



CITY OF
HAYWARD
HEART OF THE BAY

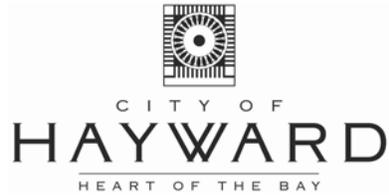
CITY COUNCIL AGENDA
MARCH 19, 2013

MAYOR MICHAEL SWEENEY
MAYOR PRO TEMPORE MARVIN PEIXOTO
COUNCIL MEMBER BARBARA HALLIDAY
COUNCIL MEMBER FRANCISCO ZERMEÑO
COUNCIL MEMBER MARK SALINAS
COUNCIL MEMBER GREG JONES
COUNCIL MEMBER AL MENDALL

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CITY COUNCIL MEETING FOR MARCH 19, 2013
777 B STREET, HAYWARD, CA 94541
WWW.HAYWARD-CA.GOV

CLOSED SESSION
Closed Session Room 2B – 4:30 PM

1. PUBLIC COMMENTS

2. Public Employment

Pursuant to Government Code 54957

➤ Performance Evaluation

City Clerk

3. Conference with Labor Negotiators

Pursuant to Government Code 54957.6

➤ Lead Negotiators: City Manager David, City Attorney Lawson, Assistant City Manager McAdoo, Human Resources Director Robustelli, Finance Director Vesely, Deputy City Attorney Vashi, Director of Maintenance Services McGrath, Jack Hughes, Esq., Liebert Cassidy Whitmore

Under Negotiation: All Groups

4. Conference with Labor Negotiators

Pursuant to Government Code 54957.6

➤ Lead Negotiators: City Attorney Lawson, Assistant City Manager McAdoo, Human Resources Director Robustelli, Finance Director Vesely, Deputy City Attorney Vashi

Under Negotiation: All Groups

5. Conference with Legal Counsel

Pursuant to Government Code 54956.9

➤ Pending Litigation

McKay, et al v. City of Hayward, et al., U.S.D.C. CV12-1613 NC

6. Conference with Legal Counsel

Pursuant to Government Code 54956.9

➤ Anticipated Litigation (Two Cases)

7. Adjourn to City Council Meeting

**CITY COUNCIL MEETING
Council Chambers – 7:00 PM**

CALL TO ORDER Pledge of Allegiance Council Member Zermeño

ROLL CALL

CLOSED SESSION ANNOUNCEMENT

PROCLAMATION American Red Cross Month

PUBLIC COMMENTS

The Public Comment section provides an opportunity to address the City Council on items not listed on the agenda or Work Session, or Informational Staff Presentation items. The Council welcomes your comments and requests that speakers present their remarks in a respectful manner, within established time limits, and focus on issues which directly affect the City or are within the jurisdiction of the City. As the Council is prohibited by State law from discussing items not listed on the agenda, your item will be taken under consideration and may be referred to staff.

ACTION ITEMS: *(The Council will permit comment as each item is called for the Consent Calendar, Public Hearings, and Legislative Business. In the case of the Consent Calendar, a specific item will need to be pulled by a Council Member in order for the Council to discuss the item or to permit public comment on the item. Please notify the City Clerk any time before the Consent Calendar is voted on by Council if you wish to speak on a Consent Item.)*

CONSENT

1. Approval of Minutes of the Special Joint City Council/Redevelopment Successor Agency Meeting on February 26, 2013
[Draft Minutes](#)
2. Resignation of Al Parso from the Council Economic Development Committee and the General Plan Update Task Force
[Staff Report](#)
[Attachment I Resolution](#)
[Attachment II Resignation Letter](#)
3. Resignation of Doug Ligibel from the Keep Hayward Clean and Green Task Force
[Staff Report](#)
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[Attachment II Resignation Letter](#)

March 19, 2013



4. Citywide Light Emitting Diode (LED) Streetlight Conversion Project - Award of Contract
[Staff Report](#)
[Attachment I](#)

 5. Adoption of Complete Streets Policy
[Staff Report](#)
[Attachment I](#)
[Attachment II](#)

 6. Hayward Executive Airport Administration Building: Certification of Negative Declaration, Approval of Plans and Specifications; and Call for Bids; and Authorization for the City Manager to Execute an Amendment to the Professional Services Agreement with WLC Architects, Inc.
[Staff Report](#)
[Attachment I](#)
[Attachment II](#)
[Attachment III](#)
[Attachment IV](#)
[Attachment V](#)
[Attachment VI](#)
[Attachment VII](#)
[Attachment VIII](#)

 7. Approve the City Manager's Action to Execute a Contract Amendment with Metropolitan Planning Group to Provide Temporary Planning Services through End of March of 2013
[Staff Report](#)
[Attachment I - Resolution](#)
-

The following order of business applies to items considered as part of Public Hearings and Legislative Business:

- *Disclosures*
 - *Staff Presentation*
 - *City Council Questions*
 - *Public Input*
 - *Council Discussion and Action*
-

LEGISLATIVE BUSINESS

8. Economic Development Strategic Plan Adoption (Report from City Manager David)
[Staff Report](#)
[Attachment I](#)
[Attachment II](#)
[Attachment III](#)
[Attachment IV](#)
[Attachment V](#)

March 19, 2013



COUNCIL REPORTS, REFERRALS, AND FUTURE AGENDA ITEMS

Oral reports from Council Members on their activities, referrals to staff, and suggestions for future agenda items.

ADJOURNMENT

NEXT MEETING – 7:00 PM, TUESDAY, MARCH 26, 2013

PUBLIC COMMENT RULES: *The Mayor may, at the beginning of the hearing, limit testimony to three (3) minutes per individual and five (5) minutes per an individual representing a group of citizens or organization. Speakers will be asked for their name and their address before speaking and are expected to honor the allotted time. A Speaker Card must be completed by each speaker and is available from the City Clerk at the meeting.*

PLEASE TAKE NOTICE *that if you file a lawsuit challenging any final decision on any public hearing or legislative business item listed in this agenda, the issues in the lawsuit may be limited to the issues that were raised at the City's public hearing or presented in writing to the City Clerk at or before the public hearing. PLEASE TAKE FURTHER NOTICE that the City Council has adopted Resolution No. 87-181 C.S., which imposes the 90 day deadline set forth in Code of Civil Procedure section 1094.6 for filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure section 1094.5.*

*****Materials related to an item on the agenda submitted to the Council after distribution of the agenda packet are available for public inspection in the City Clerk's Office, City Hall, 777 B Street, 4th Floor, Hayward, during normal business hours. An online version of this agenda and staff reports are available on the City's website. Written comments submitted to the Council in connection with agenda items will be posted on the City's website. All Council Meetings are broadcast simultaneously on the website and on Cable Channel 15, KHRT. *****

Assistance will be provided to those requiring accommodations for disabilities in compliance with the Americans with Disabilities Act of 1990. Interested persons must request the accommodation at least 48 hours in advance of the meeting by contacting the City Clerk at (510) 583-4400 or TDD (510) 247-3340.

Please visit us on:





**MINUTES OF THE SPECIAL JOINT CITY COUNCIL/REDEVELOPMENT
SUCCESSOR AGENCY MEETING OF THE CITY OF HAYWARD**

City Council Chambers

777 B Street, Hayward, CA 94541

Tuesday, February 26, 2013, 7:00 p.m.

The Special Joint City Council/Redevelopment Successor Agency meeting was called to order by Mayor/Chair Sweeney at 7:00 p.m., followed by the Pledge of Allegiance led by Council/RSA Member Salinas.

ROLL CALL

Present: COUNCIL/RSA MEMBERS Zermeño, Jones, Halliday, Peixoto, Salinas,
Mendall
MAYOR/CHAIR Sweeney
Absent: None

CLOSED SESSION ANNOUNCEMENT

Mayor Sweeney announced that Council met pursuant to Government Code 54957 regarding performance evaluations for City Attorney and City Clerk; and met with labor negotiators pursuant to Government Code 54957.6 concerning all groups. There was no reportable action.

PUBLIC COMMENTS

S.J. Samiul, Alden Gateway resident, noted that Hooters and Oakland A's were looking for places to locate and suggested that staff consider bringing them to Hayward. Mr. Samiul provided Tesla Model S electric car brochures for each Council member. Mr. Samiul requested an update of the plans for the new library and shared photographs of the new library in Los Gatos.

Mr. Francisco Abrantes, Alice Street resident, noted his street was swept on Monday after no street sweeping activity for one year and thanked Mayor Sweeney. Mr. Abrantes reiterated concerns about parking citations.

Mr. Jesús Armas, with business address on Main Street, spoke on behalf of Auto Nation regarding the Commercial Overlay zone for the former auto row included in the draft Mission Boulevard Corridor Specific Plan and noted that Auto Nation wanted the opportunity to work with the City to identify alternatives that would mutually benefit the City's goals and revitalize the corridor.

Mr. Jim Drake, Franklin Avenue resident, referred to the staff report of February 19, 2013, regarding the Annual Financial Report and urged the City to be frugal about its spending and to not borrow money from reserves.

WORK SESSION

1. Adopted Hayward Executive Airport Land Use Compatibility Plan and General Plan Consistency Requirements of the California Government Code

Staff report submitted by Lamphier Gregory Senior Planner Colin, dated February 26, 2013, was filed.

Acting Planning Manager Patenaude announced the report and introduced Kevin Colin, Senior Planner from Lamphier-Gregory, who gave a synopsis of the report.

Council Member Halliday concurred with staff's recommendation to bring back an override action with findings regarding Section 2.7.5.7 of the Airport Land Use Compatibility Plan (ALUCP) and incorporate compliance provisions into the 2014 General Plan Update in order to protect redevelopment and economic growth at Southland Mall and address public safety measures.

Council Member Zermeño agreed with an override action related to Section 2.7.5.7 of the ALUCP in order to protect the development at Southland Mall. Mr. Zermeño was glad to know that the Alameda County Airport Land Use Commission (ALUC) could not override Council's action. Mr. Zermeño suggested developing the land behind Smart and Final and La Quinta Hotel by constructing a hotel/conference center.

Council Member Mendall noted that while the City appreciated the ALUC's advice, the Council should not give the ALUC veto power over Council's decisions in achieving conformance. Mr. Mendall was reluctant to create additional review steps for potential developers. Acting Planning Manager Patenaude anticipated few cases that the ALUC would be asked to review during the course of the update of the General Plan.

Council Member Salinas was concerned that the development of a potential restaurant in the Southland Mall area could be affected by the provisions of the ALUCP.

Council Member Halliday acknowledged former Council Member Henson for his involvement during the early stages of the ALUPC.

In response to Mayor Sweeney as to next steps, Acting Planning Manager Patenaude noted that staff would present the findings to override the ALUCP at a future Council meeting.

2. Review and Discussion of the Draft Economic Development Strategic Plan

Staff report submitted by Management Fellow Thomas, dated February 26, 2013, was filed.

City Manager David announced the report and introduced the City staff team: Human Resources Director Robustelli, Senior Planner Pearson, Assistant City Manager McAdoo, and Management Fellow Thomas.



**MINUTES OF THE SPECIAL JOINT CITY COUNCIL/REDEVELOPMENT
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Council Member Zermeño commended staff for the draft Economic Development Strategic Plan and noted his personal goal was for sales tax revenue to exceed 16% and for property tax revenue to surpass 20%. Mr. Zermeño favored promoting Hayward for its international community and suggested adding ethnicity to the permit/business license application or tracking how different groups are represented in the local economy. He also requested that Council be provided with a report on the Plan's progress twice a year. He recommended that the "Business Attraction and Retention in the Industrial Sector" strategic area include/track information about businesses that are not successful and cease to do business and learn from the issues so the City can help new businesses thrive. Mr. Zermeño added that prohibiting Dance/Nightclubs should be reconsidered noting that prospective hotel guests might want to enjoy entertainment opportunities during their stay.

Council Member Halliday commended staff and the Council Economic Development Committee for the Plan and agreed that the proposed funding was appropriate to promote economic development and would prepare the City when funding opportunities occur. Ms. Halliday recommended that the City's fee structure be simplified to foster a business-friendly community. She favored mixed uses being considered in the industrial area. Ms. Halliday questioned the validity of changing the requirement for hotels and conference centers from Conditional Use Permit to Administrative Use Permit. Ms. Halliday questioned the need for Performance Measure FPM.2: "An annual increase in the number of business licenses that is half of the increase in the labor force."

Council Member Salinas commended staff and the Council Economic Development Committee for a solid Economic Development Strategic Plan. Mr. Salinas was pleased that education and the City's relationship to educational institutions was an important theme in the Plan. Mr. Salinas offered the following suggestions for the Plan: strengthen language in Goal IS6 to address doing business with educational institutions as a way to generate revenue; include language about hiring Hayward residents first; ensure zoning flexibility in the Plan; and do not over prohibit uses especially near colleges. He added that Check Cashing & Loans, Tobacco Specialty Stores, and Liquor Stores were not permitted uses in an effort to create a healthy Mission Boulevard Corridor.

Council Member Peixoto supported the Economic Development Strategic Plan and liked that it identified and incorporated major components in one document. Mr. Peixoto spoke about the importance of Council to work with staff in preserving industrial and commercial land use policies.

Council Member Mendall submitted 26 suggestions for the Plan and requested review by the Council Economic Development Committee. Mr. Mendall praised staff and the Council Economic Development Committee for the Plan and suggested the priorities, work tasks, and metrics arrangement be used as a template for Council priorities. Mr. Mendall agreed with the funding for the Plan noting it was a conservative proposal but attainable. He touched on some of his suggestions: consider the auto storage yard north of I-92 and east of Clawiter an opportunity site; consider the wholesale auto lots at I-880 and Industrial Boulevard a retail catalyst site; reframe the

Performance Measure FPM.2; achieve truly mixed use and not 100% residential for Mission Form Based Codes; and hold property owners on highly visible sites to a higher level of responsibility to maintain their properties and target community preservation enforcement in prominent areas of the city.

Council Member Jones, also a member of the Economic Development Committee, commended staff for consolidating various ideas into the Economic Development Plan. Mr. Jones pointed out he viewed the Plan as the City's proforma and the goal was to attract private investment into the community in order to improve the economic condition of the people who live, work and have businesses in Hayward. He added it was important to provide predictability to the investment community, and to protect available commercial land for that use. He suggested bringing the funding request back to Council in March along with the Economic Development Plan.

Mayor Sweeney, also a member of the Economic Development Committee, noted there was Council consensus to bring the item back in March. Mayor Sweeney cautioned staff to avoid creating an all-inclusive list, but to stay focused, accurate, and responsive to the community's needs. Mayor Sweeney concurred with Council Members Jones and Peixoto about protecting industrial areas for economic opportunity.

CONSENT

Consent Item No. 6 was pulled for discussion.

3. Approval of Minutes of the City Council Meeting on February 5, 2013

It was moved by Council Member Peixoto, seconded by Council Members Salinas, Mendall and Halliday, and unanimously carried, to approve the minutes of the City Council Meeting of February 5, 2013.

4. Adoption of an Ordinance Adding a New Section 6.36(b) to the Hayward Traffic Code Relating to BART Commuter Permit Parking on Designated Streets

Staff report submitted by City Clerk Lens, dated February 26, 2013, was filed.

It was moved by Council Member Peixoto, seconded by Council Member Salinas, Mendall and Halliday, and unanimously carried to adopt the following:

Ordinance 13-04, "An Ordinance Adding a New Section 6.36(b) to the Hayward Traffic Code Relating to BART Commuter Permit Parking on Designated Streets"

5. Pavement Rehabilitation Vehicle Registration Fee FY 14 (Districts 6, 10, 15, 19, 20) – Approval of Plans and Specifications and Call for Bids



**MINUTES OF THE SPECIAL JOINT CITY COUNCIL/REDEVELOPMENT
SUCCESSOR AGENCY MEETING OF THE CITY OF HAYWARD**

City Council Chambers

777 B Street, Hayward, CA 94541

Tuesday, February 26, 2013, 7:00 p.m.

Staff report submitted by Assistant City Engineer Owusu, dated February 26, 2013, was filed.

It was moved by Council Member Peixoto, seconded by Council Members Salinas, Mendall and Halliday, and unanimously carried to adopt the following:

Resolution 13-017, “Resolution Approving Plans and Specifications for the Pavement Rehabilitation VRF FY14 (Dist 6, 10, 15, 19, 20) Project, Project No. 5196, and Call for Bids”

6. Acceptance of Donation of Memorial Sculpture from Gail Steele

Staff report submitted by Neighborhood Partnership Manager Bristow, dated February 26, 2013, was filed.

Mayor Sweeney gave Ms. Gail Steele the opportunity to address the Council.

Ms. Gail Steele, Arlette Avenue resident, introduced parents in attendance who lost children to violence and encouraged people to get involved in programs that remembered children who lost their lives to violent crimes. Ms. Steele noted that since 1994 Alameda County lost 385 children under the age of 17 as a result of violent crimes by their caregivers. Ms. Steele requested that staff work with her to find a place to install the sculpture, preferably near the front of City Hall, to serve as a place of remembrance for Hayward children. Ms. Steele thanked staff for their assistance.

Mayor Sweeney noted the City was honored for the contribution to the City and thanked Ms. Steele and all the parents for their generosity.

Council Member Halliday offered a motion to accept the memorial sculpture donated by Ms. Gail Steele and install it in an appropriate location in the vicinity of City Hall agreed upon by Ms. Steele and staff. Council Member Zermeño seconded the motion. Ms. Halliday applauded Ms. Steele’s initiative.

It was moved by Council Member Halliday, seconded by Council Member Zermeño, and unanimously carried to adopt the following:

Resolution 13-019, “Resolution Confirming the Acceptance of the Gift from Gail Steele of a Memorial Sculpture”

7. Route 238 Corridor Improvement Project - Approve Addendum to EIR and Allow Downtown Parking on a Temporary Basis

Staff report submitted by Assistant City Engineer Owusu, dated February 26, 2013, was filed.

It was moved by Council Member Peixoto, seconded by Council Members Salinas, Mendall and Halliday, and unanimously carried to adopt the following:

Resolution 13-018, “Resolution Approving Addendum to Environmental Impact Report for the Route 238 Corridor Improvement Project and Allowing Temporary Conversion of Travel Lanes to Parking on “A” Street and Foothill Boulevard in Downtown Hayward”

8. Approval of Recognized Obligation Payment Schedule and Successor Agency Administrative Budget for the Period July Through December 2013

Staff report submitted by Assistant City Manager McAdoo, dated February 26, 2013, was filed.

It was moved by Council/RSA Member Peixoto, seconded by Council/RSA Members Salinas, Mendall and Halliday, and unanimously carried to adopt the following:

Redevelopment Successor Agency Resolution 13-01, “Resolution of the City Council of the City of Hayward, Acting as the Governing Board of the Successor Agency for the Redevelopment Agency of the City of Hayward, A Separate Legal Entity, Approving the Recognized Obligation Payment Schedule and an Administrative Budget for the Period July Through December 2013, and Directing the City Manager to Take All Actions Necessary to Effectuate Requirements Associated With This Approval”

PUBLIC HEARING

9. Site Plan Review No. PL-2012-0342 - Electric Guard Dog (Applicant)/California Auto Dealers Exchange (Owner) - An Appeal of Planning Director's Decision to Deny the Installation of an Electric Security Fence. The Project is Located at 967 Industrial Parkway West, Westerly of Huntwood Avenue, in the Industrial (I) Zoning District

Staff report submitted by Interim Planning Manager Patenaude, dated February 26, 2013, was filed.

Acting Planning Manager Patenaude provided a synopsis of the report.

In response to Council Member Peixoto’s inquiry, Acting Planning Manager Patenaude stated that Police and Fire indicated support for the fence with access to a Knox box and staff was requesting the opportunity to develop provisions and requirements for the installation of an electric fence.



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In response to Council Member Halliday's inquiry, Acting Planning Manager Patenaude noted there was no input from New England Village Mobilehome residents related to the proposed fence.

Acting Planning Manager Patenaude confirmed for Council Member Jones that Finding No. 3 of the proposed resolution would need to be amended to note that the electric fence, with access via the Knox box, would permit the Hayward Police and Fire departments to respond to calls for service. Mr. Jones said having attack dogs on site could be more dangerous for the first responders. He added it was inappropriate for the applicant to be charged for the appeal when the Planning Commission had failed to reach a decision.

In response to Council Member Salinas's inquiry as why an electric fence was needed, Acting Planning Manager Patenaude responded it was needed to protect high valued inventory and to deter crime.

Acting Planning Manager Patenaude confirmed for Council Member Zermeño that the electric fence was proposed to be black.

Mayor Sweeney opened the public hearing at 9:20 p.m.

Mr. Michael Pate, with Electric Guard Dog, noted that the electric fence used easy technology and worked best to prevent crime. Mr. Pate explained that State of California Agriculture Code required a perimeter buffer fence along the electric fence with no more than four to eight inches apart to prevent a zone of entrapment.

Mr. Mike Valderrama, Facilities Manager for Manheim San Francisco Bay, mentioned the facility had been robbed and vandalized by perpetrators who used the existing fence as a ramp. Mr. Valderrama noted the electric fence was a cost effective method to deter crime and added there was a 24-hour patrol two blocks away that could meet the first responders and would be able to disarm the electric fence.

Council Member Peixoto disclosed that he met with Mr. Pate regarding the proposed fence.

Council Member Zermeño noted there was a typo on the Spanish translation of the warning sign. He agreed that the electric fence was a good solution to prevent crime.

Mr. Kim Huggett, President of the Hayward Chamber of Commerce, noted Manheim San Francisco Bay was one of the top sales tax generators in the city. Mr. Huggett added that the proposed fence was a local business' effort to secure its location and protect high valued inventory and urged Council to support the applicant.

Mayor Sweeney closed the public hearing at 9:34 p.m.

Council Member Jones offered a motion to reverse the Planning Director's decision and direct staff to bring back findings for approval, and also asked that the appeal charges incurred by the applicant be waived because there was a breakdown in the administrative process.

Council Member Peixoto seconded the motion.

Council Member Jones disclosed meeting with Mr. Pate regarding the proposed electric fence and thanked the local business for being proactive to deter crime on their property. Mr. Jones noted he was disappointed in the process that had caused a significant cost for the applicant.

Assistant City Attorney Conneely clarified that the motion was to direct staff to conduct CEQA review and bring back findings and conditions of approval and noted that the Planning Director's decision could not be reversed because the environmental review had not been conducted.

Council Members Jones and Peixoto were amenable to the clarification offered by Assistant City Attorney Conneely.

Mayor Sweeney offered an amendment to the motion to include a requirement for the Knox box and to direct staff, as part of the Planning Division work program, to develop standards and a text amendment that would allow electric fencing in the Industrial Zoning District.

Council Members Jones and Peixoto were amenable to the amendment.

Council Member Peixoto supported the motion noting the business owner was being proactive in protecting his property. Mr. Peixoto agreed that the applicant should not pay the appeal fee because the Planning Commission did not reach a majority decision.

Council Member Zermeño supported the motion as this allowed the business owner to maintain a successful business in Hayward

Council Member Mendall offered friendly amendments: to have the warning signs spaced every 30 feet, and, on the residential side, the electric fence be behind a hedge buffer. Mr. Mendall disclosed he had met with Mr. Pate.

Council Members Jones and Peixoto accepted Council Member Mendall's friendly amendments.

Council Member Halliday commented that the process for installing an electric fence should not have taken so long and acknowledged that the industrial area was difficult to police. Ms. Halliday supported the motion.

Mayor Sweeney disclosed that he had met with Mr. Pate, Mr. Valderrama, and Mr. Cuneen, and supported the motion acknowledging there was a criminal element in the industrial area, and installing a fence was a reasonable step for a business owner to take to protect his property.

It was moved by Council Member Jones, seconded by Council Member Peixoto, and unanimously carried to direct staff to: conduct California Environmental Quality Act (CEQA) review and bring



**MINUTES OF THE SPECIAL JOINT CITY COUNCIL/REDEVELOPMENT
SUCCESSOR AGENCY MEETING OF THE CITY OF HAYWARD**

City Council Chambers

777 B Street, Hayward, CA 94541

Tuesday, February 26, 2013, 7:00 p.m.

back findings and conditions of approval; waive the appeal charges incurred by the applicant; include the requirement of a Knox Box (control panel); space warning signs for the electric fence at intervals of 30 feet; that the fence on the residential side be behind the perimeter buffer; and develop standards and a text amendment that would allow electric fencing in the Industrial Zoning District, as part of the Planning Division work program.

COUNCIL REPORTS, REFERRALS, AND FUTURE AGENDA ITEMS

Council Member Zermeño announced a friendly Hayward Basketball Fundraising Match on March 2, 2013, at the Mateo Jiménez Gym to benefit the Hayward Youth Commission. Mr. Zermeño asked that an item related to the Council priorities be added to the agenda schedule.

Council Member Salinas announced the second annual Hayward Honors Women - Teach-In on Technology, Science and Innovation on February 28, 2013, and invited all to attend.

Council Member Jones announced that Moreau Catholic High School was organizing a Relay for Life of Hayward 2013 event on June 1, 2013, and invited all to get involved.

Mayor Sweeney asked staff to prepare a report on SMASH operations.

ADJOURNMENT

Mayor Sweeney adjourned the meeting at 9:49 p.m.

APPROVED:

Michael Sweeney

Mayor, City of Hayward

Chair, Redevelopment Successor Agency

ATTEST:

Miriam Lens

City Clerk, City of Hayward

Secretary, Redevelopment Successor Agency

DATE: March 19, 2013
TO: Mayor and City Council
FROM: City Clerk
SUBJECT: Resignation of Al Parso from the Council Economic Development Committee and the General Plan Update Task Force

RECOMMENDATION

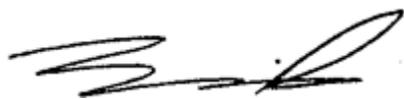
That the City Council accepts the resignation of Mr. Al Parso from the Council Economic Development Committee (CEDC) and the General Plan Update Task Force (Task Force).

BACKGROUND

Mr. Al Parso was appointed to the CEDC on September 11, 2012, and appointed to the Task Force on October 23, 2012. Mr. Parso submitted the attached resignation letter (Attachment II). His resignation is effective March 8, 2013. The vacated position on the CEDC will be filled as part of the annual appointment process for the City's Appointed Officials to Boards and Commissions. Staff recommends not filling the vacated Task Force position at this time.

Prepared and Recommended by: Miriam Lens, City Clerk

Approved by:



Fran David, City Manager

Attachments:

- Attachment I Resolution Accepting the Resignation
- Attachment II Resignation Letter

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION ACCEPTING THE WRITTEN RESIGNATION OF AL PARSO FROM THE COUNCIL ECONOMIC DEVELOPMENT COMMITTEE AND THE GENERAL PLAN UPDATE TASK FORCE

WHEREAS, Mr. Al Parso was appointed to the Council Economic Development Committee on September 11, 2012 and;

WHEREAS, Mr. Al Parso was appointed to the General Plan Update Task Force on October 23, 2012 and;

WHEREAS, Mr. Al Parso submitted his resignation on March 4, 2013, effective March 8, 2013.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Hayward that the Council hereby accepts the resignation of Mr. Al Parso; and commends him for his civic service to the City.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013.

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS: MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATTEST: _____ City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

Miriam Lens

From: Al Parso <alparso@yahoo.com>
Sent: Monday, March 04, 2013 2:16 PM
To: Miriam Lens
Cc: gregjones@gregjonesrealestate.com
Subject: Resignation Letter

Al Parso
22202 Prospect Street
Hayward, CA 94541
510-567-3664
alparso@yahoo.com

March 4, 2013

Miriam Lens, CMC, MPA

City Clerk

City of Hayward

777 B Street

Hayward, CA 94541

Dear Ms. Lens:

It is with great sadness that I have to inform you that I am resigning from my positions as a member of Council's Economic Development Committee and a member of the General Plan Update Task Force for the City of Hayward, effective March 8, 2013.

It has been a great privilege serving the community for this short time. For personal reasons I will be moving out of the City and am no longer eligible to serve. I have enjoyed working with the committees and learning about the City. I am confident that the remaining members of the committee will continue to drive Hayward towards a brighter future for all its citizens.

If I can be of any help during this transition, please let me know.

Sincerely,

Al Parso

DATE: March 19, 2013

TO: Mayor and City Council

FROM: City Clerk

SUBJECT: Resignation of Doug Ligibel from the Keep Hayward Clean and Green Task Force

RECOMMENDATION

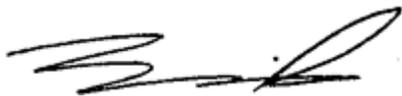
That the City Council accepts the resignation of Mr. Doug Ligibel from the Keep Hayward Clean and Green Task Force.

BACKGROUND

Mr. Doug Ligibel was appointed to the Keep Hayward Clean and Green Task Force on June 19, 2007. Mr. Ligibel submitted the attached resignation letter (Attachment II). His resignation is effective immediately and his vacated position will be filled as part of the annual appointment process for the City's Appointed Officials to Boards and Commissions.

Prepared and Recommended by: Miriam Lens, City Clerk

Approved by:



Fran David, City Manager

Attachments:

- Attachment I Resolution Accepting the Resignation
- Attachment II Resignation Letter

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION ACCEPTING THE WRITTEN RESIGNATION OF DOUG LIGIBEL FROM THE KEEP HAYWARD CLEAN AND GREEN TASK FORCE

WHEREAS, Mr. Doug Ligibel was appointed to the Keep Hayward Clean and Green Task Force on June 19, 2007 and;

WHEREAS, Mr. Doug Ligibel submitted his resignation on March 4, 2013, effective immediately.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Hayward that the Council hereby accepts the resignation of Mr. Doug Ligibel; and commends him for his civic service to the City.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013.

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS: MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATTEST: _____ City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

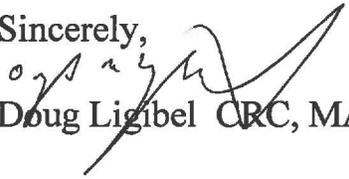
Doug Ligibel MA,CRC
Cell Phone Number (650) 245-0528

3/4/2013

Mayor Sweeney
Hayward, California 94541

Dear Mayor Sweeney:

Please make my resignation from the KHCG Task Force effective today. I had a great time working with you and our youth in Hayward over the years. We have moved out of Hayward.

Sincerely,

Doug Ligibel CRC, MA

DATE: March 19, 2013

TO: Mayor and City Council

FROM: Director of Public Works - Engineering and Transportation

SUBJECT: Citywide LED Streetlight Conversion Project – Award of Contract

RECOMMENDATION

That Council adopts the attached resolution awarding the contract to Tanko Lighting for the Citywide Light Emitting Diode (LED) Conversion Project.

BACKGROUND

On March 27, 2012, Council authorized staff to apply for a California Energy Commission (CEC) loan for \$3 million to convert the existing High Pressure Sodium (HPS) streetlights to LED lights Citywide. The loan was approved by the CEC on June 13, 2012. Council also authorized the release of Request for Proposals (RFP) to qualified contractors for a “turnkey” project to undertake this work.

The Citywide LED streetlight conversion project is the major remaining phase in the process to convert the majority of the City’s streetlights to LED. Previous efforts have included installation of LED streetlights on: Tennyson Road between Mission Boulevard and Tampa Avenue; on Jackson Street between the BART tracks and Santa Clara Street; on C Street between Watkins Street and Foothill Boulevard; and on Main Street between C Street and A Street. All of these projects were funded through the Federal Energy Efficiency and Conservation Block Grants. Additionally, LED lights were installed on Mission and Foothill Boulevards as part of the Route 238 Corridor Improvement Project.

City staff began to consider LED streetlights as a way to address the Council’s “Green” priority, by reducing greenhouse gas emissions, as well as significantly saving energy and maintenance costs. Energy experts have estimated that the conversion of high pressure sodium streetlights to LEDs can result in energy savings close to, and in some cases in excess of, 60%.

The streetlights on C Street and on Main Street were installed as part of a public outreach effort to obtain public feedback and preference on the type of lights to be installed Citywide. Five different types of lights were installed on various street segments. The public outreach component is discussed in further detail under the Public Contact section.

DISCUSSION

The scope of the project, which will convert approximately 7,750 existing streetlights to LEDs, has several components:

- Purchase and installation of new LED fixtures and disposal of old fixtures
- Community outreach and notification
- Apply for and obtain PG&E rebates
- Addressing CEC grant reporting requirements, including preparation of reimbursement forms
- Preparation of a map that can be added to the City's GIS system and which can clearly identify location and details on the City's streetlights.
- Identification of lighting improvements and upgrades needed along a few collector streets in the following neighborhoods, lighting upgrade requests from these neighborhoods:
 - a. Eldridge
 - b. Southgate
 - c. South Garden
 - d. South Hayward BART
 - e. Tennyson
 - f. Calhoun
 - g. Central Avenue/Bunker Hill

These neighborhoods were selected in response to public input received at various Neighborhood Partnership Program meetings as the most common locations where lighting deficiencies exist.

On December 14, 2012, the Request for Proposals (RFP) was released to contractors that specialize in streetlight conversion projects. The RFP required contractors to submit unit costs for completing the specified scope of work for four different types of LED fixtures: CREE/XSP; Leotek; Lumec; and one proposed by the contractors that met the City's specifications. Fixtures were required to be eligible for rebates from PG&E. On February 8, 2013, staff received eight proposals ranging in price from \$2,828,239 to \$5,208,297. Tanko Lighting of San Francisco submitted the low bid in the amount \$2,828,239. ABM, Inc, also of San Francisco, submitted the second low bid in the amount of \$2,872,410. The proposal submitted by Tanko Lighting was found to be consistent with the RFP requirements.

FISCAL IMPACT

The project is fully funded from the CEC loan. There will be no City funds spent on this project. The estimated project costs are as follows:

Contract	\$ 2,830,000
Contingency	170,000
Total	\$3,000,000

Once the contractor completes the neighborhood improvement study described in the scope of work above, staff proposes using \$170,000 of the loan balance to mitigate any remaining poor lighting conditions in the neighborhoods identified earlier. This will increase the total award to Tanko to \$3 million.

The City anticipates receiving PG&E rebates in excess of \$610,000. The rebates will be utilized to address other lighting improvement projects, such as an upgrade of the B Street decorative lights in the downtown in response to merchants and residents' requests, lighting upgrades at the various City's municipal parking lots, and at various underpasses and overcrossings.

Staff estimates that an annual energy savings of \$337,000 will be realized with this project. Based on this estimate, staff projects that the City will be able to pay off the CEC loan in less than eight years. Future savings will then continue to accrue and may be used for much needed roadway improvements. Additionally, because the LED fixtures come with a one-year warranty on labor and a ten-year warranty on the fixtures, the maintenance costs for the first year of the project will be reduced to near zero, and will be reduced significantly in subsequent years.

PUBLIC CONTACT

Staff undertook a very extensive process to receive feedback from the public on several different types of streetlight fixtures that are on the market. As mentioned above, five types of LED streetlights were installed on C Street between Watkins and Foothill and on Main Street between A Street and C Street. The public was asked to provide comments on which type of fixture they preferred and why.

Public input was sought by e-mail, phone, and written comment cards provided in drop boxes at City Hall, and at the Main and Weekes Branch libraries. The public comment period began in July 2012 and ended in mid-December 2012.

More than 40 responses were received from the public. Public opinion was generally evenly split between Leotek fixtures installed on C Street between Watkins Street and Mission Boulevard, and the CREE/XSP fixtures installed on Main Street between B and C Street. Although the City has installed Leotek fixtures in the past, the CREE/XSP fixtures have proven to be less costly and provide more energy savings. Hence, staff is recommending that CREE/XSP fixtures be installed Citywide.

Community outreach and communication was one of the services to be provided by the contractor in coordination with City staff. The contractor will prepare a schedule of the areas in the City where the lights are to be upgraded. This information will be provided on the City's website and will be distributed to residents in advance of installation commencement in each area.

