Schools?
<i>Comment: See XIII a.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Parks?
<i>Comment: See XIII a.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Other public facilities?
<i>Comment: See XIII a.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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XIV. RECREATION -

- a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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***Comment:** The project would provide internalized open space areas for passive and active use. The project would not cause such an increased use concentration as to accelerate physical deterioration of existing public park/recreational facilities.*

- b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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***Comment:** The project would provide internalized open space areas for passive and active use. The project would not include recreational facilities that might have an adverse physical effect on the environment.*

XV. TRANSPORTATION/TRAFFIC -- Would the project:

- a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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***Comment:** The proposed project would generate approximately 766 daily vehicle trips, with 60 trips during the weekday AM peak hour and 81 trips during the PM peak hour.*

The project was not found to cause a significant impact at any of the 8 study intersections. Although the worst approach at the two-way stop-controlled intersection of Soto Road/Traynor Street deteriorates from LOS D to LOS E during the PM peak hour, the intersection would not satisfy a signal warrant and, therefore, would not have a significant impact. As a whole, the intersection would operate at LOS A in both peak hours with the project.

The Mission Boulevard/Orchard Avenue intersection, which operates at LOS F in the PM peak hour, would continue to operate at LOS F. The proposed project's effect on conditions at this intersection would be considered less than significant.

With the addition of project-generated traffic, the queuing along the northbound approach to the Soto Road/Jackson Street intersection would continue to exceed the available storage, and vehicles would continue to spill back into the Soto Road/Traynor Street intersection. The queue in the northbound left-turn lanes would increase from 325 feet to 350 feet per lane during both peak hours. The queue in the northbound through-right-turn lane would increase from 550 feet to 575 feet during the AM peak hour, and from 800 feet to 850 feet during the PM peak hour.

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways? Comment: See XV a	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? Comment: The project will not affect air traffic patterns.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? Comment: The project will not substantially increase hazards.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access? Comment: The Hayward Fire Department has reviewed the project and finds the project acceptable to Hayward Fire Department requirements and standards.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Result in inadequate parking capacity? Comment: The project proposes parking in excess of the minimum number of spaces required by City regulations, as affected by State statutes.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)? Comment: Currently, the project area is not heavily served by transit. The only AC Transit bus route operating near the project area is the 77-Soto, with the nearest bus stops located on Soto Road, a few blocks east of the project site. The project is approximately 1.5 miles from the Hayward BART Station. Approximately 7.6% of the trips in the area are made on public transportation. The project, therefore, would be expected to generate approximately 4 transit trips during the AM peak hour, and 5 trips during the PM peak hour. Given the low number of transit trips generated by the project and the available transit capacity in the area, the project would have no significant adverse impacts on transit operations.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
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Existing pedestrian facilities in the project area are currently equipped to adequately handle the increase in pedestrian traffic caused by the project. Sidewalks are currently provided along most streets in the vicinity of the project site. Six- to 8-foot sidewalks exist along the south side of Traynor Street and intermittently along the north side of Traynor Street near the project site. Sidewalks exist along Orchard Avenue between Silverthorne Place and Soto Road. The project would provide sidewalks along Ashwood Place within the proposed project. These sidewalks would allow adequate pedestrian circulation within the project. The project proposes no features which would be unsafe to pedestrian travel. Therefore, there would be no significant impacts on pedestrian activity as a result of the proposed project.

Currently, a Class II bicycle lane exists on Soto Road beginning at Traynor Street a few blocks from the project site. The project proposes no features which would be unsafe to bicycle travel. Therefore, there would be no significant adverse impacts on bicycle activity as a result of the proposed project.

XVI. UTILITIES AND SERVICE SYSTEMS - Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
<i>Comment: The additional residential units will not exceed wastewater treatment requirements.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
<i>Comment: The project will not result in construction or expansion of wastewater treatment facilities.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
<i>Comment: The project will not result in construction or expansion of storm water drainage facilities that would cause significant environmental effects.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?
<i>Comment: The project will not have an impact on the water supply; therefore, it can be served by existing entitlements and resources.</i> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporation	Less Than Significant Impact	No Impact
e) Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? <i>Comment: The City of Hayward operates its own wastewater facility. This facility has the capacity to accommodate the amount of wastewater that will be generated by the project.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? <i>Comment: The project will not exceed wastewater treatment requirements.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? <i>Comment: The project will not result in construction or expansion of wastewater treatment facilities.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? <i>Comment: The project will not result in construction or expansion of storm water drainage facilities.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? <i>Comment: The project will not have an impact on the water supply; therefore, it can be served by existing entitlements and resources.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? <i>Comment: The City of Hayward operates its own wastewater facility. This facility has the capacity to accommodate the amount of wastewater that will be generated by the project.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? <i>Comment: Waste Management of Alameda County will dispose the solid waste. The Altamont landfill is available to the City of Hayward until 2009 and has sufficient capacity to handle the amount of solid waste generated by the project. The landfill recently received an approval that increases the capacity and adds 25 years to the life of the landfill to the year 2034.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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| l) Comply with federal, state, and local statutes and regulations related to solid waste? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <i>Comment: The project study area participates in the Waste Management of Alameda County recycling program. Construction and operation of the project will comply with all federal, state and local statutes and regulations related to solid waste.</i> | | | | |

XVII. MANDATORY FINDINGS OF SIGNIFICANCE --

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Sources:

Ashwood Place, Nelson Property, Hayward, CA – Environmental Noise and Vibration Study, February 14, 2007, by Dana M. Lodico, PE, Illingworth & Rodkin, Inc.

Ashwood Place Transportation Study, May 14, 2007, by DMJM Harris/Aecom

Geotechnical Investigation on Proposed Residential Development, Traynor Street and Orchard Avenue, Hayward, California for Signature Properties, July 22, 2005, by Simon Makdessi, P.E., G.E., Terrasearch, Inc.

Phase II Investigation, Nelsen Property, Traynor Street and Orchard Avenue, Hayward, California, August 29, 2005, by James P. Dunn, P.G., PES Environmental, Inc.

**Ashwood Place
Mitigation Monitoring & Reporting Program**

**General Plan Amendment Application No. PL-2007-0113
Zone Change Application No. PL-2007-0114 PD;
Tentative Tract Map Application No. PL-2007-0112 (TTM 7893);
Citation Homes (Applicant/Owner)**

June 14, 2007

Significant Environmental Impact – VII-d) HAZARDS AND HAZARDOUS MATERIALS

There are relatively low concentrations of metals and pesticides at the site. The concentrations of metals and pesticides in soils do not warrant additional investigation and do not limit future use of the property.

Shallow soil in four localized areas of the site has been affected with levels of petroleum hydrocarbons that are greater than screening levels developed by the Regional Water Quality Control Board for soil in a residential setting. The affected soil is associated with drum storage areas or rinsing of fuel tanks. It does not appear that groundwater has been affected. It is possible that other areas of localized affected soils are present and may be encountered during redevelopment. All affected soils would be properly assessed and managed.

Low levels of VOCs were detected in the groundwater samples collected from the site. The VOCs appear to be from off-site sources as no VOCs were detected in any of the soil samples. It does not appear that additional assessment or remediation of affected groundwater is warranted.

There is a potential that future residents could be impacted by the presence petroleum hydrocarbons in the soil.

Mitigation Measure

In order to reduce any potential impact of soil or groundwater contamination, the following measures will be required:

- *The drums stored on the property, along with the contents, shall be disposed off-site at a licensed disposal facility in accordance with applicable federal, state and local laws and regulations;*
- *The soils affected by petroleum hydrocarbons, in amounts greater than 100 mg/Kg for TPHd and 500 mg/Kg for TPHmo, should be further assessed to determine vertical and lateral limits of contamination and the affected soil should be remediated prior to construction of the project; and*
- *All current tenants and future tenants should conduct site closure activities in accordance with City of Hayward Hazardous Materials Program requirements under the oversight of the Fire Department and/or other federal, state or city agencies as appropriate. The closure activities shall be completed prior to the tenants vacating the property.*

Implementation of this mitigation measure will reduce hazardous materials impacts to a level of insignificance.

Implementing Responsibility

Project developers.

Monitoring Responsibility

City of Hayward Planning and Engineering & Transportation Divisions.

Timing

Removal of drums and soil remediation: prior to grading.

Site Closure: prior to vacation of premises.

Significant Environmental Impact – XI-a) NOISE

The primary noise sources at the site are vehicular traffic along West Jackson Street and trains on the adjacent Union Pacific Railroad. Intermittent noises are also generated by the existing uses on the site, occasional truck traffic along Traynor Street and Orchard Avenue, and occasional aircraft.

Two long-term noise measurements were conducted at representative locations to complete the noise monitoring survey. The first measurement (LT-1) was made approximately 90 feet from the UPRR track and about 10 feet from the edge of the cut section for Jackson Street (about 40 horizontal feet from the edge of the curb). The measurement location was about 25 feet below the grade of the site. This location documented noise levels generated during train operations, as well as noise generated by vehicular traffic along West Jackson Street. A second long-term noise measurement (LT-2) was conducted at the same setback from the railroad track, but at a distance of about 90 feet from the edge of the cut section for West Jackson Street. The location was well-shielded from traffic noise along West Jackson Street by the cut section. Train movements dominated the noise environment here, with ambient background noise levels generated by traffic on West Jackson Street. The Ldn noise level over the course of the measurement period ranged from 72 to 74 dBA at LT-1 and 65-71 dBA at LT-2.

Approximately 126 train movements took place over a 5-day period. The number of train passages per day ranged from about 20 to 30, with about 72% of the trains occurring during daytime hours, 13% during evening hours, and 15% during nighttime hours. Through trains do not sound their horns at this location. Trains stopped on the siding do sound their horns when they begin to accelerate after stopping. Instantaneous maximum noise levels generated by train passages typically ranged from 80 to 90 dBA, with a few trains generating noise levels as high as 99 dBA Lmax. Instantaneous maximum noise levels in the range of 94 to 99 dBA, which occurred 7 times over the 5-day measurement period, are most likely associated with the occasional horn soundings.

The primary industrial noise sources were truck movements on the project site and along Traynor Street, construction activities to the south across the UPRR tracks, and occasional manufacturing equipment operations on the project site. The majority of these noise sources are temporary in nature or would be removed with the development of the project. It is assumed that the majority of the truck movements along Traynor Street are also associated with the current site usage, as trucks accessing the industrial sites to the north would not need to pass by the site. No significant noise-generating activities were identified on the adjacent properties to the north. Storage facilities do not typically generate substantial noise

and the truss company does not appear to be in operation at this time. The only notable noise source generated by the commercial use to the east would be parking lot noise, which would not make a significant contribution to overall noise levels and would be removed if the site were to be developed with residences.

The future weekday/Saturday Ldn noise levels at the height of an observer at ground level are:

- *76 dBA Ldn at the northwest corner of the site, which is exposed to West Jackson Street and is located about 15 feet above the roadway;*
- *72 to 74 dBA Ldn at the southwest corner of the site, which is exposed to West Jackson Street and the railroad and is located about 25 feet above the roadway;*
- *66 to 71 dBA Ldn at the southeast corner of the site depending on railroad activity; and*
- *Less than 60 dBA Ldn in well-shielded areas within the interior of the site.*

The City specifies a maximum acceptable interior noise level of 45 dBA Ldn in new housing units. In addition, maximum instantaneous noise levels generated by the railroad are limited to 50 dBA in bedrooms during nighttime hours, 55 dBA in bedrooms during daytime hours and 55 dBA in other rooms during all hours of the day. Standard California residential construction typically provides about 15 dBA of noise reduction with windows partially open and 20 to 25 dBA of reduction with windows closed. Interior noise levels can typically be maintained below 45 dBA Ldn, 50 dBA for instantaneous noise levels, with incorporation of forced air mechanical; ventilation systems.

Mitigation Measure

In order to reduce any potential impact from noise, the following measures will be required:

- *All project residences shall be provided forced-air mechanical ventilation so that windows may be closed at the discretion of the occupants to control noise intrusion; and*
- *The project plans shall be reviewed during detail design to determine the specific sound transmission class ratings necessary to obtain the prescribed overall A-weighted noise reduction values. A report shall be prepared and submitted to the Building Official, prior to issuance of any building permit, which stipulates the noise control treatments that have been included in the design to demonstrate compliance with the local noise standards.*

Implementation of these mitigation measures will reduce hazardous materials impacts to a level of insignificance.