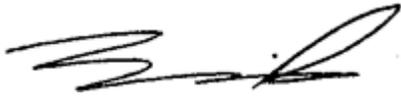
SCHEDULE

Notice to Proceed	April 1, 2013
Begin LED light installation	June 1, 2013
Complete Work	September 30, 2013

Prepared by: Don Frascinella, Transportation Manager
Yaw Owusu, Assistant City Engineer

Recommended by: Morad Fakhrai, Director of Public Works– Engineering and Transportation

Approved by:



Fran David, City Manager

Attachments:

Attachment I: Resolution

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-_____

Introduced by Council Member _____

RESOLUTION AWARDING THE DESIGN AND CONSTRUCTION CONTRACT TO TANKO LIGHTING FOR THE CITYWIDE LED RETROFIT PROJECT, PROJECT NO. 5139

WHEREAS, by resolution on March 27, 2012, the City Council authorized staff to submit an application to the California Energy Commission (CEC) and to distribute a Request for Proposals (RFP) for a Citywide LED Streetlight Retrofit Project; and

WHEREAS, the CEC Loan was approved on June 13, 2012; and

WHEREAS, an RFP was distributed on December 14, 2012; and

WHEREAS, on February 8, 2012, the City received eight (8) bids for this project ranging from \$2,828,239 to \$5,208,297 for the Base Bid; and

WHEREAS, Tanko Lighting San Francisco submitted the low Base Bid in the amount of \$2,828,239 and demonstrated the ability to capably carry out the scope of work identified in the RFP; and

WHEREAS, The City wishes to spend an additional \$170,800 to mitigate poor street lighting conditions in seven residential neighborhoods.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Hayward that Tanko Lighting is hereby awarded the contract for the Citywide LED Streetlight Retrofit Project, Project No. 5139 in an amount not to exceed \$3,000,000, in accordance with the RFP on file in the office of the City Clerk of the City of Hayward at and for the price named and stated in the bid of the hereinabove specified bidder, and all other bids are hereby rejected.

BE IT FURTHER RESOLVED that the City Manager is hereby authorized and directed to execute the contract with Tanko Lighting, in the name of and for and on behalf of the City of Hayward, in a form to be approved by the City Attorney.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS:

MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATTEST: _____
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward



DATE: March 19, 2013
TO: Mayor and City Council
FROM: Director of Public Works – Engineering and Transportation
SUBJECT: Adoption of Complete Streets Policy

RECOMMENDATION

That Council adopts the attached resolution for a Complete Streets Policy for the City of Hayward, to be effective June 30, 2013.

BACKGROUND

Complete Streets are generally defined as streets that are safe and convenient for all users of the roadway, including pedestrians, bicyclists, motorists, persons with disabilities, users and operators of public transit, seniors, children, and movers of commercial goods. A Complete Street is the result of comprehensive planning, programming, design, construction, operation, and maintenance; and should be appropriate to the function and context of the street. Over 400 communities in the U.S. have supported building complete streets, through the adoption of complete streets policies.

In recent Alameda County history, there has been tremendous growth in the number of people bicycling and walking. While specific data for the City of Hayward is not available, staff believes that the trend in Hayward will be consistent with what is being experienced countywide. Counts done by Alameda County Transportation Commission (Alameda CTC) show that since 2002 bicycling has increased by seventy-five percent and walking by forty-seven percent. As more facilities are built, evidence shows that even more people will likely be attracted to these modes. At the same time, transit ridership has also been increasing, and this trend is expected to continue; the Alameda Countywide Transportation Plan projects that there will be a 130% increase in all daily transit trips in the County by 2035.

In 2005, ten percent of Alameda County residents were 65 and older, but by 2035, seniors will make up almost twenty percent of the County's population. As the population ages, individuals become more dependent upon alternatives to cars for their mobility needs, including transit and paratransit services, and additional curb cuts. Additionally, in greater Alameda County, more and more children are walking and bicycling to school, and this trend is expected to continue.

DISCUSSION

Complete streets support safe and convenient travel by all of these existing users (walkers, bicyclists, transit riders, seniors, and children), plus the many other users of the roadway. Both the Metropolitan Transportation Commission (MTC) and Alameda CTC have recently enacted requirements that local jurisdictions must have an adopted Complete Streets policy to be eligible for certain transportation funding. The MTC and Alameda CTC requirements are described below:

- **MTC Requirements:** With Resolution 4035, MTC established the requirement that any jurisdiction that wishes to receive One Bay Area Grant (OBAG) funding must either adopt a complete streets policy resolution that is consistent with regional guidelines, or have a general plan circulation element that is in compliance with the state Complete Streets Act.
- **Alameda CTC Requirements:** The current Master Program Funding Agreement (MPFA) between Alameda CTC and the City of Hayward, that was signed on March 2, 2012 and allows the distribution of Measure B and Vehicle Registration Fee (VRF) pass-through funding, includes a complete streets policy requirement. Local agencies are required to adopt a complete streets policy that includes ten required elements. Alameda CTC developed its required policy elements to be complementary to the MTC requirement, so that jurisdictions only need to adopt one policy to be in compliance with both the Alameda CTC and MTC requirements. Staff recommends adoption of the attached policy, which meets these requirements. The adopted Alameda CTC policy requires that a Complete Streets policy include certain key elements, which are further described in Attachment II as part of the City's draft policy.

The California Complete Streets Act of 2008(Assembly Bill 1358), which took effect in January 2011, requires cities and counties to include Complete Streets policies as part of their general plans when any substantive revisions of the circulation element in the general plan are considered. To be eligible for future transportation funding cycles, MTC's Resolution 4035 also requires that local jurisdictions update their General Plans to comply with the Act by October 31, 2014. The City of Hayward is currently embarking on this effort and will include the Complete Streets policy in the General Plan Update, which is scheduled for adoption by June 2014. In conjunction with this update, Council may choose to make modifications to the adopted policy, as long as the basic Alameda CTC requirements are still met.

Attachment II is the recommended Complete Streets Policy. The focus of the Policy is to identify the actions that the City will take to ensure the needs of all potential users of a transportation project are accommodated during project development, design, and implementation. In its application of the policy, the City must look at what improvements, such as sidewalk repair, installation of curb cuts, and signing and striping improvements will be needed. The policy also identifies when exceptions from the policy may need to be granted. Finally, a key feature of the policy is public outreach and notification.

As is apparent, staff acknowledges that the steps needed to implement this policy still need to be defined. In order to assist jurisdictions with the implementation of their Complete Streets policies, MTC will be holding a workshop in May to provide further guidance and clarification. Additionally, the Alameda CTC will provide resources to assist jurisdictions in this effort.

Although Hayward has not yet adopted a Complete Streets policy, the City has been proactive over the years in addressing the needs of the varied users of the roadways. The City's adopted Bicycle Master Plan has been utilized to the great benefit of bicyclists and pedestrians. Furthermore, in response to a specific need or public support for additional facilities that are not part of the Bicycle Master Plan, staff has proceeded to implement such improvements. A recent example is the installation of bike lanes on Whitman Avenue from Tennyson Road to Harder Road. Other examples, such as the planned C Street Streetscape project, the enhanced senior pedestrian crossings at A Street and Happyland Avenue, and the installation of bicycle lanes and wider sidewalks as part of the Route 238 Corridor Improvement Project, all speak to the City's commitment to developing an infrastructure that benefits the myriad users of the roadway network.

Even beyond current efforts, the City has included the Complete Streets concept in its long range planning. The City has adopted a Form Based Code for the South Hayward BART area and is working on a form based code for the Mission corridor north of Harder Road. In part, the Form Based Code evaluates the form and function of streets and includes most of the goals of a Complete Streets Program, such as providing a street network that is more conducive to pedestrians and bicycle use.

FISCAL AND ECONOMIC IMPACT

Adopting the policy in a timely manner will ensure that the City will maintain its eligibility to receive approximately \$1.9 million in annual Measure B Local Streets and Roads funds, \$380,000 in annual bicycle and pedestrian funds, \$680,000 in annual paratransit funding and \$720,000 in annual Vehicle Registration Fee funding. Furthermore, the City will maintain its eligibility to receive approximately \$350,000 annually in additional funding for local streets and roads. However, depending upon individual projects, implementation of the policy could likely require additional design and planning costs relative to how the Complete Streets policy is implemented, as well as additional costs for public outreach, and additional construction costs.

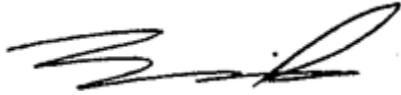
PUBLIC CONTACT

The Alameda CTC Complete Streets policy was adopted at a public meeting of the Alameda CTC Board on October 25, 2012 and was discussed at numerous prior public meetings. Additionally, City staff presented the Complete Streets concept to the Council-appointed General Plan Update Task Force on November 7, 2012.

Prepared by: Don Frascinella, Transportation Manager

Recommended by: Morad Fakhrai, Director of Public Works, Engineering & Transportation

Approved by:



Fran David, City Manager

Attachments:

Attachment I:	Resolution
Attachment II:	Complete Streets Policy

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION OF THE CITY OF HAYWARD ADOPTING A
COMPLETE STREETS POLICY

WHEREAS, the term “Complete Streets” describes a comprehensive, integrated transportation network with infrastructure and design that allows safe and convenient travel along and across streets for all users, including pedestrians, bicyclists, persons with disabilities, motorists, movers of commercial goods, users and operators of public transportation, emergency vehicles, seniors, children, youth, and families;

WHEREAS, the lack of Complete Streets is dangerous for pedestrians, bicyclists, and public transportation riders, particularly children, older adults, and persons with disabilities; more than 4,000 pedestrians and bicyclists died on roads in America in 2009, and more than 110,000 were injured, and more than 20% of traffic-related fatalities in California involved bicyclists or pedestrians; many of these injuries and fatalities are preventable, and the severity of these injuries could readily be decreased by implementing Complete Streets approaches; and City of Hayward wishes to ensure greater safety for those traveling its streets and roads;

WHEREAS, City of Hayward acknowledges the benefits and value for the public health and welfare of reducing vehicle miles traveled and increasing transportation by walking, bicycling, and public transportation, which can help address a wide variety of challenges, including pollution, climate change, traffic congestion, social isolation, obesity, physical inactivity, limited recreational opportunities, sprawl, safety, and excessive expenses;

WHEREAS, sedentary lifestyles and limited opportunities to integrate exercise into daily activities are factors contributing to increased obesity among adults and children and the consequences of obesity, such as diabetes, heart disease, stroke, high blood pressure, high cholesterol, certain cancers, asthma, low self-esteem, reduced academic performance, depression, and other debilitating diseases;

WHEREAS, City of Hayward recognizes that the careful planning and coordinated development of Complete Streets infrastructure provides long-term cost savings for local governments by reducing road construction, repair, and maintenance costs and expanding the tax base; improves public health and lowers health care expenses; provides financial benefits to property owners and businesses; and decreases air and water pollution; in contrast, the lack of Complete Streets imposes significant costs on government, employers, and individuals, including the cost of obesity, overweight, and physical inactivity, which likely amount to \$28 billion annually in California in medical expenses, workers’ compensation, and lost productivity;

WHEREAS, the State of California has emphasized the importance of Complete Streets by enacting the California Complete Streets Act of 2008 (known as AB 1358), which requires that when cities or counties revise general plans, they identify how they will provide for the routine accommodation of all users of the roadways, as well as through Deputy Directive 64, in which the California Department of Transportation explained that it “views all transportation improvements as opportunities to improve safety, access, and mobility for all travelers in California and recognizes bicycle, pedestrian, and transit modes as integral elements of the transportation system”;

WHEREAS, the California Global Warming Solutions Act of 2006 (known as AB 32) sets a mandate for the reduction of greenhouse gas emissions in California to slow the onset of human-induced climate change, and the Sustainable Communities and Climate Protection Act of 2008 (known as SB 375) requires emissions reductions through coordinated regional planning that integrates transportation, housing, and land-use policy, and achieving the goals of these laws will require significant increases in travel by public transit, bicycling, and walking;

WHEREAS, 35% of Californians do not drive, including a disproportionate number of older adults, low-income people, people of color, people with disabilities, and children, and the insufficient and inequitable availability of safe alternative means of travel adversely affects their daily lives;

WHEREAS, the dramatic increase in the population of older and very old adults that will be seen by 2020 and 2030, with the concomitant decrease in driving, requires that changes begin to occur now to street design and transportation planning to accommodate more walking, bicycling and public transit;

WHEREAS, numerous California counties, cities, and agencies have adopted Complete Streets policies and legislation in order to further the health, safety, welfare, economic vitality, and environmental well-being of their communities;

WHEREAS, City of Hayward therefore, in light of the foregoing benefits and considerations, wishes to improve its commitment to Complete Streets and desires that its streets form a comprehensive and integrated transportation network promoting safe, equitable, and convenient travel for all users while preserving flexibility, recognizing community context, and using the latest and best design guidelines and standards;

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of Hayward, State of California, as follows:

1. That the City of Hayward adopts the Complete Streets Policy attached hereto as Exhibit A, and made part of this Resolution, and that said exhibit is hereby approved and adopted.
2. That the next substantial revision of the City of Hayward General Plan circulation element shall take place by June, 2014, and shall incorporate Complete Streets policies and principles consistent with the California Complete Streets Act of 2008 (AB 1358) and with the Complete Streets Policy adopted by this resolution.

BE IT FURTHER RESOLVED, that this Complete Streets policy is effective June 30, 2013.

IN COUNCIL, HAYWARD, CALIFORNIA March 19, 2013.

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS:
MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATEST: _____
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

Attachment: Attachment I-a – Complete Streets Policy

Exhibit A

This Complete Streets Policy was adopted by Resolution No. _____ by the City Council of the City of Hayward on _____, 2013.

COMPLETE STREETS POLICY OF CITY OF HAYWARD

A. Complete Streets Commitments.

- 1. Complete Streets Serving All Users.** The City of Hayward expresses its commitment to creating and maintaining Complete Streets that provide safe, comfortable, and convenient travel along and across streets (including streets, roads, highways, bridges, and other portions of the transportation system) through a comprehensive, integrated transportation network that serves all categories of users, including pedestrians, bicyclists, persons with disabilities, motorists, emergency vehicles, movers of commercial goods, users and operators of public transportation, seniors, children, youth, and families.
- 2. Complete Streets Infrastructure.** The City of Hayward recognizes the importance of Complete Streets infrastructure and modifications that enable safe, convenient, and comfortable travel for all categories of users, including but not limited to sidewalks, shared use paths, bicycle lanes, bicycle routes, paved shoulders, street trees and landscaping, planting strips, accessible curb ramps, crosswalks, refuge islands, pedestrian signals, signs, street furniture, bicycle parking facilities, public transportation stops and facilities, transit priority signalization, and other features assisting in the provision of safe travel for all users, such as traffic calming circles, raised medians, dedicated transit lanes, transit bulb outs, and road diets.
- 3. Context Sensitivity.** In planning and implementing street projects, departments of the City shall maintain sensitivity to local conditions in both residential and business districts as well as urban, suburban, and rural areas, and shall work with residents, merchants, and other stakeholders to ensure that a strong sense of place ensues.

B. Safe Travel Requirements.

- 1. Complete Streets Routinely Addressed by All Departments.** All relevant departments of the City shall work towards making Complete Streets practices a routine part of everyday operations, approach every relevant project, program, and practice as an opportunity to improve streets and the transportation network for all categories of users, and work in coordination with other departments, agencies, and jurisdictions to maximize opportunities for Complete Streets, connectivity, and cooperation.

2. Complete Streets Required.

- a. **All Projects and Phases.** Complete Streets infrastructure sufficient to enable reasonably safe travel along and across the right of way for each category of users shall be incorporated into all planning, funding, design, approval, and implementation processes for any construction, reconstruction, retrofit, maintenance, operations, alteration, or repair of streets (including streets, roads, highways, bridges, and other portions of the transportation system), except that specific infrastructure for a given category of users may be excluded if an exemption is approved via the process set forth in section B.3 of this policy.
 - b. **Complete Streets in Routine Work and Projects.** Relevant departments shall improve Complete Streets and street functionality for all categories of users as part of routine work or projects involving pavement resurfacing, restriping, accessing above and underground utilities, signalization operations, or maintenance of landscaping or other features, unless an exemption is approved via the process set forth in section B.3 of this policy.
 - c. **Plan Consultation and Consistency.** Maintenance, planning, and design of projects affecting the transportation system shall be consistent with local bicycle, pedestrian, transit, multimodal, and other relevant plans, except that where such consistency cannot be achieved without negative consequences, consistency shall not be required if the Director of Public Works - Engineering and Transportation provides written approval explaining the basis of such deviation.
3. **Leadership Approval for Exemptions.** Specific infrastructure for a given category of users may be excluded where all of the following conditions are met:
- a. Supporting data and documentation are assembled indicating one of the following bases for the exemption:
 1. Use by a specific category of users is prohibited by law;
 2. The cost for specific infrastructure would be excessively disproportionate to the need and probable future use over the long term (costs in excess of 20% of project total may be regarded as evidence that cost is excessively disproportionate, as set forth by the United States Department of Transportation in its policy statement on accommodating bicycle and pedestrian travel);
 3. There is an absence not only of current need, but also of future need (absence of future need may be shown via demographic, school, employment, and public transportation route data that demonstrate a low likelihood of bicycle, pedestrian, or transit activity in an area over the next 10 to 20 years); or
 4. Significant adverse impacts outweigh the positive effects of the infrastructure; and

- b. The proposed exemption, as well as the supporting data and documentation, is made publicly available prior to approval of the project design by the Director of Public Works – Engineering and Transportation; and
 - c. The proposed exemption is approved by the City Council; and
- 4. Street Network/Connectivity.** As feasible, the City shall incorporate Complete Streets infrastructure into existing streets to improve the safety and convenience of users and to create employment, with the particular goal of creating a connected network of facilities accommodating each category of users, and increasing connectivity across jurisdictional boundaries and for existing and anticipated future areas of travel origination or destination.

C. Policies, Plans, and Studies.

- 1. Revising Policies and Plans.** All relevant departments are hereby directed to assess additional steps and potential obstacles to implementing Complete Streets in the City of Hayward and to recommend proposed revisions to all appropriate plans, zoning and subdivision codes, laws, procedures, rules, regulations, guidelines, programs, templates, and design manuals to integrate, accommodate, and balance the needs of all categories of users in all projects.
- 2. Studies.** All initial planning and design studies, health impact assessments, environmental reviews, and other project reviews for projects requiring funding or approval by the City Council shall: (1) evaluate the effect of the proposed project on safe, comfortable, and convenient travel by all categories of users, and (2) identify measures to mitigate any adverse impacts on such travel that are detected.

D. Performance Standards, Evaluation, and Reporting. The following steps shall be taken to support implementation of Complete Streets goals:

- 1. Performance Standards.** All relevant agencies or departments shall put into place performance standards with measurable outcomes to assess safety, comfort, actual use, and functionality, particularly with regard to the development of a bicycle and pedestrian network, for each category of users.
- 2. Evaluation.** All relevant departments shall perform evaluations of how well the streets and transportation network of the City are serving each category of users by collecting baseline data in 2013 and collecting follow-up data on a biannual basis, including data that:
 - a. Track performance standards, including new miles of bicycle lanes, sidewalks, and street trees or plantings, number of new curb ramps, improved crossings, and signage;
 - b. Measure latent demand and existing levels of service for different modes of transport and categories of users, including public transportation ridership;

- c. Track collision statistics by neighborhood and mode of transportation, and bicycle and pedestrian injuries and fatalities;
 - d. Assess the safety, functionality, and actual use of the neighborhoods and areas within the City of Hayward by each category of users.
 - e. Assess the number of bicycle, pedestrian and transit users and how this changes over time as more infrastructures are built to create a network.
- 3. Reporting.** The Director of Public Works – Engineering and Transportation shall provide an annual report to the City Council summarizing how well the City is implementing Complete Streets, with the report including: the performance standards and goals from section D.1 of this policy; the evaluations from section D.2 of this policy, with an assessment of the evaluation data; and a list and map of street projects undertaken in the past year, with a brief summary of the Complete Streets infrastructure used in those projects and, if applicable, the basis for excluding Complete Streets infrastructure from any projects.

COMPLETE STREETS POLICY OF THE CITY OF HAYWARD

Vision: To create and maintain a safe and efficient transportation system that promotes the health and mobility of the City of Hayward citizens and visitors, support better access to businesses and neighborhoods and foster new opportunities.

A. Complete Streets Principles

1. Complete Streets Serving All Users and Modes. The City of Hayward expresses its commitment to creating and maintaining Complete Streets that provide safe, comfortable, and convenient travel along and across streets (including streets, roads, highways, bridges, and other portions of the transportation system) through a comprehensive, integrated transportation network that serves all categories of users, including pedestrians, bicyclists, persons with disabilities, motorists, movers of commercial goods, users and operators of public transportation, emergency responders, seniors, children, youth, and families.

2. Context Sensitivity. In planning and implementing street projects, the City of Hayward will maintain sensitivity to local conditions in both residential and business districts as well as urban, suburban, and rural areas, and will work with residents, merchants, and other stakeholders to ensure that a strong sense of place ensues. Improvements that will be considered include sidewalks, shared use paths, bicycle lanes, bicycle routes, paved shoulders, street trees and landscaping, planting strips, accessible curb ramps, crosswalks, refuge islands, pedestrian signals, signs, street furniture, bicycle parking facilities, public transportation stops and facilities, transit priority signalization, and other features assisting in the provision of safe travel for all users, such as those features identified in the Bicycle Master Plan, General Plan, Form Based Code, etc.

3. Complete Streets Routinely Addressed by All Departments. All relevant departments and agencies of the City of Hayward will work towards making Complete Streets practices a routine part of everyday operations, approach every relevant project, program, and practice as an opportunity to improve streets and the transportation network for all categories of users, and work in coordination with other departments, agencies, and jurisdictions to maximize opportunities for Complete Streets, connectivity, and cooperation.

4. All Projects and Phases. Complete Streets infrastructure sufficient to enable reasonably safe travel along and across the right of way for each category of users will be incorporated into all planning, funding, design, approval, and implementation processes for any construction, reconstruction, retrofit, maintenance, operations, alteration, or repair of streets (including streets, roads, highways, bridges, and other portions of the transportation system), except that specific infrastructure for a given category of users may be excluded if an exception is approved via the process set forth in section C.1 of this policy.

B. Implementation

1. Design. The City of Hayward will generally follow its own accepted or adopted design standards, including the General Plan, Specific Plans, Form Based Code and Standard Details and will also evaluate using the latest design standards and innovative design options, with a goal of balancing user needs.

2. Network/Connectivity. The City of Hayward will incorporate Complete Streets infrastructure into existing streets to improve the safety and convenience of all users, with the particular goal of creating a connected network of facilities accommodating each category of users, and increasing connectivity across jurisdictional boundaries and for anticipated future transportation investments.

3. **Implementation.** The City of Hayward will take the following specific next steps to implement this Complete Streets Policy:

- A. Plan Consultation and Consistency: Maintenance, planning, and design of projects affecting the transportation system will be consistent with the City of Hayward Bicycle Master Plan, General Plan, Specific Plans, Form Based Codes, and other appropriate plans.
- B. Stakeholder Consultation: The City of Hayward will utilize its existing stakeholder notification process to allow for stakeholder involvement on projects and to support implementation of this Complete Streets policy.
- C. Developers and landowners will be encouraged to implement complete streets in their developments through consistent application of this complete streets policy/

4. **Performance Measures.** All relevant departments will perform evaluations of how well the streets and transportation network of the City of Hayward are serving each category of users by collecting baseline data and collecting follow-up data on a regular basis. Examples will include the linear feet of sidewalk constructed, miles of bicycle lanes constructed, roadway miles maintained, etc.

C. Exceptions

1. **Exception Approvals.** A process will be developed for approving exceptions that require deviation from existing plans. Written findings for exceptions will be included in the staff report approving the projects to Council, signed off by the Public Works Director. Exceptions will explain why accommodations for all users and modes were not included in the plan or project.

2. **Specific Exceptions:** Accommodation under the complete streets policy for the City of Hayward may not be necessary on transportation corridors where:

- A. Specific users are prohibited.
- B. The cost of establishing Complete Streets features would be excessively disproportionate to the need or probable use.
- C. Documented absence of current and likely future need as determined by the local and/or regional planning documents.
- D. No existing or planned transit service.
- E. Routine maintenance of the transportation network that does not change the roadway geometry or operations such as, sweeping, spot repair, and slurry seal.
- F. A reasonable and equivalent project is already planned and/or programmed in the future to provide the necessary facilities, such as a sidewalk.
- G. Accommodations are physically impossible to construct or implement.
- H. Accommodations would provide gaps in the existing transportation network, such as requiring a bike lane on a street that would not connect to other bike facilities.
- I. Accommodations would require right-of-way acquisition beyond what is required for the base project.



DATE: March 19, 2013

TO: Mayor and City Council

FROM: Director of Public Works – Engineering and Transportation

SUBJECT: Hayward Executive Airport Administration Building: Certification of Negative Declaration, Approval of Plans and Specifications; and Call for Bids; and Authorization for the City Manager to Execute an Amendment to the Professional Services Agreement with WLC Architects, Inc.

RECOMMENDATION

That Council adopts the attached resolutions that:

1. Certify the Negative Declaration for the project;
2. Approve the plans and specifications for the new Hayward Executive Airport Administration Building Project and call for bids to be received on April 16, 2013; and
3. Authorize an increase of \$80,000 to the additional services portion of the professional services agreement with WLC Architects, Inc.

BACKGROUND

The five-story Hayward Municipal Airport Air Traffic Control Tower was dedicated in 1961. The Federal Aviation Administration (FAA) has occupied the top three floors of the building since it opened, and the Airport management offices have been located on the first and second floors for approximately twenty-five years. As the needs of Airport customers have changed over time, issues with the current building have developed and the Airport operation has outgrown the limited 1,800 square feet of available floor space. New and modern building features are needed to effectively administer Airport operations and provide a high level of customer service.

These features include: a pilot briefing room with facilities to review enroute weather and file flight plans; a lounge for pilots to relax between flights and wait for passengers; a refreshment area with vending machines and seating for the use by the public; a public meeting room with seating for up to fifty people, where future public events such as the Council Airport Committee meetings will be held; and an expanded reception area in the administrative offices. An Airport historical display and an airplane viewing area will also be provided as part of the City’s community outreach and education efforts. Features such as these are now considered standard at general aviation airports across the country, and none of them are available given the size of the current building.

Recognizing the shortcomings of the current building, staff identified potential design features for a new administration building in 2002. The results of an Airport Customer Survey Report completed on June 3, 2010 confirmed the building was a high priority need that should be pursued. A project was subsequently programmed in the Airport Capital Fund of the Capital Improvement Program.

On December 6, 2011, Council authorized the City Manager to negotiate and enter into an agreement with WLC Architects, Inc. for the design of the New Hayward Executive Airport Administration Building in an amount not-to-exceed \$180,000.

DISCUSSION

The site identified for the new 8,739 square foot administration building is adjacent to the existing Control Tower building (see Attachment IV). The Airport Administration building has been designed to be constructed in two phases. Phase One is approximately 4,957 square feet with 55% of the space devoted to common public areas and 45% to Airport staff offices. Features include a public waiting room, vending area, weather briefing room, restrooms, office space for the Police Department, and a fifty-seat meeting room (see Attachment V). Also included with phase one is the installation of conduits for a security system. However, the security system itself would be installed by a different contractor under a separate contract. Phase Two includes the future development of 3,782 square feet of commercial, leasable office space. This phase will be built in the future when sufficient demand materializes for the space.

The 2010 Airport Layout Plan Update designates this site for construction of the administration building. Vehicle parking is proposed on an existing turf area east of the existing control tower. The site plan includes a new parking lot, a pedestrian bridge that will connect the new building to the runway, an outdoor patio area, trash enclosures, and landscaping (see Attachment V).

The Airport Administration Building was part of the Hayward Executive Airport Master Plan Environmental Assessment/Environmental Impact Report (EA/EIR) prepared for the Airport Master Plan, which was certified in 2002. The Master Plan EIR indicated the project site as a location for the construction of a 12,000 square foot public terminal building and parking lot of portions of the proposed project.

On January 17, 2013, a Notice of Intent to Adopt a Negative Declaration was published in The Daily Review. On January 17, 2013, the City submitted the Initial Study/Negative Declaration to the State Clearinghouse and Planning Unit. This State agency submitted the environmental document to select agencies for a review and comment period of 30 days ending on February 20, 2013. The document was also posted at the Hayward Public Library, the Weekes Branch Library, City Hall, and the Airport for a public review period of 30 days.

Three letters commenting on the draft Initial Study/Negative Declaration were received. The letters were from the San Francisco Bay Regional Water Quality Control Board (Water Board), the California Department of Fish and Wildlife (CDFW), and one from a resident of San Lorenzo.

Staff reviewed and responded to those comments in the form of letters sent to the agencies and the individual commenting on the environmental document. Attachment VII provides the comments followed by the City's responses. Essentially, the comments covered concerns about an assumed alteration of Sulphur Creek, specifically as it relates to streambed, riparian, biological resources, and water quality issues associated with site drainage. All concerns were addressed in detail, including how project design features would ensure that the project would not result in any significant environmental impacts, thus precluding the need for mitigation measures. To avoid conducting work in the creek, staff has designed the pedestrian bridge with its pile footings placed outside of the top of bank. Furthermore, the project's design would not significantly impact biological resources within the Project Area or result in significant impacts related to hydrology/water quality.

The proposed project would not likely be required to obtain any permits from agencies with jurisdiction over Sulphur Creek, such as a United States Army Corps of Engineers (Corps) Clean Water Act (CWA) Section 404 permit, a Regional Water Quality Control Board (RWQCB) Water Quality Certification, or a California Department of Fish and Wildlife (CDFW) Lake or Streambed Alteration Agreement (LSAA). However, these regulatory agencies would make the final determination whether or not a permit would be required. A Corps Section 404 permit and RWQCB Section 401 permit would not be required because the proposed project does not include filling wetlands or non-wetland waters. However, CDFW may determine that a LSAA is required for impacts to vegetation; if so, then the City will be required to complete any conditions included in the agreement.

With respect to water quality, all runoff on the proposed building site would be treated on-site in compliance with the Clean Water Program (C.3, Storm Water Technical Guidance) before it is discharged into an existing drainage inlet. Attachment VII provides all the comments received, and detailed responses to each. These responses have also been sent in the form of official letters addressed to the agencies and the individual that submitted the comments. They were also notified of the City Council's planned action at today's meeting. Approval of the Negative Declaration is recommended, based on the findings of the Initial Study that there is no substantial evidence that the project will have any significant effect on the environment.

FISCAL IMPACT

The estimated project costs are as follows:

Design & Construction	
Support Services - Consultant	\$260,000
(\$180,000 initial contract plus \$80,000 additional)	
Design Administration – City Staff	160,000
Construction Contract	2,120,000
Furniture and Equipment	180,000
Inspection and Testing	<u>160,000</u>
TOTAL:	\$2,880,000

The Adopted FY 2013 Capital Improvement Program includes \$2,600,000 for the Hayward Executive Airport Administration Building Project in the Airport Capital Improvement Fund. After bids are received, an additional appropriation from the existing Airport fund balance will be requested, if needed. The initial contract with WLC Architects for \$180,000 was solely for design services. Additional design and construction support services with WLC Architects, which are estimated at \$80,000, will be required during the construction phase. Staff requests authorization for these additional services.

PUBLIC CONTACT

On April 26, 2012, the preliminary design of the New Airport Administration Building was presented to the Council Airport Committee (CAC), and was well-received by the Committee and those members of the public who attended the meeting. Immediately after, the PowerPoint presentation from the meeting and exterior renderings were made available to the public on the City's website at: www.hayward-ca.gov/departments/publicworks/HEA/NAB.shtm . The preliminary design was also reported to Council in the City Manager's Bi-weekly Report on May 25, 2012.

Prior to the start of construction, staff will send notification letters to all Airport tenants and other interested parties, with information regarding the project and the construction schedule.

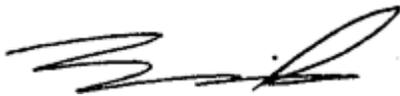
SCHEDULE

Award Construction Contract	May 7, 2013
Begin Construction	May 28, 2013
End Construction	March 19, 2014

Prepared by: Yaw Owusu, Assistant City Engineer

Recommended by: Morad Fakhrai, Director of Public Works – Engineering and Transportation

Approved by:



Fran David, City Manager

Attachments:

- Attachment I: Resolution
- Attachment II: Resolution
- Attachment III: Resolution
- Attachment IV: Location Map
- Attachment V: Architectural Drawings
- Attachment VI: Negative Declaration and Initial Study

Attachment VII: Initial Study/Negative Declaration Summary of Comments and Responses
Attachment VIII: Public Comments on initial study of the Negative Declaration

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION CERTIFYING THAT THE INITIAL STUDY AND NEGATIVE DECLARATION FOR THE HAYWARD EXECUTIVE AIRPORT ADMINISTRATION BUILDING PROJECT HAVE BEEN COMPLETED IN COMPLIANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT

WHEREAS, an Environmental Assessment/Program Environmental Impact Report (EA/EIR) for the Hayward Executive Airport Master Plan was certified by the City Council on April 16, 2002; and

WHEREAS, an Initial Study and Negative Declaration (IS/ND) for the Hayward Executive Airport Administration Building have been prepared and processed in accordance with City and CEQA guidelines; and

WHEREAS, on January 17, 2013, a Notice of Intent to Adopt Negative Declaration was published in The Daily Review. Copies of the Negative Declaration and Initial Study Checklist were posted 30 days, from January 17 to February 20, 2013 for public review in accordance with CEQA; and

WHEREAS, three letters commenting on the draft Initial Study/Negative Declaration were received from the Bay Regional Water Quality Control Board (Water Board), the California Department of Fish and Wildlife (CDFW) and from Mr. Howard Beckman, a resident of San Lorenzo; and

WHEREAS, staff reviewed and responded to all comments received in the form of letters sent to the agencies and the individual commenting on the environmental document; and

WHEREAS, the City Council has reviewed the Initial Study and Negative Declaration, and the comments thereon and the responses thereto by staff.

NOW, THEREFORE, BE IT RESOLVED THAT the City Council of the City of Hayward, having considered the information contained in the Initial Study upon which the Negative Declaration is based and having reviewed the comments thereon and the responses thereto by staff, hereby certifies that the Negative Declaration has been completed in compliance with the requirements of the California Environmental Quality Act, and further finds that the project will not have a significant effect on the environment and the Negative Declaration reflects the independent judgment of the City of Hayward.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES:

NOES:

ABSTAIN:

ABSENT:

ATTEST: _____
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-_____

Introduced by Council Member _____

RESOLUTION APPROVING PLANS AND SPECIFICATIONS FOR THE HAYWARD EXECUTIVE AIRPORT ADMINISTRATION BUILDING PROJECT – PROJECT NO. 6815, AND CALL FOR BIDS

BE IT RESOLVED by the City Council of the City of Hayward as follows:

WHEREAS, those certain plans and specifications for the Hayward Executive Airport Administration Building Project, Project No. 6815, on file in the office of the City Clerk, are hereby adopted as the plans and specifications for the project;

WHEREAS, the City Clerk is hereby directed to cause a notice calling for bids for the required work and material to be made in the form and manner provided by law;

WHEREAS, sealed bids therefor will be received by the City Clerk’s office at City Hall, 777 B Street, 4th Floor, Hayward, California 94541, up to the hour of 2:00 p.m. on Tuesday, April 16, 2013, and immediately thereafter publicly opened and declared by the City Clerk in the Public Works Conference Room, 4D, located on the 4th Floor of City Hall, Hayward, California;

NOW, THEREFORE, BE IT FURTHER RESOLVED, the City Council will consider a report on the bids at a regular meeting following the aforesaid opening and declaration of same.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES:

NOES:

ABSTAIN:

ABSENT:

ATTEST: _____
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION AUTORIZING AN INCREASE IN ADDITIONAL PROFESSIONAL SERVICES WITH WLC ARCHITECTS, INC. FOR THE HAYWARD EXECUTIVE AIRPORT ADMINISTRATION BUILDING, PROJECT NO. 6815.