Implementing Responsibility

Project developers and City of Hayward Building Division.

Monitoring Responsibility

City of Hayward Planning and Building Divisions.

Timing

Prior to issuance of building permits.

Significant Environmental Impact – XI-d) NOISE

During the construction period, the project would cause a temporary increase in the ambient noise level.

Mitigation Measure

Construction vehicles shall be required to use Traynor Street to access the project site.

Implementation of this mitigation measure will reduce noise impacts to a level of insignificance.

Implementing Responsibility

Project developers and City of Hayward Engineering & Transportation Divisions.

Monitoring Responsibility

City of Hayward Planning and Engineering & Transportation Divisions.

Timing

During construction activities.

SHEET INDEX

NO.

1 COVER SHEET - SHEET INDEX

CIVIL PLANS

- 2 TM-01 VESTING TENTATIVE MAP - EXISTING PROPERTY
- 3 TM-02 VESTING TENTATIVE MAP - SITE & CIRCULATION PLAN
- 4 TM-03 VESTING TENTATIVE MAP - GRADING PLAN
- 5 TM-04 VESTING TENTATIVE MAP - UTILITY PLAN
- 6 C1 PLANNED DEVELOPMENT SITE PLAN

LANDSCAPE PLANS

- 7 L-01 OVERALL LANDSCAPE PLAN
- 8 L-02 LANDSCAPE ENLARGEMENTS
- 9 L-03 LANDSCAPE ENLARGEMENT/ELEVATION

NO.

ARCHITECTURAL PLANS

- 10 A1-1 PLAN 1725 FRONT ELEVATIONS AND ROOF PLANS
- 11 A1-2 PLAN 1725 FLOOR PLANS
- 12 A1-3 PLAN 1725 SIDES AND REAR ELEVATIONS
- 13 A1-4 PLAN 1725 ENHANCED SIDES AND REAR ELEVATIONS
- 14 A2-1 PLAN 1900 FRONT ELEVATIONS AND ROOF PLANS
- 15 A2-2 PLAN 1900 FLOOR PLANS
- 16 A2-3 PLAN 1900 SIDES AND REAR ELEVATIONS
- 17 A2-4 PLAN 1900 ENHANCED SIDES AND REAR ELEVATIONS
- 18 A3-1 PLAN 2060 FRONT ELEVATIONS AND ROOF PLANS
- 19 A3-2 PLAN 2060 FLOOR PLANS
- 20 A3-3 PLAN 2060 SIDES AND REAR ELEVATIONS
- 21 A3-4 PLAN 2060 ENHANCED SIDES AND REAR ELEVATIONS

OWNER/DEVELOPER

CITATION HOMES CENTRAL
404 SARATOGA AVENUE, SUITE 100
SANTA CLARA, CA 95050
(408) 985-6000
CONTACT: CHARLES McKEAG

CIVIL ENGINEER

CARLSON, BARBEE & GIBSON, INC.
6111 BOLLINGER CANYON ROAD, SUITE 150
SAN RAMON, CA 94583
(925) 866-0322
CONTACT: LEE ROSENBLATT

LANDSCAPE ARCHITECT

AAM DESIGN, INC.
3730 MT. DIABLO BOULEVARD, SUITE 310
LAFAYETTE, CA 94549
(925) 283-8886
CONTACT: TIM McKIMSON

ARCHITECT

DANIELIAN ASSOCIATES
60 CORPORATE PARK
IRVINE, CA 92606
(949) 474-6030
CONTACT: JOANN LEE-KIM



RECEIVED

JUN 07 2007

PLANNING DIVISION

PROJECT #
PL-2007-0113/0114/0112
GAP/ZC/TTM 7893

MAY 4, 2007

AAM Arthur McKimson
AAM DESIGN, INC. CORPORATION
Landscape Architecture and Civil Engineering
3730 Mount Diablo Boulevard Suite 310
Lafayette, California 94549
Telephone: 925-283-8886
Facsimile: 925-283-7057
Email: aamdesign@aacpbaci.net

DANIELIAN ASSOCIATES
ARCHITECTURE + PLANNING
Sixty Corporate Park Irvine, CA 92606 P 949 474 6030 F 949 474 1422 www.danielian.com
000000.00 11.21.05

ASHWOOD PLACE
PLANNED DEVELOPMENT
HAYWARD, CALIFORNIA



cbg Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS • SURVEYORS • PLANNERS
6111 Bollinger Canyon Road, Suite 150 • San Ramon, CA 94583
925-866-0322 • fax 925-866-8575
www.cbang.com

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	SUBDIVISION BOUNDARY
---	---	PROPERTY LINE
---	---	CURB, CUTTER AND SIDEWALK
---	---	CENTERLINE
•	•	SPOT ELEVATIONS
---	---	PUBLIC SERVICE UTILITY EASEMENT
---	---	PRIVATE STORM DRAIN EASEMENT
---	---	PRIVATE ACCESS EASEMENT
---	---	PG&E EASEMENT
---	---	RAILROAD EASEMENT
•	•	MONUMENT
1		LOT NUMBER

TANGENT TABLE

NO.	BEARING	LENGTH
L1	S53.74°W 70'	17.28'
L2	N22.5°W 74.7'	11.14'

CURVE TABLE

NO.	RADIUS	BEARING	LENGTH
C1	15.00'	S33°04' 18.71"	18.71'
C2	50.00'	N22°28' 46"	25.73'

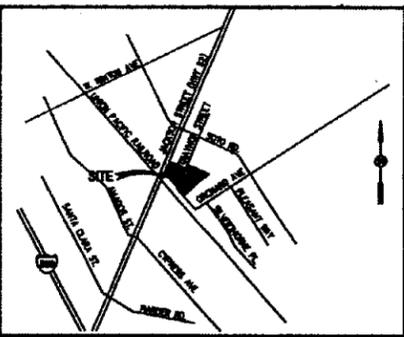
NOTE: ALL EXISTING EASEMENTS TO BE OBTAINED OR ABANDONED UNLESS OTHERWISE SPECIFIED.

WE, CITATION HOMES CENTRAL, AGREE TO THE FILING OF SAID MAP AND TO COMPLY WITH THE PROVISIONS OF THE CITY OF HAYWARD SUBDIVISION ORDINANCE AND THE STATE MAP ACT AS THEY APPLY TO THE PROCESSING AND APPROVAL OF SAID MAP.

BY: _____ DATE: _____

CONTACTS

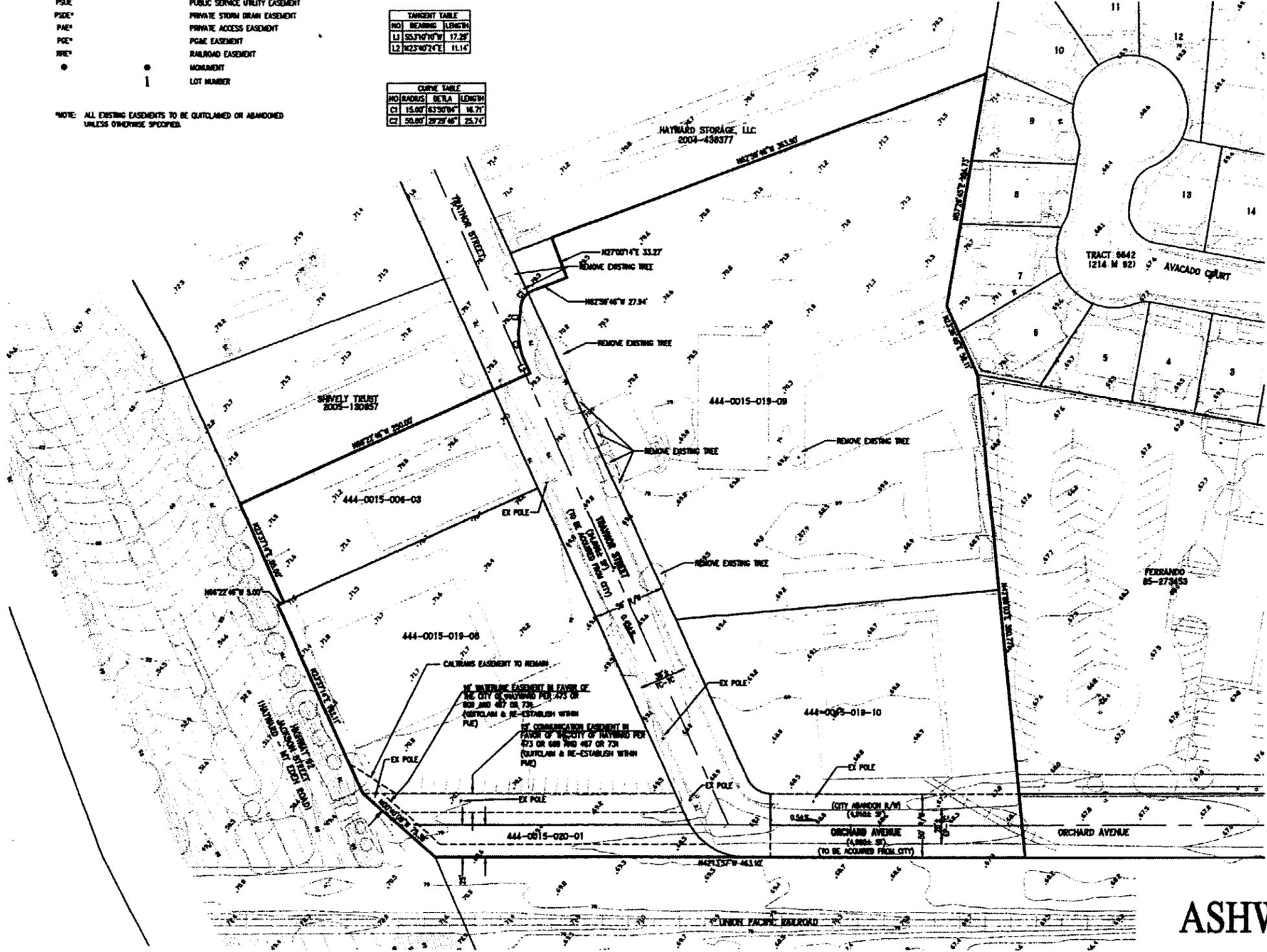
- OWNER/SUBDIVIDER: CITATION HOMES CENTRAL
404 SHASTA AVENUE, SUITE 100
SANTA CLARA, CA 95050
(408) 965-6000
CHARLES MCGEE
- ENGINEER: CARLSON, BARBEE & GIBSON, INC.
6711 BOLLINGER CANYON ROAD, SUITE 150
SAN RAMON, CA 94583
(925) 888-4322
LEE ROSENBLATT, P.E. 65489
- SOILS ENGINEER: TERRARESEARCH INC.
257 BRIGHT BROTHERS AVE.
LINEMORE, CA 94551
(925) 243-8882
- TOPOGRAPHIC SOURCE: AERIAL TOPOGRAPHY COMPILED BY:
AEROMETRIC SURVEYS
915 CLAREMONT STREET
SAN MATEO, CA 94402



VICINITY MAP (ITS)