BE IT RESOLVED by the City Council of the City Council of Hayward that the City Manager is hereby authorized and directed to negotiate and execute an amendment to the existing Professional Services Agreement with WLC Architects, Inc. for additional design and construction support services related to the Hayward Executive Airport Administration Building, Project 6815, in an amount not to exceed \$80,000, in a form to be approved by the City Attorney

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES:

NOES:

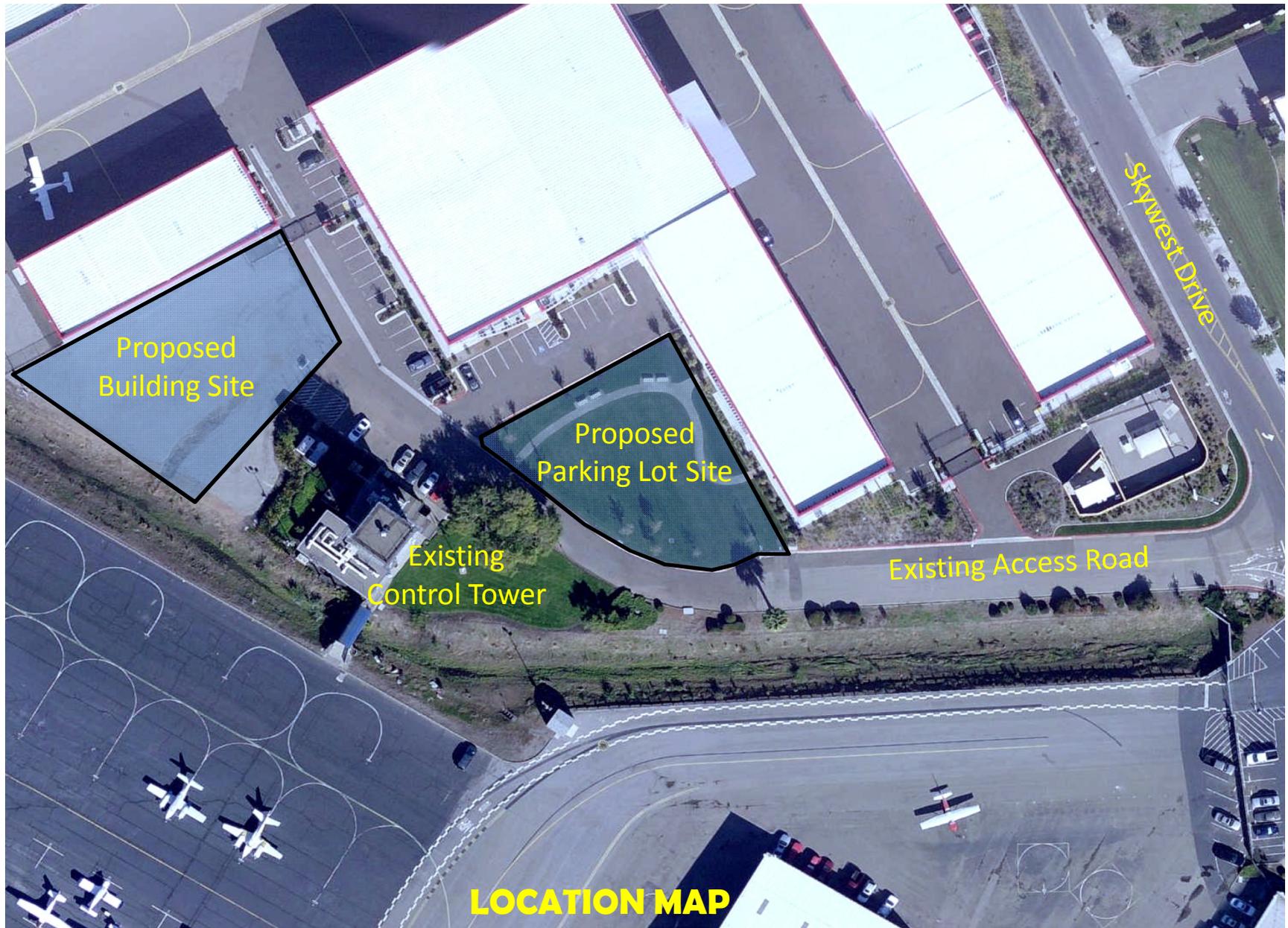
ABSTAIN:

ABSENT:

ATTEST: _____
City Clerk of the City of Hayward

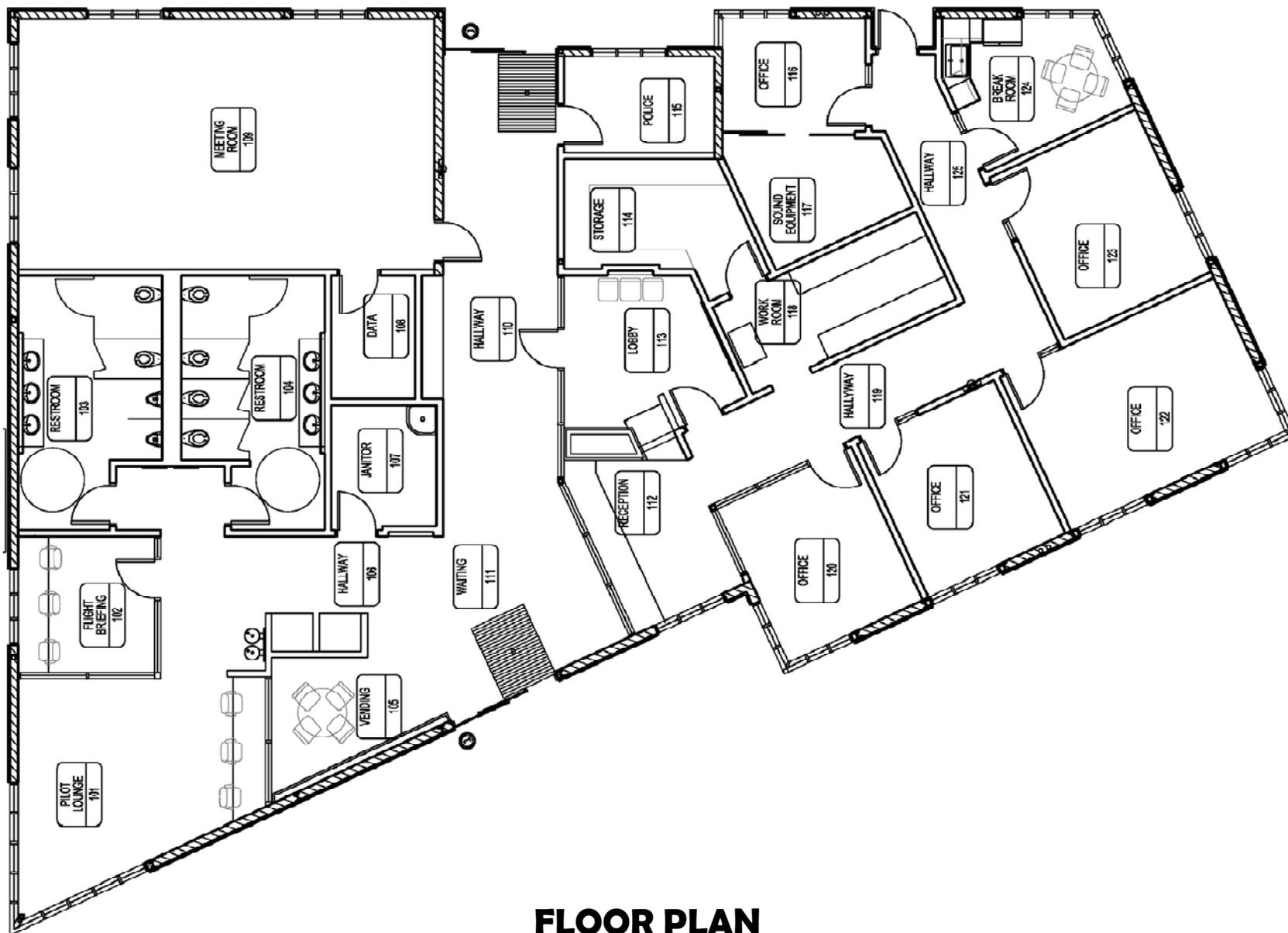
APPROVED AS TO FORM:

City Attorney of the City of Hayward





SITE PLAN



FLOOR PLAN



WEST ELEVATION FACING THE RUNWAY



**DEPARTMENT OF PUBLIC WORKS
Engineering and Transportation**

**NEGATIVE DECLARATION
Hayward Executive Airport Administration Building**

Notice is hereby given that the City of Hayward finds that no significant impact on the environment as prescribed by the California Environmental Quality Act of 1970, as amended will occur for the following proposed project:

I. PROJECT DESCRIPTION:

One story airport administration building containing administration offices, pilot lounge, flight planning, meeting room, lobby, reception, and other necessary facilities. The site includes parking lot for personnel and visitors, pedestrian bridge to access runway, utility enclosure, trash enclosure, site lighting, landscaping, and irrigation.

II. FINDING PROJECT WILL NOT SIGNIFICANTLY IMPACT ENVIRONMENT:

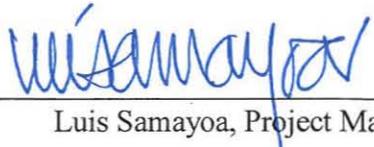
The proposed project will not have a significant impact on the environment.

III. FINDINGS SUPPORTING DECLARATION:

1. The proposed project has been reviewed according to the standards and requirements of the California Environmental Quality Act (CEQA) and an Initial Study has determined that the proposed project could not result in significant impact on the environment.
2. The project will not result in any development that would adversely impact any scenic resources.
3. The project will not result in any development that would have an adverse impact on agricultural land.
4. The project will not result in any development that would have impacts related to changes in air quality.
5. The project will not result in any development that would have impacts to biological resources such as wildlife and wetlands.
6. The project will not result in any development that would have impacts to known cultural resources including historical resources, archaeological resources, paleontological resources, unique topography or disturb human remains.
7. The project will not impact geological hazards.
8. The project will not generate significant greenhouse gas emissions or be in conflict with an applicable plan, policy or regulating adopted to reduce greenhouse gas emissions.
9. The project will not have significant impacts on hazards or hazardous materials.
10. The project will adhere to all applicable water quality standards.
11. The project is not in conflict with the policies of the Hayward General Plan, Hayward Zoning Ordinance or Alameda County Airport Land Use Policy Plan.
12. The project will not result in significant impact to mineral resources since no such resources are located within the project area or vicinity.
13. The project will not result in significant noise impacts.

14. The project will have no impact on population or housing.
15. The project will not result in a significant impact to public services.
16. The project will have no impact on recreational facilities.
17. The project will have no impact on vehicular traffic including those relating to emergency access.
18. The project will have no impact with respect to traffic circulation.
19. The project will have no impact on utilities or service systems.

IV. PERSON WHO PREPARED INITIAL STUDY:

Signature:  Dated: 1-11-13
Luis Samayoa, Project Manager

V. COPY OF INITIAL STUDY IS ATTACHED

For additional information, please contact the City of Hayward Engineering and Transportation, 777 B Street, Hayward, CA 94541-5007 or telephone (510) 583-4740.

When submitting a comment, please include the name of a contact person in your agency or organization. Comments may be submitted by mail, e-mail, or fax to the address below:

Luis Samayoa, P.E.
Project Manager
City of Hayward, Engineering and Transportation
777 B Street
Hayward, CA 94541-5007
E-mail: luis.samayoa@hayward-ca.gov
Fax: (510) 583-3620

All comments must be received by **5:00 p.m. on February 15, 2013**



**DEPARTMENT OF PUBLIC WORKS
Engineering and Transportation Division**

INITIAL STUDY CHECKLIST

Project Title: Hayward Executive Airport Administration Building

Lead agency name and address: City of Hayward, 777 "B" Street, Hayward, CA 94541-5007

Contact person: Luis Samayoa, Associate Civil Engineer
(510) 583-4769 Luis.samayoa@hayward-ca.gov

Project location: 20301 Skywest Drive, Hayward, California 94541, APN # 432-0104-002-00

Project sponsor's name and address: City of Hayward
777 B Street
Hayward, CA 94541

General Plan: Public and Quasi-Public (PQP)

Zoning: Air Terminal - Airport Commercial (AT-AC) District

Description of project: One story airport administration building containing administration offices, pilot lounge, flight planning, meeting room, lobby, reception, and other necessary facilities. parking for personnel, pedestrian bridge to access runway, utility enclosure, trash enclosure, site lighting, landscaping, irrigation work, and parking lot

Surrounding land uses and setting: The uses surrounding the subject site include retail and office commercial to the west, air terminal-airport commercial to the south, north and east.

Other public agencies whose approval is required: None.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology /Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation/Traffic | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Luis Samayoa
 Signature
Luis Samayoa
 Printed Name

January 4, 2013
 Date

 For

EVALUATION OF ENVIRONMENTAL IMPACTS:

ENVIRONMENTAL ISSUES:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
I. AESTHETICS -- Would the project:				
a) Have a substantial adverse effect on a scenic vista? <i>Comment: The proposed improvements would not affect any scenic vista. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? <i>Comment: No scenic resources exist in the area. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings? <i>Comment: The project will enhance the visual character of the site; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area? <i>Comment: The light generated from the proposed administration building is less than significant given that the proposal will be an extension of the adjacent airport developed area with an airport zoning; no mitigation is required.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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I. AGRICULTURE AND FOREST

RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. -- Would the project:

a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resource Agency, to non-agricultural use?

***Comment:** According to "A Guide to the Farmland Mapping and Monitoring Program, 2004 Edition, the Alameda County Board of Supervisors determined that there is no Farmland of Local Importance for Alameda County. Therefore there is no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?

***Comment:** The project is not located in an agricultural zoning district nor is it subject to a Williamson Act contract. Therefore is no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

***Comment** There are no forest lands in this area and the project does not involve the rezoning of forest land or timberland; therefore, no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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d) Result in the loss of forest land or conversion of forest land to non-forest use?

Comment *There are no forest lands in this area and the project does not involve the loss of forest land or involve conversion of forest land; therefore, no impact.*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?

Comment *The project does not involve, nor is it located near, any commercially operated agricultural lands, The project is not located near any forest land. Therefore, no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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III. AIR QUALITY -- Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a) Conflict with or obstruct implementation of the applicable air quality plan?

Comment *The Bay Area Air Quality Management District (BAAQMD) has established screening criteria as part of its CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria, the proposed project screens below what would require additional evaluation; therefore the proposed project will not conflict with the goals of the air quality plan and there is no impact.*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?

Comment *The Bay Area Air Quality Management District (BAAQMD) has established screening criteria as part of its CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria, the proposed project screens below what would require additional evaluation. There are no existing or projected air quality violations that affect this property; therefore the proposed project will not violate any air quality*

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<i>standard and there is no impact.</i>				
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment <i>The proposed project meets the screening criteria in Table 3-1 of the Air District's CEQA Guidelines; therefore, no impact.</i>				
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment <i>(Refer to III a).</i>				
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment <i>The Bay Area Air Quality Management District (BAAQMD) has established screening criteria as part of its CEQA guidance to assist in determining if a proposed project could result in potentially significant air quality impacts. Based on the District's criteria, the proposed project screens below what would require additional evaluation; therefore, there is no impact.</i>				
IV. BIOLOGICAL RESOURCES -- Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>The project will not have any adverse effect on biological resources; therefore, no impact.</i>				
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer to IV a).</i>				

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Comment: Refer to IV a)

d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: Refer to IV a)

e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: The project site does not contain any significant stands of trees. Any removal of tress would be required to comply with the City's Tree Preservation Ordinance including providing replacement tees. It will be required that an arborist report, that meets the approval of the City's Landscape Architect, be submitted. With this condition, the impacts on the project would comply with local policies and ordinances; therefore, there would be a less than significant impact.

f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: There are no habitat conservation plans affecting the property. Therefore, no impact.

V. CULTURAL RESOURCES -- Would the project:

a) Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Comment: There are no historical resources associated with the improvements on the site or the affected parcels. In addition, the surrounding properties have no historical significance; therefore, no impact.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5? <i>Comment: No known archaeological resources exist on the site. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature? <i>Comment: No known paleontological resources exist on the site. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries? <i>Comment: There are no records of any human remains located on the subject sites. Standardized procedure for evaluation accidental finds and discovery of human remains shall be followed as prescribed in Sections 15064.f and 151236.4 of the California Environmental Quality Act. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
VI. GEOLOGY AND SOILS -- Would the project:				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. <i>Comment: The site is not located near any known fault traces. Therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking? <i>Comment: The affected parcels are not located near any known fault traces; however, future buildings will be designed and constructed to withstand ground shaking in the event of an earthquake; therefore, there is less than a significant impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction? <i>Comment: The project site is within an area identified as subject to liquefaction movement. A soils investigation with recommendations on foundation design that addresses the effects of</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<i>liquefaction has been prepared; therefore, there is less than significant impact.</i>				
iv) Landslides?				
Comment <i>The project site is a flat lot located along the wetlands and will not be subject to landslides; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?				
Comment <i>The project site is a flat lot. The applicant proposed to place engineered fill on the site to specifications of a geotechnical engineer to ensure stability and to raise the property above the flood zone level; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment <i>Refer to VIb.</i>				
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer VIb.</i>				
e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>The project will be connected to the City's sanitary sewer system and will not involve septic tanks or other alternative wastewater; therefore, no impact.</i>				
VII. GREENHOUSE GAS EMISSIONS -- Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?				
Comment: <i>The project falls below the allowable screening criteria established by the Bay Area Air Quality Management District and will not exceed the threshold of significance for Greenhouse gas emissions; therefore no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? Comment: <i>Future construction will conform to the City's Green Building Ordinance which includes measures regarding greenhouse gasses; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
VIII. HAZARDS AND HAZARDOUS MATERIALS -- Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? Comment: <i>The proposed development will consist of building government use. No use or storage of hazardous materials/waste will be allowed in the building; therefore, no significant impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? Comment: <i>See VIII a).</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? Comment: <i>There are no schools within one-quarter mile of the project site; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? Comment: <i>It has been determined that there are no hazardous materials on the project site and the project site is not on a list of hazardous materials site; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? Comment: <i>The project is located within an airport</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

land and conforms to the airport master plan thus it would not result in a safety hazard for people working in the project area; therefore, no impact.

f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?

Comment: See VIII e).

g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Comment: The project site is located at the terminus of a street and will not interfere with an adopted emergency response plans or evacuation plan; therefore, no impact.

h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

Comment: The project site is not located within the City's Wildland Interface Area; therefore no impact.

IX. HYDROLOGY AND WATER QUALITY --

Would the project:

a) Violate any water quality standards or waste discharge requirements?

Comment: The project will comply with all water quality and wastewater discharge requirements of the City; therefore, no impact.

b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

Comment: The project will be connected to the existing water supply and will not involve the use of water wells and will not deplete groundwater supplies or substantially interfere with groundwater recharge; therefore, no impact.

c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? Comment: See VIII e).	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? Comment: The project site is located at the terminus of a street and will not interfere with an adopted emergency response plans or evacuation plan; therefore, no impact.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? Comment: The project site is not located within the City's Wildland Interface Area; therefore no impact.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IX. HYDROLOGY AND WATER QUALITY -- Would the project:				
a) Violate any water quality standards or waste discharge requirements? Comment: The project will comply with all water quality and wastewater discharge requirements of the City; therefore, no impact.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? Comment: The project will be connected to the existing water supply and will not involve the use of water wells and will not deplete groundwater supplies or substantially interfere with groundwater recharge; therefore, no impact.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
would result in substantial erosion or siltation on- or off-site? Comment: <i>The proposed drainage system for the project is designed to accept all off-site drainage that is directed towards the project site; therefore, no significant impact.</i>				
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? Comment: <i>Refer to IX c.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? Comment: <i>Refer to IX c.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality? Comment: <i>It will be required that the proposed drainage design shall be treated to meet the Alameda County Flood Control and Water Conservation District's C-3 requirements before entering an existing drainage facility; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? Comment: <i>The proposed project would consist of airport use. There are no new homes proposed; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows? Comment: <i>The project site is not located in an area that would be subject to flooding from the failure of a dam or levee. The project site is currently located within a 100-year flood hazard area; however, engineered fill will be placed on the site to specifications of a geotechnical engineer to ensure stability and to raise the property above the flood zone level; therefore, it would be less than significant.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? Comment: <i>(Refer to IX h.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
j) Inundation by seiche, tsunami, or mudflow? Comment: <i>Refer to IX h.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
X. LAND USE AND PLANNING -- Would the project:				
a) Physically divide an established community? Comment: <i>The project site is located within airport land. There is no established community that would be physically divided by the proposed project; therefore no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? Comment: <i>The property lies within the airport as identified on the General Plan. The use of public building is consistent with the abutting properties; therefore there is no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan? Comment: <i>The project site is not covered by any habitat conservation plan or natural community conservation plan; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XI. MINERAL RESOURCES -- Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? Comment: <i>There are no known mineral resources on the project site; therefore no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? Comment: <i>The project site is not identified as a site known to have mineral resources; therefore, no</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<i>impact.</i>				
XII. NOISE -- Would the project result in:				
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>All uses within the proposed buildings will be required to comply with the City's Noise Regulations as defined in Section Chapter 4 Article 1 of the Hayward Municipal Code; therefore, no impact.</i>				
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer to XII a).</i>				
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer to XII a).</i>				
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer to XII a).</i>				
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>The project is within an airport land use but the building design will incorporate features to mitigate the outside noise .</i>				
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>Refer to XII e).</i>				
XIII. POPULATION AND HOUSING -- Would the project:				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment: <i>. The project is located at the end of a</i>				

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<i>cul-de-sac. All properties surrounding the site are either zoned public, quasi public or commercial. The project would not induce population growth; therefore, no impact.</i>				
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment. <i>There is no existing housing in the airport land and, as such, the project would not displace housing. Therefore, no impact.</i>				
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Comment. <i>Refer to XIIIb.</i>				
XIV. PUBLIC SERVICES --				
a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other public facilities?				
Comment <i>The project is proposing an airport and airport commercial use within an urbanized area that is already served by police and fire services and no additional public services facilities would be needed to adequately serve the proposed project. Since the use is airport (public) it would not have an impact on schools or parks. .</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XV. RECREATION --				
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

accelerated?

Comment *The proposed project is an airport use. As there is no housing proposed there would not be a need to use neighborhood and regional parks or other recreational facilities, therefore, no impact.*

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

Comment *The project is proposing an airport use which does not include require recreational facilities or require the construction or expansion of recreational facilities; therefore, no impact.*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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XVI. TRANSPORTATION/TRAFFIC -- Would the project:

a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?

Comment *The project will not conflict with any plan regarding the circulation system. The project is an airport project which would not create a new street for access. The project would create a minimal impact to the existing traffic system, therefore, no significant impact*

b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

Comment. *No level of service will be significantly impacted by the new airport administration building.*

c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?

Comment *The project involves no change to air traffic patterns; therefore, no impact.*

d) Substantially increase hazards due to a design

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? Comment <i>The project has been designed to meet all City requirements regarding street design will not increase any hazards; therefore no impact.</i>				
e) Result in inadequate emergency access? Comment <i>The project is at the end of an existing public street; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities? Comment <i>The project does not involve any conflicts or changes to policies, plans or programs related to public transit, bicycle or pedestrian facilities; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XVII. UTILITIES AND SERVICE SYSTEMS --				
Would the project:				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? Comment <i>The project will not exceed wastewater treatment requirements; therefore no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? Comment <i>There is sufficient capacity to accommodate the proposed project; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? Comment <i>There is sufficient capacity in the existing storm drain system to accommodate the proposed project; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? Comment <i>There is sufficient capacity to accommodate the proposed project; therefore, no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

treatment provider which serves or may serve the project that it has adequate capacity to serve the project' s projected demand in addition to the provider' s existing commitments?

Comment *There is sufficient capacity to accommodate the proposed project; therefore, no impact.*

f) Be served by a landfill with sufficient permitted capacity to accommodate the project' s solid waste disposal needs?

Comment *There is sufficient capacity to accommodate the proposed project; therefore, no impact.*

g) Comply with federal, state, and local statutes and regulations related to solid waste?

Comment *The project will be subject to the regulations stipulated in Chapter 5, Article 1 Solid Waste Collection and Disposal in the City's Municipal Code; therefore, no impact.*

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE --

a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Comment *As evidenced in Section IV a and IV d of the checklist above, it has been determined that the project will not have any significant impacts to fish, wildlife species or plant life; therefore no impact.*

b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

Comment *As evidenced in Sections IX c, IX f, XIII a, and XV a of the checklist above, it has been determined that the project will not have any significant impacts; therefore no impact..*

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? Comment <i>The project will not have any environmental impacts therefore will not cause substantial adverse effects on human beings; therefore no impact.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

February 15, 2013

Luis Samayoa
Engineering and Transportation Div.
City of Hayward
777 B Street
Hayward, CA 94541

Initial Study for New Airport Administration Building

Dear Mr. Samayoa,

Herewith are comments on the Initial Study for the new airport administration building dated Jan. 4, 2013. These same comments were sent to you by e-mail at 2 p.m. today.

The Initial Study Checklist prepared by the governor's Office of Planning and Research or OPR (and included as Appendix G to the CEQA Guidelines) is intended to document the conclusion that a proposed project does not appear to have potentially significant environmental impacts that would require an Environmental Impact Report (under CEQA). The checklist prepared by the OPR is a suggested checklist of issues; the introduction to the checklist states that it is a "sample checklist" and that it is "intended to encourage thoughtful assessment of impacts." Moreover, the OPR states in the introduction to its checklist: "Substantial evidence of potential impacts that are not listed on this form must also be considered."

A1

Thus the OPR checklist is not literally a checklist that, if filled out by "checking the boxes," will satisfy CEQA. It is meant to guide agencies in a serious contemplation of the possible adverse impacts of a proposed project.

The proposed new airport administration building, like the existing building, would lie immediately adjacent to a portion of the lowest stretch of Sulphur Creek before the creek traverses the runway area of the airport. Yet this important feature of the project site is nowhere to be found in the project description in the Initial Study, which is no more than one sentence. (Nor is it shown or identified in the site plan prepared by WLC Architects, dated Oct. 20, 2012.) Indeed, in describing past projects that touch on or include streams, whether natural or engineered, the City of Hayward has habitually failed to identify the location of such streams in relation to proposed projects.

A2

In addition, the Initial Study refers to Sulphur Creek as a "flood control facility". This language conjures up images of engineered channels with cement walls or underground culverts, when in fact the creek at the point it passes next to the proposed building is a part of a natural segment of Sulphur Creek that emerges from underneath Hesperian Boulevard and courses across the airport toward the bay. This language, too, is characteristic of City of Hayward's predisposition toward the remaining natural streams that are part of the watershed draining the city; in the past city planning documents have referred to streams as "ditches" (viz the West A Street extension

A3

/ Continued ...

project or the Cannery Project). The manner in which some feature of the environment is characterized has much to do with an assessment of impacts on that feature.

A3

Section IV, Biological Resources. City provides a naked, unsupported assertion: "The projects will not have any adverse effect on biological resources; therefore, no impact." Not only is this a mere assertion, it is a tautology. This identical statement is used to dismiss the issue of impacts on riparian habitat (sec. IV b) or on movement of fish or wildlife (sec. IV c).

A4

Section IX, Hydrology and Water Quality. Here, too, City provides only naked assertions. With respect to the issue of water quality and discharge into adjacent Sulphur Creek (para. a), City states: "The project will comply with all water quality and wastewater discharge requirements of the City; therefore, no impact." A promise to uphold standards does not qualify as even a cursory assessment of possible adverse impacts. Moreover, the City's assertion does not make clear whether the City's wastewater discharge requirements as applied to this project constitute mitigation of potentially adverse impacts. Any mitigation must be spelled out at this stage, and City would be required to issue a Mitigated Negative Declaration rather than a simple Negative Declaration.

A5

With respect to impacts of the project on drainage or surface water runoff (paras. c, d, e), City relies exclusively on the assertion: "The proposed drainage system for the project is designed to accept all off-site drainage that is directed towards the project site; therefore, no significant impact." City does not provide information on whether the proposed drainage system will discharge into Sulphur Creek or how much impermeable surface will be created by the project and, as a result, how much, if any, stormwater will run into the creek. City's comment at para. f indicates that water from the proposed drainage system will discharge into the creek (identified there as "an existing drainage facility").

A6

Section XVIII, Mandatory Findings of Significance. Para. b concerns the question of cumulative impacts of the proposed project. City concludes "no impact" and for support cites its "evidence" in sections IXc, IXf, XIIIa, and XVa of the Initial Study. However, City provides no documentation to support its conclusion of no impacts in those sections. City does not describe or discuss the possible cumulative effects of the proposed project in light of earlier projects that impacted the lower arm of Sulphur Creek (construction of Home Depot on airport property, the Cannery Project, West A St. extension, as well as other, unknown projects).

A7

City cannot avoid the intent and requirement of the Initial Study to document its conclusions of no significant environmental impacts of the new administration building by merely asserting no impacts, thereby shifting the burden of analysis and documentation of substantial evidence to the public.

Howard Beckman
1261 via Dolorosa
San Lorenzo 94580
Tel: 510.278.7238

cc: Brian Wines, Regional Water Quality Control Board



State of California – The Natural Resources Agency
 DEPARTMENT OF FISH AND WILDLIFE
 Bay Delta Region
 7329 Silverado Trail
 Napa, CA 94558
 (707) 944-5500
www.wildlife.ca.gov

EDMUND G. BROWN JR., Governor
 Attachment VII
 CHARLTON H. BONHAM, Director



Comment Letter B

February 22, 2013

Mr. Luis Samayoa
 Project Manager
 City of Hayward, Engineering and Transportation
 777 B Street
 Hayward, CA 94541-5007

Dear Mr. Samayoa:

Subject: Hayward Executive Airport Administration Building, Initial Study/Negative Declaration, SCH #2013012050, City of Hayward, Alameda County

The California Department of Fish and Wildlife (CDFW) has reviewed the subject document prepared for the Hayward Executive Airport Administration Building (Project). The Project is located at 20301 Skywest Drive, in the City of Hayward, Alameda County. The Project includes construction of a one-story administration building, parking for personnel, pedestrian bridge to access runway, utility and trash enclosures, site lighting, landscaping and irrigation, and a parking lot.

The Project description does not provide adequate details, such as the size of the building or construction area or the existing conditions, to allow for CDFW to assess the biological impacts of the Project. Section 15063 (a)(1) of the California Environmental Quality Act (CEQA) Guidelines states "All phases of project planning, implementation, and operation must be considered in the Initial Study of the project." The Initial Study should include a description of the environmental setting and potential environmental impacts with a brief explanation to support findings.

B1

The Initial Study checklist, Biological Resources, IV(a) states "The Project will not have any adverse effect on biological resources, therefore, no impact." The Initial Study fails to disclose the Project location in relation to Sulphur Creek, which flows through the Hayward Executive Airport. CDFW recommends that a set-back buffer be established for some creeks measuring from the top of the stream bank or riparian canopy. No construction, including roads, should be allowed within the buffer area to provide adequate protection of the resources and to minimize the need for future maintenance and bank armoring in the channel. Many negative impacts to creek systems are associated with attempts to stabilize creek banks that are failing beneath poorly located structures. For example, a structure placed too close to the top of bank, or even below the top of bank, may become threatened by natural erosion of the creek bank, as the centerline of the creek meanders within the channel. All wetlands and watercourses, whether intermittent or perennial, should be retained and provided with substantial setbacks to preserve the riparian and aquatic values and maintain their value to on-site and off-site fish and wildlife.

B2

Mr. Luis Samayoa
February 22, 2013
Page 2

For any activity that will divert or obstruct the natural flow, or change the bed, channel, or bank (which may include associated riparian resources) of a river or stream, or use material from a streambed, CDFW may require a Lake and Streambed Alteration Agreement (LSAA), pursuant to Section 1600 et seq. of the Fish and Game Code. Issuance of an LSAA is subject to CEQA. The CDFW, as a responsible agency under CEQA, will consider the local jurisdiction's (lead agency) Negative Declaration or Environmental Impact Report for the Project. The CEQA document should fully identify the potential impacts to the stream or riparian resources and provide adequate avoidance, mitigation, monitoring and reporting commitments for completion of the agreement.

B3

The Initial Study, Biological Resources, IV(e) implies that some trees will be removed, but does not disclose how many, their size, species, or location. CDFW recommends that for each native tree that is removed or destroyed, trees shall be replaced with native trees on-site at a minimum 3:1 ratio (replacement:loss). For each non-native tree that is removed or destroyed, trees should be replaced with native trees on-site at a minimum 1:1 ratio (replacement:loss). Impacts to nesting birds should be avoided by scheduling construction and tree removal activities outside of the nesting season.

B4
B5

If mitigation is required for the Project, it should be approved by CDFW and be of sufficient quality and quantity to offset the impacts.

B6

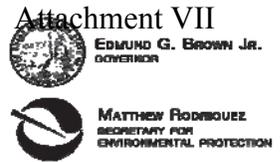
CDFW is unable to determine if the Project will not have a significant impact on the environment. CDFW recommends that the Initial Study be recirculated after a thorough Project description and, if necessary, a complete environmental analysis has been completed.

We are further available to discuss our concerns, if requested. If you have any questions, please call Ms. Marcia Grefsrud, Environmental Scientist, at (707) 644-2812; or Mr. Craig Weightman, Acting Environmental Program Manager, at (707) 944-5577.

Sincerely,

Scott Wilson
Acting Regional Manager
Bay Delta Region

cc: State Clearinghouse



Comment Letter C

San Francisco Bay Regional Water Quality Control Board

February 19, 2013
CIWQS Place No. 776459

Sent via electronic mail: No hardcopy to follow

City of Hayward
Engineering and Transportation Division
777 B Street
Hayward, CA 94541

Attn: Luis Samayoa (Luis.samayoa@hayward-ca.gov)

Subject: Initial Study and Proposed Mitigated Negative Declaration for the Hayward Executive
Airport Administration Building
SCH No. 2013012050

Dear Mr. Samayoa:

San Francisco Bay Regional Water Quality Control Board (Water Board) staff have reviewed the
Initial Study / Mitigated Negative Declaration (ISMND) for the Hayward Executive Airport
Administration Building. The proposed Project will construct a new administration building and
parking lot at the Hayward Executive Airport in the City of Hayward, in Alameda County.
Water Board staff have the following comments on the ISMND.

Comment 1, Timing of CEQA Clearinghouse Compliance and the Proposed Adoption of
the ISMND by the City of Hayward City Council

According to the Notice of Completion & Environmental Document Transmittal, which was
provided to the Water Board by the State Clearinghouse, the State Clearinghouse Compliance
date for the end of the comment period is February 20, 2013. However, the Hayward City
Council has proposed to adopt the ISMND at their meeting on February 19, 2013. It is not clear
why the City is proposing to adopt the ISMND before the end of the State Clearinghouse
comment period.

C1

Comment 2, Biological Resources

The new Administration Building will be constructed adjacent to Sulphur Creek and the
proposed new pedestrian bridge will cross Sulphur Creek. The reach of Sulphur Creek in the
vicinity of the existing control tower was used to provide riparian mitigation for two previous
City of Hayward Projects: the West A Street Realignment Project (Water Board Site No. 02-01-
0861) and the Cannery Area Public Improvement Project (Water Board Site No. 02-01-C0880).