GENERAL NOTES

- ASSESSORS PARCEL NO.: 444-0015-008-03
444-0015-010-08
444-0015-010-09
444-0015-010-10
444-0015-020-01
- SITE ADDRESS: TRAYNOR STREET @ ORCHARD AVE.
HAYWARD, CA
- SITE AREA: 5.822 ACRES
- EXISTING ZONING: MIXED INDUSTRIAL
PROPOSED ZONING: PLANNED DEVELOPMENT
- EXISTING USE: MIXED INDUSTRIAL
PROPOSED USE: SINGLE-FAMILY RESIDENTIAL
80 SINGLE FAMILY LOTS, IN PARCELS
- MINIMUM LOT SIZE: 32' X 46' (1,472 SF)
- BENCHMARK: USCGAS BRASS DISC IN DRILL HOLE AT SOUTH END OF EAST CURB OF UNION PACIFIC RAILROAD OVERPASS AT JACKSON STREET, CITY OF HAYWARD ELEVATION = 74.58
- EXISTING STRUCTURES: ALL EXISTING BUILDINGS, PAVEMENT, AND TREES WITHIN BOUNDARY TO BE REMOVED.
- STREETS: ALL STREETS AND DRIVE ASILES WITHIN THE SUBDIVISION WILL BE PRIVATE AND WILL BE PRIVATELY MAINTAINED. ALL STREETS WILL BE WITHIN PUE'S (MINIMUM LONGITUDINAL SLOPE = 0.5%).
- STREET TREES: STREET TREES SHALL BE PROVIDED AS REQUIRED BY THE CITY, INSTALLED PER CITY STD 30-122 AND WILL BE PRIVATELY MAINTAINED BY THE HOMEOWNERS ASSOCIATION.
- STREET LIGHTS: STREET LIGHTS ON PRIVATE STREETS WILL BE PRIVATELY MAINTAINED.
- WALLS: ALL WALLS WILL BE PRIVATE FACILITIES AND PRIVATELY MAINTAINED.
- STORM DRAIN: PROPOSED ON-SITE STORM DRAIN FACILITIES WILL BE PRIVATE FACILITIES AND WILL BE PRIVATELY MAINTAINED BY THE HOMEOWNERS ASSOCIATION, EXCEPT AS NOTED BELOW. PROPOSED STORM DRAIN FACILITIES CONNECTING THE PUBLIC STORM DRAIN IN TRAYNOR STREET TO THE PUBLIC STORM DRAIN IN ORCHARD AVENUE WILL BE WITHIN PUBLIC STORM DRAINAGE EASEMENTS AND WILL BE OWNED AND MAINTAINED BY THE CITY OF HAYWARD.
- PUBLIC UTILITIES: PROPOSED WATER AND SANITARY SEWER FACILITIES WITHIN ROADWAYS WILL BE CONSTRUCTED AS PER CITY OF HAYWARD STANDARDS AND DEDICATED TO THE CITY.
- FLOOD ZONE: ZONE B - BETWEEN LIMITS OF 100 YEAR AND 500 YEAR FLOOD
ZONE C - AREAS OF MINIMAL FLOODING
REFER TO:
FLOOD INSURANCE RATE MAP
PANEL 080003-0011 E (FEBRUARY 9, 2000)
- PRIVATE UTILITIES: N/A
- WELLS ON-SITE: NONE
- WATER: CITY OF HAYWARD
- SEWER: CITY OF HAYWARD
- GAS & ELECTRIC: PG&E
- TELEPHONE: SBC
- CABLE TV: COMCAST CABLE
- DIMENSIONS: ALL DIMENSIONS ARE PRELIMINARY AND SUBJECT TO FINAL MAP.
- FINAL MAP: MULTIPLE MAPS MAY BE FILED FOR THIS SITE.

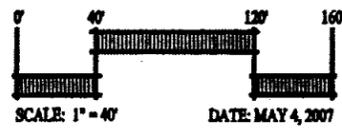


VESTING TENTATIVE MAP EXISTING PROPERTY ASHWOOD PLACE-TRACT 7893

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

SHEET INDEX

- TM-01 TITLE SHEET-EXISTING CONDITIONS
- TM-02 SITE & CIRCULATION PLAN
- TM-03 GRADING PLAN
- TM-04 UTILITY PLAN

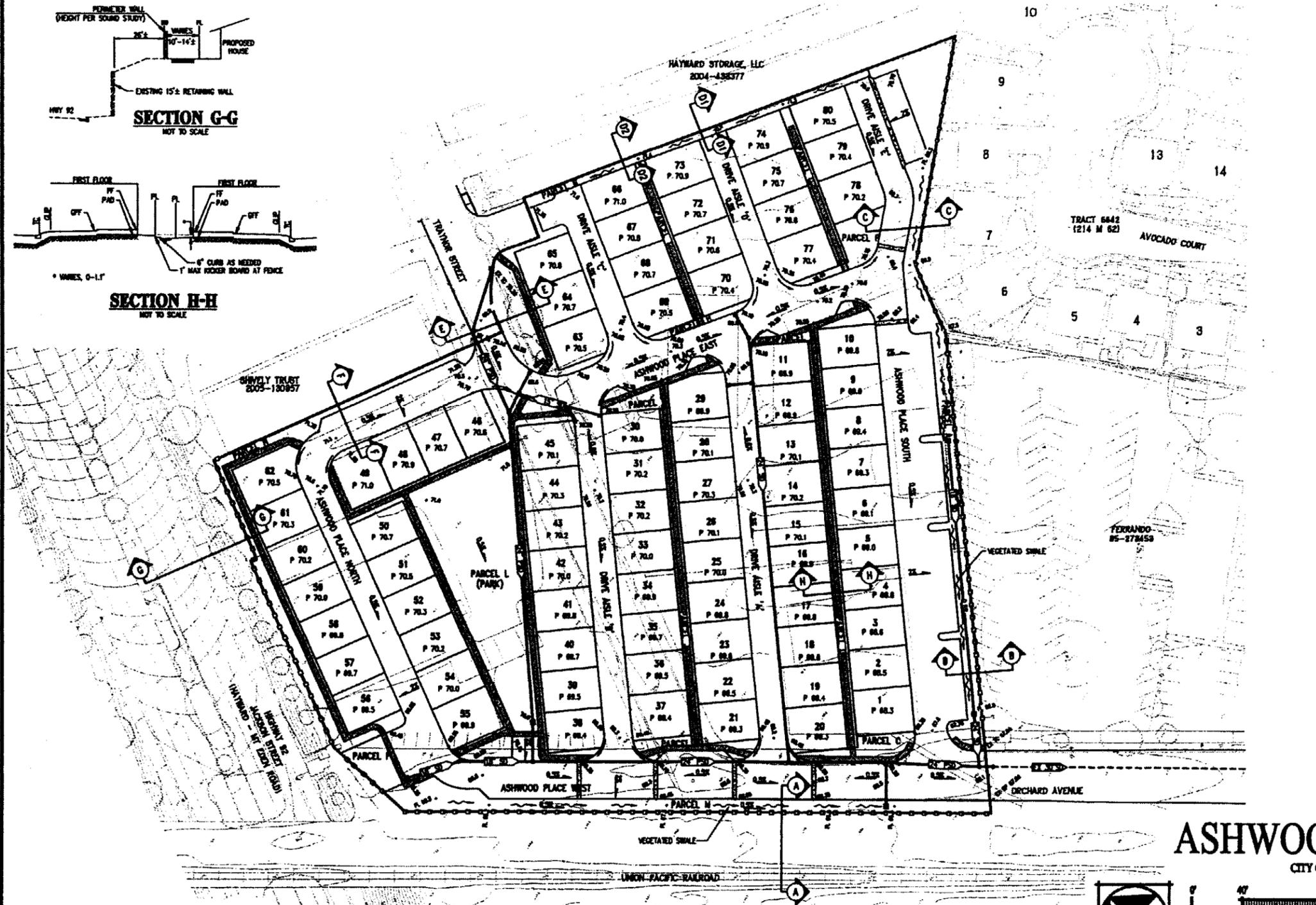
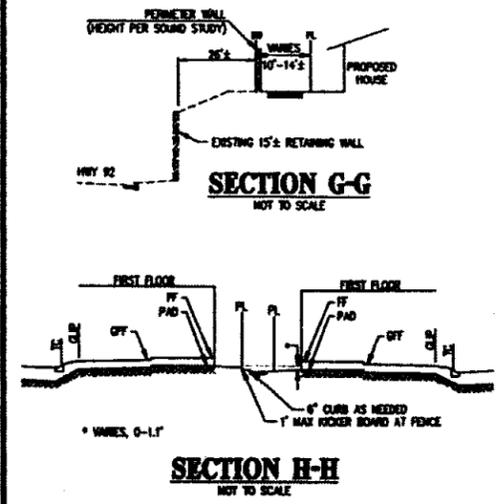
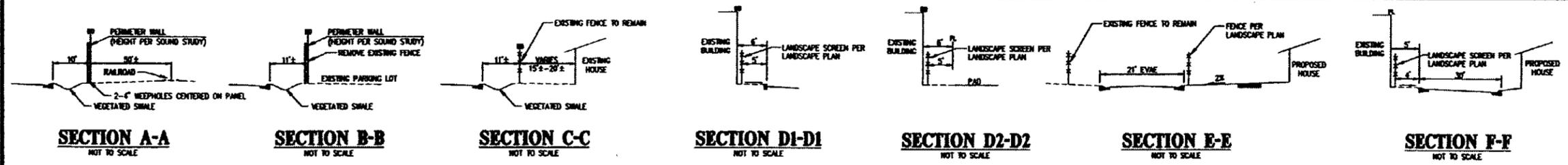


Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS - SURVEYORS - PLANNERS

6711 BOLLINGER CANYON ROAD, SUITE 150
SAN RAMON, CALIFORNIA 94583

TEL: (925) 888-4322
FAX: (925) 888-4329

SHEET NUMBER
TM-01
2 OF 21



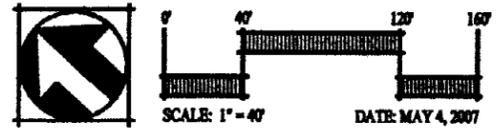
- GRADING NOTES**
- GRADING: GRADING WILL BE DONE AS PART OF THIS TRACT
 - LIMIT OF GRADING: THE LIMIT OF GRADING IS THE PROPOSED SUBDIVISION BOUNDARY EXCEPT ANY IMPROVEMENTS AT THE TRAINER STREET SUBWALL AND THE ORCHARD AVENUE PROJECT ENTRANCE
 - SOILS REPORT: ELEMENTS OF GRADING ARE IN CONFORMANCE WITH PRELIMINARY SOILS REPORT

LEGEND

EXISTING	PROPOSED	DESCRIPTION
- - - - -	- - - - -	PROPERTY LINE
- - - - -	- - - - -	CURB, GUTTER AND SIDEWALK
- - - - -	- - - - -	VALLEY GUTTER
- - - - -	- - - - -	CENTRELINE
•	•	SPOT ELEVATIONS
- - - - -	- - - - -	STORM DRAIN LINE (PRIVATE)
- - - - -	- - - - -	STORM DRAIN LINE (PUBLIC)
- - - - -	- - - - -	SANITARY SEWER
- - - - -	- - - - -	WATER
- - - - -	- - - - -	PERIMETER MASONRY WALL
- - - - -	- - - - -	EXISTING FENCE/WALL
•	•	SANITARY SEWER MANHOLE
•	•	STORM DRAIN MANHOLE
•	•	CURB INLET
•	•	FIELD INLET
•	•	FIRE HYDRANT
•	•	MONUMENT
*	*	STREETLIGHT
20		LOT NUMBER
PL		PUBLIC UTILITY EASEMENT
CV		EMERGENCY VEHICLE ACCESS EASEMENT
OFF		CARAGE FINISHED FLOOR
GLP		CARAGE LP
FF		FRESHED FLOOR
BD		BOUNDARY
PL		PROPERTY LINE

VESTING TENTATIVE MAP GRADING PLAN
ASHWOOD PLACE-TRACT 7893
 CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

NOTE: RETAINING WALLS ADJACENT TO LOTS AND PARKS AND IN SOYARDS NOT SHOWN

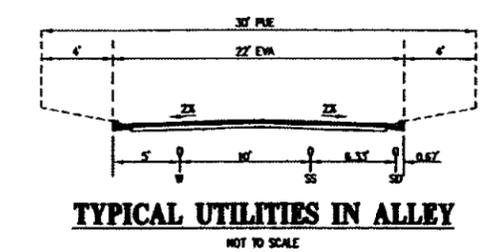
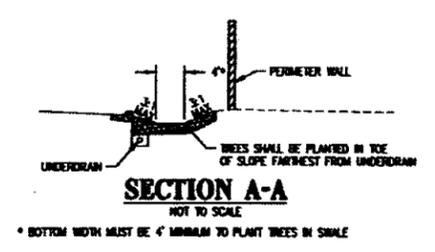
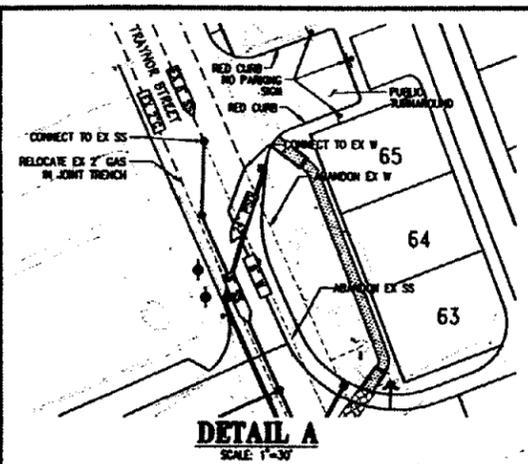


Carlson, Barbee & Gibson, Inc.
 CIVIL ENGINEERS - SANITARY - PLUMBERS

8711 BULLHORN CANYON ROAD, SUITE 100
 SAN JOSE, CALIFORNIA 95138

TEL: 950.880.0000
 FAX: 950.880.0000

SHEET NUMBER
TM-03
 4 OF 71

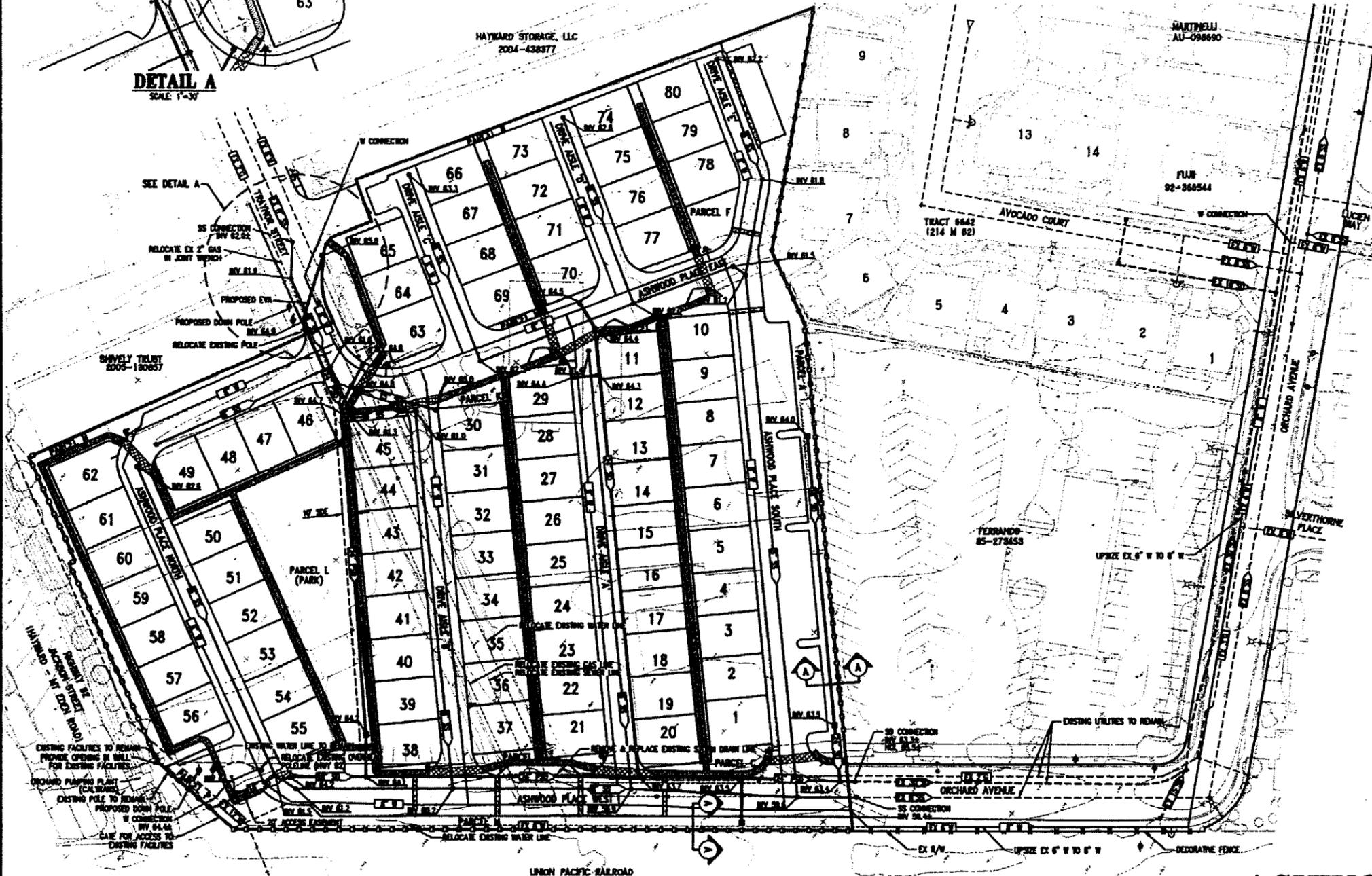


UTILITY NOTES

1. EXISTING UTILITIES: ALL EXISTING UTILITIES IN TRAYNOR STREET AND ORCHARD AVENUE WITHIN THE SUBDIVISION BOUNDARY TO BE REMOVED AND RELOCATED PER CITY OF HAYWARD STANDARDS.
2. PUBLIC UTILITIES: PROPOSED WATER FACILITIES WITHIN THE SUBDIVISION WILL BE CONSTRUCTED PER LOCAL STANDARDS AND DEDICATED TO THE CITY OF HAYWARD. PROPOSED SANITARY SEWER WITHIN THE SUBDIVISION WILL BE CONSTRUCTED PER LOCAL STANDARDS AND DEDICATED TO THE CITY OF HAYWARD. PRIVATE ROADWAYS WILL BE WITHIN A PUE FOR ACCESS.
3. PRIVATE UTILITIES: PROPOSED STORM DRAIN FACILITIES WITHIN THE SUBDIVISION WILL BE PRIVATELY MAINTAINED, EXCEPT AS NOTED.
4. STORM DRAIN: PRIVATE (NOA MAINTAINED) MIN SLOPE OF PROPOSED STORM DRAIN PIPE = 0.002
PUBLIC (CITY OF HAYWARD) MIN SLOPE OF PROPOSED STORM DRAIN PIPE = 0.002
PUBLIC STORM DRAIN FACILITIES TO BE CONSTRUCTED TO CITY OF HAYWARD STANDARDS. ALL STORM PIPE TO BE RCP OR PLASTIC PER CITY OF HAYWARD STANDARDS.
5. WATER: CITY OF HAYWARD RELOCATE ALL EXISTING FIRE HYDRANTS ON OLD TRAYNOR STREET & ORCHARD AVENUE PER CITY REQUIREMENTS. WATER SHALL BE CONSTRUCTED OF PVC PER CITY ORDINANCE.
6. SEWER: CITY OF HAYWARD STANDARD MIN SLOPE OF PROPOSED SEWER PIPE = 0.0033
MIN SLOPE OF PROPOSED SEWER PIPE CONNECTING EXISTING SEWER AT TRAYNOR TO EXISTING SEWER AT ORCHARD = 0.002
MIN SIZE OF PROPOSED SEWER MAIN IS 8\"/>
- 7. GAS & ELECTRIC: PUE
- 8. TELEPHONE: SIC
- 9. CABLE TV: COMCAST CABLE
- 10. UTILITIES: UTILITIES SHOWN ARE TO BE USED AS A GUIDE AND MAY CHANGE DURING FINAL DESIGN. DESIGN SHALL ADHERE TO CITY OF HAYWARD STANDARDS.

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROPERTY LINE
---	---	CURB, GUTTER AND SIDEWALK
---	---	WALLEY GUTTER
---	---	COVERLINE
777.0	777.70	SPOT ELEVATIONS
---	---	STORM DRAIN LINE (PRIVATE)
---	---	STORM DRAIN LINE (PUBLIC)
---	---	SANITARY SEWER
---	---	WATER
---	---	PERIMETER MASONRY WALL
---	---	EXISTING FENCE/WALL
---	---	DECORATIVE FENCE
●	●	SANITARY SEWER MANHOLE
●	●	STORM DRAIN MANHOLE
●	●	CURB INLET
●	●	FIELD INLET
●	●	FIRE HYDRANT
●	●	MONUMENT
*	*	STREETLIGHT
+	+	OVER HEAD POLES
20	20	LOT NUMBER
PUE	PUE	PUBLIC UTILITY EASEMENT
EVE	EVE	EMERGENCY VEHICLE ACCESS EASEMENT
SE	SE	STORM DRAIN EASEMENT



VESTING TENTATIVE MAP UTILITY PLAN
ASHWOOD PLACE-TRACT 7893
 CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

SCALE: 1" = 40'

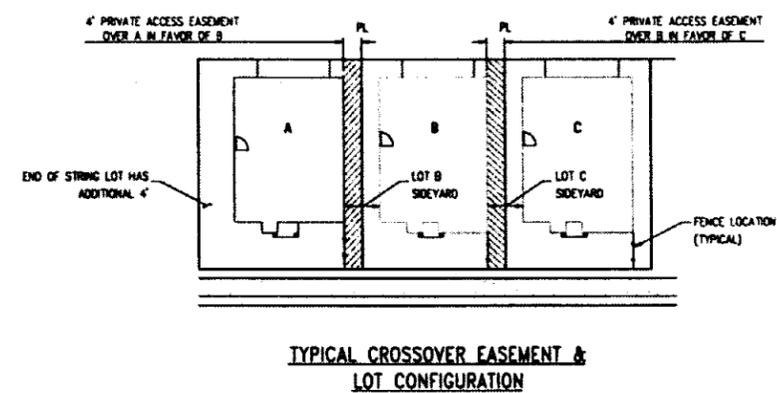
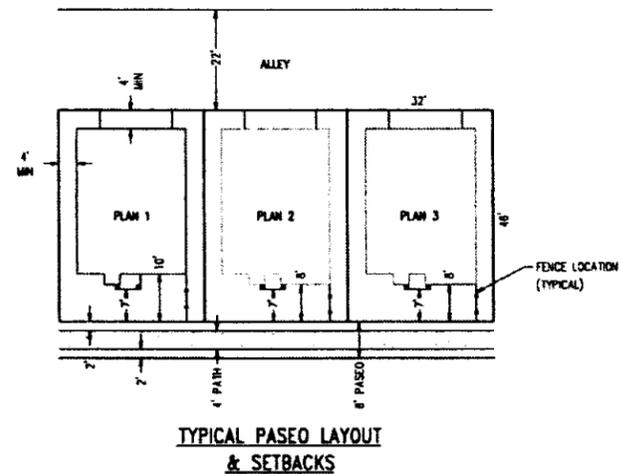
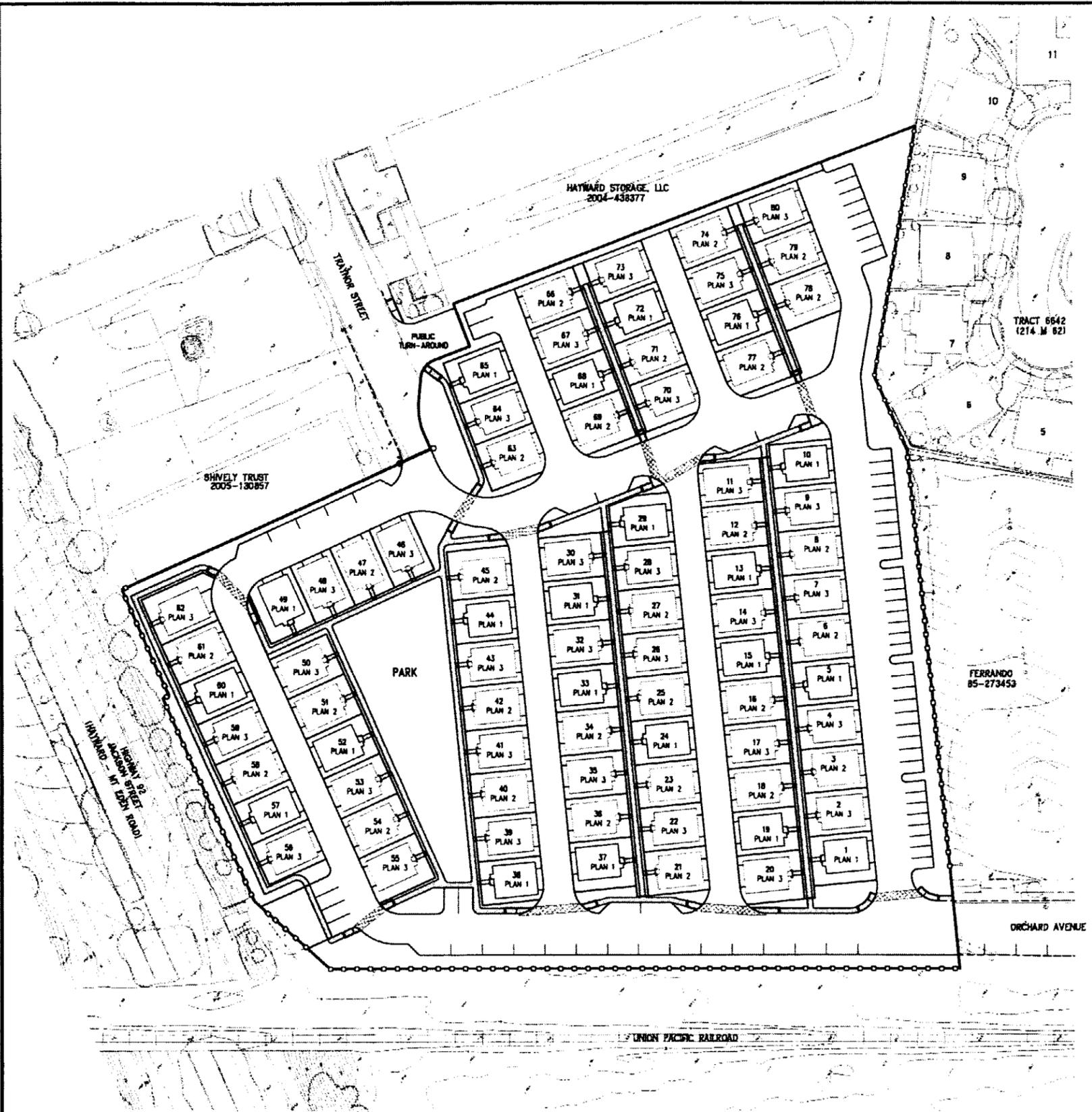
DATE: MAY 4, 2007