C2

Mitigation for West A Street included:

Onsite mitigation will enhance and improve 89 linear feet of Line K-1. About 58 linear
feet of Line K-1 that is currently enclosed in a 65-inch by 40-inch corrugated metal arch
culvert will be daylighted. The daylighted segment of the channel, along with an
additional 31 linear feet segment of the channel that is currently open, will be enhanced by
widening the channel bottom and planting the channel banks with native trees, shrubs, and

JOHN MULLER, CHAIR | BRUCE H. WOLFE, EXECUTIVE OFFICER

grasses. Trees planted at the Project site as part of the on-site mitigation shall include box elder (*acer negundo*), California buckeye (*aesculus californica*), western sycamore (*platanus recemosa*), and California bay (*umbellularia californica*). Shrubs and grasses shall include California coffeeberry (*rhamnus californica*), coyote bush (*baccharis pilularis*), flowering currant (*ribes sanguineum*), California lilac (*ceanthos gloriosus*), Berkeley sedge (*carex tumulicola*), and sky lupine (*lupinus nanus*). A selection of ornamental tree and shrub species will be planted in a buffer strip between the mitigation area and the street, including: trident maple (*acer buergeranum*), brisbane box (*lophostemon confertus*), crape myrtle (*lagerstroemia natchez*), chanticleer pear (*pyrus calleryan*), Japanese barberry (*berberis thunderbergii*), strawberry tree (*arbutus unedo*), dwarf flax (*phormium*), white India hawthorn (*rhapiolepis*), Point Reyes kinnikinnick (*arctostaphylos uva*), white gazania (*gazania rigens leucolaena*), and dwarf fountain grass (*pennistetum alopecuroides*).

Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 570 linear feet are to be provided as mitigation for the West A Street Project, while the remaining portion of this reach will be used as mitigation for impacts to Sulfur Creek at the Cannery Area Project (Site No. 02-01-C0880). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the mitigation reach of Sulphur Creek. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek mitigation is included as Figure 6 of the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006). Plantings at the Sulphur Creek mitigation site shall include arroyo willow (*salix lasiolepis*), California coffeeberry (*rhamnus californica*), toyon (*heteromeles arbutifolia*), and coyote bush (*baccharis pilularis*).

Mitigation for the Cannery Area included:

At the Cannery Area, a 20-foot segment and a 39-foot segment of existing enclosed creek channel will be daylighted and about 60 linear feet of an existing open, concrete-lined segment of the creek will be converted to an earthen channel. The channel in these areas will be widened, and meanders will be designed to improve the hydrologic and hydraulic function of the channel. The bank will be expanded and vegetated with an appropriate assemblage of native riparian species to improve habitat value and to shade the channel to reduce the growth of emergent wetland vegetation, which can reduce flow capacity.

Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 850 linear feet are to be provided as mitigation for the Cannery Area Project, while the remaining portion of this reach will be used as

C2

mitigation for impacts to Line K-1 at the West A Street Realignment Project (Site No. 02-01-C0861). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the offsite mitigation reach of Sulphur Creek at the Hayward Executive Airport. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek mitigation is included as Figure 6 of the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006). Plantings at the Sulphur Creek mitigation site shall include arroyo willow (*salix lasiolepis*), California coffeeberry (*rhamnus californica*), toyon (*heteromeles arbutifolia*), and coyote bush (*baccharis pilularis*).

In addition, because the site layout at the Cannery Area does not provide opportunities for 1:1 in-kind mitigation for the culverting of Sulfur Creek, the Applicant will provide enhancement of downstream water quality in Sulfur Creek by providing Clean Water Act maximum extent practicable (MEP) treatment for stormwater runoff from the new Burbank School and impermeable areas of Cannery Park. Prior to constructing improvements at Cannery Park and prior to constructing the new Burbank School, the Applicant shall submit proposed plans with landscape-based stormwater treatment best management practices (BMPs) to the Executive Officer of the Regional Water Quality Control Board for review and approval. Landscape-based BMPs shall be consistent with the design standards in Provision C.3 of the Alameda County Clean Water Program's National Pollutant Discharge Elimination System (NPDES) Municipal Stormwater Permit (Order R2-2003-0021; NPDES Permit No. CAS0029831).

Based on the limited information provided in the ISMND, it is not clear whether or not any of the channel reconfigurations and riparian vegetation required as riparian mitigation for the prior City of Hayward projects may be impacted by the proposed Project. In addition, constructing a new pedestrian bridge across a mitigation reach of Sulphur Creek may be considered a violation of the conditions of certification for the prior City of Hayward projects.

Finally, Water Board staff have concerns about the status of the required mitigation for the two prior City projects. We do not appear to have been provided with documentation that the Cannery Area day-lighting was implemented. At the airport, the banks of Sulphur Creek were to have been " regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1". Based on monitoring reports submitted to the Water Board, it appears that the City filled in the thalweg of Sulphur Creek to provide gentler bank slopes. That is clearly not what was required by the prior water quality certifications, and Water Board staff are concerned that the fill placed in the channel thalweg is very likely to be washed away by high flow events, which would re-create the overly steepened channel slopes along Sulphur

C2

C3

Creek. Also, we are concerned that flood control maintenance projects may have removed some of the required mitigation vegetation. Water Board staff will need to visit the airport in the near future to determine whether or not the City is in compliance with the prior conditions of certification. The City may be required to expand the top-of-bank area along Sulphur Creek to achieve compliance with the requirement to have the creek banks “regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1”. The proposed Project should leave adequate setbacks from the existing top-of-bank to allow the City to take appropriate actions to return to compliance with the requirements of the prior mitigation projects.

C3

The ISMND should have evaluated the consistency of the proposed Project with the required mitigation projects along Sulphur Creek at the airport. Since the ISMND failed to do this, it should be revised and recirculated before it is adopted by the City Council.

Comment 3, Potential Impacts to a Tributary Channel

The annotated aerial photograph of the proposed Project site appears to indicate that a tributary channel to Sulphur Creek may be present in the footprint of the new Administration Building. If a tributary channel is present, mitigation will be required for placing a building on top of it, and the ISMND should have identified this potential impact and proposed mitigation.

C4

Comment 4, Hydrology and Water Quality,

The proposed Project will be required to comply with the post-construction stormwater treatment requirements of the National Pollutant Discharge Elimination System (NPDES) Municipal Regional Permit (MRP) for the management of stormwater runoff (Order R2-2009-0074; NPDES Permit No. CAS612008). The ISMND does not describe how the proposed Project will comply with the MRP, or if new outfalls to Sulphur Creek will be needed (Note: New outfalls to Sulphur Creek at this location may not be consistent with the mitigation project along this reach of the creek). The ISMND should have demonstrated that sufficient surface area has been set aside at the airport to provide the required treatment measures and discussed the potential need for new outfalls.

C5

Comment 5

As is noted above, the ISMND does not demonstrate that impacts associated with the proposed Project have been correctly identified. The ISMND also lacks mitigation proposals for these potential impacts. Unless these omissions are corrected, the ISMND may not be sufficient to support the issuance of any permits needed for the proposed Project, including the construction of the new pedestrian bridge.

C6

Please contact Brian Wines at (510) 622-5680 or bwines@waterboards.ca.gov if you have any questions. All future correspondence regarding this Project should reference the Site Number indicated at the top of this letter.

Sincerely,

Dale Bowyer
Section Leader
South/East Bay Section, Watershed Division

cc: CDFG, Bay Delta Region, Attn: Marcia Grefsrud (mgrefsrud@dfg.ca.gov)
USACE, Cameron Johnson (Cameron.L.Johnson@usace.army.mil)
State Clearinghouse (state.clearinghouse@opr.ca.gov)

Initial Study/Negative Declaration Comments and Responses

Comments from MR. HOWARD BECKMAN (Comment Letter A)

Comment A1

The Initial Study Checklist prepared by the governor's Office of Planning and Research or OPR (and included as Appendix G to the CEQA Guidelines) is intended to document the conclusion that a proposed project does not appear to have potentially significant environmental impacts that would require an Environmental Impact Report (under CEQA). The checklist prepared by the OPR is a suggested checklist of issues; the introduction to the checklist states that it is a "sample checklist" and that it is "intended to encourage thoughtful assessment of impacts." Moreover, the OPR states in the introduction to its checklist: "Substantial evidence of potential impacts that are not listed on this form must also be considered."

Thus the OPR checklist is not literally a checklist that, if filled out by "checking the boxes," will satisfy CEQA. It is meant to guide agencies in a serious contemplation of the possible adverse impacts of a proposed project.

Response to Comment A1

As a part of responding to comments submitted on the Initial Study/Negative Declaration for the proposed project, additional information has been provided to further describe the various characteristics of the proposed project, including design features that would ensure that the project would not result in any significant environmental impacts, thus precluding the need for mitigation measures. Also, the Airport Administration Building and associated parking lot was part of the Hayward Executive Airport Master Plan Environmental Assessment/Environmental Impact Report (EA/EIR) prepared for the Airport Master Plan, which was certified in 2002. The Master Plan EIR indicated the project site as a location for the construction of a 12,000 square foot public terminal building and parking lot of portions of the proposed project. Hence, the Initial Study/Negative Declaration is not the first time the project has been subject to review pursuant to CEQA.

Comment A2

The proposed new airport administration building, like the existing building, would lie immediately adjacent to a portion of the lowest stretch of Sulphur Creek before the creek traverses the runway area of the airport. Yet this important feature of the project site is nowhere to be found in the project description in the Initial Study, which is no more than one sentence. (Nor is it shown or identified in the site plan prepared by WLC Architects, dated Oct. 20, 2012.) Indeed, in describing past projects that touch on or include streams, whether natural or engineered, the City of Hayward has habitually failed to identify the location of such streams in relation to proposed projects.

Response to Comment A2

The proposed project includes the construction of a new 8,739 square foot administration building complex adjacent to the existing Control Tower building (refer to Figure 1 included as part of this Exhibit). The Airport Administration building complex has been designed to be constructed in two phases. Phase one is approximately 4,920 square feet with 55% of the space devoted to common public areas and 45% to Airport staff offices (Figure 2, Overall Site Plan). Features include a public waiting

room, vending area, weather briefing room, restrooms, office space for the Police Department, and a fifty-seat meeting room (refer to Attachment IV). Phase two includes the future development of 3,782 square feet of commercial, leasable office space; this phase will be built in the future when sufficient demand materializes for the space. Future build-out of Phase 2 was included as part of the proposed project.

The 2010 Airport Layout Plan Update designates this site for construction of the administration building. Vehicle parking is proposed on an existing turf area east of the existing control tower. The site plan includes a new parking lot, a pedestrian bridge that will connect the new building to the runway, an outdoor patio area, trash enclosures, and landscaping (see Attachment IV). As indicated in Figure 1, the proposed project includes a minimum setback of approximately 15 feet from the Sulphur Creek top of bank.

The proposed pedestrian footbridge would be a prefabricated concrete clear span bridge with abutments and foundations placed outside of the bed, bank, and channel of Sulphur Creek (Figures 3 and 11). Specific project design elements are described below. Construction of the proposed pedestrian bridge would have less-than-significant impacts on existing biological resources. The bridge would be positioned to avoid riparian trees recently planted to enhance this reach of the creek channel. The spanning bridge design would also serve to avoid impeding wildlife movement that may occur within the creek corridor including aquatic species movement. Shading from the 8-foot-wide bridge is expected to be minimal on the herbaceous bank vegetation below. The narrow width of the bridge would mean that only small areas at the very top of the bank on either end of the bridge would receive prolonged shading while other areas under the bridge would see limited periods of shading each day.

Drainage and Utilities

As illustrated in the Grading and Drainage Plan (Figure 3) and the Utility Plan (Figure 4), the proposed project includes the installation of a new on-site storm drainage system that captures and treats rainwater and surface runoff through bio-retention structures and the installation of concrete pathways, curbs, and gutters. The project would install new six inch sanitary sewer laterals, new water service per City standards, and a new fire sprinkler system, and conduits for a new security system. The utilities would be underground via a new joint trench that includes gas, cable television, and electricity. Detailed plans are illustrated in Figure 6.

Stormwater Treatment and Erosion Control

The proposed project has been designed to treat stormwater on-site and prevent erosion through design features such as sediment basins and tree protection measures. The erosion control plan (Figures 7 and 8) and stormwater treatment plan (Figure 9) illustrate best management practices (BMPs) that would be used to prevent impacts to water quality. The purpose of the plan is to stabilize the site to prevent erosion of graded areas and to prevent sedimentation from leaving the construction areas and affecting neighboring sites, natural areas, public facilities or any other area that might be affected by sedimentation. All measures shown in the plan should be considered the minimum requirements necessary. Should field conditions dictate additional measures, such measures shall be per California Regional Water Quality Control Boards (RWQCB) field manual for erosion and sedimentation control and the California Storm Water Quality Association BMPs handbook for construction. Further measures are described in Figures 8 and 9.

Comment A3

In addition, the Initial Study refers to Sulphur Creek as a "flood control facility". This language conjures up images of engineered channels with cement walls or underground culverts, when in fact the creek at the point it passes next to the proposed building is a part of a natural segment of Sulphur Creek that emerges from underneath Hesperian Boulevard and courses across the airport toward the bay. This language, too, is characteristic of City of Hayward's predisposition toward the remaining natural streams that are part of the watershed draining the city; in the past city planning documents have referred to streams as "ditches" (viz the West A Street extension project or the Cannery Project). The manner in which some feature of the environment is characterized has much to do with an assessment of impacts on that feature.

Response to Comment A3

Sulphur Creek is a perennial drainage with at least some degree of continuous flow present all year. The creek is part of the flood control facilities around the City and includes its natural creek channel and man-made open channels and boxed culverts. In describing the creek within the Project Area as a flood control facility, Staff was referring to its current form and function; and did not intend to minimize the environmental issues as they particularly relate to sections on the creek in its natural form. As stated in Response to Comment A2, the bridge would be positioned to avoid riparian trees recently planted to enhance this reach of the creek channel. The spanning bridge design would also serve to avoid impeding wildlife movement that may occur within the creek corridor including aquatic species movement.

Shading from the 8-foot-wide bridge is expected to be minimal on the herbaceous bank vegetation below. The narrow width of the bridge would mean that only small areas at the very top of the bank on either end of the bridge would receive prolonged shading while other areas under the bridge would see limited periods of shading each day. The limited amount of shading generated by the new bridge does not represent a significant impact on biological resources.

The proposed project would not impact the bed, bank, or channel of Sulphur Creek. However, the proposed pedestrian bridge would be located within upland habitat associated with the creek. The majority of the upland portions of Sulphur Creek within the Project Area are composed of annual non-natives and assorted weedy species. These upland portions can be thought of as ruderal vegetation, or a disturbed site mainly comprised of non-native weedy species that do not provide suitable habitat for special-status species.

Comment A4

Section IV, Biological Resources. City provides a naked, unsupported assertion: "The projects will not have any adverse effect on biological resources; therefore, no impact." Not only is this a mere assertion, it is a tautology. This identical statement is used to dismiss the issue of impacts on riparian habitat (sec. IV b) or on movement of fish or wildlife (sec. IV c).

Response to Comment A4

As described in Response to Comment A3, portions of Sulphur Creek within the Project Area that have the potential to be impacted by the proposed project are composed of ruderal vegetation. These areas do not support special-status species. Also, the proposed project has been designed to avoid impacts to Sulphur Creek through stormwater retention and erosion control measures, thus avoiding potential water quality issues associated with surface runoff.

Due to the ruderal nature of the site, the abundance of open space adjacent to the Project Area, and the larger home range or territory of local wildlife, few individuals of the various vertebrate species presently utilizing the creek as a corridor would be impacted by the project. Additionally, many carnivores hunt under the cover of darkness, and the proposed project does not include nighttime lighting or glare into the creek that could cause stress to these animals, including limiting their ability to hunt normally. Furthermore, the proposed project's building is set back from the top of bank and all riparian vegetation by a minimum of 15 feet on the south side to a maximum of 32 feet on the north side of the future building as illustrated in Figures 1, 2, and 10.

Comment A5

Section IX, Hydrology and Water Quality. Here, too, City provides only naked assertions. With respect to the issue of water quality and discharge into adjacent Sulphur Creek (para. a), City states: "The project will comply with all water quality and wastewater discharge requirements of the City; therefore, no impact." A promise to uphold standards does not qualify as even a cursory assessment of possible adverse impacts. Moreover, the City's assertion does not make clear whether the City's wastewater discharge requirements as applied to this project constitute mitigation of potentially adverse impacts. Any mitigation must be spelled out at this stage, and City would be required to issue a Mitigated Negative Declaration rather than a simple Negative Declaration.

Response to Comment A5

The existing administrative building site is developed with compacted dirt and gravel. Under current conditions, rain water does not percolate into the ground area but instead drains directly over the top of bank into the Sulphur Creek as sheet flow. The area designated for the proposed parking lot is currently developed as a manicured lawn and walkway. The City is required by law to comply with the Alameda County Flood Control District and Water Conservation District's National Pollution Discharge Elimination System (NPDES) C.3 Stormwater Technical Guidance requirements. The C.3 requirements address the implementation of specific erosion control, sediment control, and waste management BMPs.

Although the conversion of the lawn area to a parking lot would increase the impermeable surface area thus increasing the amount of potential runoff, the proposed bio-retention stormwater facilities (Figures 2, 3, and 4) and the fact that the project is subject to requirements of the County's C.3 stormwater requirements avoids any potentially significant impacts related to water quality and stormwater runoff. The commenter also questions whether the City's wastewater discharge requirements constitute mitigation. The wastewater from the proposed building would be directed through pipelines to the existing sanitary sewer system and eventually to the Wastewater Treatment Plant as indicated on Figures 3, 4, 6, and 10. The project's connection to the City's wastewater system is also regulated by standards. Compliance with these standards is required by law and as such is not considered mitigation. Therefore, based on the project's required adherence to such standards, and the various project design features intended to avoid impacts, the City's use of a Negative Declaration is allowed under CEQA.

Comment A6

With respect to impacts of the project on drainage or surface water runoff (paras. c, d, e), City relies exclusively on the assertion: "The proposed drainage system for the project is designed to accept all off-site drainage that is directed towards the project site; therefore, no significant impact." City does not provide information on whether the proposed drainage system will discharge into Sulphur Creek or how

much impermeable surface will be created by the project and, as a result, how much, if any, stormwater will run into the creek. City's comment at para. f indicates that water from the proposed drainage system will discharge into the creek (identified there as "an existing drainage facility").

Response to Comment A6

Currently, untreated runoff flows directly across the compacted gravel area and into the creek through overland sheet flow. As described above under Response to Comment A5, the proposed project's infrastructure would retain pollutant laden runoff from the increased impermeable surface area before it enters the creek. Thus, the proposed project reduces the amount of pollutants entering the creek and improves water quality from the existing baseline condition.

Comment A7

Section XVIII, Mandatory Findings of Significance. Para. b concerns the question of cumulative impacts of the proposed project. City concludes "no impact" and for support cites its "evidence" in sections IXc, IXf, XIIIa, and XVa of the Initial Study. However, City provides no documentation to support its conclusion of no impacts in those sections. City does not describe or discuss the possible cumulative effects of the proposed project in light of earlier projects that impacted the lower arm of Sulphur Creek (construction of Home Depot on airport property, the Cannery Project, West A St. extension, as well as other, unknown projects).

City cannot avoid the intent and requirement of the Initial Study to document its conclusions of no significant environmental impacts of the new administration building by merely asserting no impacts, thereby shifting the burden of analysis and documentation of substantial evidence to the public.

Response to Comment A7

Staff believes that the Initial Study findings support the conclusion that the proposed project, as described in Responses to Comments A2 – A6, would have no significant environmental impacts. The proposed project would neither encroach nor modify the creek's bed, bank, or channel; nor would the proposed project significantly degrade water quality through the discharge of pollutants into the creek. The proposed project would not degrade the quality of the environment, substantially reduce the habitat of fish or wildlife species, or significantly impact a special-status species. Project components and proposed runoff and water quality improvements are likely to benefit Sulphur Creek's existing condition. As the project would not result in any significant impacts, its contribution to any significant cumulative impacts would not be cumulatively considerable.

Comments from CDFW (Comment Letter B)

Comment B1

The Project description does not provide adequate details, such as the size of the building or construction area or the existing conditions, to allow for CDFW to assess the biological impacts of the Project. Section 15063 (a)(1) of the California Environmental Quality Act (CEQA) Guidelines states "All phases of project planning, implementation, and operation must be considered in the Initial Study of the project." The Initial Study should include a description of the environmental setting and potential environmental impacts with a brief explanation to support findings.

Response to Comment B1

Please refer to Response to Comment A2 and Figures 1-11 for a complete Project Description.

Comment B2

The Initial Study checklist, Biological Resources, IV(a) states "The Project will not have any adverse effect on biological resources, therefore, no impact." The Initial Study fails to disclose the Project location in relation to Sulphur Creek, which flows through the Hayward Executive Airport. CDFW recommends that a set-back buffer be established for some creeks measuring from the top of the stream bank or riparian canopy. No construction, including roads, should be allowed within the buffer area to provide adequate protection of the resources and to minimize the need for future maintenance and bank armoring in the channel. Many negative impacts to creek systems are associated with attempts to stabilize creek banks that are failing beneath poorly located structures. For example, a structure placed too close to the top of bank, or even below the top of bank, may become threatened by natural erosion of the creek bank, as the centerline of the creek meanders within the channel. All wetlands and watercourses, whether intermittent or perennial, should be retained and provided with substantial setbacks to preserve the riparian and aquatic values and maintain their value to on-site and off-site fish and wildlife.

Response to Comment B2

As stated in Responses to Comments A1-A6, the proposed project would not significantly impact the creek system or species that rely on the creek for habitat or movement. Figures 1, 3, and 4 indicate the proposed building and parking lot locations in relation to Sulphur Creek. The proposed project would be set back a minimum of 15 feet from the top of bank and up to 32 feet at some locations. Furthermore, the proposed project has been designed to avoid impacting the bed, bank, or channel of Sulphur Creek through the use of a prefabricated free span bridge with foundations outside of top of bank and within ruderal upland habitat. Detailed conceptual plans of the pedestrian bridge are illustrated in Figure 11. The proposed bridge foundations include 24-inch diameter drilled cast-in-place reinforced concrete piers that are setback from the top of bank, thus avoiding potential undercutting caused by natural erosion of the creek bank.

Comment B3

For any activity that will divert or obstruct the natural flow, or change the bed, channel, or bank (which may include associated riparian resources) of a river or stream, or use material from a streambed, CDFW may require a Lake and Streambed Alteration Agreement (LSAA), pursuant to Section 1600 et seq. of the Fish and Game Code. Issuance of an LSAA is subject to CEQA. The CDFW, as a responsible agency under CEQA, will consider the local jurisdiction's (lead agency) Negative Declaration or Environmental Impact Report for the Project. The CEQA document should fully identify the potential impacts to the stream or riparian resources and provide adequate avoidance, mitigation, monitoring and reporting commitments for completion of the agreement.

Response to Comment B3

The proposed project would not divert or obstruct the natural flow, or change the bed, channel, or bank (which may include associated riparian resources) of Sulphur Creek. As stated in Responses to Comments A1-A6, through the project's design and conditions required by law, the proposed project would not impact biological resources within the Project Area. Therefore, the proposed project would not likely be required to obtain an LSAA and no mitigation measures are necessary. However, the CDFW

makes the final determination of whether an LSAA is required. If the CDFW determines an LSAA is required, then the City will be required to complete any conditions included in the agreement.

Comment B4

The Initial Study, Biological Resources, IV(e) implies that some trees will be removed, but does not disclose how many, their size, species, or location. CDFW recommends that for each native tree that is removed or destroyed, trees shall be replaced with native trees on-site at a minimum 3:1 ratio (replacement: loss). For each non-native tree that is removed or destroyed, trees should be replaced with native trees on-site at a minimum 1:1 ratio (replacement: loss).

Response to Comment B4:

Eleven (11) non-native trees located in the proposed parking lot area are to be removed. Those trees have diameters ranging in size from 3 to 4 inches. Of the trees to be removed, only three (3) are in good health. No trees exist on the site of the proposed building itself. The proposed project would replace the eleven trees removed with 20 new trees: eight (8) Japanese Maples, eight (8) Windmill Palms, four (4) Fruitless Chinese Pistachios, and a large amount of shrubs and groundcovers as illustrated in the Planting Plan (Figure 10).

Comment B5:

Impacts to nesting birds should be avoided by scheduling construction and tree removal activities outside of the nesting season.

Response to Comment B5

The project will fully comply with the terms of the Migratory Bird Treaty Act (MBTA) which provides guidelines to deal with nesting birds if found during construction. Unpermitted “take” of migratory birds or their eggs, which also may result from nest destruction, is prohibited under the MBTA. The regulatory definition of “take” is to pursue, hunt, shoot, wound, kill, trap, capture, or collect; or attempt to pursue, hunt, shoot, wound, kill, trap, capture, or collect. Under the regulations of the MBTA, depredation permits are required for the destruction of any active migratory bird nests. “Active” nests are defined as those with eggs or birds in them. Nests that are in the process of being constructed or nests that have been abandoned after a breeding season are not active nests (they are inactive). The MBTA does not prohibit the destruction of a bird nest alone (an inactive nest), provided that no possession occurs during the destruction. No permit is needed for this activity. Preventing the birds from nesting by knocking down unfinished nests is acceptable to the USFWS, which considers this to be nonlethal harassment.

Continuing to knock down nests that are in the process of being built (prior to occupancy) will eventually persuade the birds to nest elsewhere.

Comment B6:

If mitigation is required for the Project, it should be approved by CDFW and be of sufficient quality and quantity to offset the impacts. CDFW is unable to determine if the Project will not have a significant impact on the environment. CDFW recommends that the Initial Study be recirculated after a thorough Project description and, if necessary, a complete environmental analysis has been completed.

Response to Comment C6: Please refer to Response to Comment A2 and Figures 21-11 for a complete Project Description. As described in Response to Comments B1-B5 above, the proposed project would

not result in potentially significant impacts to biological resources, including water quality and habitat for wildlife. The proposed project would be subject to all Federal, State, and local policies and standards. Therefore, as no new impacts have been identified, the proposed project's Initial Study would not require recirculation.

Comments from WATER BOARD (Comment Letter C):

Comment C1

Timing of CEQA Clearinghouse Compliance and the Proposed Adoption of the ISMND by the City of Hayward City Council. According to the Notice of Completion & Environmental Document Transmittal, which was provided to the Water Board by the State Clearinghouse, the State Clearinghouse Compliance date for the end of the comment period is February 20, 2013. However, the Hayward City Council has proposed to adopt the ISMND at their meeting on February 19, 2013. It is not clear why the City is proposing to adopt the ISMND before the end of the State Clearinghouse comment period.

Response to Comment C1

The timing of the CEQA process on the Hayward City Council agenda was an unintentional oversight. The Notice of Completion & Environmental Document Transmittal for the Project Negative Declaration was submitted to the State Clearinghouse on January 17, 2013. The error was made using this date in calculating the 30-day notice requirement instead of the date the Clearinghouse circulated the document to the agencies. The Hayward City Council's consideration of the Project and the environmental document has been postponed to March 19, 2013.

Comment C2

Biological Resources. The new Administration Building will be constructed adjacent to Sulphur Creek and the proposed new pedestrian bridge will cross Sulphur Creek. The reach of Sulphur Creek in the vicinity of the existing control tower was used to provide riparian mitigation for two previous City of Hayward Projects: the West A Street Realignment Project (Water Board Site No. 02-01- 0861) and the Cannery Area Public Improvement Project (Water Board Site No. 02-01-C0880).

Mitigation for West A Street included:

Onsite mitigation will enhance and improve 89 linear feet of Line K-1. About 58 linear feet of Line K-1 that is currently enclosed in a 65-inch by 40-inch corrugated metal arch culvert will be daylighted. The daylighted segment of the channel, along with an additional 31 linear feet segment of the channel that is currently open, will be enhanced by widening the channel bottom and planting the channel banks with native trees, shrubs, and grasses. Trees planted at the Project site as part of the on-site mitigation shall include box elder (*acer negundo*), California buckeye (*Aesculus californica*), western sycamore (*Platanus racemosa*), and California bay (*Umbellularia californica*). Shrubs and grasses shall include California coffeeberry (*Rhamnus californica*), coyote bush (*Baccharis pilularis*), flowering currant (*Ribes sanguineum*), California lilac (*ceanthos gloriosus*), Berkeley sedge (*Carex tumulicola*), and sky lupine (*Lupinus nanus*). A selection of ornamental tree and shrub species will be planted in a buffer strip between the mitigation area and the street, including: trident maple (*acer buergeranum*), brisbane box (*lophostemon confertus*), crape myrtle (*lagerstroemia natchez*), chanticleer pear (*Pyrus calleryan.*) Japanese barberry (*Berberis thunderbergii*), strawberry tree (*arbutus unedo*), dwarf flax

(phormium), white India hawthorn (*rhapiolepis*), Point Reyes kinnikinnick (*Arctostaphylos uva*), white gazania (*gazania rigens leucolaena*), and dwarf fountain grass (*pennisetum alopecuroides*).

Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 570 linear feet are to be provided as mitigation for the West A Street Project, while the remaining portion of this reach will be used as mitigation for impacts to Sulfur Creek at the Cannery Area Project (Site No. 02-01- C0880). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the mitigation reach of Sulphur Creek. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006). Plantings at the Sulphur Creek mitigation site shall include arroyo willow (*Salix lasiolepis*), California coffeeberry (*Rhamnus californica*), toyon (*Heteromeles arbutifolia*), and coyote bush (*Baccharis pilularis*).

Mitigation for the Cannery Area included:

At the Cannery Area, a 20-foot segment and a 39-foot segment of existing enclosed creek channel will be daylighted and about 60 linear feet of an existing open, concrete-lined segment of the creek will be converted to an earthen channel. The channel in these areas will be widened, and meanders will be designed to improve the hydrologic and hydraulic function of the channel. The bank will be expanded and vegetated with an appropriate assemblage of native riparian species to improve habitat value and to shade the channel to reduce the growth of emergent wetland vegetation, which can reduce flow capacity. Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 850 linear feet are to be provided as mitigation for the Cannery Area Project, while the remaining portion of this reach will be used as mitigation for impacts to Line K-1 at the West A Street Realignment Project (Site No. 02- 01-C0861). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the offsite mitigation reach of Sulphur Creek at the Hayward Executive Airport. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be

graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek mitigation is included as Figure 6 of the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006). Plantings at the Sulphur Creek mitigation site shall include arroyo willow (*salix lasiolepis*), California coffeeberry (*rhamnus californica*), toyon (*heteromeles arbutifolia*), and coyote bush (*baccharis pilularis*).

In addition, because the site layout at the Cannery Area does not provide opportunities for 1:1 in-kind mitigation for the culverting of Sulfur Creek, the Applicant will provide enhancement of downstream water quality in Sulfur Creek by providing Clean Water Act maximum extent practicable (MEP) treatment for stormwater runoff from the new Burbank School and impermeable areas of Cannery Park. Prior to constructing improvements at Cannery Park and prior to constructing the new Burbank School, the Applicant shall submit proposed plans with landscape-based stormwater treatment best management practices (BMPs) to the Executive Officer of the Regional Water Quality Control Board for review and approval. Landscape-based BMPs shall be consistent with the design standards in Provision C.3 of the Alameda County Clean Water Program's National Pollutant Discharge Elimination System (NPDES) Municipal Stormwater Permit (Order R2-2003-0021; NPDES Permit No. CAS0029831).

Based on the limited information provided in the ISMND, it is not clear whether or not any of the channel reconfigurations and riparian vegetation required as riparian mitigation for the prior City of Hayward projects may be impacted by the proposed Project. In addition, constructing a new pedestrian bridge across a mitigation reach of Sulphur Creek may be considered a violation of the conditions of certification for the prior City of Hayward projects.

Response to Comment C2

As described in Responses to Comments A2-A6 and B1-B6, the proposed project would not impact Sulphur Creek. The proposed project would neither modify the bed, bank, or channel of the creek nor significantly alter the vegetation associated with the creek. The commenter states that the City may be in violation of the conditions of certification for the prior City of Hayward projects. The City acknowledges this comment but this comment does not concern potential environmental impacts of the proposed project or the inadequacy of the Initial Study/Negative Declaration, and therefore no response is required.

Comment C3

Finally, Water Board staff have concerns about the status of the required mitigation for the two prior City projects. We do not appear to have been provided with documentation that the Cannery Area day-lighting was implemented. At the airport, the banks of Sulphur Creek were to have been "regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1". Based on monitoring reports submitted to the Water Board, it appears that the City filled in the thalweg of Sulphur Creek to provide gentler bank slopes. That is clearly not what was required by the prior water quality certifications, and Water Board staff are concerned that the fill placed in the channel thalweg is very likely to be washed away by high flow events, which would re-create the overly steepened channel slopes along Sulphur Creek. Also, we are concerned that flood control maintenance projects may have removed some of the required mitigation vegetation. Water Board staff will need to visit the airport in the near future to determine whether or not the City is in compliance with the prior conditions of certification. The

City may be required to expand the top-of-bank area along Sulphur Creek to achieve compliance with the requirement to have the creek banks “regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1”. The proposed Project should leave adequate setbacks from the existing top-of-bank to allow the City to take appropriate actions to return to compliance with the requirements of the prior mitigation projects.

The ISMND should have evaluated the consistency of the proposed Project with the required mitigation projects along Sulphur Creek at the airport. Since the ISMND failed to do this, it should be revised and recirculated before it is adopted by the City Council.

Response to Comment C3

The City acknowledges this comment but this comment does not concern potential environmental impacts of the proposed project or the inadequacy of the Initial Study/Negative Declaration, and therefore no response is required. However, the City adds that a review of the September 10, 2008 Section 401 Water Quality Certification (Site No. 02-01-1027) finds that no restrictions were placed on bridging the restored creek channel. Furthermore, the City would like to have a more in-depth discussion, and provide a more detailed response on the Adjacent Mitigation Projects, which are not a part of the proposed project, in a separate forum. This is a brief response to the key points made by the Water Board:

- The Cannery Area creek day-lighting was completed as planned.
- The banks of Sulphur Creek were pulled back to 2.5:1 per the construction documents and no channel filling occurred except as shown on permitted plans.
- City flood control maintenance has included trimming of some of the willow shrubs planted for mitigation to improve sight lines and channel flow; in addition non-native cattails have been removed from the channel bottom.
- The proposed project's building is set back from the top of bank and all riparian vegetation by a minimum of 15 feet on the south side to a maximum of 32 feet on the north side of the future building as indicated in Figures 1 and 11.

Since the proposed project is not a part of, and will not impact the Adjacent Mitigation Projects, a detailed discussion of these mitigation projects was not included in the Airport Administration Building Initial Study/Negative Declaration. Please refer to Response to Comment A2 and Figures 21-11 for a complete Project Description. As described in Responses to Comments B1-B5 above, the proposed project would not result in potentially significant impacts to biological resources, including water quality and habitat for wildlife. The proposed project would be subject to all Federal, State, and local policies and standards. Therefore, as no new impacts have been identified, the proposed project's Initial Study/Negative Declaration does not require recirculation.

Comment C4:

Potential Impacts to a Tributary Channel. The annotated aerial photograph of the proposed Project site appears to indicate that a tributary channel to Sulphur Creek may be present in the footprint of the new Administration Building. If a tributary channel is present, mitigation will be required for placing a building on top of it, and the ISMND should have identified this potential impact and proposed mitigation.

Response to Comment C4

There is not a tributary to Sulphur Creek present in the footprint of the proposed building or elsewhere on the Project site. The mitigation project that completed in 2009 included a crib wall outfall to receive the small amount of runoff from the existing empty lot (site of new building). The new project will divert this runoff to a bio-retention system and then to the existing storm drain system. (See Figures 1, 3 and 4)

Comment C5

Hydrology and Water Quality. The proposed Project will be required to comply with the post-construction stormwater treatment requirements of the National Pollutant Discharge Elimination System (NPDES) Municipal Regional Permit (MRP) for the management of stormwater runoff (Order R2-2009-0074; NPDES Permit No. CAS612008). The ISMND does not describe how the proposed Project will comply with the MRP, or if new outfalls to Sulphur Creek will be needed (Note: New outfalls to Sulphur Creek at this location may not be consistent with the mitigation project along this reach of the creek). The ISMND should have demonstrated that sufficient surface area has been set aside at the airport to provide the required treatment measures and discussed the potential need for new outfalls.