Carlson, Barbee & Gibson, Inc.
CIVIL ENGINEERS - SURVEYORS - PLANNERS

897 HOLLAND AVENUE, SUITE 100
 HAYWARD, CALIFORNIA 94541

TEL: 925-885-8888
 FAX: 925-885-8888

SHEET NUMBER
TM-04
 5 OF 21



SETBACKS:
 GARAGE: 4' MIN
 SIDE: 4' MIN
 FRONT LIVING SPACE: 8' MIN
 NOTE: PORCH OR ENTRY MAY ENCR OACH UP TO 3' INTO FRONT SETBACK

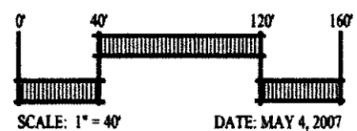
LOTSIZE:
 46'x32' (1,472 SF)

UNIT MIX

PLAN	SF	TOTAL	%
1	1,725	21	26%
2	1,900	28	35%
3	2,060	31	39%
TOTAL		80	100%

**PLANNED DEVELOPMENT
 SITE PLAN
 ASHWOOD PLACE-TRACT 7893**

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



DATE: MAY 4, 2007

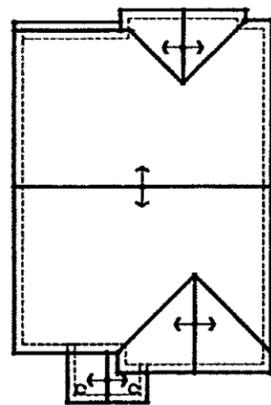
Carlson, Barbee & Gibson, Inc.
 CIVIL ENGINEERS - SURVEYORS - PLANNERS

8111 BOLLINGER CANYON ROAD, SUITE 100
 SAN RAMON, CALIFORNIA 94583

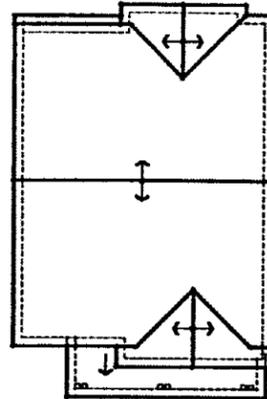
TEL: (925) 888-4333
 FAX: (925) 888-8079

SHEET NUMBER
C1
 6 OF 21

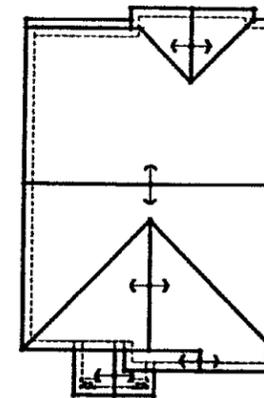
DUE TO THE SIZE AND
COLOR OF THE FOLLOWING
ATTACHMENTS, THEY
HAVE BEEN INCLUDED AS
SEPARATE LINKS



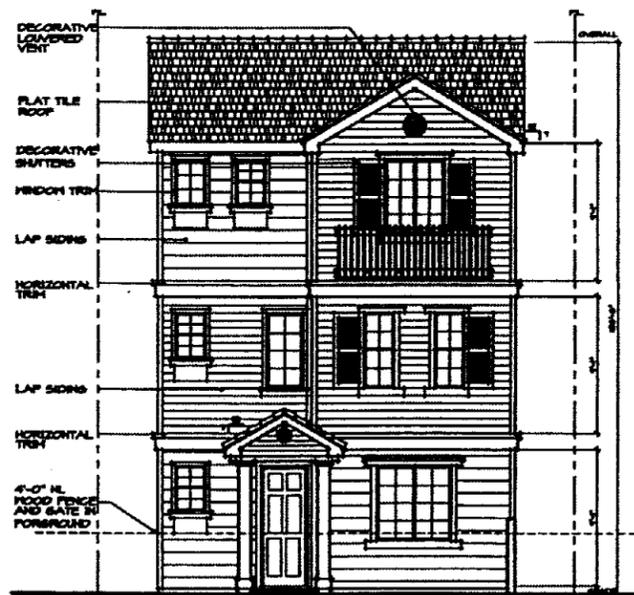
ROOF
SCALE: 1/8"=1'-0"



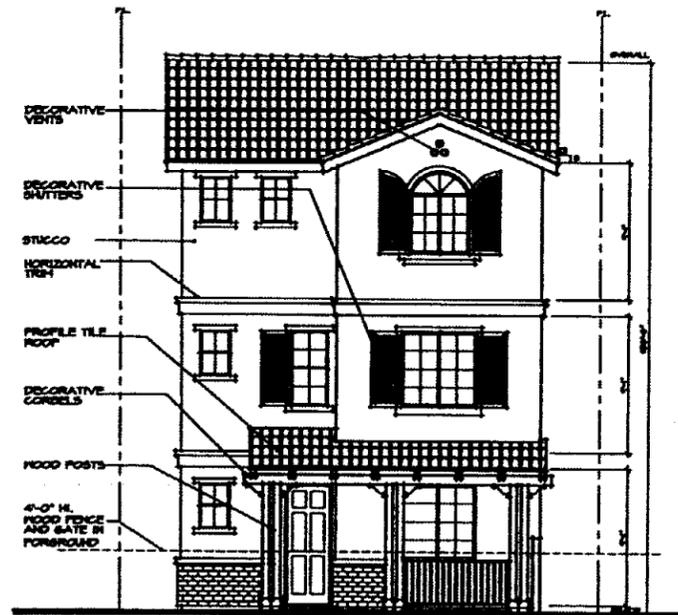
ROOF
SCALE: 1/8"=1'-0"



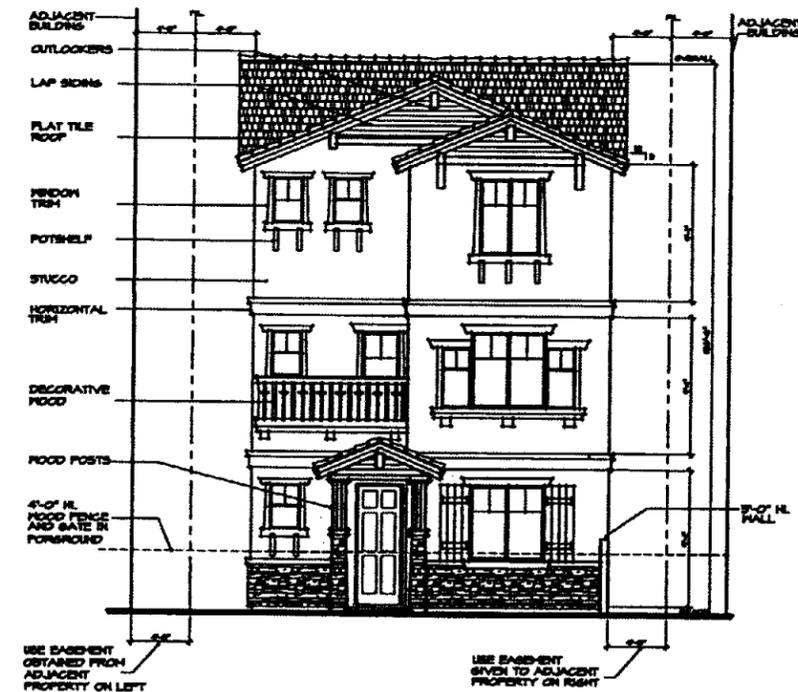
ROOF
SCALE: 1/8"=1'-0"



CAPE COD
SCALE: 3/16"=1'-0"

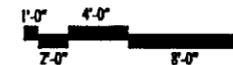


MONTEREY
SCALE: 3/16"=1'-0"



CRAFTSMAN
SCALE: 3/16"=1'-0"

1725
FRONT ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

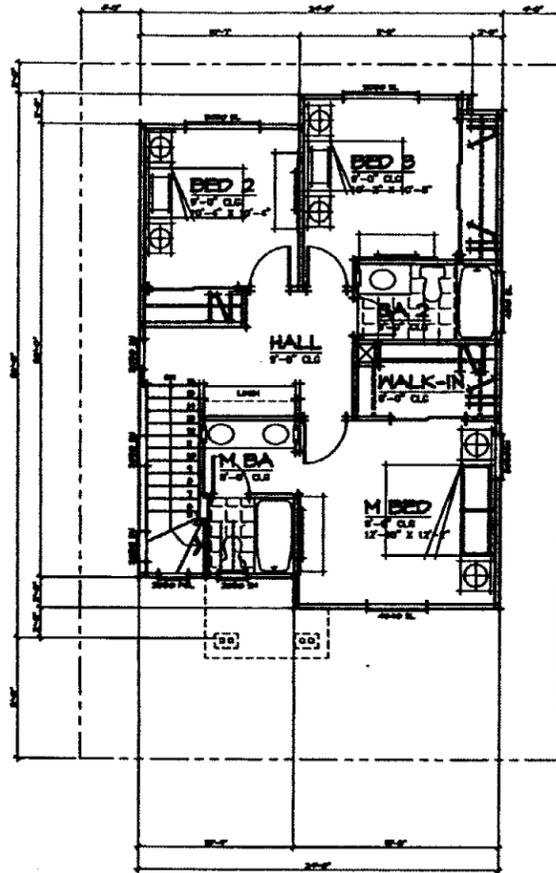


SCALE: 3/16" = 1'-0"