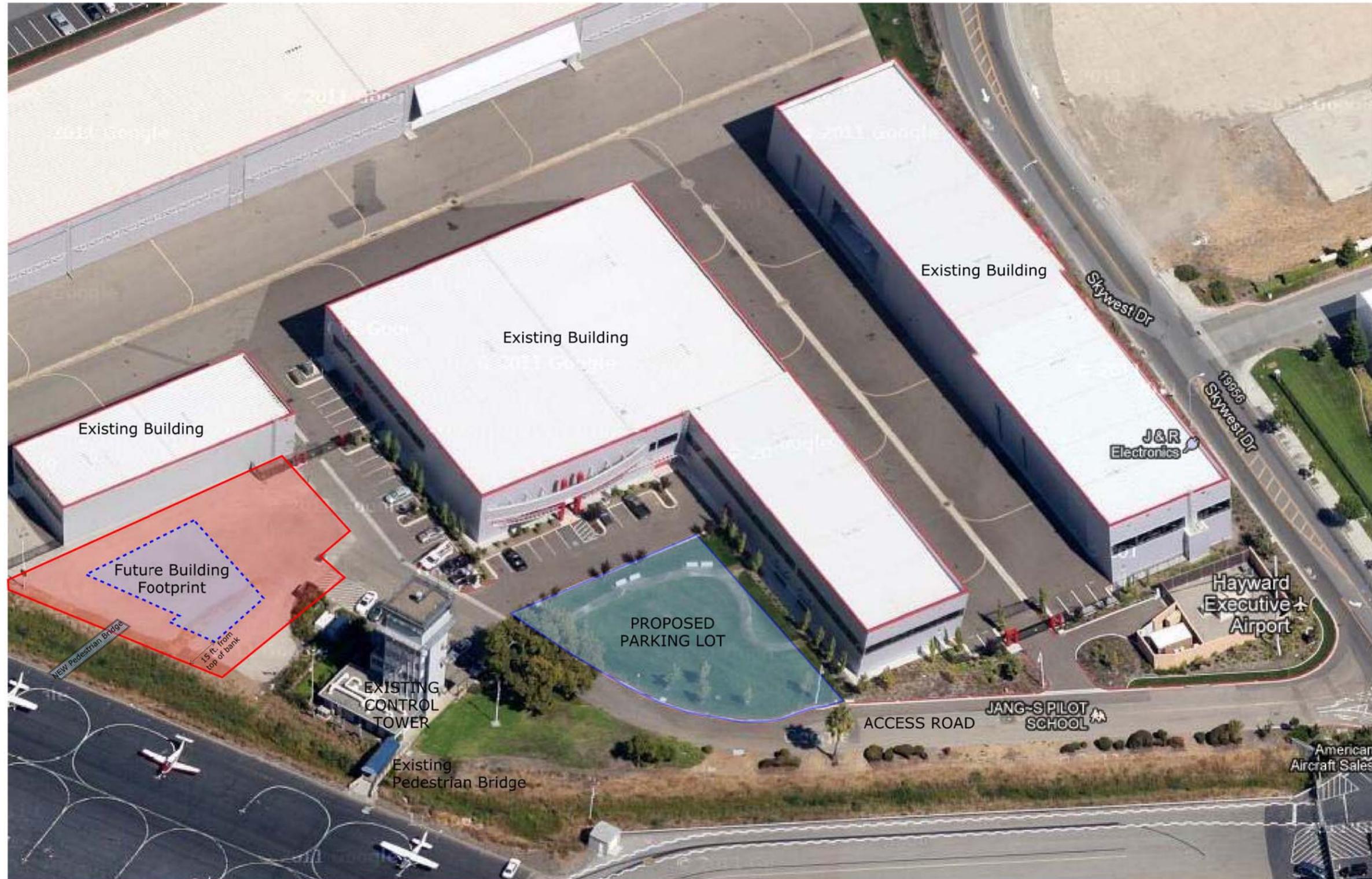
Response to Comment C5

The proposed drainage design would comply with the Alameda County Flood Control and Water Conservation District's C.3 stormwater requirements. As described in Response to Comment A5, stormwater currently sheet flows overland and into the creek. The proposed stormwater system would not drain directly into the creek because the proposed landscaped areas between the proposed building and the creek would be graded such that surface runoff would be directed toward the bio-retention facilities. After collection of stormwater runoff and treatment in these facilities, the treated water would be directed to the existing storm drain system that eventually drains to the creek. The need for a new or reconstructed outfall is not anticipated. Project details are illustrated in Figures 3, 4, 6, 7, 8, and 9. Please also refer to Response to Comment A5 for a detailed description of the project's storm drainage system and associated Best Management Practices.

Comment C6

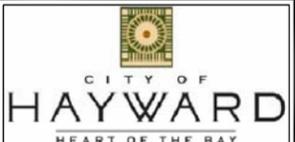
As is noted above, the ISMND does not demonstrate that impacts associated with the proposed Project have been correctly identified. The ISMND also lacks mitigation proposals for these potential impacts. Unless these omissions are corrected, the ISMND may not be sufficient to support the issuance of any permits needed for the proposed Project, including the construction of the new pedestrian bridge.

Response to Comment C6: Staff believes that the proposed project will have no significant environmental impacts and that the additional information provided in the Responses to Comments above further substantiates that the project would not result in significant impacts. Construction on an existing hard-packed gravel site, and removal of 11 non-native landscape trees located outside of the creek's riparian area were not seen as significant impacts. Features that will be included in the project were not included as mitigation proposals, they are permanent project features that the City normally requires from developers to comply with the Alameda County Flood Control and Water Conservation District's C.3 stormwater requirements. For example, the bio-retention areas were designed to have a surface ponding area to allow for evapotranspiration and to filter water through 18 inches of engineered bio-treatment soil. The bio-retention facilities were included as part of the project design and not as mitigation for an environmental impact.



HAYWARD EXECUTIVE AIRPORT NEW ADMINISTRATION BUILDING
PROPOSED BUILDING AND PARKING LOT LOCATION

RANCHO CUCAMONGA - FOLSOM - EMERYVILLE
 NORTHERN CALIFORNIA
 1110 Iron Point Road, Suite 200
 Folsom, California 95630-8301
 tel: 916-355-9922
 fax: 916-355-9950



DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

DESIGNED BY:	CHECKED BY:
DRAWN BY:	APPROVAL REC'D:
APPROVED BY:	
ASST. CITY ENGINEER	DIRECTOR PUBLIC WORKS
PROJ. NO. 6815	FILE NO. E-2021

CITY OF HAYWARD
EXECUTIVE AIRPORT ADMIN. BUILDING
 100% CONSTRUCTION DRAWINGS



CONSULTANT

2/13/13	JULY	BLDG. DEPT COMMENTS
NO	DATE	BY DESCRIPTION
REVISIONS		

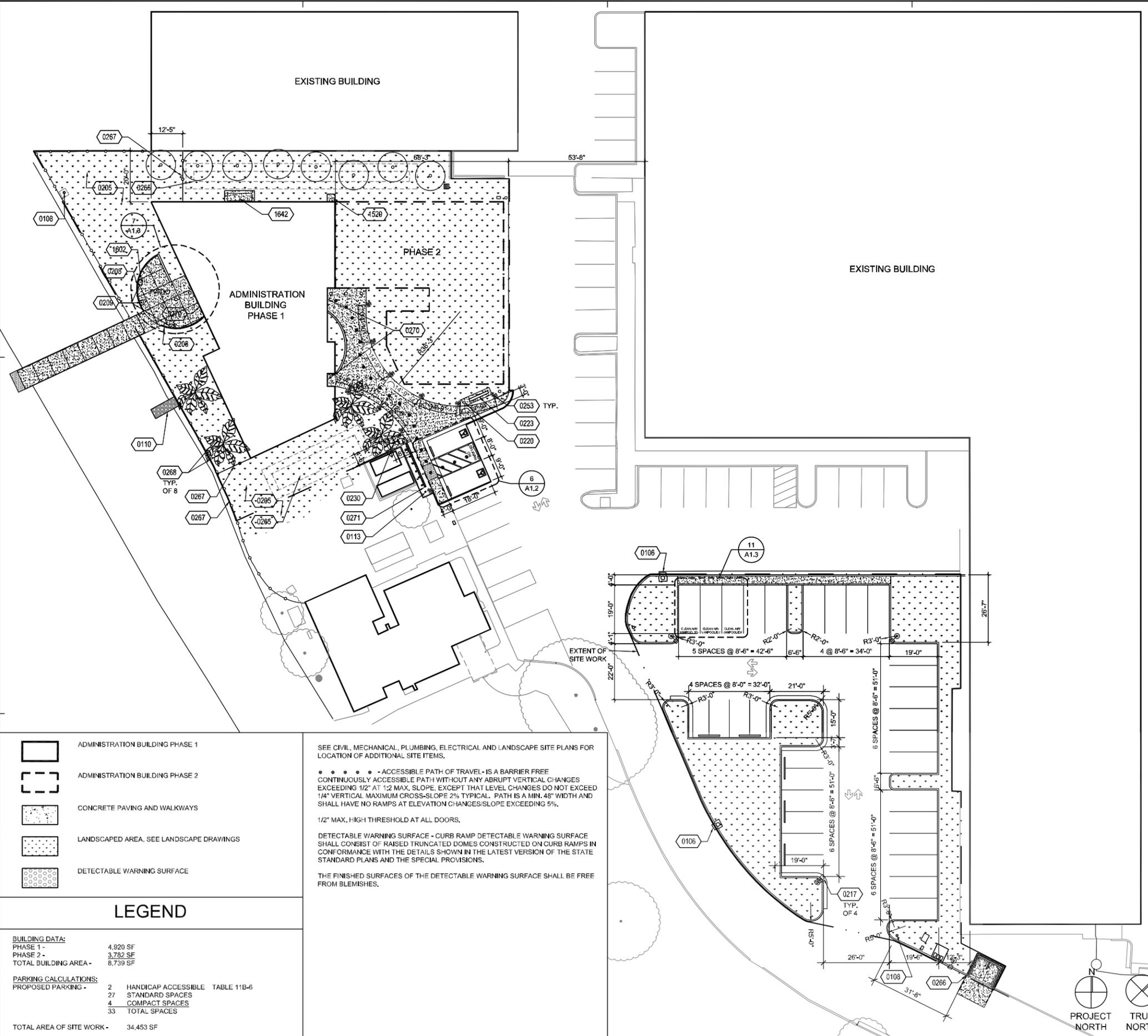
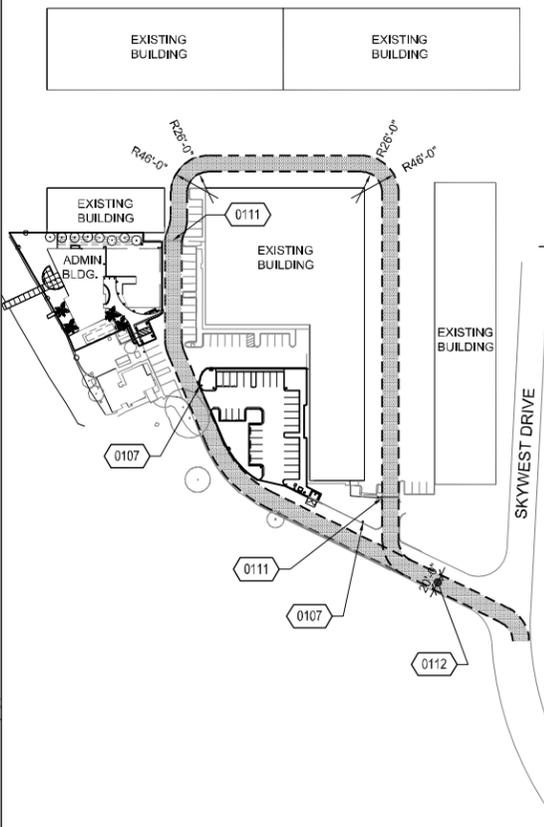
DRAWN: JLY/AD	CHECKED: BL
DATE: 01/11/2013	SCALE: AS SHOWN
PROJECT NUMBER: 1118000	

OVERALL SITE PLAN

25 of 34
 DRAWING NUMBER: **A1.1**

- 0106 EXISTING MANHOLE & CATCH BASIN TO REMAIN
- 0107 EXISTING FIRE HYDRANT TO REMAIN
- 0108 EXISTING LIGHT POLE TO REMAIN
- 0109 EXISTING WATER SYSTEM APPURTENANCE TO REMAIN. SEE CIVIL DWGS
- 0110 EXISTING RIPRAP TO REMAIN, SEE CIVIL DWGS
- 0111 EXISTING GATE WITH KEY SWITCH FOR FIRE DEPT ACCESS
- 0112 20'-0" WIDE FIRE ACCESS ROUTE
- 0113 EXISTING BOLLARDS TO REMAIN
- 0205 LANDSCAPING
- 0208 36" HIGH BANISTER, SEE 6/A1.3
- 0209 METAL PEDESTAL SIGN, SEE 4/A1.3
- 0217 LIGHT POLE, SEE 12/E3.1
- 0220 FLAG POLE, SEE 15/A1.2
- 0223 MONUMENT SIGN, SEE 1/A1.2
- 0230 BICYCLE RACK SET IN CONCRETE PAVING, SEE 10/A1.3
- 0253 LIGHT BOLLARD, SEE 11/E3.1
- 0265 BIORETENTION PLANTER, SEE CIVIL DWGS.
- 0266 TRASH ENCLOSURE, METAL PANEL FACADE TO MATCH ADJ. HANGAR, SEE 10/A1.2
- 0267 WROUGHT IRON FENCING TO MATCH EXISTING FENCING, SEE 5/A1.3
- 0268 UPLIGHT FOR PALM TREES, SEE 10/E3.1
- 0270 "NO SMOKING" SIGN, SEE 14/A10.4
- 0271 GENERATOR SCREEN
- 1520 FIRE SPRINKLER RISER, SEE PLUMBING DRAWING
- 1602 VHF TOWER RADIO SPEAKER
- 1642 MAIN SWITCHBOARD AND PAD, SEE ELECTRICAL DRAWINGS

REFERENCE NOTES



LEGEND

	ADMINISTRATION BUILDING PHASE 1
	ADMINISTRATION BUILDING PHASE 2
	CONCRETE PAVING AND WALKWAYS
	LANDSCAPED AREA, SEE LANDSCAPE DRAWINGS
	DETECTABLE WARNING SURFACE

LEGEND

BUILDING DATA:
 PHASE 1 - 4,920 SF
 PHASE 2 - 3,782 SF
 TOTAL BUILDING AREA - 8,739 SF

PARKING CALCULATIONS:
 PROPOSED PARKING - 2 HANDICAP ACCESSIBLE TABLE 11B-6
 27 STANDARD SPACES
 4 COMPACT SPACES
 33 TOTAL SPACES

TOTAL AREA OF SITE WORK - 34,453 SF

SEE CIVIL, MECHANICAL, PLUMBING, ELECTRICAL AND LANDSCAPE SITE PLANS FOR LOCATION OF ADDITIONAL SITE ITEMS.

• • • • • ACCESSIBLE PATH OF TRAVEL - IS A BARRIER FREE CONTINUOUSLY ACCESSIBLE PATH WITHOUT ANY ABRUPT VERTICAL CHANGES EXCEEDING 1/2" AT 1:2 MAX. SLOPE, EXCEPT THAT LEVEL CHANGES DO NOT EXCEED 1/4" VERTICAL MAXIMUM CROSS-SLOPE 2% TYPICAL. PATH IS A MIN. 48" WIDTH AND SHALL HAVE NO RAMPS AT ELEVATION CHANGES/SLOPE EXCEEDING 5%.

1/2" MAX. HIGH THRESHOLD AT ALL DOORS.

DETECTABLE WARNING SURFACE - CURB RAMP DETECTABLE WARNING SURFACE SHALL CONSIST OF RAISED TRUNCATED DOMES CONSTRUCTED ON CURB RAMPS IN CONFORMANCE WITH THE DETAILS SHOWN IN THE LATEST VERSION OF THE STATE STANDARD PLANS AND THE SPECIAL PROVISIONS.

THE FINISHED SURFACES OF THE DETECTABLE WARNING SURFACE SHALL BE FREE FROM BLEMISHES.

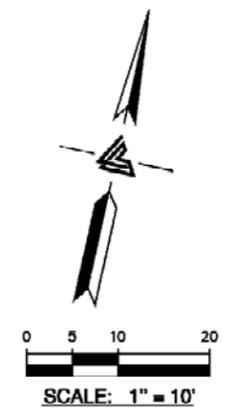
GENERAL INFORMATION

GENERAL NOTES

98 **SITE PLAN** 1" = 20'-0" 1

FIRE TRUCK ACCESS ROUTE

RANCHO CUCAMONGA - FOLSOM - EMERYVILLE
 NORTHERN CALIFORNIA
 1110 Iron Point Road, Suite 200
 Folsom, California 95630-8301
 tel: 916-355-9922
 fax: 916-355-9950



SEE SHEET C-1.1,
 C-6.1 AND C-6.2 FOR
 NOTES AND LEGENDS.

CITY OF
HAYWARD
 HEART OF THE BAY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

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PROJ. NO. 8816	FILE NO. E-2021

CITY OF HAYWARD
EXECUTIVE AIRPORT ADMIN. BUILDING
 100% CONSTRUCTION DRAWINGS



CONSULTANT (LB # 2120114)
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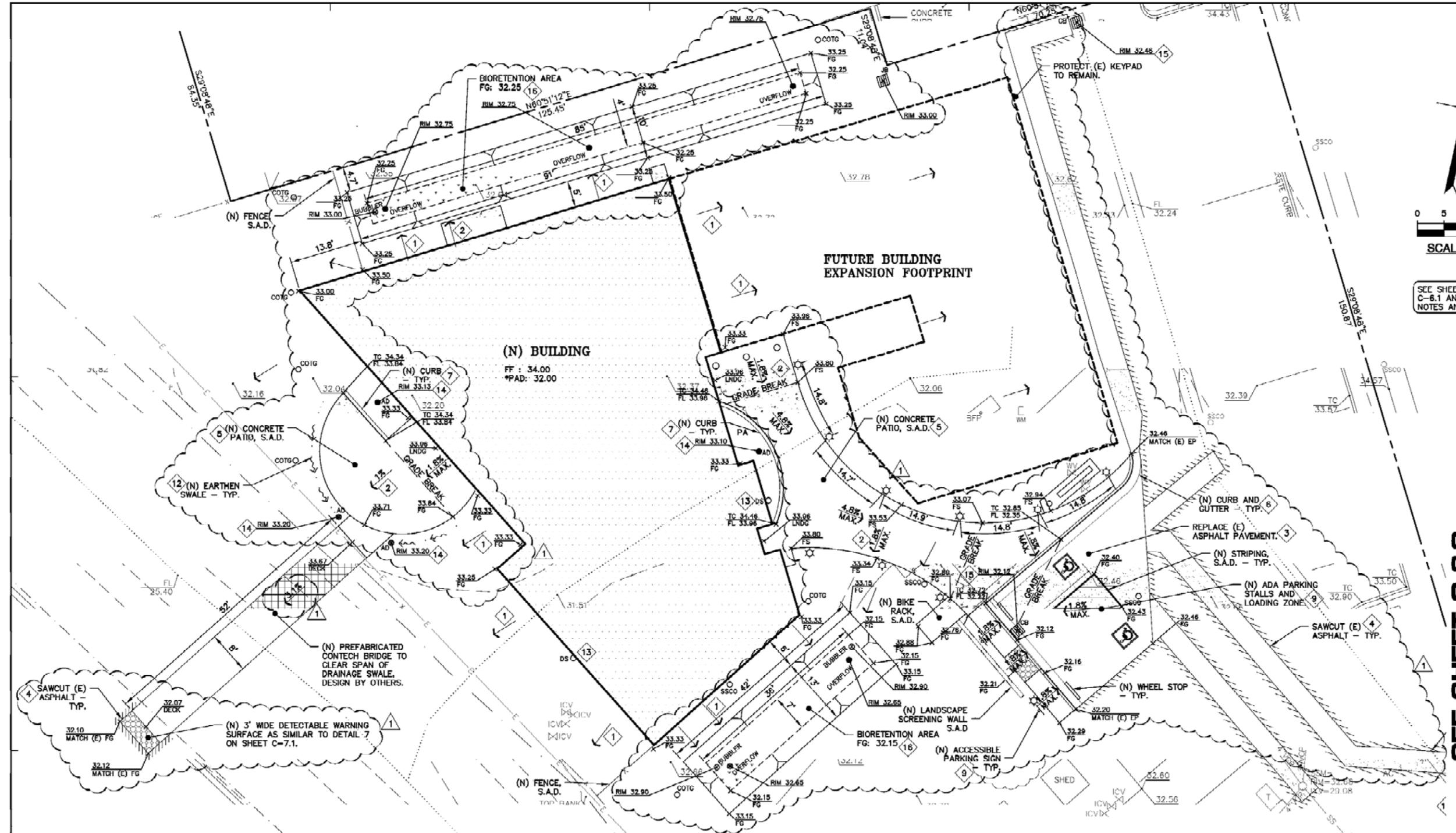
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 DATE: 01/11/2013 SCALE: 1"=10'
 PROJECT NUMBER: 1118000

GRADING AND DRAINAGE PLAN

DRAWING NUMBER: **C-3.1**
 6 OF 30



SEE SHEET C-3.2

FLATWORK

- 1 FINISHED GRADES AT BUILDING PERIMETER SHALL BE SLOPED AT A MINIMUM OF 5% FOR THE FIRST 10' AWAY FROM THE BUILDING PER CBC 1804.3 OR TO AN APPROVED DRAINAGE SWALE OR STRUCTURE. GRADES SHALL CONTINUE TO SLOPE TOWARDS POSITIVE DRAINAGE AND A POSITIVE OUTFALL. MAINTAIN 8" CLEARANCE BETWEEN FINISH EARTHEN GRADE AND BOTTOM OF MUD SILL AT ALL TIMES PER CBC 2304.11.2 UNLESS STRUCTURAL DETAILING ALLOWS LESS. REFER TO STRUCTURAL PLANS FOR FOUNDATION DESIGN AND DETAILS.
- 2 PROVIDE 2% (1% MIN.) SLOPE ACROSS FLAT WORK AND/OR PAVING PER CBC 2304.11.2. SLOPE TOWARDS POSITIVE DRAINAGE AS SHOWN ON PLAN.
- 3 (N) AC PAVING. SEE PAVEMENT PLAN, SHEETS C-5.1 & C-5.2.
- 4 GRIND AC TO TIE (N) AC INTO (E) AC PAVING. APPLY SS-1H BINDER TO (E) AC BEFORE PLACING (N) AC
- 5 (N) CONCRETE PATIOS/WALKWAYS. SEE PAVEMENT PLAN, SHEETS C-5.1 & C-5.2.
- 6 (N) CONCRETE CURB & GUTTER. SEE DETAIL 2 ON SHEET C-7.1.
- 7 (N) CONCRETE CURB. SEE DETAIL 2 ON SHEET C-7.1.
- 8 (N) CONCRETE VALLEY GUTTER. SEE DETAIL 5 ON SHEET C-7.1. (NOT USED ON THIS SHEET)
- 9 (N) ADA ACCESSIBLE PARKING & LOADING ZONE. SEE DETAIL 1 ON SHEET C-7.1

STORM DRAIN

- 10 INSTALL (N) ON-SITE STORM DRAIN SYSTEM. USE MINIMUM 6" PVC (SDR 35) OR HDPE (ADS N-12 W/ SMOOTH INTERIOR WALLS). MAINTAIN 24" MINIMUM COVER AND SLOPED AT 1% AT ALL TIMES UNLESS OTHERWISE NOTED. PROVIDE CLEAN OUT TO GRADE AT MAJOR CHANGES IN DIRECTION. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS AND WYE CONNECTIONS. (NOT USED ON THIS SHEET)
- 11 INSTALL (N) SUBDRAIN. USE PERFORATED 4" PVC (SDR-35) WITH HOLE DOWN AND SLOPED AT 1% MINIMUM SURROUND WITH 3/4" DRAIN ROCK. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS AND WYE CONNECTIONS. PROVIDE CLEANOUT TO GRADE AT MAJOR CHANGES IN DIRECTION AND AT 100' MAXIMUM INTERVALS. SUBDRAIN SHALL REMAIN A DEDICATED SEPARATE SYSTEM UNTIL IT CONNECTS TO STORM DRAIN SYSTEM OR OUTFALL AS SHOWN. SEE DETAILS 1 & 2 ON SHEET C-7.2. (NOT USED ON THIS SHEET)
- 12 CONSTRUCT (N) EARTHEN SWALE SLOPED AT 1% MINIMUM TOWARDS POSITIVE OUTFALL. SEE DETAIL 6 ON SHEET C-7.1.
- 13 CONNECT RAIN WATER DOWNSPOUTS TO 6" PVC (SDR-35) TIGHTLINE, SLOPED AT 1% MINIMUM. DIRECT TO NEAREST STORM DRAIN LINE. PROVIDE CLEAN OUT TO GRADE AT MAJOR CHANGES IN DIRECTION. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS & WYE. TIGHTLINE MAY BE PLACED IN COMMON TRENCH WITH SUBDRAIN LINES, HOWEVER, NOT CONNECT TO SUBDRAIN LINES. CONNECT TO NEAREST STORM DRAIN LINE AS SHOWN ON PLAN.

- 14 INSTALL (N) "CHRISTY V-1" AREA DRAINS. CONNECT TO ON-SITE STORM DRAIN SYSTEM. SEE DETAIL 4 ON SHEET C-7.1.
- 15 INSTALL (N) "CHRISTY U-21" CATCH BASIN W/ CONCRETE BOTTOM FLUSH W/ LOWEST OUTGOING INVERT. PLACE BOX ON 6" CLASS 2 AGGREGATE BASE MATERIAL.
- 16 (N) BIO-RETENTION STORM WATER TREATMENT FACILITY. SEE DETAILS 1 ON SHEET C-7.2. SEE LANDSCAPE PLAN FOR PLANTING.

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HAYWARD	
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APPROVED BY:	
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CITY OF HAYWARD
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UTILITY PLAN

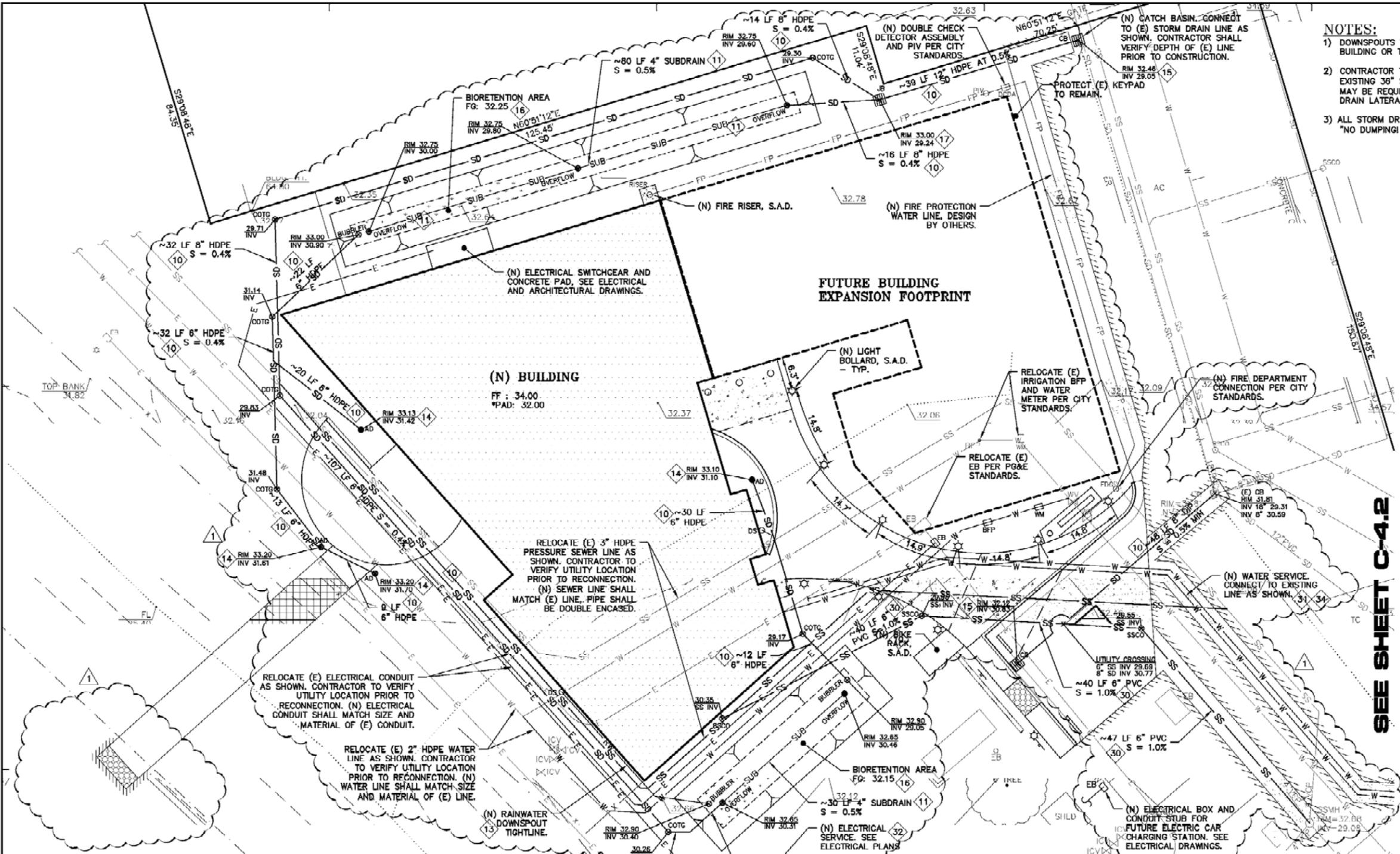
DRAWING NUMBER OF 30 **C-4.1**

- NOTES:**
- 1) DOWNSPOUTS TO DAYLIGHT ON PAVEMENT NEXT TO BUILDING OR THRU CURB FACE.
 - 2) CONTRACTOR TO TAKE CAUTION WHEN WORKING BY EXISTING 36" STORM DRAIN LATERAL HAND DIGGING MAY BE REQUIRED WHEN WORKING NEAR 36" STORM DRAIN LATERAL.
 - 3) ALL STORM DRAIN INLETS TO BE MARKED WITH "NO DUMPING" DRAINS TO BAY"



SEE SHEET C-1.1, C-6.1 AND C-6.2 FOR NOTES AND LEGENDS.

SEE SHEET C-4.2



- FLATWORK**
- 1 FINISH GRADES AT BUILDING PERIMETER SHALL BE SLOPED AT A MINIMUM OF 5% FOR THE FIRST 10' AWAY FROM THE BUILDING PER CBC 1804.3 OR TO AN APPROVED DRAINAGE SWALE OR STRUCTURE. GRADES SHALL CONTINUE TO SLOPE TOWARDS POSITIVE DRAINAGE AND A POSITIVE OUTFALL. MAINTAIN 8" CLEARANCE BETWEEN FINISH EARTHEN GRADE AND BOTTOM OF MUD SILL AT ALL TIMES PER CBC 2304.11.2 UNLESS STRUCTURAL DETAILING ALLOWS LESS. REFER TO STRUCTURAL PLANS FOR FOUNDATION DESIGN AND DETAILS. (NOT USED ON THIS SHEET)
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 - 3 (N) AC PAVING. SEE PAVEMENT PLAN, SHEETS C-5.1 & C-5.2. (NOT USED ON THIS SHEET)
 - 4 GRIND AC TO TIE (N) AC INTO (E) AC PAVING. APPLY SS-1 H BINDER TO AC BEFORE PLACING (N) AC (NOT USED ON THIS SHEET)
 - 5 (N) CONCRETE PATIOS/WALKWAYS. SEE PAVEMENT PLAN, SHEETS C-5.1 & C-5.2. (NOT USED ON THIS SHEET)
 - 6 (N) CONCRETE CURB & GUTTER. SEE DETAIL 2 ON SHEET C-7.1. (NOT USED ON THIS SHEET)
 - 7 (N) CONCRETE CURB. SEE DETAIL 2 ON SHEET C-7.1. (NOT USED ON THIS SHEET)

- STORM DRAIN**
- 8 (N) CONCRETE VALLEY GUTTER. SEE DETAIL 5 ON SHEET C-7.1. (NOT USED ON THIS SHEET)
 - 9 (N) ADA ACCESSIBLE PARKING & LOADING ZONE. SEE DETAIL 1 ON SHEET C-7.1 (NOT USED ON THIS SHEET)
 - 10 INSTALL (N) ON-SITE STORM DRAIN SYSTEM. USE MINIMUM 6" PVC (SDR 35) OR HDPE (ADS N-12 W/ SMOOTH INTERIOR WALLS). MAINTAIN 24" MINIMUM COVER AND SLOPED AT 1% AT ALL TIMES UNLESS OTHERWISE NOTED. PROVIDE CLEAN OUT TO GRADE AT MAJOR CHANGES IN DIRECTION. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS AND WYE CONNECTIONS.
 - 11 INSTALL (N) SUBDRAIN. USE PERFORATED 6" PVC (SDR-35) WITH HOLES DOWN AND SLOPED AT 1% MINIMUM SURROUND WITH 3/4" DRAIN ROCK. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS AND WYE CONNECTIONS. PROVIDE CLEANOUT TO GRADE AT MAJOR CHANGES IN DIRECTION AND AT 100' MAXIMUM INTERVALS. SUBDRAIN SHALL REMAIN A DEDICATED SEPARATE SYSTEM UNTIL IT CONNECTS TO STORM DRAIN SYSTEM OR OUTFALL AS SHOWN. SEE DETAILS 1 & 2 ON SHEET C-7.2.

- UTILITIES**
- 12 CONSTRUCT (N) EARTHEN SWALE SLOPED AT 1% MINIMUM TOWARDS POSITIVE OUTFALL. SEE DETAIL 6 ON SHEET C-7.1. (NOT USED ON THIS SHEET)
 - 13 CONNECT RAIN WATER DOWNSPOUTS TO 6" PVC (SDR-35) TIGHTLINE, SLOPED AT 1% MINIMUM, DIRECT TO NEAREST STORM DRAIN LINE. PROVIDE CLEAN OUT TO GRADE AT MAJOR CHANGES IN DIRECTION. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS & WYE. TIGHTLINE MAY BE PLACED IN COMMON TRENCH WITH SUBDRAIN LINES. HOWEVER, NOT CONNECT TO SUBDRAIN LINES. CONNECT TO NEAREST STORM DRAIN LINE AS SHOWN ON PLAN.
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 - 16 (N) BIO-RETENTION STORM WATER TREATMENT FACILITY. SEE DETAILS 1 & 2 ON SHEET C-7.2. SEE LANDSCAPE PLAN FOR PLANTING.
 - 17 INSTALL (N) "CHRISTY V-24" JUNCTION BOX W/ SILT BASIN. CONNECT TO ON-SITE STORM DRAIN SYSTEM. SEE DETAIL 2 ON SHEET C-7.2.

- 30 INSTALL (N) SANITARY SEWER LATERALS. USE 6" PVC (SDR-18) SLOPED AT 1% MINIMUM. CONNECT TO (E) SEWER MAIN AS SHOWN. PROVIDE CLEANOUT TO GRADE AT BUILDING AND BEHIND PROPERTY LINE AND AT MAJOR CHANGES IN DIRECTION AS SHOWN. REUSE (E) LATERAL IF POSSIBLE. CONNECT PER CITY STANDARDS.
- 31 CONNECT (N) WATER SERVICE AND METER PER CITY STANDARDS. INSTALL (N) 2" MINIMUM SERVICE LINE TO (N) BUILDING OR AS DIRECTED BY FIRE SPRINKLER DESIGNER. WATER METER CAN ONLY BE INSTALLED BY THE HAYWARD WATER DEPARTMENT.
- 32 INSTALL (N) JOINT TRENCH FOR SERVICES INCLUDING GAS, CATV & ELECTRIC FROM NEAREST POINT OF CONNECTION. SEE DRAWINGS BY MECHANICAL & ELECTRICAL CONSULTANTS.
- 33 INSTALL AREA DRAIN FOR COVERED TRASH ENCLOSURE. SEE DETAIL 9 ON SHEET C-7.1. (NOT USED ON THIS SHEET)
- 34 INSTALL A REDUCED PRESSURE BACKFLOW PREVENTION ASSEMBLY ON EACH DOMESTIC AND IRRIGATION WATER METERS PER CITY STANDARD SD-202.

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HAYWARD

DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

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CITY OF HAYWARD
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 WWW.LEABRAZE.COM

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		DESCRIPTION
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HORIZONTAL CONTROL AND PAVEMENT PLAN

DRAWING NUMBER: **C-5.1**
 28 of 34

PAVEMENT LEGEND

PAVEMENT SECTION TO BE APPROVED BY GEOTECHNICAL ENGINEER.

CONCRETE PAVEMENT SURFACES TO HAVE 28 DAY COMPRESSIVE STRENGTH OF AT LEAST 3,700 PSI.
 CONCRETE PAVING IN AUTO/TRUCK AREA TO MEET ADTT (AVERAGE DAILY TRUCK TRAFFIC) OF 1.

EXISTING AC PAVEMENT TO BE GRINDED AND REUSED FOR FILL ON THE SITE.

PROPOSED DESCRIPTION

AC PAVING

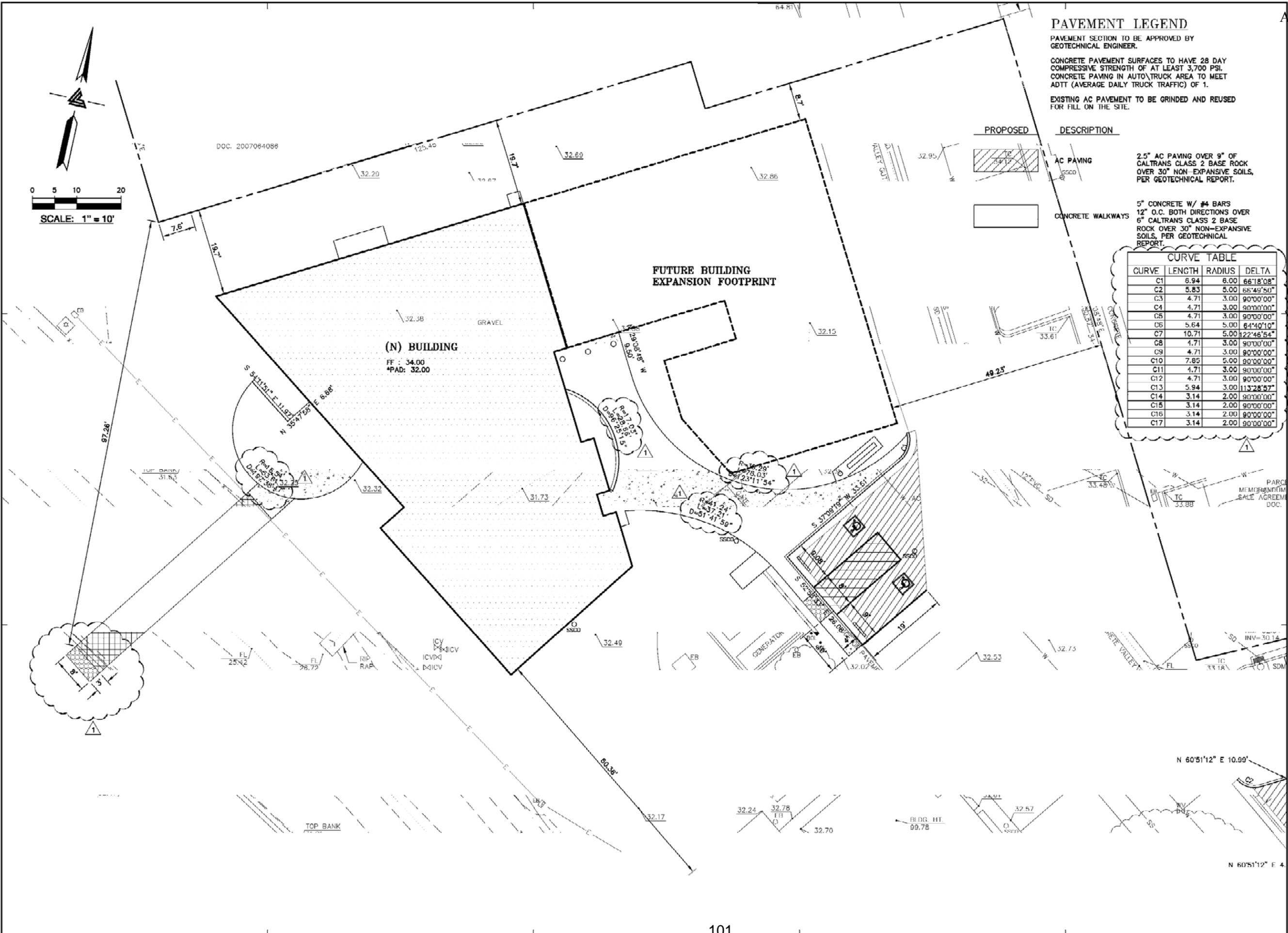
2.5" AC PAVING OVER 9" OF CALTRANS CLASS 2 BASE ROCK OVER 30" NON-EXPANSIVE SOILS, PER GEOTECHNICAL REPORT.

CONCRETE WALKWAYS

5" CONCRETE W/ #4 BARS 12" O.C. BOTH DIRECTIONS OVER 6" CALTRANS CLASS 2 BASE ROCK OVER 30" NON-EXPANSIVE SOILS, PER GEOTECHNICAL REPORT.

CURVE TABLE

CURVE	LENGTH	RADIUS	DELTA
C1	6.94	6.00	66°18'08"
C2	5.83	5.00	66°49'50"
C3	4.71	3.00	90°00'00"
C4	4.71	3.00	90°00'00"
C5	4.71	3.00	90°00'00"
C6	5.64	5.00	64°40'10"
C7	10.71	5.00	122°46'54"
C8	4.71	3.00	90°00'00"
C9	4.71	3.00	90°00'00"
C10	7.85	5.00	90°00'00"
C11	4.71	3.00	90°00'00"
C12	4.71	3.00	90°00'00"
C13	5.94	3.00	113°28'57"
C14	3.14	2.00	90°00'00"
C15	3.14	2.00	90°00'00"
C16	3.14	2.00	90°00'00"
C17	3.14	2.00	90°00'00"



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CITY OF HAYWARD
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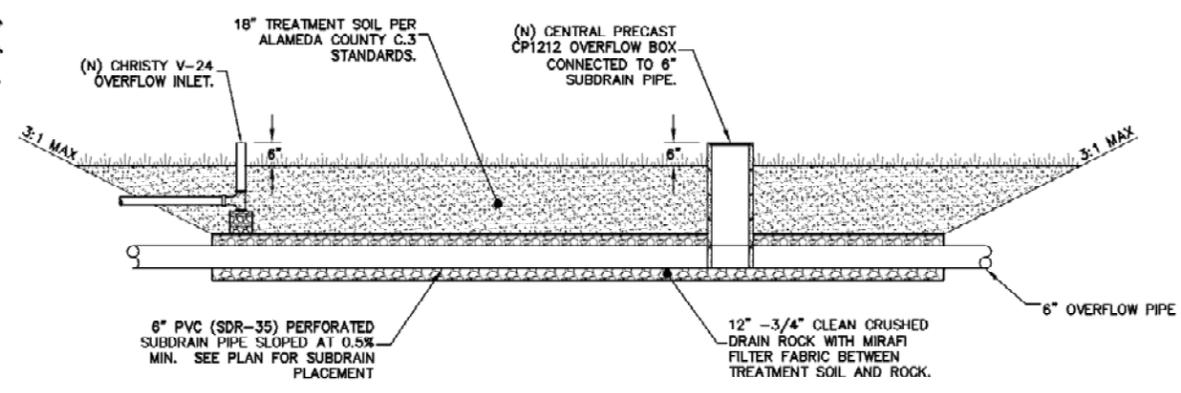
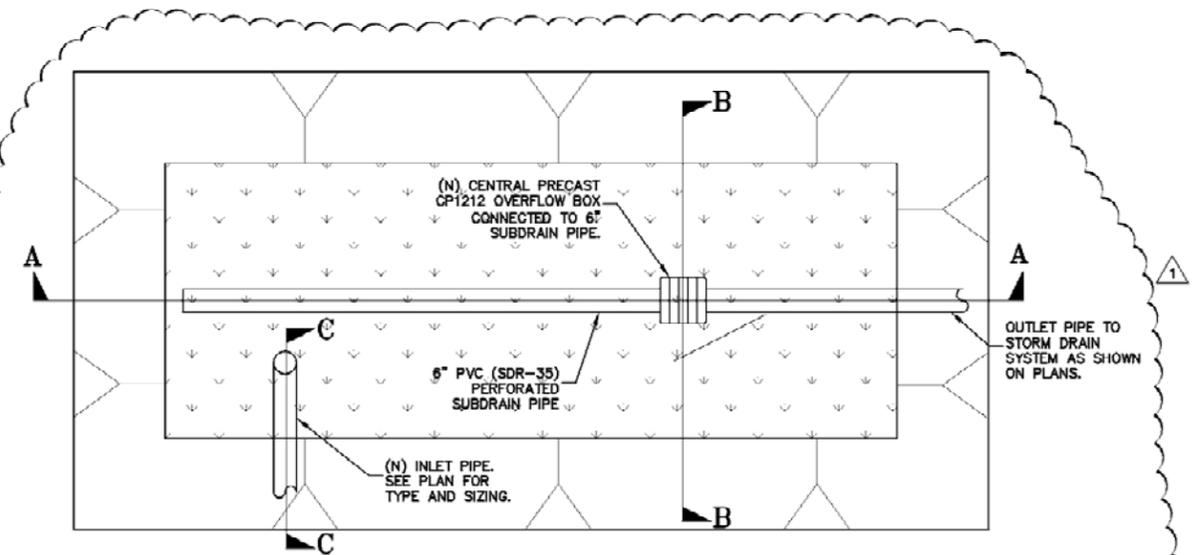
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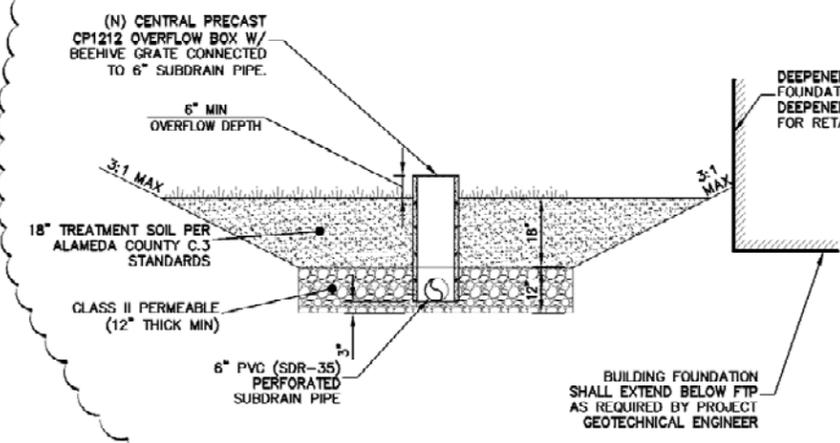
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DETAILS

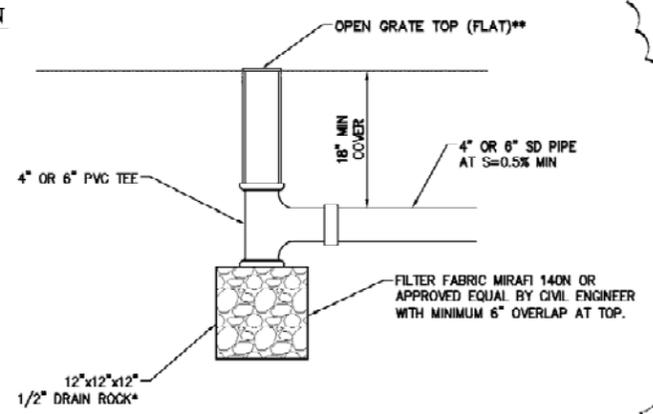
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 29 of 34



A SECTION
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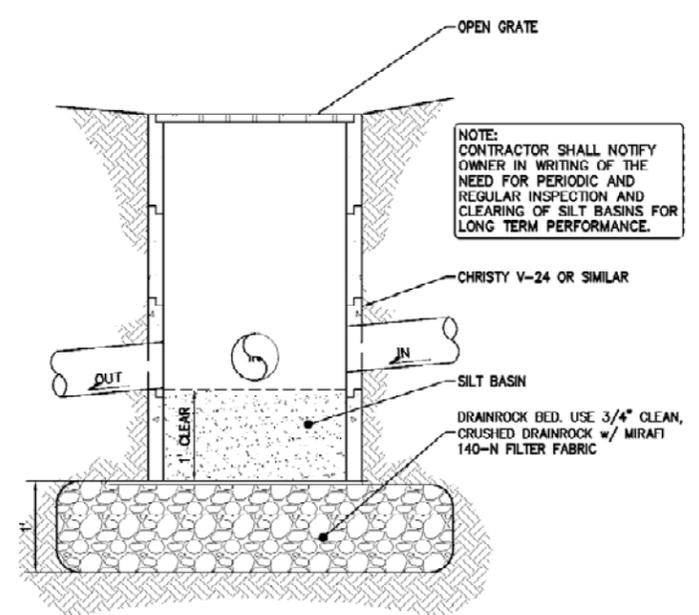


B SECTION
 NTS



C SECTION
 NTS

1 BIO-RETENTION TREATMENT AREA
 C-7.2 NTS



2 JUNCTION BOX
 C-7.2 NTS

*USE CLASS II PERM IF USED TO DRAIN INTO TREATMENT AREAS. NO FABRIC WILL BE NEEDED IN THAT CASE.
 **USE FLAT TOPS FOR LAWN AREAS & DOMED TOPS WHERE MOWING OR TRIPPING IS NOT A CONCERN.

NOTE: CONTRACTOR SHALL NOTIFY OWNER IN WRITING OF THE NEED FOR PERIODIC AND REGULAR INSPECTION AND CLEARING OF SILT BASINS FOR LONG TERM PERFORMANCE.

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DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

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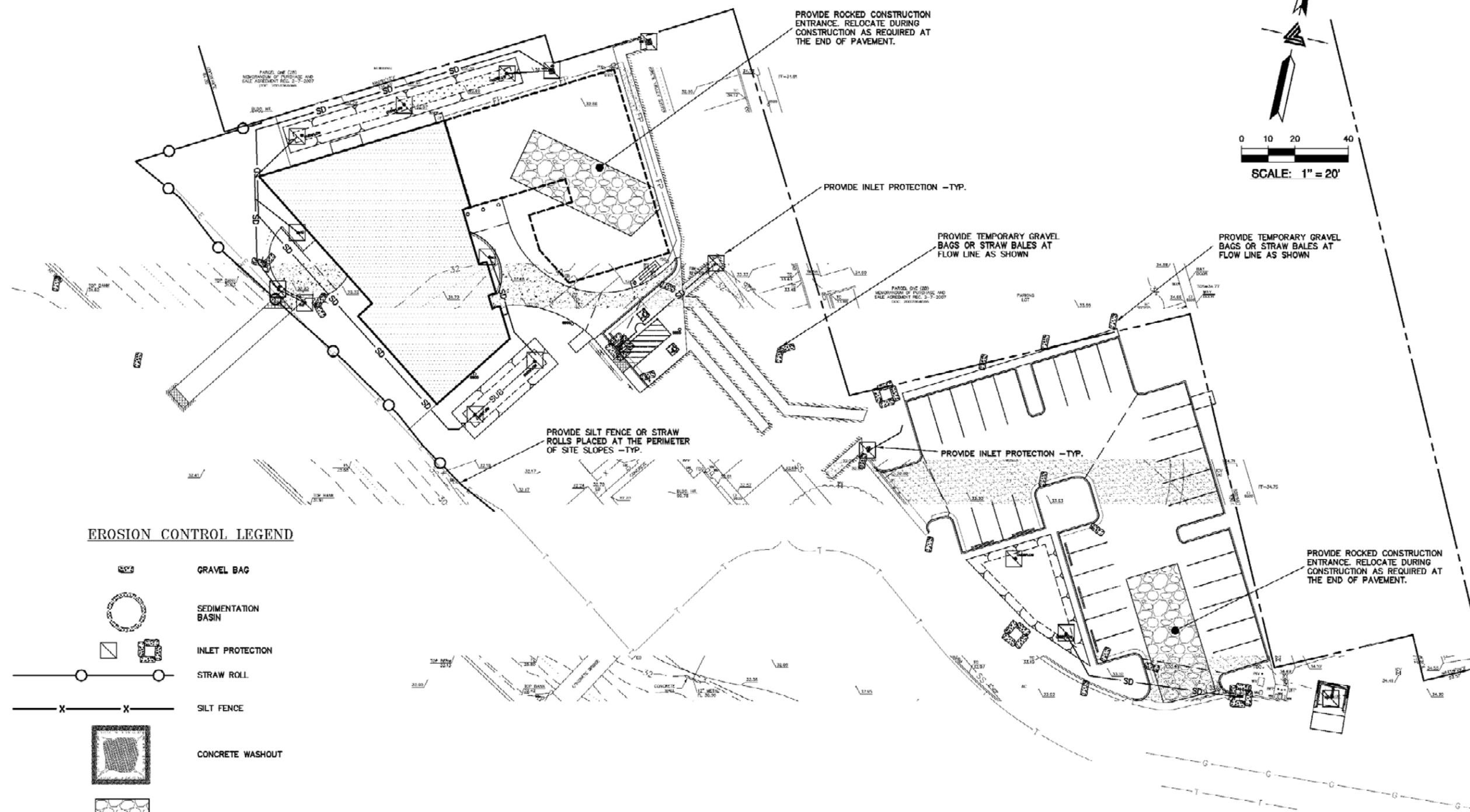
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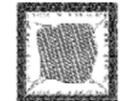
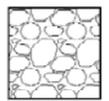
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PROJECT NUMBER: 1118000	

**EROSION
 CONTROL
 PLAN**

DRAWING
 NUMBER: 30 of 34 **ER-1**



EROSION CONTROL LEGEND

-  GRAVEL BAG
-  SEDIMENTATION BASIN
-  INLET PROTECTION
-  STRAW ROLL
-  SILT FENCE
-  CONCRETE WASHOUT
-  CONSTRUCTION ENTRANCE
-  TREE PROTECTION

NOTE:
 SEAL ALL OTHER INLETS NOT INTENDED
 TO ACCEPT STORM WATER AND DIRECT
 FLOWS TEMPORARILY TO FUNCTIONAL
 SEDIMENTATION BASIN INLETS. -TYP

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CITY OF
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 CITY ENGINEER DIRECTOR PUBLIC WORKS

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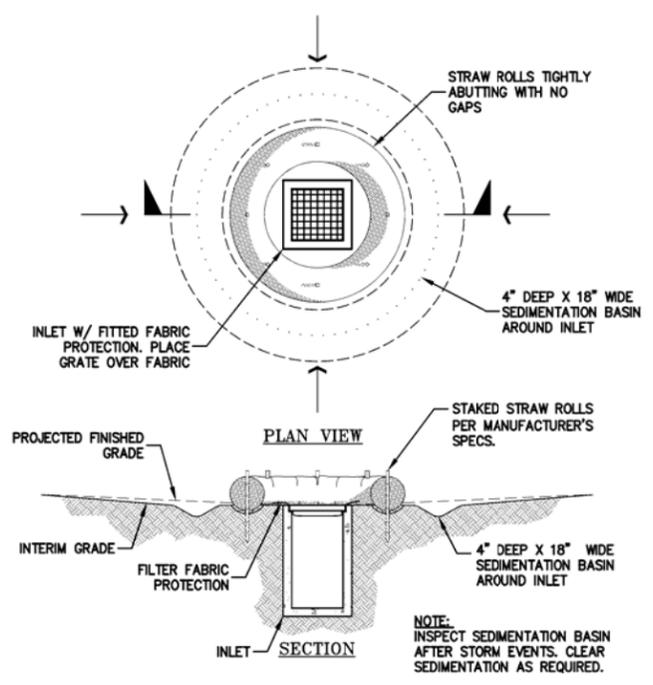
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 DATE: 01/11/2013 SCALE: NTS
 PROJECT NUMBER: 1118000

EROSION CONTROL DETAILS

DRAWING NUMBER: 31 of 31 **ER-2**

EROSION CONTROL MEASURES:

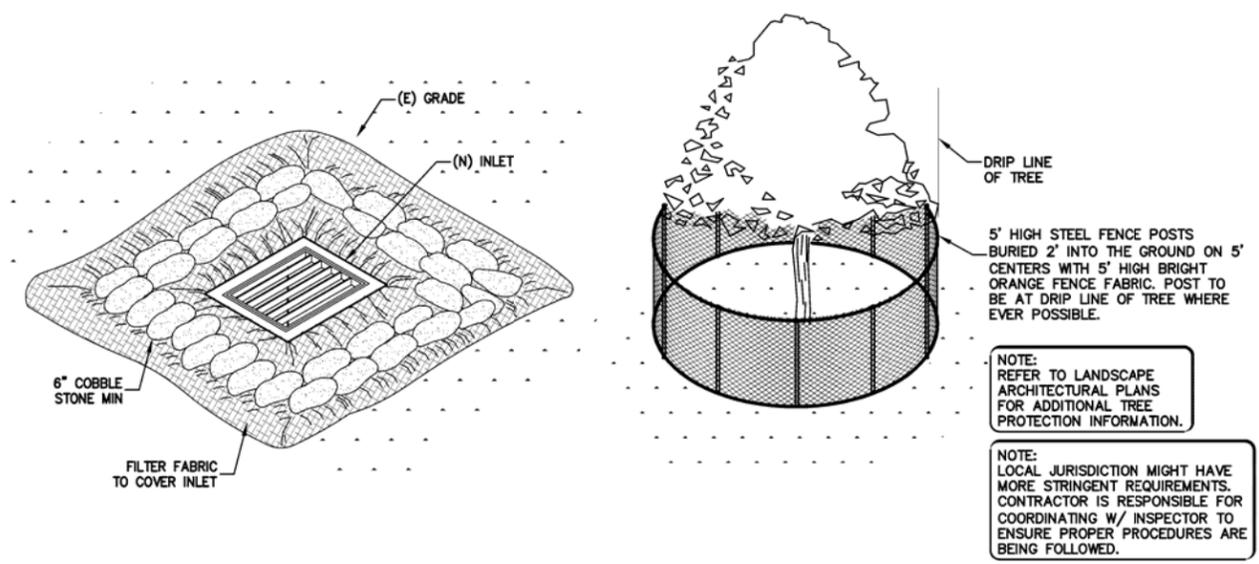
- THE FACILITIES SHOWN ON THIS PLAN ARE DESIGNED TO CONTROL EROSION AND SEDIMENT DURING THE RAINY SEASON, OCTOBER 15TH TO APRIL 15. EROSION CONTROL FACILITIES SHALL BE IN PLACE PRIOR TO OCTOBER 15TH OF ANY YEAR. GRADING OPERATIONS DURING THE RAINY SEASON WHICH LEAVE DENUDE SLOPES SHALL BE PROTECTED WITH EROSION CONTROL MEASURES IMMEDIATELY FOLLOWING GRADING ON THE SLOPES.
- SITE CONDITIONS AT TIME OF PLACEMENT OF EROSION CONTROL MEASURES WILL VARY. APPROPRIATE ACTION INCLUDING TEMPORARY SWALES, INLETS, HYDROSEEDING, STRAW BALES, ROCK SACKS, ETC. SHALL BE TAKEN TO PREVENT EROSION AND SEDIMENTATION FROM LEAVING SITE. EROSION CONTROL MEASURES SHALL BE ADJUSTED AS THE CONDITIONS CHANGE AND THE NEED OF CONSTRUCTION SHIFT.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. ALL CONSTRUCTION TRAFFIC ENTERING ONTO THE PAVED ROADS MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCES. CONTRACTOR SHALL MAINTAIN STABILIZED ENTRANCE AT EACH VEHICLE ACCESS POINT TO EXISTING PAVED STREETS. ANY MUD OR DEBRIS TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED DAILY AND AS REQUIRED BY THE CITY.
- ALL EXPOSED SLOPES THAT ARE NOT VEGETATED SHALL BE HYDROSEEDING. IF HYDROSEEDING IS NOT USED OR IS NOT EFFECTIVE BY OCTOBER 15, THEN OTHER IMMEDIATE METHODS SHALL BE IMPLEMENTED, SUCH AS EROSION CONTROL BLANKETS, OR A THREE-STEP APPLICATION OF 1) SEED, MULCH, FERTILIZER 2) BLOWN STRAW 3) TACKIFIER AND MULCH. HYDROSEEDING SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF SECTION 20" EROSION CONTROL AND HIGHWAY PLANTING" OF THE STANDARD SPECIFICATION OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION, AS LAST REVISED. REFER TO THE EROSION CONTROL SECTION OF THE GRADING SPECIFICATIONS THAT ARE A PART OF THIS PLAN SET FOR FURTHER INFORMATION.
- INLET PROTECTION SHALL BE INSTALLED AT OPEN INLETS TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAIN SYSTEM. INLETS NOT USED IN CONJUNCTION WITH EROSION CONTROL ARE TO BE BLOCKED TO PREVENT ENTRY OF SEDIMENT. MINIMUM INLET PROTECTION SHALL CONSIST OF A ROCK SACKS OR AS SHOWN ON THIS PLAN
- THIS EROSION AND SEDIMENT CONTROL PLAN MAY NOT COVER ALL THE SITUATIONS THAT MAY ARISE DURING CONSTRUCTION DUE TO UNANTICIPATED FIELD CONDITIONS. VARIATIONS AND ADDITIONS MAY BE MADE TO THIS PLAN IN THE FIELD. CONTRACTOR SHALL NOTIFY THE CITY OF ANY CHANGES.
- THE EROSION CONTROL MEASURES SHALL CONFORM TO THE CITY'S STANDARDS AND THE APPROVAL OF THE CITY ENGINEER.



3 **SEDIMENTATION BASIN**
 ER-2 NTS

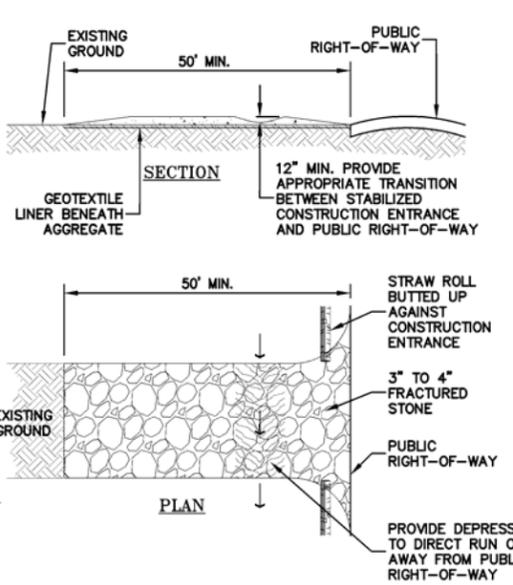
EROSION CONTROL NOTES:

- IT SHALL BE THE OWNER'S/CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CONTROL OF THE ENTIRE CONSTRUCTION OPERATION AND TO KEEP THE ENTIRE SITE IN COMPLIANCE WITH THIS EROSION CONTROL PLAN.
- THE INTENTION OF THIS PLAN IS FOR INTERIM EROSION AND SEDIMENT CONTROL ONLY. ALL EROSION CONTROL MEASURES SHALL CONFORM TO CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION, AND THE CITY OF HAYWARD.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MONITORING EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO, DURING, AND AFTER STORM EVENTS. PERSON IN CHARGE OF MAINTAINING EROSION CONTROL MEASURES SHOULD WATCH LOCAL WEATHER REPORTS AND ACT APPROPRIATELY TO MAKE SURE ALL NECESSARY MEASURES ARE IN PLACE.
- SANITARY FACILITIES SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT-LADEN RUNOFF TO ANY STORM DRAINAGE SYSTEM, INCLUDING EXISTING DRAINAGE SWALES AND WATERCOURSES.
- CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION WILL BE MINIMIZED. COMPLIANCE WITH FEDERAL, STATE AND LOCAL LAWS CONCERNING POLLUTION SHALL BE MAINTAINED AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE DUST CONTROL AS REQUIRED BY THE APPROPRIATE FEDERAL, STATE AND LOCAL AGENCY REQUIREMENTS.
- ALL MATERIALS NECESSARY FOR THE APPROVED EROSION CONTROL MEASURES SHALL BE IN PLACE BY OCTOBER 15TH.
- EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON, OR FROM OCTOBER 15TH THROUGH APRIL 15TH, WHICHEVER IS LONGER.
- IN THE EVENT OF RAIN, ALL GRADING WORK IS TO CEASE IMMEDIATELY AND THE SITE IS TO BE SEALED IN ACCORDANCE WITH THE APPROVED EROSION CONTROL MEASURES AND APPROVED EROSION CONTROL PLAN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING AND REPAIRING EROSION CONTROL SYSTEMS AFTER EACH STORM.
- ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY THE CITY ENGINEER.
- MEASURES SHALL BE TAKEN TO COLLECT OR CLEAN ANY ACCUMULATION OR DEPOSIT OF DIRT, MUD, SAND, ROCKS, GRAVEL OR DEBRIS ON THE SURFACE OF ANY STREET, ALLEY OR PUBLIC PLACE OR IN ANY PUBLIC STORM DRAIN SYSTEMS. THE REMOVAL OF AFORESAID SHALL BE DONE BY STREET SWEEPING OR HAND SWEEPING. WATER SHALL NOT BE USED TO WASH SEDIMENTS INTO PUBLIC OR PRIVATE DRAINAGE FACILITIES.
- EROSION CONTROL MEASURES SHALL BE ON-SITE FROM SEPTEMBER 15TH THRU APRIL 15TH.



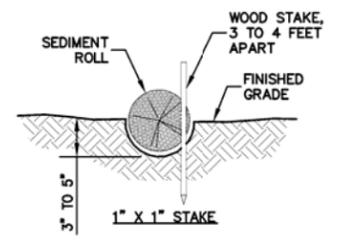
1 **INLET PROTECTION**
 ER-2 NTS

2 **EXISTING TREE PROTECTION DETAIL**
 ER-2 NTS



4 **CONSTRUCTION ENTRANCE**
 ER-2 NTS

NOTES:
 STABILIZED CONSTRUCTION SITE ACCESS SHALL BE CONSTRUCTED OF 3" TO 4" WASHED, FRACTURED STONE AGGREGATE.
 MATERIAL SHALL BE PLACED TO A MINIMUM THICKNESS OF 12". LENGTH OF ENTRANCE SHALL BE A MINIMUM OF 50'.
 WIDTH SHALL BE A MIN. OF 20' OR GREATER IF NECESSARY TO COVER ALL VEHICULAR INGRESS AND EGRESS. PROVIDE AMPLE TURNING RADI.
 THE ENTRANCE SHALL BE KEPT IN GOOD CONDITION BY OCCASIONAL TOP DRESSING WITH MATERIAL AS SPECIFIED IN ABOVE NOTE.
 ACCESSES SHALL BE INSPECTED WEEKLY DURING PERIODS OF HEAVY USAGE, MONTHLY DURING NORMAL USAGE, AND AFTER EACH RAINFALL, WITH MAINTENANCE PROVIDED AS NECESSARY.
 PERIODIC TOP DRESSING SHALL BE DONE AS NEEDED.



5 **STRAW ROLLS**
 ER-2 NTS

PURPOSE:

THE PURPOSE OF THIS PLAN IS TO STABILIZE THE SITE TO PREVENT EROSION OF GRADED AREAS AND TO PREVENT SEDIMENTATION FROM LEAVING THE CONSTRUCTION AREA AND AFFECTING NEIGHBORING SITES, NATURAL AREAS, PUBLIC FACILITIES OR ANY OTHER AREA THAT MIGHT BE AFFECTED BY SEDIMENTATION. ALL MEASURES SHOWN ON THIS PLAN SHOULD BE CONSIDERED THE MINIMUM REQUIREMENTS NECESSARY. SHOULD FIELD CONDITIONS DICTATE ADDITIONAL MEASURES, SUCH MEASURES SHALL BE PER CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL AND THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION. THE CITY SHOULD BE NOTIFIED IMMEDIATELY SHOULD CONDITIONS CHANGE.