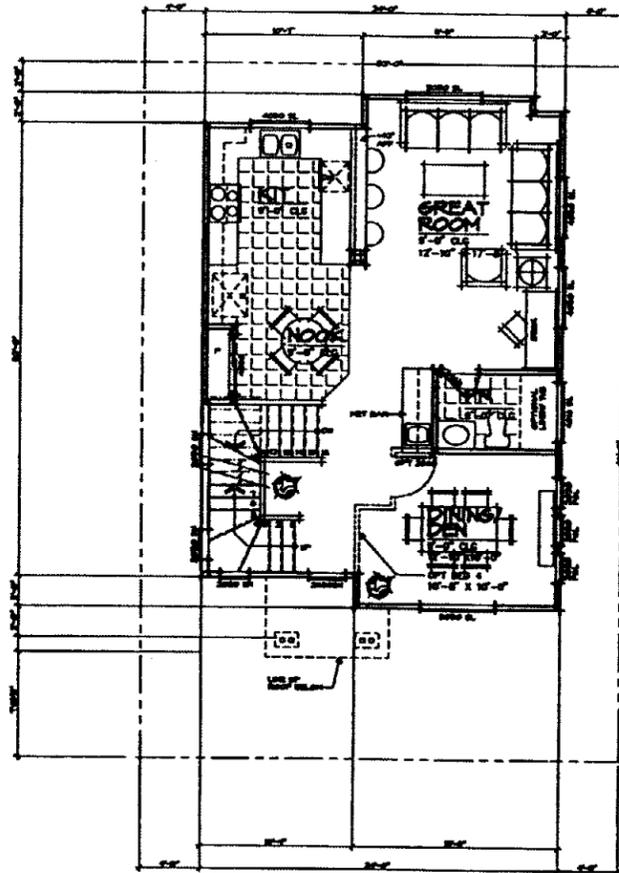
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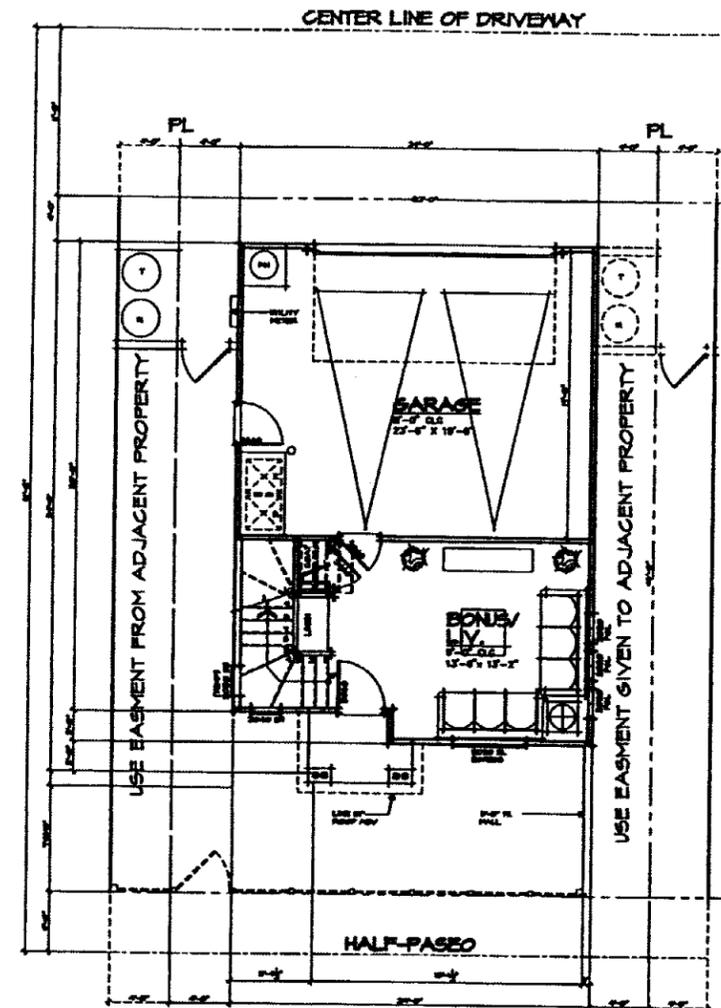
SHEET NUMBER
A1-1
10 OF 21



THIRD FLOOR PLAN

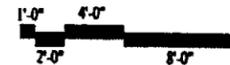


SECOND FLOOR PLAN



FIRST FLOOR PLAN
SCALE: 3/16"=1'-0"

1725 (CRAFTSMAN) 3 BR/2.5 BA
+ BONUS 1725 SQ. FT.
FLOOR PLANS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

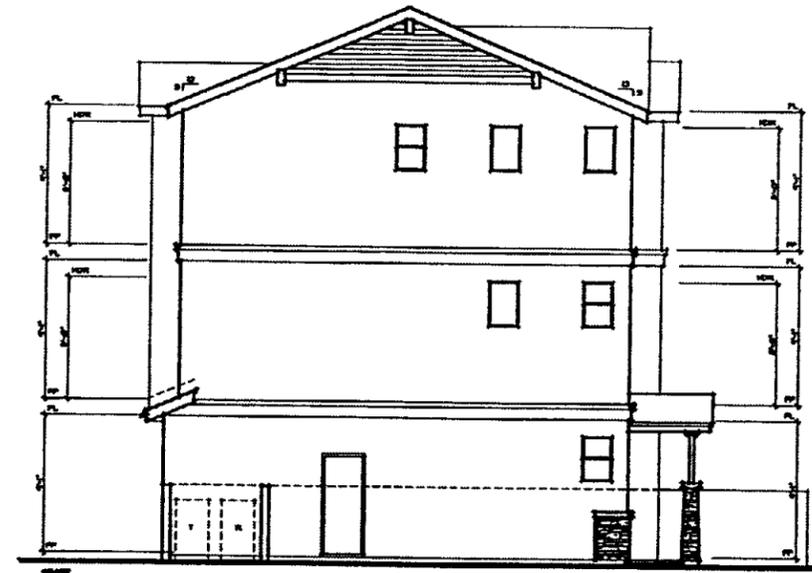


SCALE: 3/16" = 1'-0"

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SHEET NUMBER
A1-2
11 OF 21



LEFT
SCALE: 3/16"=1'-0"

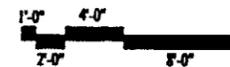


REAR
SCALE: 3/16"=1'-0"



RIGHT
SCALE: 3/16"=1'-0"

1725 (CRAFTSMAN)
ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16" = 1'-0"

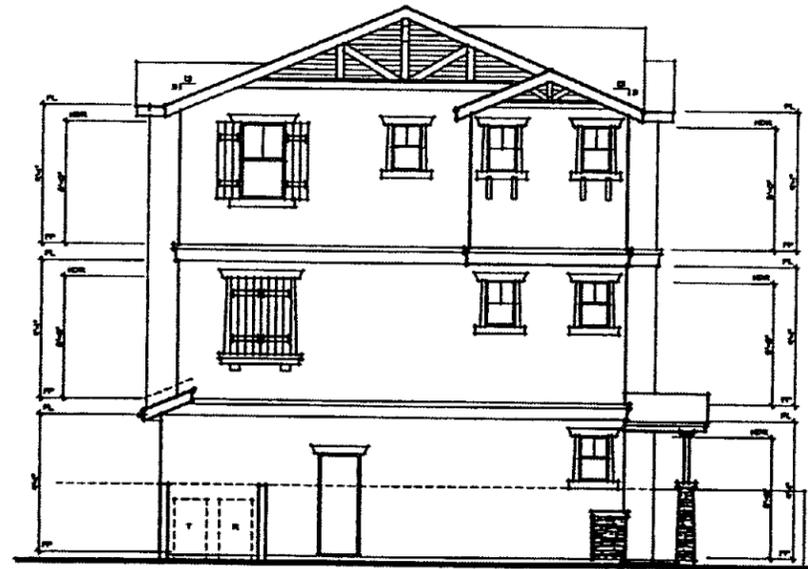
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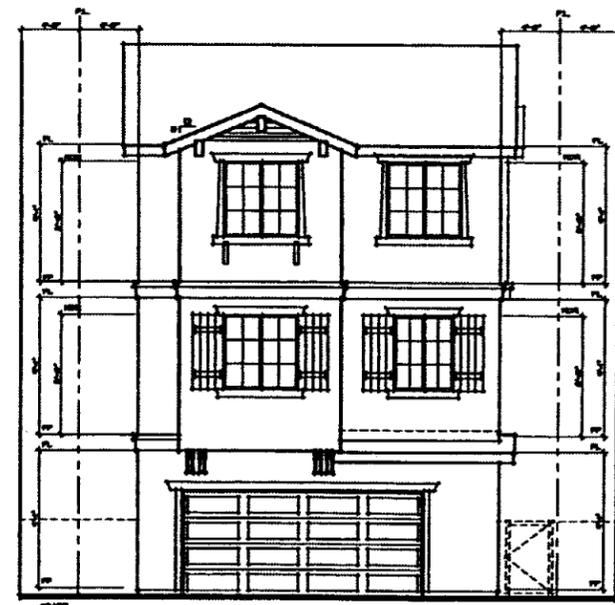
SHEET NUMBER

A1-3

12 OF 21



LEFT
SCALE: 3/16"=1'-0"

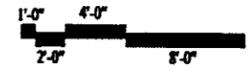


REAR
SCALE: 3/16"=1'-0"



RIGHT
SCALE: 3/16"=1'-0"

1725 (CRAFTSMAN)
ENHANCED SIDE
ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

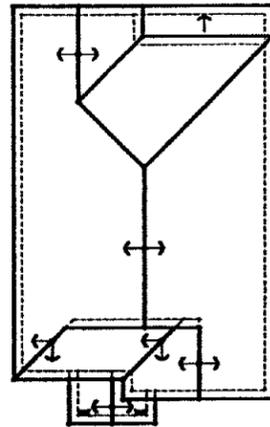


SCALE: 3/16" = 1'-0"

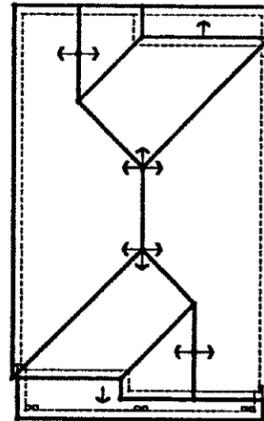
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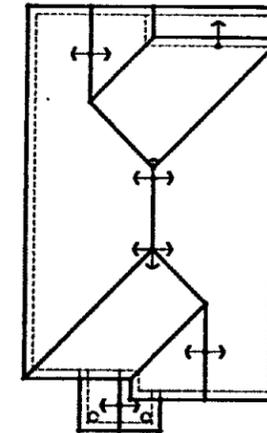
SHEET NUMBER
A1-4
13 OF 21



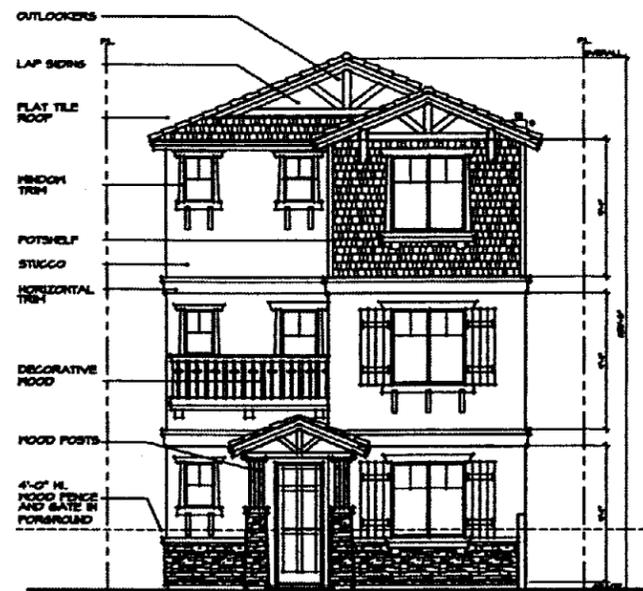
ROOF
SCALE: 1/8"=1'-0"



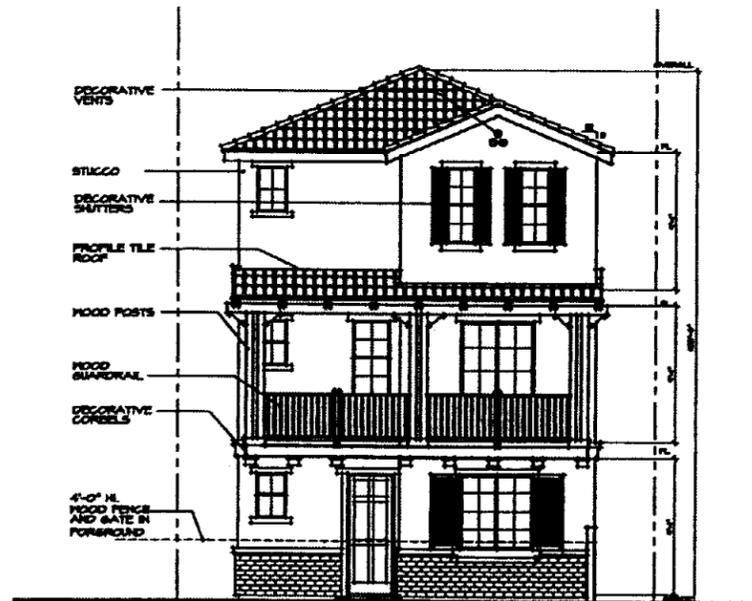
ROOF
SCALE: 1/8"=1'-0"



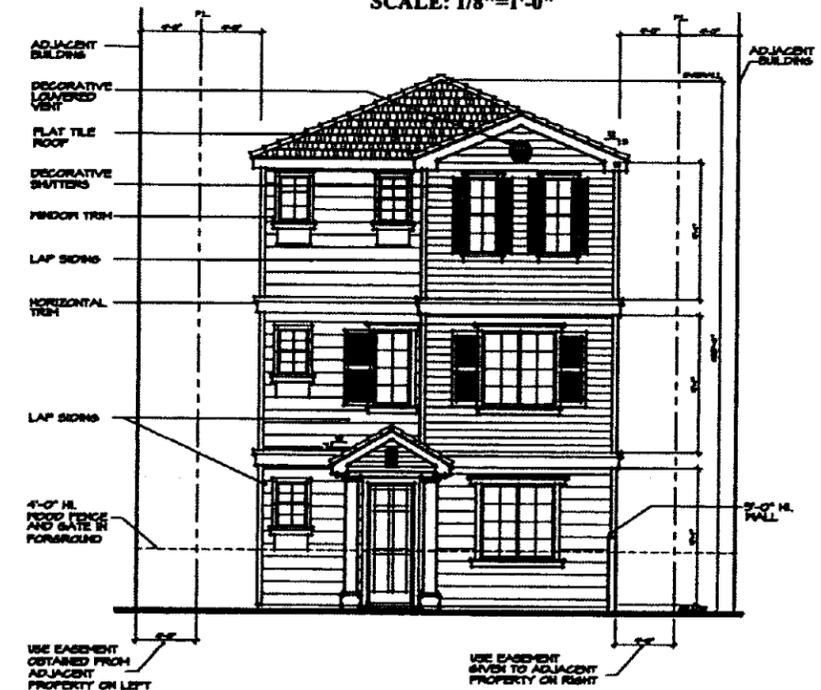
ROOF
SCALE: 1/8"=1'-0"



CRAFTSMAN
SCALE: 3/16"=1'-0"

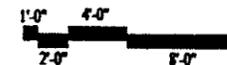


MONTEREY
SCALE: 3/16"=1'-0"



CAPE COD
SCALE: 3/16"=1'-0"

1900
FRONT ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

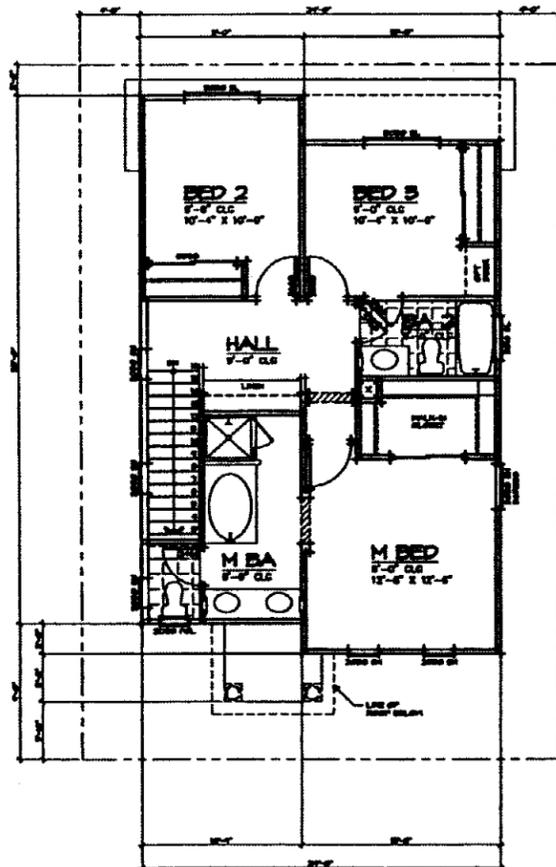


SCALE: 3/16"=1'-0"

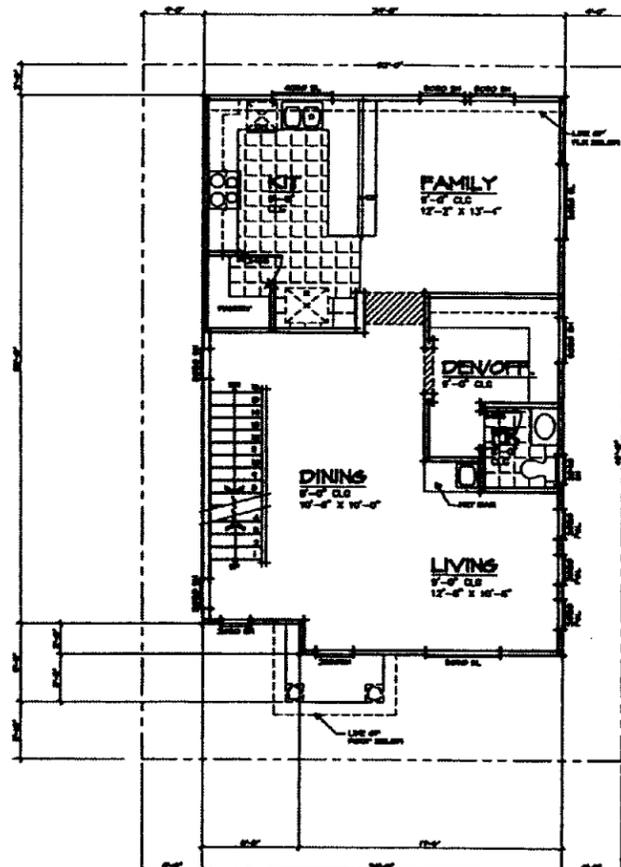
APRIL 30, 2007

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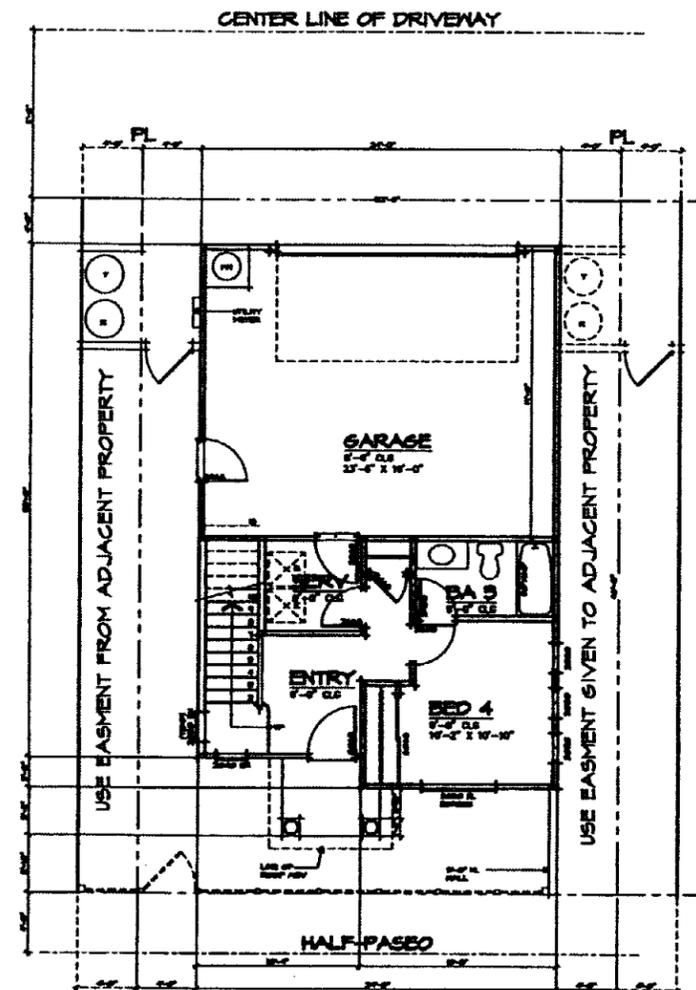
SHEET NUMBER
A2-1
14 OF 21



THIRD FLOOR PLAN

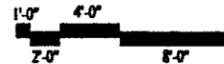


SECOND FLOOR PLAN



FIRST FLOOR PLAN
SCALE: 3/16"=1'-0"

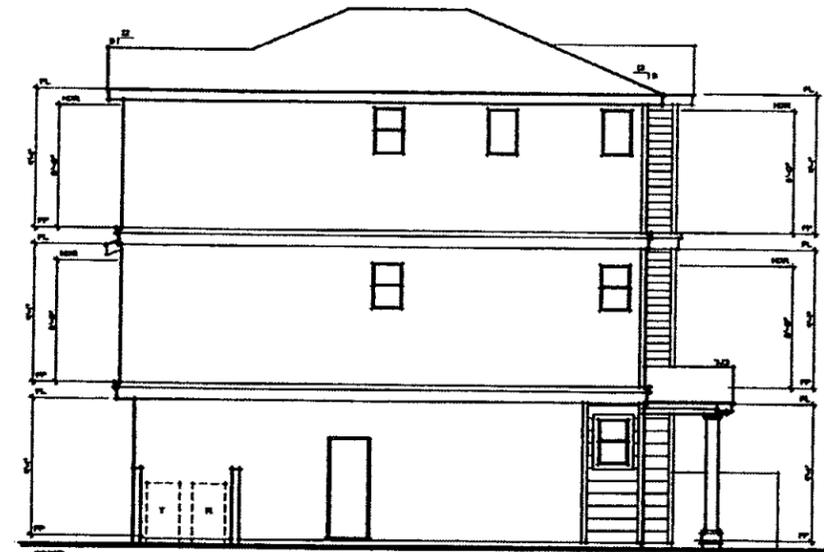
1900 (CAPE COD) 4 BR/3.5 BA
 FLOOR PLANS 1900 SQ. FT.
ASHWOOD PLACE
 CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16" = 1'-0" APRIL 30, 2007

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SHEET NUMBER
A2-2
 15 OF 21



LEFT
SCALE: 3/16"=1'-0"



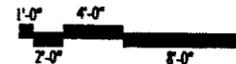
REAR
SCALE: 3/16"=1'-0"



RIGHT
SCALE: 3/16"=1'-0"

1900 (CAPE COD)
ELEVATIONS
ASHWOOD PLACE

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16" = 1'-0"

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SHEET NUMBER
A2-3



LEFT
SCALE: 3/16"=1'-0"

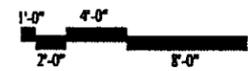


REAR
SCALE: 3/16"=1'-0"



RIGHT
SCALE: 3/16"=1'-0"

1900 (CAPE COD)
ENHANCED SIDE
ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA

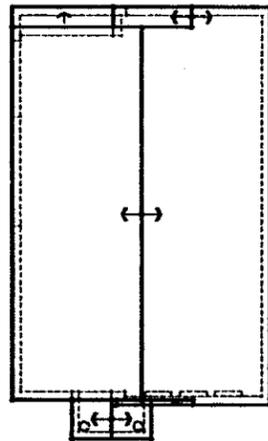


SCALE: 3/16" = 1'-0"