REFERENCES:

- CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENT CONTROL
- CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION

RANCHO CUCAMONGA - FOLSOM - EMERYVILLE
 NORTHERN CALIFORNIA
 1110 Iron Point Road, Suite 200
 Folsom, California 95630-8301
 tel: 916-355-9922
 fax: 916-355-9950



HAYWARD

DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

DESIGNED BY: _____ CHECKED BY: _____
 DRAWN BY: _____ APPROVAL RECD: _____
 APPROVED BY: _____
 CITY ENGINEER: _____ DIRECTOR PUBLIC WORKS: _____
 PROJ. NO. 6816 FILE NO. E-2021

CITY OF HAYWARD
EXECUTIVE AIRPORT ADMIN. BUILDING

100% CONSTRUCTION DRAWINGS



CONSULTANT (LB # 2120114)

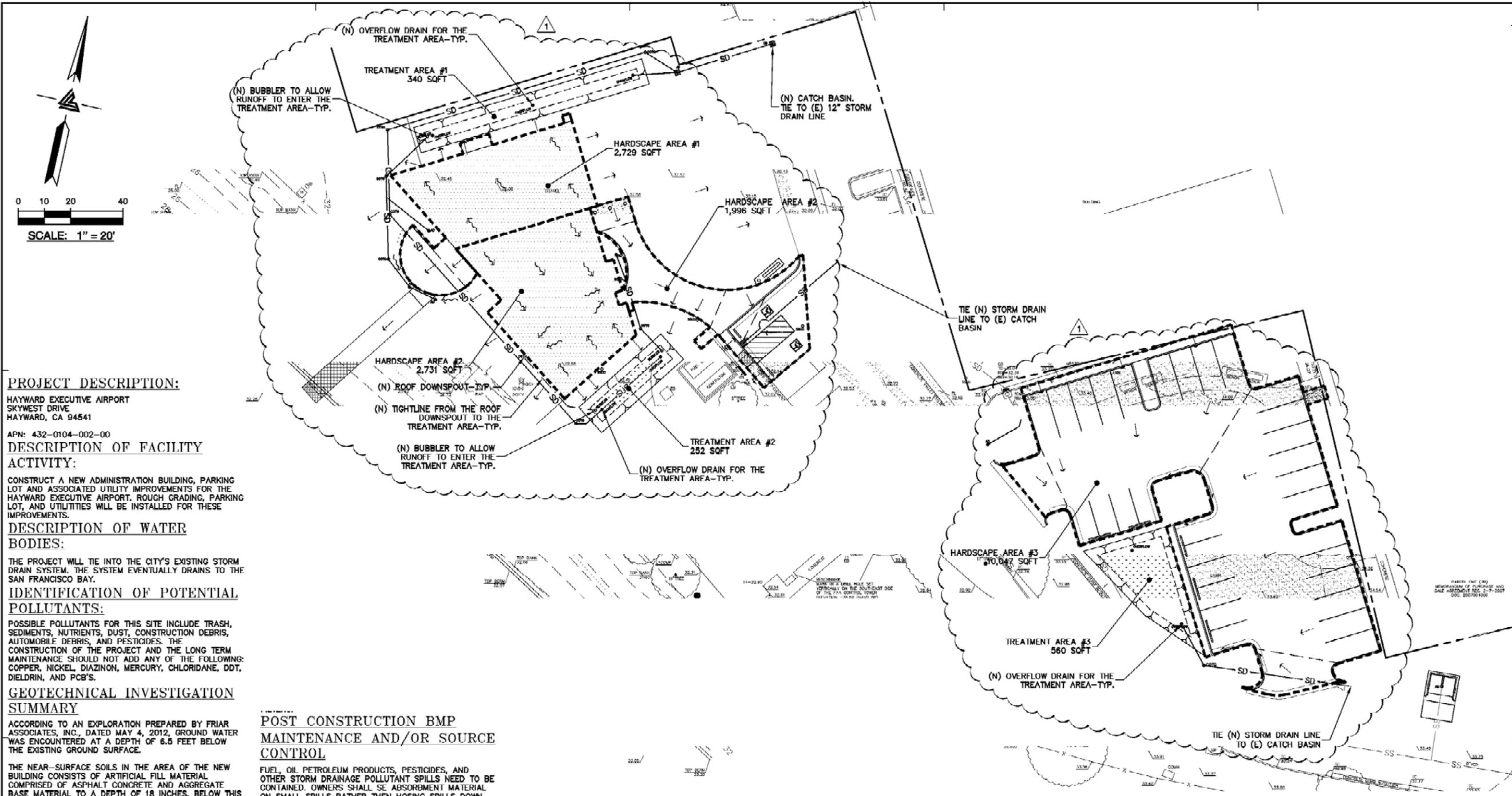
LEA & BRAZE ENGINEERING, INC.
 CIVIL ENGINEERS LAND SURVEYORS
 2495 INDUSTRIAL PARKWAY WEST
 HAYWARD, CALIFORNIA 94545
 (510) 887-4086 VOICE
 (510) 887-3019 FAX
 WWW.LEABRAZE.COM

NO	DATE	BY	DESCRIPTION
2/13/13	PT		BLDG. DEPT COMMENTS
REVISIONS			

DRAWN: TB/PT CHECKED: PC
 DATE: 01/11/2013 SCALE: 1"=20'
 PROJECT NUMBER: 1118000

STORM WATER TREATMENT PLAN

DRAWING NUMBER: 2 of 3 SW-1



PROJECT DESCRIPTION:

HAYWARD EXECUTIVE AIRPORT
 SKYWEST DRIVE
 HAYWARD, CA 94541

APN: 432-0104-002-00
DESCRIPTION OF FACILITY ACTIVITY:

CONSTRUCT A NEW ADMINISTRATION BUILDING, PARKING LOT AND ASSOCIATED UTILITY IMPROVEMENTS FOR THE HAYWARD EXECUTIVE AIRPORT. ROUGH GRADING, PARKING LOT, AND UTILITIES WILL BE INSTALLED FOR THESE IMPROVEMENTS.

DESCRIPTION OF WATER BODIES:

THE PROJECT WILL TIE INTO THE CITY'S EXISTING STORM DRAIN SYSTEM. THE SYSTEM EVENTUALLY DRAINS TO THE SAN FRANCISCO BAY.

IDENTIFICATION OF POTENTIAL POLLUTANTS:

POSSIBLE POLLUTANTS FOR THIS SITE INCLUDE TRASH, SEDIMENTS, NUTRIENTS, DUST, CONSTRUCTION DEBRIS, AUTOMOBILE DEBRIS, AND PESTICIDES. THE CONSTRUCTION OF THE PROJECT AND THE LONG TERM MAINTENANCE SHOULD NOT ADD ANY OF THE FOLLOWING: COPPER, NICKEL, DIAZINON, MERCURY, CHLORIDANE, DDT, DIELDRIN, AND PCB'S.

GEOTECHNICAL INVESTIGATION SUMMARY

ACCORDING TO AN EXPLORATION PREPARED BY FRIAR ASSOCIATES, INC., DATED MAY 4, 2012, GROUND WATER WAS ENCOUNTERED AT A DEPTH OF 6.5 FEET BELOW THE EXISTING GROUND SURFACE.

THE NEAR-SURFACE SOILS IN THE AREA OF THE NEW BUILDING CONSISTS OF ARTIFICIAL FILL MATERIAL COMPRISED OF ASPHALT CONCRETE AND AGGREGATE BASE MATERIAL TO A DEPTH OF 18 INCHES. BELOW THIS DEPTH TO 15 FEET BELOW THE SURFACE, MATERIAL CONSISTED OF STIFF SILTY CLAY TO SANDY CLAY. BELOW THIS DEPTH TO 28 FEET BELOW THE SURFACE, MATERIAL CONSISTED OF SAND. THE GEOTECHNICAL INVESTIGATION DID NOT INCLUDE INFORMATION ON PERMEABILITY OF THE SOIL.

BASED UPON A STUDY PREPARED BY FEMA, PER FIRM MAP 06001G0286G DATED AUGUST 3, 2009, IT WAS STATED THAT THE AREA OF THE PROPOSED NEW BUILDING IS DETERMINED TO BE WITHIN A SPECIAL FLOOD HAZARD AREA SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD WITH A BASE FLOOD ELEVATION OF ~32 FEET. THE AREA OF THE NEW PARKING LOT IS DETERMINED TO BE IN AREAS OF 0.2% ANNUAL CHANCE FLOOD; AREAS OF 1% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQ. MILE; AND AREAS PROTECTED BY LEVEES FROM 1% ANNUAL CHANCE FLOOD.

BMP DESCRIPTION

THIS PROJECT USES 4 BIO-RETENTION AREAS.
 THE SITE IS APPROXIMATELY 43,337 SF. THE DEVELOPMENT WILL ADD APPROXIMATELY 33,296 SF OF ADDITIONAL IMPERVIOUS SURFACE. THE SITE WILL RETAIN APPROXIMATELY 23% PERVIOUS SURFACES, WHICH INCLUDES LANDSCAPING AREAS.
 THE SITE IS DIVIDED INTO 7 DRAINAGE MANAGEMENT AREAS (DMA). AREAS A THROUGH F UTILIZE FLOW THROUGH PLANTERS, AND AREA G USES TREE WELL FILTERS.

POST CONSTRUCTION BMP MAINTENANCE AND/OR SOURCE CONTROL

FUEL, OIL, PETROLEUM PRODUCTS, PESTICIDES, AND OTHER STORM DRAINAGE POLLUTANT SPILLS NEED TO BE CONTAINED. OWNERS SHALL USE ABSORBENT MATERIAL ON SMALL SPILLS RATHER THAN HOSING SPILLS DOWN. REMOVE THE ABSORBENT MATERIAL PROMPTLY AND DISPOSE OF PROPERLY, AS REQUIRED BY CITY, STATE AND FEDERAL REGULATIONS.

DRAINAGE INLETS SHALL BE INSPECTED MONTHLY AND KEPT CLEAN OF ANY TRASH THAT MAY HAVE ACCUMULATED. IT IS THE RESPONSIBILITY OF THE PROPERTY MANAGER/OWNER TO HAVE THOSE INSPECTIONS PERFORMED, DOCUMENTED AND ANY REPAIRS MADE.

A. LANDSCAPE MAINTENANCE

LANDSCAPE AREAS SHALL BE COVERED WITH PLANTS OR SOME TYPE OF GROUND COVER TO MINIMIZE EROSION. NO AREAS ARE TO BE LEFT AS BARE DIRT THAT COULD ERODE. MOUNDING SLOPES SHALL NOT EXCEED 2 HORIZONTAL TO 1 VERTICAL.

PESTICIDES AND FERTILIZERS SHALL BE STORED AS HAZARDOUS MATERIALS AND IN APPROPRIATE PACKAGING. OVER SPRAYING ONTO PAVED AREAS SHALL BE AVOIDED WHEN APPLYING FERTILIZERS AND PESTICIDES. PESTICIDES AND FERTILIZERS WILL BE PROHIBITED FROM STORAGE OUTSIDE.

THE LANDSCAPE AREAS SHALL BE INSPECTED AND ALL TRASH PICKED UP AND OBSTRUCTIONS TO THE DRAINAGE FLOW REMOVED ON A MONTHLY BASIS MINIMUM. THIS SITE HAS BEEN DESIGNED WITH EFFICIENT IRRIGATION AND DRAINAGE TO REDUCE PESTICIDE USE. PLANTS HAVE BEEN SELECTED BASED ON SIZE AND ARE SITUATED TO REDUCE MAINTENANCE AND ROUTINE PRUNING.

B. DRAINAGE COLLECTION MANAGEMENT

THE STORM DRAINAGE SYSTEM CONSISTS OF AREA DRAINS, CATCH BASINS, BUBBLERS, COLLECTION AND DISTRIBUTION PIPING, SWALES, AND CLEAN OUTS.

THE STORM DRAINAGE COLLECTION SYSTEM SHALL BE CLEANED YEARLY BY THE PROPERTY MANAGEMENT/OWNER. THE INSPECTION SHALL BE PERFORMED DURING THE DRY SEASON. THIS INCLUDES THE FOLLOWING:

*ALL TRASH AND OBSTRUCTIONS SHALL BE REMOVED FROM AREA DRAINS, BUBBLERS, CLEAN OUTS, AND CATCH BASINS.

C. BIO-RETENTION AREAS

MAINTENANCE AGREEMENT UPON ACCEPTANCE OF THE DESIGN CONCEPT, A MAINTENANCE AGREEMENT WILL BE DEVELOPED REQUIRING THE PROPERTY MANAGER/OWNER TO PROVIDE THE FOLLOWING INFORMATION ON A ROUTINE BASIS.

MAINTENANCE STANDARDS:

*SOILS AND PLANTINGS MUST BE MAINTAINED, INCLUDING ROUTINE PRUNING, MOWING, IRRIGATION, REPLENISHMENT OF MULCH, WEEDING, AND FERTILIZING WITH A SLOW-RELEASE FERTILIZER WITH TRACE ELEMENTS.

*REMOVE OBSTRUCTIONS AND TRASH FROM FLOW THROUGH PLANTERS.

*ONLY PESTICIDES AND FERTILIZERS THAT ARE ACCEPTED BY THE CITY AND COUNTY FOR USE IN FLOW THROUGH PLANTERS SHALL BE USED.

*EROSION AT INFLOW POINTS MUST BE REPAIRED.

FLOW THROUGH PLANTERS SHALL BE INSPECTED AND MAINTAINED MONTHLY TO REVIEW:

*OBSTRUCTION AND TRASH

*IF PONDED WATER IS OBSERVED, THE SURFACE SOILS SHALL BE REMOVED AND REPLACED AND SUBDRAIN SYSTEM INSPECTED.

*CONDITION OF GRASSES.

D. TRAINING PROGRAM

A COPY OF THE STORM WATER MANAGEMENT PLANS (SWMP) WILL BE MADE AVAILABLE TO PERSONNEL IN CHARGE OF FACILITY MAINTENANCE AND WILL BE DISTRIBUTED TO THE SUBCONTRACTOR REPRESENTATIVE ENGAGED IN THE MAINTENANCE OR INSTALLATION OF THE BMP'S.

A COPY OF THE YEARLY INSPECTION REPORTS SHALL BE MANAGED BY THE PROPERTY MANAGER/OWNER.

DRAINAGE ZONE DESIGNATION	IMPERVIOUS AREA TREATED (SQ FT)	TREATMENT AREA REQUIRED (4% OF IMPERVIOUS AREA) (SQ FT)	TREATMENT AREA PROVIDED (SQ FT)	EXCESS TREATMENT AREA (SQ FT)
1	2,729	110	340	230
2	4,727	189	252	63
3	10,047	402	560	158
TOTAL	17,503	701	1,152	451

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CITY OF
HAYWARD
 HEART OF THE BAY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

DESIGNED BY: CHECKED BY:
 DRAWN BY: APPROVAL REC'D:
 APPROVED BY:

ASST. CITY ENGINEER DIRECTOR PUBLIC WORKS
 PROJ. NO. 6815 FILE NO. E-2021

CITY OF HAYWARD
**EXECUTIVE
 AIRPORT
 ADMIN.
 BUILDING**

100% CONSTRUCTION
 DRAWINGS



CONSULTANT
HARRIS DESIGN
 Landscape Architecture
 Urban Design
 755 Folger Avenue
 Berkeley, CA 94710
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 www.hd-ia.com

NO	DATE	BY	DESCRIPTION
1	2/13/13	JULY	BLDG. DEPT COMMENTS
REVISIONS			

DRAWN: LF	CHECKED: BH
DATE: 01/11/2013	SCALE: 1"=20'-0"
PROJECT NUMBER: 1118000	

PLANTING PLAN

DRAWING NUMBER: 33 of 34
1.1

TREE MITIGATION SUMMARY CHART

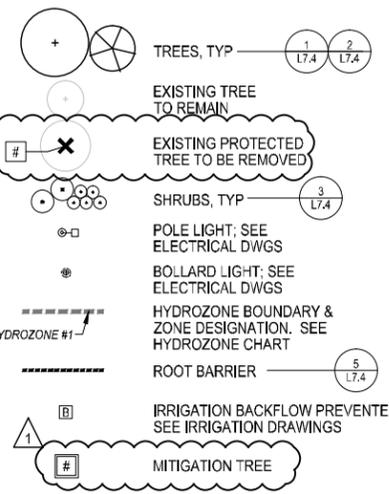
PROTECTED TREES TBR	MITIGATION TREES
1 3" <i>Platanus x hispanica</i>	1 <i>Trachycarpus fortunei</i> , 12'-0" exposed trunk
2 3" <i>Platanus x hispanica</i>	2 <i>Trachycarpus fortunei</i> , 12'-0" exposed trunk
3 3" <i>Platanus x hispanica</i>	3 <i>Trachycarpus fortunei</i> , 12'-0" exposed trunk
4 3" <i>Platanus x hispanica</i>	4 <i>Trachycarpus fortunei</i> , 12'-0" exposed trunk
5 3" <i>Platanus x hispanica</i>	
6 3" <i>Platanus x hispanica</i>	
7 3" <i>Platanus x hispanica</i>	
8 3"-4" <i>Prunus cerasifera</i>	
9 3"-4" <i>Prunus cerasifera</i>	
10 3"-4" <i>Prunus cerasifera</i>	
11 3"-4" <i>Prunus cerasifera</i>	

NOTE
 REFER TO ARBORIST REPORT: "TREE EVALUATION & APPRAISAL OF VALUE, HAYWARD EXECUTIVE AIRPORT (PROJ. #6815)" DATED 1/14/13, BY HORTSCIENCE, INC

PLANT MATERIAL KEY & SCHEDULE OF SIZES

ABBREVIATION	BOTANIC NAME	COMMON NAME	CONT SIZE & HT/SPD	SPACING	WATER USE
TREES					
ACE PAL	ACER PALMATUM "SANGOKAKU"	JAPANESE MAPLE	24" BOX; 6-7/4-5' MULTI-TRUNK	PER PLAN	MEDIUM
TRA FOR	TRACHYCARPUS FORTUNEI	WINDMILL PALM	12'-0" EXPOSED TRUNK HT	PER PLAN	LOW
PIS CHI	PISTACIA CHINENSIS "KIETH DAVEY"	FRUITLESS CHINESE PISTACHE	24" BOX; 9-11' / 4-5'	PER PLAN	LOW
SHRUBS					
ASP ELA	ASPIDISTRA ELATIOR "ASAHI"	CAST IRON PLANT	1 GAL	3'-0" OC	LOW
BER THU	BERBERIS THUNBERGII "ORANGE ROCKET"	JAPANESE BARBERRY	5 GAL	2'-6" OC	LOW
CIS PUR	CISTUS X PURPUREA	ORCHID ROCKROSE	5 GAL	5'-0" OC	LOW
CEA SKY	CEANOTHUS THYRSIFLORUS "SKYLARK"	SKYLARK WILD LILAC	5 GAL	5'-0" OC	LOW
DIE IRI	DIETES IRIDIODES	FORTNIGHT LILY	1 GAL	2'-6" OC	LOW
HEM MON	HEMEROCALLIS X MONOLD	EVERGREEN DAYLILY - GOLD	5 GAL	2'-0" OC	MEDIUM
HEM STE	HEMEROCALLIS X "STELLA DE ORO"	EVERGREEN DAYLILY	5 GAL	2'-0" OC	MEDIUM
LAV HID	LAVANDULA ANGUSTIFOLIA "HIDCOTE"	ENGLISH LAVENDER	1 GAL	2'-0" OC	LOW
MUH RIG	MUHLENBERGIA RIGENS	DEER GRASS	5 GAL	4'-0" OC	LOW
NAS TEN	NASSELLA TENUISSIMA	FEATHER GRASS	1 GAL	2'-0" OC	LOW
NAN DOM	NANDINA DOMESTICA "COMPACTA"	HEAVENLY BAMBOO	5 GAL	2'-0" OC	LOW
PHO COO	PHORMIUM COOKIANUM "CREAM DELIGHT"	VARIEGATED NEW ZEALAND FLAX	5 GAL	5'-0" OC	LOW
SAL LEU	SALVIA LEUCANTHA "SANTA BARBARA"	MEXICAN SAGE	5 GAL	3'-0" OC	LOW
SAL GRE	SALVIA GREGGII "FURMAN'S RED"	MAGENTA SAGE	5 GAL	3'-0" OC	LOW
TEU CHA	TEUCHRIUM CHAMAEDRIS	GERMANDER	1 GAL	3'-0" OC	LOW
VER BON	VERBENA BONARIENSIS	VERBENA	5 GAL	3'-0" OC	VERY LOW
GROUNDCOVERS					
CAR DIV	CAREX DIVULSA	BERKELEY SEDGE	1 GAL	2'-0" OC	LOW - MED
CEA ANC	CEANOTHUS "ANCHOR BAY"	WILD LILAC	1 GAL	5'-0" OC	LOW
COP KIR	COPROSMA KIRKII "VARIEGATA"	VARIEGATED COPROSMA	1 GAL	4'-6" OC	LOW
CIS SAL	CISTUS SALVIFOLIUS	SAGELEAF ROCKROSE	1 GAL	5'-0" OC	LOW
JUN PAT	JUNCUS PATENS	CALIFORNIA GRAY RUSH	1 GAL	2'-0" OC	LOW
MYO PAR	MYOPORUM PARVIFOLIUM	MYOPORUM	1 GAL	5'-0" OC	LOW

LEGEND

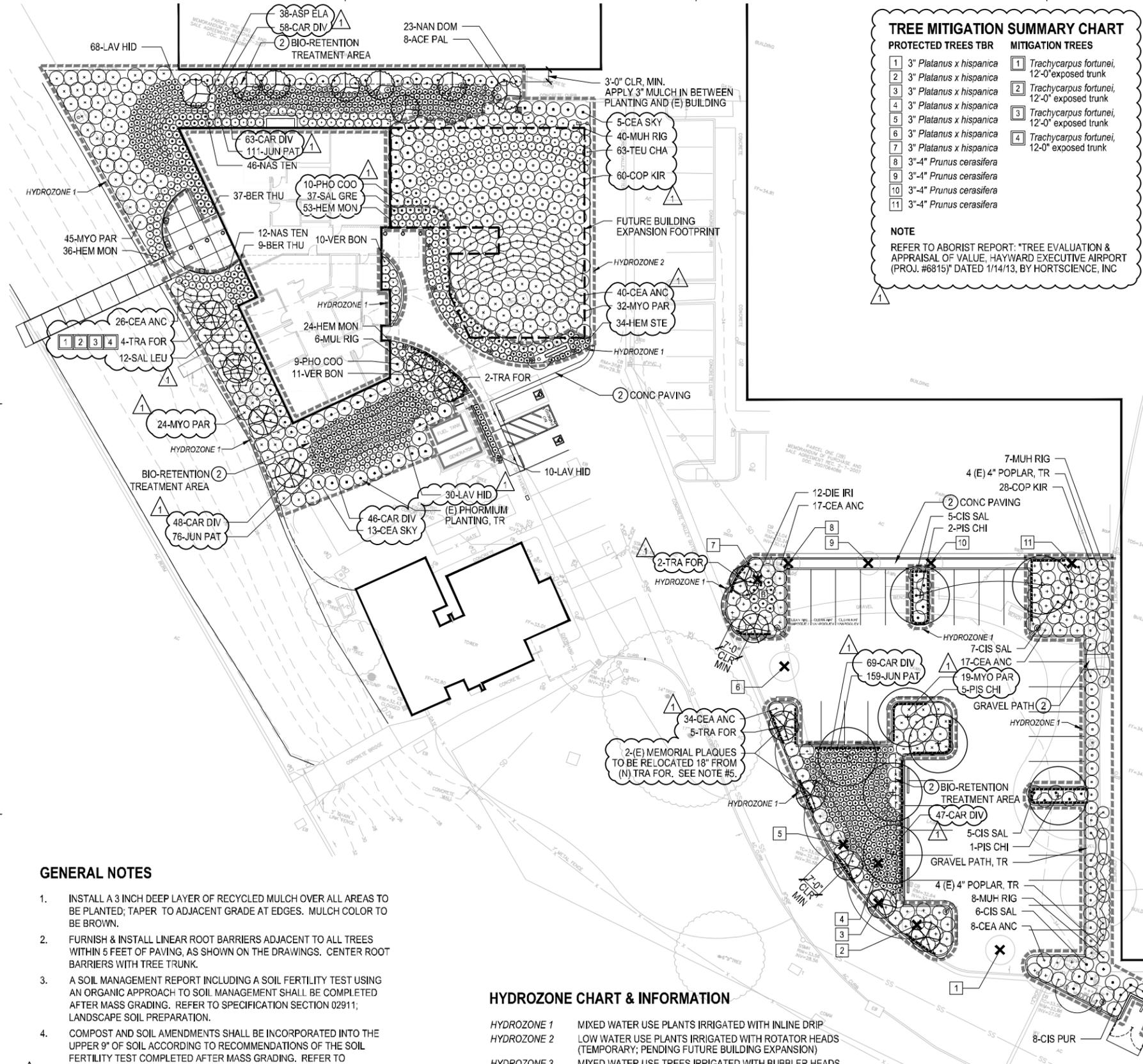
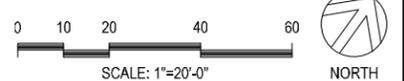


NUMBERED NOTES

- 1 REFER TO ARCHITECTURAL DRAWINGS
- 2 REFER TO CIVIL DRAWINGS

ABBREVIATIONS

CAL	CALIPER
CLR	CLEARANCE
CONC	CONCRETE
(E)	EXISTING
GAL	GALLON
HDR	REDWOOD HEADER
HT	HEIGHT
MT	MULTI-TRUNK
N)	NEW
OC	ON CENTER
SPD	SPREAD
TYP	TYPICAL
TBR	TO BE REMOVED
TR	TO REMAIN



HYDROZONE CHART & INFORMATION

HYDROZONE 1	MIXED WATER USE PLANTS IRRIGATED WITH INLINE DRIP
HYDROZONE 2	LOW WATER USE PLANTS IRRIGATED WITH ROTATOR HEADS (TEMPORARY; PENDING FUTURE BUILDING EXPANSION)
HYDROZONE 3	MIXED WATER USE TREES IRRIGATED WITH BUBBLER HEADS (HYDROZONE 3 NOT INDICATED WITH DASHED LINE; ALL TREES ARE INCLUDED IN THIS ZONE)

NOTES

- PLANT WATER USE DESIGNATIONS ARE TAKEN FROM GUIDE TO ESTIMATING IRRIGATION WATER NEEDS OF LANDSCAPE PLANTINGS IN CA, BY UC COOPERATIVE EXTENSION AND CALIFORNIA NATIVE PLANTS FOR THE GARDEN, BY CAROL BORNSTEIN AND DAVID FROSS
- REFER TO IRRIGATION PLANS FOR ADDITIONAL HYDROZONE INFORMATION

GENERAL NOTES

- INSTALL A 3 INCH DEEP LAYER OF RECYCLED MULCH OVER ALL AREAS TO BE PLANTED; TAPER TO ADJACENT GRADE AT EDGES. MULCH COLOR TO BE BROWN.
- FURNISH & INSTALL LINEAR ROOT BARRIERS ADJACENT TO ALL TREES WITHIN 5 FEET OF PAVING, AS SHOWN ON THE DRAWINGS. CENTER ROOT BARRIERS WITH TREE TRUNK.
- A SOIL MANAGEMENT REPORT INCLUDING A SOIL FERTILITY TEST USING AN ORGANIC APPROACH TO SOIL MANAGEMENT SHALL BE COMPLETED AFTER MASS GRADING. REFER TO SPECIFICATION SECTION 02911; LANDSCAPE SOIL PREPARATION.
- COMPOST AND SOIL AMENDMENTS SHALL BE INCORPORATED INTO THE UPPER 9" OF SOIL ACCORDING TO RECOMMENDATIONS OF THE SOIL FERTILITY TEST COMPLETED AFTER MASS GRADING. REFER TO SPECIFICATION SECTION 02911; LANDSCAPE SOIL PREPARATION.
- LOCATIONS OF RELOCATED (E) MEMORIAL PLAQUES TO BE CONFIRMED WITH LANDSCAPE ARCHITECT PRIOR TO PLACEMENT.
- REFER TO "TREE/PLANT PROTECTION NOTES" ON THE CIVIL PLANS FOR EXISTING TREE PROTECTION MEASURES
- I HAVE COMPLIED WITH THE CRITERIA OF THE CITY OF HAYWARD BAY-FRIENDLY WATER EFFICIENT LANDSCAPE ORDINANCE, HAYWARD MUNICIPAL CODE, CHAPTER 10, ARTICLE 12, AND APPLY THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN.

CITY OF HAYWARD
HEART OF THE BAY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

DESIGNED BY: _____ CHECKED BY: _____
DRAWN BY: _____ APPROVAL RECD: _____
APPROVED BY: _____
CITY ENGINEER DIRECTOR PUBLIC WORKS

PROJ. NO. 6815 FILE NO. E-2021

EXECUTIVE AIRPORT ADMIN. BUILDING

100% CONSTRUCTION DRAWINGS

REGISTERED ARCHITECT
Max Ivan Medina
C-24882
Exp. 07/31/13
STATE OF CALIFORNIA

REGISTERED PROFESSIONAL ENGINEER
JOHN M. MANNINGER
No. 3824
Exp. 09/30/12
STRUCTURAL
STATE OF CALIFORNIA

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STRUCTURAL ENGINEERS INC
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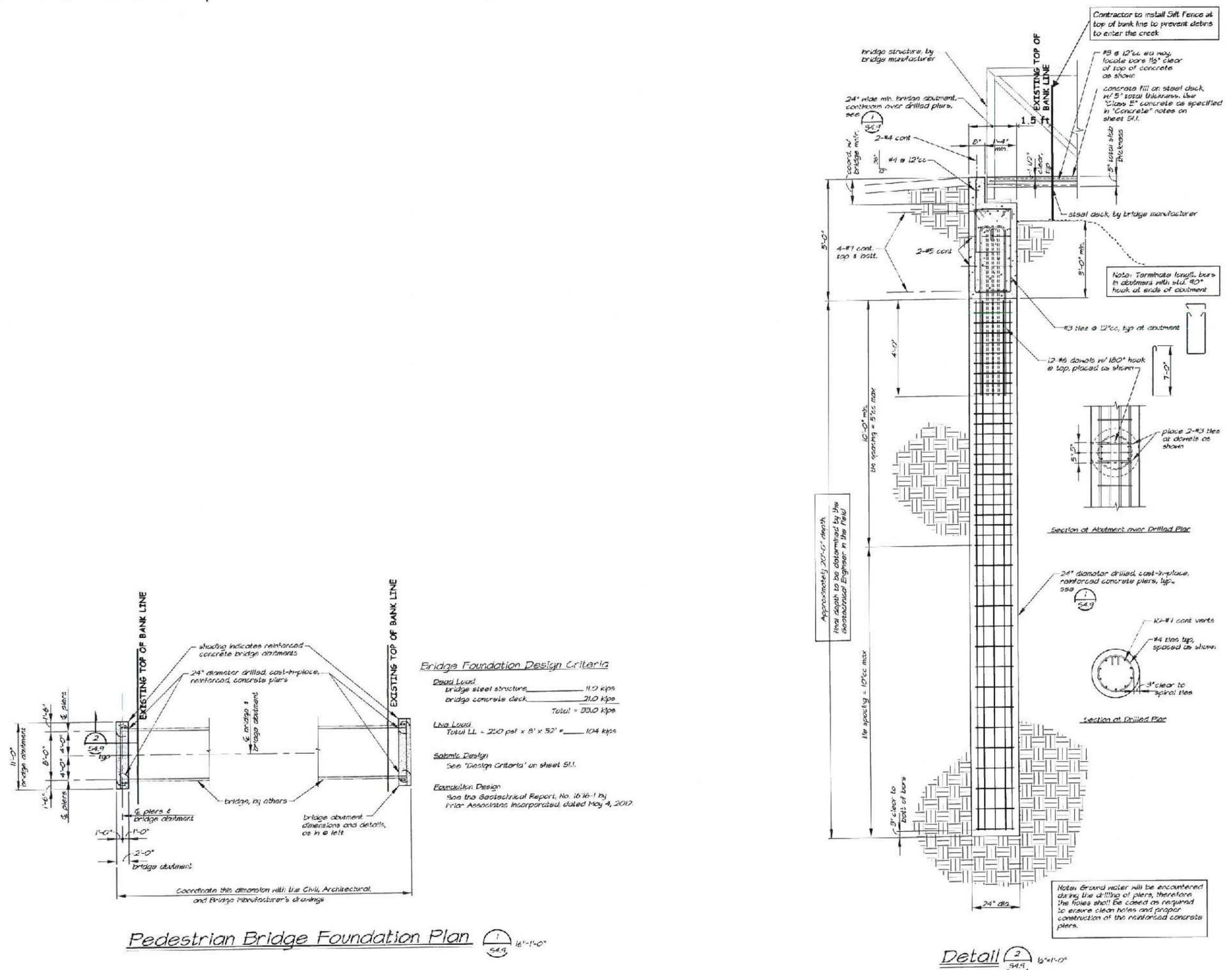
NO	DATE	BY	DESCRIPTION
△	2/4/13	JLY	B.L.D.G. DEPT COMMENTS
△			
△			

REVISIONS

DRAWN:	CHECKED:
DATE: 01/11/2013	SCALE:
PROJECT NUMBER: 1118000	

Pedestrian Bridge Foundation and Details

DRAWING NUMBER of 3 \$4.9



Pedestrian Bridge Foundation Plan 54.9 1/8"=1'-0"

Detail 2 54.9 1/2"=1'-0"

February 15, 2013

Luis Samayoa
 Engineering and Transportation Div.
 City of Hayward
 777 B Street
 Hayward, CA 94541

Initial Study for New Airport Administration Building

Dear Mr. Samayoa,

Herewith are comments on the Initial Study for the new airport administration building dated Jan. 4, 2013. These same comments were sent to you by e-mail at 2 p.m. today.

The Initial Study Checklist prepared by the governor's Office of Planning and Research or OPR (and included as Appendix G to the CEQA Guidelines) is intended to document the conclusion that a proposed project does not appear to have potentially significant environmental impacts that would require an Environmental Impact Report (under CEQA). The checklist prepared by the OPR is a suggested checklist of issues; the introduction to the checklist states that it is a "sample checklist" and that it is "intended to encourage thoughtful assessment of impacts." Moreover, the OPR states in the introduction to its checklist: "Substantial evidence of potential impacts that are not listed on this form must also be considered."

Thus the OPR checklist is not literally a checklist that, if filled out by "checking the boxes," will satisfy CEQA. It is meant to guide agencies in a serious contemplation of the possible adverse impacts of a proposed project.

The proposed new airport administration building, like the existing building, would lie immediately adjacent to a portion of the lowest stretch of Sulphur Creek before the creek traverses the runway area of the airport. Yet this important feature of the project site is nowhere to be found in the project description in the Initial Study, which is no more than one sentence. (Nor is it shown or identified in the site plan prepared by WLC Architects, dated Oct. 20, 2012.) Indeed, in describing past projects that touch on or include streams, whether natural or engineered, the City of Hayward has habitually failed to identify the location of such streams in relation to proposed projects.

In addition, the Initial Study refers to Sulphur Creek as a "flood control facility". This language conjures up images of engineered channels with cement walls or underground culverts, when in fact the creek at the point it passes next to the proposed building is a part of a natural segment of Sulphur Creek that emerges from underneath Hesperian Boulevard and courses across the airport toward the bay. This language, too, is characteristic of City of Hayward's predisposition toward the remaining natural streams that are part of the watershed draining the city; in the past city planning documents have referred to streams as "ditches" (viz the West A Street extension

/ Continued ...

project or the Cannery Project). The manner in which some feature of the environment is characterized has much to do with an assessment of impacts on that feature.

Section IV, **Biological Resources**. City provides a naked, unsupported assertion: "The projects will not have any adverse effect on biological resources; therefore, no impact." Not only is this a mere assertion, it is a tautology. This identical statement is used to dismiss the issue of impacts on riparian habitat (sec. IV b) or on movement of fish or wildlife (sec. IV c).

Section IX, **Hydrology and Water Quality**. Here, too, City provides only naked assertions. With respect to the issue of water quality and discharge into adjacent Sulphur Creek (para. a), City states: "The project will comply with all water quality and wastewater discharge requirements of the City; therefore, no impact." A promise to uphold standards does not qualify as even a cursory assessment of possible adverse impacts. Moreover, the City's assertion does not make clear whether the **City's wastewater discharge requirements** as applied to this project constitute mitigation of potentially adverse impacts. Any mitigation must be spelled out at this stage, and City would be required to issue a Mitigated Negative Declaration rather than a simple Negative Declaration.

With respect to **impacts of the project on drainage or surface water runoff** (paras. c, d, e), City relies exclusively on the assertion: "The proposed drainage system for the project is designed to accept all off-site drainage that is directed towards the project site; therefore, no significant impact." City does not provide information on whether the proposed drainage system will discharge into Sulphur Creek or how much impermeable surface will be created by the project and, as a result, how much, if any, stormwater will run into the creek. City's comment at para. f **indicates that water from the proposed drainage system will discharge into the creek** (identified there as "an existing drainage facility").

Section XVIII, Mandatory Findings of Significance. Para. b concerns the question of cumulative impacts of the proposed project. City concludes "no impact" and for support cites its "evidence" in sections IXc, IXf, XIIIa, and XVa of the Initial Study. However, City provides no documentation to support its conclusion of no impacts in those sections. City does not describe or discuss the possible cumulative effects of the proposed project in light of earlier projects that impacted the lower arm of Sulphur Creek (construction of Home Depot on airport property, the Cannery Project, West A St. extension, as well as other, unknown projects).

City cannot avoid the intent and requirement of the Initial Study to document its conclusions of no significant environmental impacts of the new administration building by merely asserting no impacts, thereby shifting the burden of analysis and documentation of substantial evidence to the public.

Howard Beckman
1261 via Dolorosa
San Lorenzo 94580
Tel: 510.278.7238

cc: Brian Wines, Regional Water Quality Control Board



Sent via electronic mail: Hardcopy to follow

March 1, 2013

Mr. Howard Beckman
1261 Via Dolorosa
San Lorenzo, California 94580

Dear Mr. Beckman:

Thank you for your comment letter dated February 15, 2013, which pertained to the City's proposed new Administration Building and parking lot at the Hayward Executive Airport (the "Project"). We will respond to each comment individually below, but, as a preliminary matter, we want to assure you that neither the building nor the associated pedestrian bridge will impact the Sulphur Creek channel improvements and associated riparian plantings implemented as mitigation in 2009 (the "Adjacent Mitigation Projects").