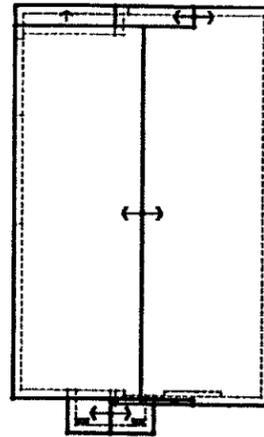
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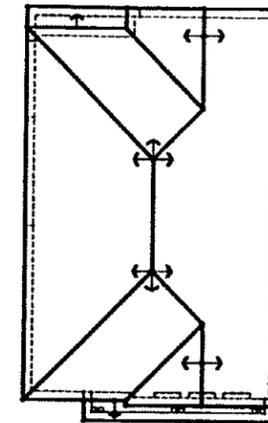
SHEET NUMBER
A2-4
17 OF 21



ROOF
SCALE: 1/8"=1'-0"



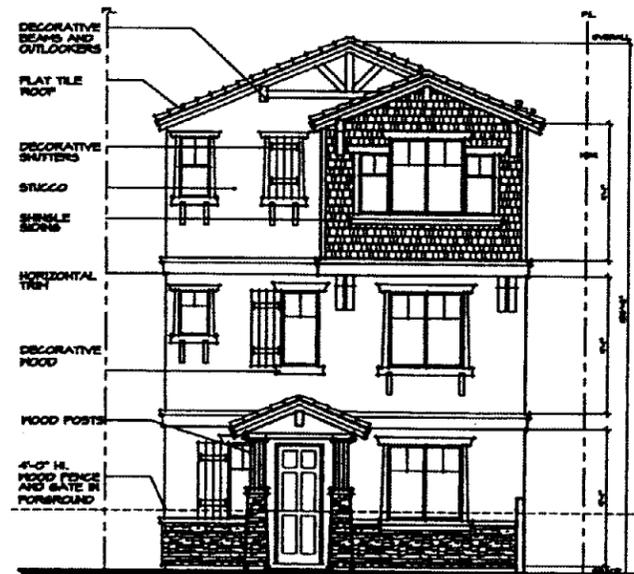
ROOF
SCALE: 1/8"=1'-0"



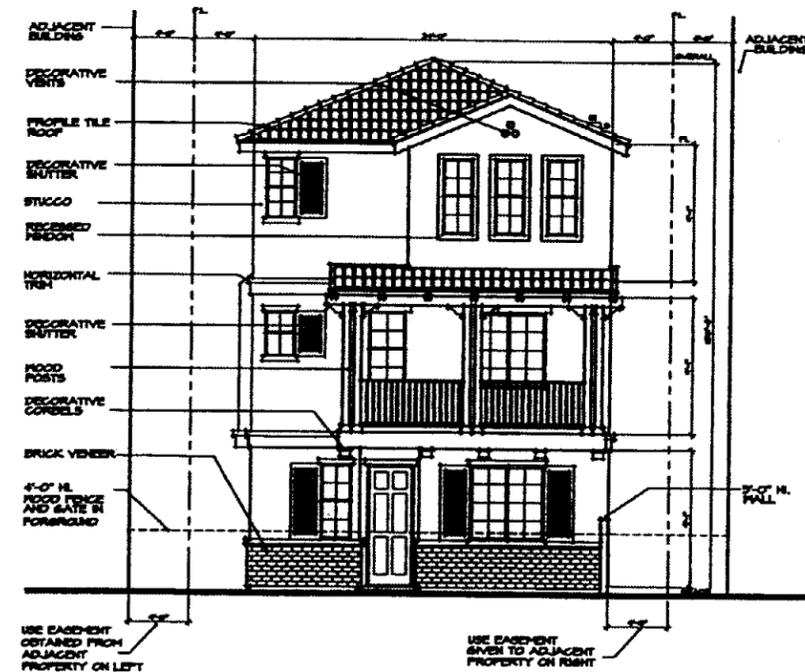
ROOF
SCALE: 1/8"=1'-0"



CAPE COD
SCALE: 3/16"=1'-0"

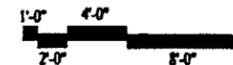
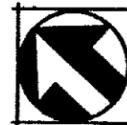


CRAFTSMAN
SCALE: 3/16"=1'-0"



MONTEREY
SCALE: 3/16"=1'-0"

2060
FRONT ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16"=1'-0"

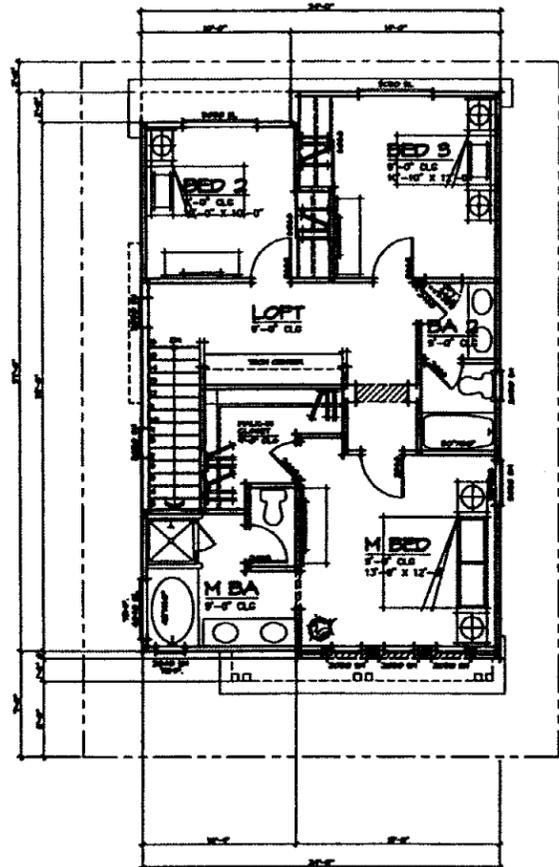
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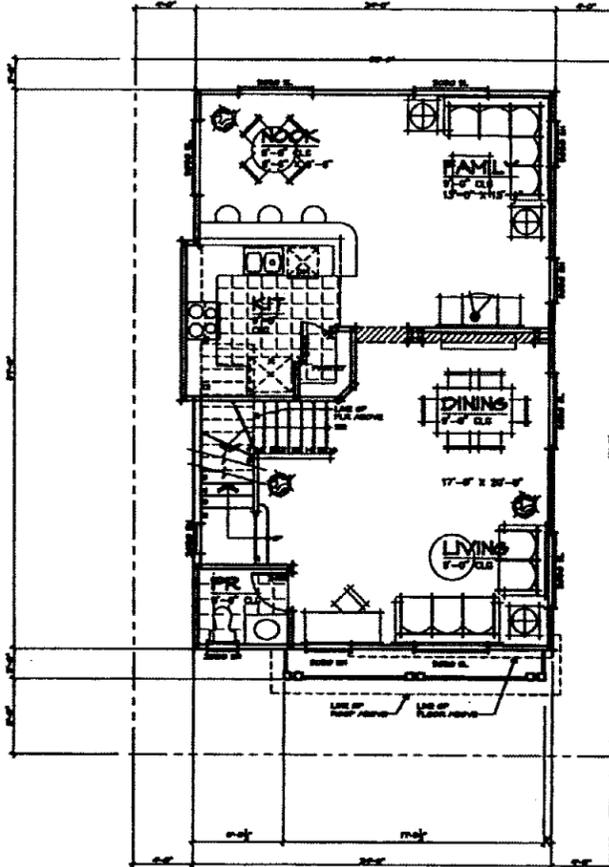
SHEET NUMBER

A3-1

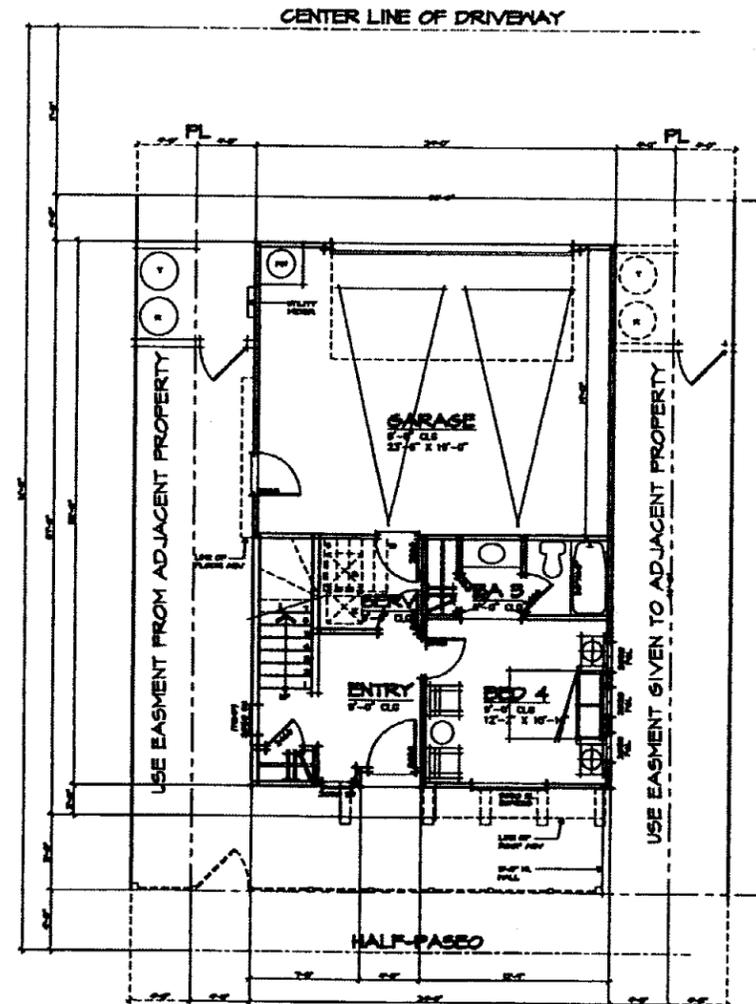
11 OF 21



THIRD FLOOR PLAN



SECOND FLOOR PLAN

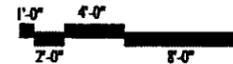


FIRST FLOOR PLAN

SCALE: 3/16"=1'-0"

2060 (MONTEREY) 4 BR/3.5 BA
 + LOFT
 2060 SQ. FT.
ASHWOOD PLACE

CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16" = 1'-0"

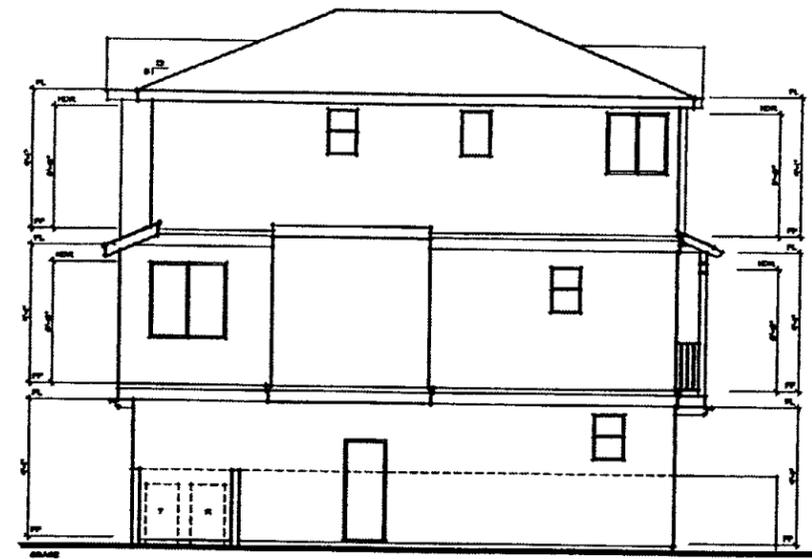
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SHEET NUMBER

A3-2

11 OF 21



LEFT
SCALE: 3/16"=1'-0"

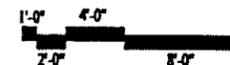


REAR
SCALE: 3/16"=1'-0"



RIGHT
SCALE: 3/16"=1'-0"

2060 (MONTEREY)
ELEVATIONS
ASHWOOD PLACE
CITY OF HAYWARD ALAMEDA COUNTY CALIFORNIA



SCALE: 3/16" = 1'-0"

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SHEET NUMBER
A3-3