Comment 1 – No mention that project will lie immediately adjacent to Sulphur Creek

Response – The proposed Airport Administrative Building is set back from the Sulphur Creek top back a distance ranging from 15 to 35 feet. There is no runoff from the proposed improvement area to the Creek or any other impact from this proposed structure. Please see the attached plans.

Comment 2 – Reference to Sulphur Creek as a "flood control facility"

Response – Sulphur Creek is part of the flood control facilities around the City and includes its natural creek channel and man-made open channels and boxed culverts. This is how the creek exists in its current condition. In describing the creek as a flood control facility, the city was referring to its current form and function; and did not intend to minimize the environmental issues as they particularly relate to sections on the creek in its natural form.

Comment 3 – Biological Resources

Response – The proposed project's building is set back from the top of bank and all riparian vegetation by a minimum of 15 feet on the south side to a maximum of 32 feet on the north side of the future building.

DEPARTMENT OF PUBLIC WORKS
ENGINEERING & TRANSPORTATION

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Comment 4 – Hydrology and Water Quality

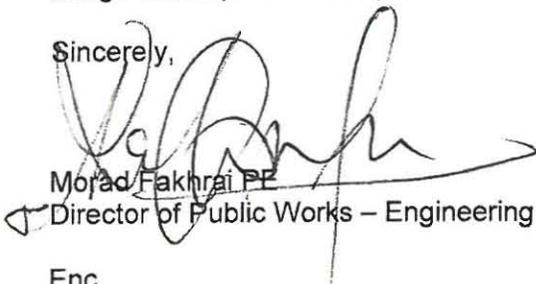
Response – The existing site is currently covered with very well-compacted dirt with gravel on top. Under current site conditions, rain water does not percolate into the ground area; it drains directly over the top of bank into the Sulphur Creek. The project proposes bio-retention storm water facilities in three separate locations within the site to address the increase in developed areas. The drainage system will not drain back into the creek as currently occurs; instead, the landscaped areas between the proposed Airport Administration Building and the creek bank are designed to direct drainage runoff towards the building and into the aforementioned bio-retention facilities. After collection of storm water runoff and treatment in these facilities, the treated water will be directed to the existing storm drain system that finally drains to the creek. By filtering rain water and associated runoff through the bio-retention areas the Project infrastructure will be able to catch many of the pollutants that derive from the runoff before they enter the creek, unlike now where the untreated runoff sheets directly across a compacted gravel area into the creek. Also, the volume of water entering the creek will be reduced because the bio-retention facility will enable greater volumes of water to infiltrate into the ground. The wastewater from the building will be directed through pipelines to the existing sanitary sewer system and eventually to the Wastewater Treatment Plant. Please see attached plans.

Comment 5– Mandatory Findings of Significance

Response - The City believes that the Initial Study findings support the conclusion that the proposed Project will have no adverse environmental impacts. The Project will not encroach into the creek and therefore will not have significant environmental impacts. Construction on an existing hard-packed gravel site, and removal of a few small landscaping trees well away from the creek are not seen as significant impacts. In addition, as discussed above, the improvements in runoff and water quality that will result from the project are likely to provide an overall improvement in Sulphur Creek water quality.

A separate discussion with CDFW will determine whether the proposed creek-spanning bridge will require a 1602 permit.

Sincerely,



Mojad Fakhrai PE

Director of Public Works – Engineering and Transportation

Enc.

DEPARTMENT OF PUBLIC WORKS
ENGINEERING & TRANSPORTATION

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TEL: 510/583-4730 • FAX: 510/583-3620 • TDD: 510/247-3340

EDMUND G. BROWN JR.
GOVERNORMATTHEW RODRIGUEZ
SECRETARY FOR
ENVIRONMENTAL PROTECTION

San Francisco Bay Regional Water Quality Control Board

February 19, 2013
CIWQS Place No. 231404

Sent via electronic mail: No hardcopy to follow

City of Hayward
Engineering and Transportation Division
777 B Street
Hayward, CA 94541

Attn: Luis Samayoa (Luis.samayoa@hayward-ca.gov)

Subject: Initial Study and Proposed Mitigated Negative Declaration for the Hayward Executive Airport Administration Building
SCH No. 2013012050

Dear Mr. Samayoa:

San Francisco Bay Regional Water Quality Control Board (Water Board) staff have reviewed the *Initial Study / Mitigated Negative Declaration (ISMND) for the Hayward Executive Airport Administration Building*. The proposed Project will construct a new administration building and parking lot at the Hayward Executive Airport in the City of Hayward, in Alameda County. Water Board staff have the following comments on the ISMND.

Based on the limited information provided in the ISMND, it is not clear whether or not any of the channel reconfigurations and riparian vegetation Water Board required as riparian mitigation for two prior City of Hayward projects may be impacted by the proposed Project. In addition, constructing a new pedestrian bridge across a Water Board required mitigation reach of Sulphur Creek may undermine the required mitigation and may be inconsistent with the conditions of certification for the prior City of Hayward projects.

Furthermore, Water Board staff have concerns about the status of the required mitigation for the two prior Water Board certified City projects. We do not appear to have been provided with documentation that the Cannery Area day-lighting was implemented. At the airport, the banks of Sulphur Creek were to have been "regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1". Based on monitoring reports submitted to the Water Board, it appears that the City filled in the thalweg of Sulphur Creek to provide gentler bank slopes. The placement of fill in the thalweg of the channel constituted unauthorized fill of a water of the State. Filling the thalweg is clearly not what was certified by the prior water quality certifications, and the Water Board staff are concerned that the fill placed in the channel thalweg is very likely to be washed away by high flow events, which would re-create the overly steepened channel slopes along Sulphur Creek. Also, we are concerned that the City's flood control maintenance projects may have removed some of the required mitigation vegetation.

 JOHN MULLER, CHAIR | BRUCE H. WOLFE, EXECUTIVE OFFICER

 1515 Clay St., Suite 1400, Oakland, CA 94612 | www.waterboards.ca.gov/sanfranciscobay

The ISMND has to evaluate the consistency of the proposed Project with the required mitigation projects along Sulphur Creek at the airport to be considered complete before City Council should consider for adoption.

Comment 1, Timing of CEQA Clearinghouse Compliance and the Proposed Adoption of the ISMND by the City of Hayward City Council

According to the Notice of Completion & Environmental Document Transmittal, which was provided to the Water Board by the State Clearinghouse, the State Clearinghouse Compliance date for the end of the comment period is February 20, 2013. However, the Hayward City Council has proposed to adopt the ISMND at their meeting on February 19, 2013. It is not clear why the City is proposing to adopt the ISMND before the end of the State Clearinghouse comment period.

Comment 2, Biological Resources

The new Administration Building will be constructed adjacent to Sulphur Creek and the proposed new pedestrian bridge will cross Sulphur Creek. The reach of Sulphur Creek in the vicinity of the existing control tower was used to provide riparian mitigation for two previous City of Hayward Projects: the West A Street Realignment Project (Water Board Site No. 02-01-0861; U.S.A.C.E File No. File No. 28665S) and the Cannery Area Public Improvement Project (Water Board Site No. 02-01-C0880; U.S.A.C.E File No. File No. 29782S).

Mitigation for West A Street included:

Onsite mitigation will enhance and improve 89 linear feet of Line K-1. About 58 linear feet of Line K-1 that is currently enclosed in a 65-inch by 40-inch corrugated metal arch culvert will be daylighted. The daylighted segment of the channel, along with an additional 31 linear feet segment of the channel that is currently open, will be enhanced by widening the channel bottom and planting the channel banks with native trees, shrubs, and grasses.

Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 570 linear feet are to be provided as mitigation for the West A Street Project, while the remaining portion of this reach will be used as mitigation for impacts to Sulfur Creek at the Cannery Area Project (Site No. 02-01-C0880). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the mitigation reach of Sulphur Creek. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek mitigation is included as Figure 6 of the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006).

Mitigation for the Cannery Area included:

At the Cannery Area, a 20-foot segment and a 39-foot segment of existing enclosed creek channel will be daylighted and about 60 linear feet of an existing open, concrete-lined segment of the creek will be converted to an earthen channel. The channel in these areas will be widened, and meanders will be designed to improve the hydrologic and hydraulic function of the channel. The bank will be expanded and vegetated with an appropriate assemblage of native riparian species to improve habitat value and to shade the channel to reduce the growth of emergent wetland vegetation, which can reduce flow capacity.

Offsite mitigation will be performed along a 1,423-foot long reach of Sulphur Creek at the Hayward Executive Airport. Of this reach, 850 linear feet are to be provided as mitigation for the Cannery Area Project, while the remaining portion of this reach will be used as mitigation for impacts to Line K-1 at the West A Street Realignment Project (Site No. 02-01-C0861). This reach of Sulphur creek will be reconfigured to provide a more meandering low flow channel and more stable bank slopes. Native riparian vegetation will be planted along the re-contoured banks and exotic species will be removed to provide better quality riparian habitat along Sulphur Creek.

Significant erosion is present along the offsite mitigation reach of Sulphur Creek at the Hayward Executive Airport. To establish stable channel banks, the channel will be regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1. Where practicable, slopes may be decreased to a minimum slope of 4:1. The stabilized slopes will be planted with native vegetation. In addition, to the extent practicable, small meanders will be added to the channel. Areas adjacent to the creek that are available for mitigation may also be graded down to near floodplain elevation to allow for regular ponding between flow events and to allow for a variety of elevations on which plantings may succeed. A conceptual plan for the Sulphur Creek mitigation is included as Figure 6 of the Sulphur Creek Mitigation and Monitoring Plan, Hayward, Alameda County (WRA Environmental Consultants, June 6, 2006). In addition, because the site layout at the Cannery Area does not provide opportunities for 1:1 in-kind mitigation for the culverting of Sulfur Creek, the Applicant will provide enhancement of downstream water quality in Sulfur Creek by providing Clean Water Act maximum extent practicable (MEP) treatment for stormwater runoff from the new Burbank School and impermeable areas of Cannery Park. Prior to constructing improvements at Cannery Park and prior to constructing the new Burbank School, the Applicant shall submit proposed plans with landscape-based stormwater treatment best management practices (BMPs) to the Executive Officer of the Regional Water Quality Control Board for review and approval. Landscape-based BMPs shall be consistent with the design standards in Provision C.3 of the Alameda County Clean Water Program's National Pollutant Discharge Elimination System (NPDES) Municipal Stormwater Permit (Order R2-2003-0021; NPDES Permit No. CAS0029831).

Water Board staff have concerns about the status of the required mitigation for the two prior City projects. We do not appear to have been provided with documentation that the Cannery Area day-lighting was implemented. At the airport, the banks of Sulphur Creek were to have been " regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1". Based on monitoring reports submitted to the Water Board, it appears that the City filled in the thalweg of Sulphur Creek to provide gentler bank slopes. The

placement of fill in the thalweg of the channel constituted unauthorized fill of a water of the State. Filling the thalweg is clearly not what was certified by the prior water quality certifications, and the Water Board staff are concerned that the fill placed in the channel thalweg is very likely to be washed away by high flow events, which would re-create the overly steepened channel slopes along Sulphur Creek. Also, we are concerned that the City's flood control maintenance projects may have removed some of the required mitigation vegetation. Water Board staff will need to visit the airport in the near future to determine whether or not the City is in compliance with the prior conditions of certification. The City may be required to expand the top-of-bank area along Sulphur Creek to achieve compliance with the requirement to have the creek banks "regraded by pulling the top of the streambanks back from the channel centerline to achieve a maximum slope of 2.5:1". The proposed Project should leave adequate setbacks from the existing top-of-bank to allow the City to take appropriate actions to return to compliance with the requirements of the prior mitigation projects.

Comment 3, Potential Impacts to a Tributary Channel

The annotated aerial photograph of the proposed Project site appears to indicate that a tributary channel to Sulphur Creek may be present in the footprint of the new Administration Building. If a tributary channel is present, mitigation will be required for placing a building on top of it, and the ISMND should have identified this potential impact and proposed mitigation.

Comment 4, Hydrology and Water Quality,

The proposed Project will be required to comply with the post-construction stormwater treatment requirements of the National Pollutant Discharge Elimination System (NPDES) Municipal Regional Permit (MRP) for the management of stormwater runoff (Order R2-2009-0074; NPDES Permit No. CAS612008). The ISMND does not describe how the proposed Project will comply with the MRP, or if new outfalls to Sulphur Creek will be needed (Note: New outfalls to Sulphur Creek at this location may not be consistent with the mitigation project along this reach of the creek). The ISMND should have demonstrated that sufficient surface area has been set aside at the airport to provide the required treatment measures and discussed the potential need for new outfalls.

Comment 5, Sufficiency of ISMND to Support Future Permits

As is noted above, the ISMND does not demonstrate that impacts associated with the proposed Project have been correctly identified. The ISMND also lacks mitigation proposals for these potential impacts. Unless these omissions are corrected, the ISMND may not be sufficient to support the issuance of any permits needed for the proposed Project, including the construction of the new pedestrian bridge.

Prior to taking any action in the vicinity of Sulphur Creek, the City of Hayward must first take concrete steps to come into full compliance with the previous water quality certifications that required mitigation along Sulphur Creek.

Please contact Brian Wines at (510) 622-5680 or bwines@waterboards.ca.gov if you have any questions. All future correspondence regarding this Project should reference the Site Number indicated at the top of this letter.

Sincerely,



Shin-Roei Lee

2013.02.19

15:00:02 -08'00'

Shin-Roei Lee
Division Chief
Watershed Division

cc: CDFG, Bay Delta Region, Attn: Marcia Grefsrud (mgregsrud@dfg.ca.gov)
USACE, Cameron Johnson (Cameron.L.Johnson@usace.army.mil)
State Clearinghouse (state.clearinghouse@opr.ca.gov)



Sent via electronic mail: Hardcopy to follow

February 25, 2013

Ms. Shin-Roei Lee, Division Chief
 Watershed Division
 S.F. Bay Regional Water Quality Control Board
 1515 Clay Street, Suite 1400
 Oakland, California 94612

Subject: Initial Study and Proposed Mitigated Negative Declaration for the Hayward Executive Airport Administration Building
 SCH No. 2013012050

Dear Ms. Lee:

Thank you for your comment letter dated February 19, 2013, which pertained to the City's proposed new Administration Building and parking lot at the Hayward Executive Airport (the "Project"). We will respond to each comment individually below, but, as a preliminary matter, we want to assure you that neither the building nor the associated pedestrian bridge will impact the Sulphur Creek channel improvements and associated riparian plantings implemented as mitigation in 2009 (the "Adjacent Mitigation Projects"). In addition, our review of the September 10, 2008 Section 401 Water Quality Certification (Site No. 02-01-1027) finds that no restrictions were placed on bridging the restored creek channel.

At this time our response will focus on your concerns about the proposed administrative building project. We would like to discuss your questions about the adjacent mitigation site in a separate forum, either in person or on the phone later next month. We agree that the Project must be entirely consistent with the requirements and goals of the Adjacent Mitigation Projects.

Comment 1 – Timing of CEQA process

Response – The timing of the CEQA process on the Hayward City Council agenda was an unintentional oversight. The Notice of Completion & Environmental Document Transmittal for the Project Negative Declaration was submitted to the State Clearinghouse on January 17, 2013. The error we made was using this date in calculating the 30-day notice requirement instead of the date the Clearinghouse circulated the document to the agencies. The Hayward City Council's consideration of the Project and the environmental document has been postponed to March 5, 2013, to allow concerned agencies time to comment on the document. Staff would appreciate getting your comments no later than close of business on Wednesday, February 27, which will allow us enough time to include your comments to the report to Council.

**DEPARTMENT OF PUBLIC WORKS
 ENGINEERING & TRANSPORTATION**

777 B STREET, HAYWARD, CA 94541-5007
 TEL: 510/583-4730 • FAX: 510/583-3620 • TDD: 510/247-3340

Comment 2 – Biological Resources

Response – As stated above, we would like to have a more in-depth discussion, and provide a more detailed response on the Adjacent Mitigation Projects, which are unrelated to the proposed Project, in a separate forum. However, a brief response to the key points made in your final Comment 2 paragraph is provided below.

- The Cannery Area creek day-lighting was completed as planned.
- The banks of Sulphur Creek were pulled back to 2.5:1 per the construction documents and no channel filling occurred.
- City flood control maintenance has included trimming of some of the willow shrubs planted for mitigation to improve sight lines and channel flow; in addition non-native cattails have been removed from the channel bottom.
- The proposed project's building is set back from the top of bank and all riparian vegetation by a minimum of 15 feet on the south side to a maximum of 32 feet on the north side of the future building.

Since the proposed Project is unrelated to, and will not impact the Adjacent Mitigation Projects, a detailed discussion of these mitigation projects were not included in the Airport Administration Building Initial Study/Negative Declaration ("ISND").

Comment 3 – Potential Impacts to a Tributary Channel

Response – There is not a tributary to Sulphur Creek present in the footprint of the proposed building or elsewhere on the Project site. The Sulphur Creek Enhancement Project that was completed in 2009 included a cribwall outfall to receive the small amount of runoff from the existing empty lot (site of new building). The new Project will divert this runoff to a bio-retention system and then to the existing storm drain system. The existing tributary is south of the existing control tower building and will not be affected. Therefore there was no consideration or discussion of this topic in the ISND.

Comment 4 – Hydrology and Water Quality

Response - The Initial Study states "[i]t will be required that the proposed drainage design shall be treated to meet the Alameda County Flood Control and Water Conservation District's C-3 requirements before entering an existing drainage facility; therefore, no impact." The existing site is currently covered with very well-compacted dirt with gravel on top. Under current site conditions, rain water does not seep into the ground area; it drains directly over the top of bank into the Sulphur Creek area or through the cribwall mentioned in Comment 3. The Project proposes bio-retention storm water facilities in three separate locations within the site to address the increase in developed areas. The drainage system will not drain back into the creek as currently occurs; instead, the landscaped areas between the proposed Airport Administration Building and the creek bank will be graded so that drainage runoff will be directed toward the building and into the aforementioned bio-retention facilities. After collection of storm water runoff and treatment in these facilities, the treated water will be directed to the *existing storm drain system* that eventually drains to the creek. The need for a new or reconstructed outfall is not anticipated.

By filtering rain water and associated runoff through the bio-retention areas the Project infrastructure will be able to catch many of the pollutants that derive from the runoff before they

enter the creek, unlike now where the untreated runoff sheets directly across a compacted gravel area into the creek. Also, the volume of water entering the creek will be reduced because the bio-retention facility will enable greater volumes of water to infiltrate into the ground. Please see attached plans.

Comment 5– Sufficiency of ISND to Support Future Permits

Response - The City believes that the Initial Study findings support the conclusion that the proposed Project will have no adverse environmental impacts. The Project will not encroach into the creek and it was therefore concluded that the Project will not have significant environmental impacts. Construction on an existing hard-packed gravel site, and removal of a few small landscaping trees well away from the creek were not seen as significant impacts. In addition, as discussed above, the improvements in runoff water quality that will result from the project are likely to provide an overall increase in Sulphur Creek water quality.

Department of Fish & Games' Issues

Additionally, the California Department of Fish and Wildlife (CDFW) has expressed concern about the removal of trees and its effect on nesting birds. Eleven (11) trees located in the future parking lot area are to be removed. These trees have diameters ranging in size from 3 to 4 inches. Of the trees to be removed, only three (3) are in good health. No trees exist on the site of the proposed building itself. The project will replace the eleven trees removed with 20 new trees: eight (8) Japanese Maples, eight (8) Windmill Palms, four (4) Fruitless Chinese Pistachios; and a large amount of shrubs and groundcovers. The removal of the trees will be done during the winter season (October 15 through February), which is outside the nesting period in California, which is approximately from March through August.

A separate discussion with CDFW will determine whether the proposed creek-spanning bridge will require a 1602 permit.

Sincerely,



Morad Fakhrai PE
Director of Public Works – Engineering and Transportation

Enc.

Cc: Water Board, Brian Wines, (bwines@waterboards.ca.gov)
CDFG, Bay Delta Region, Marcia Grefsrud (mgrefsrud@dfg.ca.gov)
USACE, Cameron Johnson (Cameron.L.Johnson@usace.army.mil)
State Clearinghouse (state.clearinghouse@opr.ca.gov)



State of California – The Natural Resources Agency
 DEPARTMENT OF FISH AND WILDLIFE
 Bay Delta Region
 7329 Silverado Trail
 Napa, CA 94558
 (707) 944-5500
www.wildlife.ca.gov

EDMUND G. BROWN JR., Governor
 CHARLTON H. BONHAM, Director



February 22, 2013

Mr. Luis Samayoa
 Project Manager
 City of Hayward, Engineering and Transportation
 777 B Street
 Hayward, CA 94541-5007

Dear Mr. Samayoa:

Subject: Hayward Executive Airport Administration Building, Initial Study/Negative Declaration, SCH #2013012050, City of Hayward, Alameda County

The California Department of Fish and Wildlife (CDFW) has reviewed the subject document prepared for the Hayward Executive Airport Administration Building (Project). The Project is located at 20301 Skywest Drive, in the City of Hayward, Alameda County. The Project includes construction of a one-story administration building, parking for personnel, pedestrian bridge to access runway, utility and trash enclosures, site lighting, landscaping and irrigation, and a parking lot.

The Project description does not provide adequate details, such as the size of the building or construction area or the existing conditions, to allow for CDFW to assess the biological impacts of the Project. Section 15063 (a)(1) of the California Environmental Quality Act (CEQA) Guidelines states "All phases of project planning, implementation, and operation must be considered in the Initial Study of the project." The Initial Study should include a description of the environmental setting and potential environmental impacts with a brief explanation to support findings.

The Initial Study checklist, Biological Resources, IV(a) states "The Project will not have any adverse effect on biological resources, therefore, no impact." The Initial Study fails to disclose the Project location in relation to Sulphur Creek, which flows through the Hayward Executive Airport. CDFW recommends that a set-back buffer be established for some creeks measuring from the top of the stream bank or riparian canopy. No construction, including roads, should be allowed within the buffer area to provide adequate protection of the resources and to minimize the need for future maintenance and bank armoring in the channel. Many negative impacts to creek systems are associated with attempts to stabilize creek banks that are failing beneath poorly located structures. For example, a structure placed too close to the top of bank, or even below the top of bank, may become threatened by natural erosion of the creek bank, as the centerline of the creek meanders within the channel. All wetlands and watercourses, whether intermittent or perennial, should be retained and provided with substantial setbacks to preserve the riparian and aquatic values and maintain their value to on-site and off-site fish and wildlife.

Mr. Luis Samayoa
February 22, 2013
Page 2

For any activity that will divert or obstruct the natural flow, or change the bed, channel, or bank (which may include associated riparian resources) of a river or stream, or use material from a streambed, CDFW may require a Lake and Streambed Alteration Agreement (LSAA), pursuant to Section 1600 et seq. of the Fish and Game Code. Issuance of an LSAA is subject to CEQA. The CDFW, as a responsible agency under CEQA, will consider the local jurisdiction's (lead agency) Negative Declaration or Environmental Impact Report for the Project. The CEQA document should fully identify the potential impacts to the stream or riparian resources and provide adequate avoidance, mitigation, monitoring and reporting commitments for completion of the agreement.

The Initial Study, Biological Resources, IV(e) implies that some trees will be removed, but does not disclose how many, their size, species, or location. CDFW recommends that for each native tree that is removed or destroyed, trees shall be replaced with native trees on-site at a minimum 3:1 ratio (replacement:loss). For each non-native tree that is removed or destroyed, trees should be replaced with native trees on-site at a minimum 1:1 ratio (replacement:loss). Impacts to nesting birds should be avoided by scheduling construction and tree removal activities outside of the nesting season.

If mitigation is required for the Project, it should be approved by CDFW and be of sufficient quality and quantity to offset the impacts.

CDFW is unable to determine if the Project will not have a significant impact on the environment. CDFW recommends that the Initial Study be recirculated after a thorough Project description and, if necessary, a complete environmental analysis has been completed.

We are further available to discuss our concerns, if requested. If you have any questions, please call Ms. Marcia Grefsrud, Environmental Scientist, at (707) 644-2812; or Mr. Craig Weightman, Acting Environmental Program Manager, at (707) 944-5577.

Sincerely,



Scott Wilson
Acting Regional Manager
Bay Delta Region

cc: State Clearinghouse



Sent via electronic mail: Hardcopy to follow

Mr. Scott Wilson, Acting Regional Manager
Bay Delta Region
California Department of Fish and Wildlife
1515 Clay Street, Suite 1400
Oakland, California 94612

February 25, 2013

Dear Mr. Wilson:

Thank you for your comment letter dated February 22, 2013, which pertained to the City's proposed new Administration Building and parking lot at the Hayward Executive Airport (the "Project"). We will respond to each comment individually below, but, as a preliminary matter, we want to assure you that neither the building nor the associated pedestrian bridge will impact the Sulphur Creek channel improvements and associated riparian plantings implemented as mitigation in 2009. In addition, our review of the June 21, 2006, Notification Number 1600-2005-0636-3 finds that no restrictions were placed on bridging the restored creek channel but we would like to discuss with CDFW to determine whether the proposed creek-spanning bridge will require a 1602 permit.

At this time our response will focus on your concerns about the proposed administrative building project. We agree that the Project must be entirely consistent with the requirements and goals of the Adjacent Mitigation Projects.

Comment 1 – The Project description does not provide adequate details.

Response – We are attaching the most relevant project plans that provide a better description of the project.

Comment 2 – Biological Resources, IV(a)

Response – The drawing titled Proposed Building and Parking Lot Location shows the Project location in relation to Sulphur Creek.

DEPARTMENT OF PUBLIC WORKS
ENGINEERING & TRANSPORTATION
777 B STREET, HAYWARD, CA 94541-5007
TEL: 510/583-4730 • FAX: 510/583-3620 • TDD: 510/247-3340

Comment 3 – Set-back buffer

Response – The drawing title Planting Plans shows a landscaped buffer between the proposed building and the top of bank that range from 15 to 32 feet..

Comment 4 – Lake and Streambed Alteration Agreement

Response - The City does not believe there is a need for an alteration agreement since the Project will not divert or obstruct the natural flow, or change the bed, channel, or bank of the Sulphur Creek. No activity is planned within the creek.

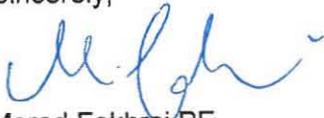
Comment 5– Removal of Trees

Response - Eleven (11) non-native trees located in the future parking lot area are to be removed. These trees have diameters ranging in size from 3 to 4 inches. Of the trees to be removed, only three (3) are in good health. No trees exist on the site of the proposed building itself. The project will replace the eleven trees removed with 20 new trees: eight (8) Japanese Maples, eight (8) Windmill Palms, four (4) Fruitless Chinese Pistachios; and a large amount of shrubs and groundcovers.

Comment 6– Nesting Birds

Response – The project will not impact nesting birds. The removal of the trees will be done during the winter season (October 15 through February), which is outside the nesting period in California, which is approximately from March through August.

Sincerely,



Morad Fakhrai PE
Director of Public Works – Engineering and Transportation

Enc.

Cc: CDFG, Bay Delta Region, Marcia Grefsrud (mgrefsrud@dfg.ca.gov)
Water Board, Brian Wines, (bwines@waterboards.ca.gov)
USACE, Cameron Johnson (Cameron.L.Johnson@usace.army.mil)
State Clearinghouse (state.clearinghouse@opr.ca.gov)

**DEPARTMENT OF PUBLIC WORKS
ENGINEERING & TRANSPORTATION**

777 B STREET, HAYWARD, CA 94541-5007
TEL: 510/583-4730 • FAX: 510/583-3620 • TDD: 510/247-3340

DATE: March 19, 2013

TO: Mayor and City Council

FROM: Director of Development Services

SUBJECT: Approve the City Manager’s Action to Execute a Contract Amendment with Metropolitan Planning Group to Provide Temporary Planning Services through End of March of 2013

RECOMMENDATION

That Council adopts the attached resolution (Attachment I) approving the City Manager’s action to amend the contract with Metropolitan Planning Group to increase the originally approved \$25,000 contract by up to \$10,000 for the provision of additional emergency temporary planning services through the end of March of 2013.

BACKGROUND

The Planning Division over the course of the last several months has had a severe shortage of staff as described below.

Associate Planner	Previous planner unexpectedly passed away in mid-October, 2012	New Associate Planner scheduled to start March 25
Senior Planner (current planning)	Was on maternity leave since September 21, 2012	Returned to work on March 5 and is now lead on the General Plan Update project
Associate Planner	Was on medical leave since November 30, 2012	Returned to work on March 6
Senior Planner (advanced planning)	Previous Senior Planner is now City’s new Environmental Services Manager in the Public Works – Utilities & Environmental Services Department, effective March 11	Recruitment is ongoing, with new Senior Planner expected to start in mid to late April
Planning Manager	Previous Planning Manager has retired effective 12-31-12 and is on contract until new Planning Manager is on board	New Planning Manager is expected to start in mid-April

DISCUSSION

In response to the staffing shortage, the City hired part-time temporary planners from three firms and through a temporary employment agency to help with the functions and daily operations of the Planning Division. One of the temporary planners is from the firm, Metropolitan Planning Group, who works three days a week. Because of the unexpected delay in the Associate Planner returning to work from medical leave, and because of the successful application of the former Senior Planner who was appointed to Environmental Services Manager, the City Manager approved an additional \$10,000 for the planner from the Metropolitan Planning Group to provide temporary planning services through the end of March. The staffing need was critical and timing on the projects did not allow time for the normal procedure of seeking Council authorization prior to executing the amendment.

ECONOMIC IMPACT

No economic impacts are anticipated, other than allowing the Planning Division to continue with providing municipal planning services to residents, business owners, and potential developers in Hayward.

FISCAL IMPACT

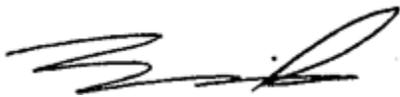
The \$10,000 contract amendment does not require any additional appropriation and will be funded out of the existing departmental budget.

NEXT STEPS

If approved, the temporary planner will work through the end of March.

Prepared and Recommended by: David Rizk, AICP, Development Services Director

Approved by:



Fran David, City Manager

Attachments:

Attachment I Resolution

HAYWARD CITY COUNCIL

RESOLUTION NO. 12-

Introduced by Council Member _____

RESOLUTION APPROVING THE CITY MANAGER’S ACTION TO EXECUTE A CONTRACT AMENDMENT WITH METROPOLITAN PLANNING GROUP TO PROVIDE TEMPORARY PLANNING SERVICES THROUGH END OF MARCH OF 2013

BE IT RESOLVED by the City Council of the City of Hayward that the City Manager’s action to execute a contract amendment with Metropolitan Planning Group to provide temporary planning services through the end of March of 2013 is approved, in an additional amount not to exceed \$10,000 above the originally approved amount of \$25,000 for such services, in a form to be approved by the City Attorney.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS:
MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATTEST: _____
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

DATE: March 19, 2013
TO: Mayor and City Council
FROM: City Manager
SUBJECT: Economic Development Strategic Plan Adoption

RECOMMENDATION

That the City Council adopts the attached resolution finding the Economic Development Strategic Plan (the Plan) exempt from CEQA review, approving the Plan, and appropriating General Fund funding in the amount of \$500,000 for FY 2014 for the first phase of the Plan.

SUMMARY

The purpose of this report is to present the final Economic Development Strategic Plan, which is provided as Attachment II, and the associated resolution to approve the Plan and the budget appropriations to fund the first phase of the Plan, which is Attachment I.

Once adopted, the Plan will be used as a strong, dynamic basis for implementing active and successful economic development activities for Hayward over the next five years. The proposed funding and staffing for the plan supports the identified work effort in a reasonable and thoughtful manner.

BACKGROUND

An integrated City staff team (the Team) was formed in August 2012 to work with the Council Economic Development Committee (CEDC) and key stakeholders in the community to develop an Economic Development Strategic Plan to present to Council. Staff met with the CEDC eight times over six months to seek feedback on all major areas of the planning process, including stakeholder engagement, the Vision and Mission Statement, Strategic Focus Areas, and key geographic areas.

Staff presented a final draft of the Plan to the City Council in a work session on February 26, 2013. At the work session, Councilmembers expressed general support for the draft Plan, as well as appreciation for the effort that the CEDC and staff have put into the Plan's development. Staff returned to the CEDC on March 4, 2013 to give an overview of Council's feedback (see Attachment III for the staff report to the CEDC).

In addition to feedback on the draft Plan, staff sought Council feedback on uses for two catalyst sites that fall within the boundaries of the draft Mission Boulevard Corridor Specific Plan (MBCSP). At the work session, Councilmembers expressed support for a commercial overlay for the two catalyst sites. Council raised a few of concerns about the changes proposed to Table 9 of the MBCSP, specifically around hospitals and dance/nightclubs. Planning staff will return to Council to discuss the MBCSP further in a work session later this spring.

DISCUSSION

Changes made to the final Plan

The majority of the Plan has not been changed since it was last presented to Council at the February 26, 2013 work session. Staff made some modifications and additions to the Plan based on Councilmember feedback, which are reflected in Attachment IV. These include five new work tasks, two new metrics, and an additional opportunity site in the industrial area.

Program funding

At the work session, Council requested that staff bring the funding and staffing model to the Council along with the Plan adoption in order to consider the funding the first phase of the Plan. The resolution in Attachment I appropriates funding necessary to support the staffing and funding model identified in the February 26, 2013 staff report to Council. The recommended funding strategy includes the use of FY 2014 property tax increment revenue that will be redistributed to the City as a result of the dissolution of the former Hayward Redevelopment Agency. The initial projection of the redistributed tax increment revenue was \$350,000 annually, which has been verified as described below.

Recently, staff received more current data from the County Auditor Controller regarding the amount of property tax dollars redistributed to the City and other taxing entities during the first eighteen months following dissolution of the Redevelopment Agency (January 2012 to June 2013). Staff presented this data at a recent Oversight Board meeting and there was a question from Board members as to whether the \$350,000 annual revenue projection in the Strategic Plan would actually materialize based on this data.

Staff revisited the original projections and incorporated the recently received data from the County (Attachment V). The revised analysis not only confirms the original projection of an annual receipt of \$350,000 in redistributed tax increment, but concludes that this may be a conservative projection. Table 1 summarizes the projected range of annual redistributed increment revenue for the five year timeframe of the Economic Development Strategic Plan.

Table 1

Redevelopment Property Tax Trust Fund Allocations (RPTTF)	FY 2014	FY2015	FY2016	FY2017	FY2018
RPTTF Distribution to All Taxing Entities	3,360,870	2,431,075	2,511,741	2,596,616	2,683,349
RPTTF Hayward Share (17.6%) <i>*Current Projection</i>	591,513	427,869	442,066	457,004	472,270
Typical City Share (15%)	504,130	364,661	376,761	389,492	402,502

Staff has made two assumptions with respect to the amount of property tax the City can expect to receive from the dissolution of the former RDA.

1. For the past eighteen months, the County Auditor Controller has redistributed about 17.6% of the residual amount to the City. The numbers on the second line of Table 1 (RPTTF Hayward share) utilize this percentage for the projections going forward.
2. The third line of the table (Typical City share) reflects the residual amounts the City could expect to receive if the County Auditor Controller changed the formula to the property tax allocation percentage that the City typically receives (15%). Staff included this more conservative percentage for comparison purposes only. As more obligations of the former Redevelopment Agency are paid off and retired, these annual revenues will continue to increase.

In addition to the appropriation of anticipated RPTTF, staff is asking for an additional appropriation of \$150,000 in FY 2014 (and \$120,000 in FY 2015 and \$90,000 in FY 2016) for program start-up costs. While this is a temporary allocation from the General Fund, staff is projecting that actual RPTTF revenues received by the City in excess of the initial projection of \$350,000 in FY 2014 and beyond, will fund all or part of these start-up costs.

PUBLIC OUTREACH

Staff collected stakeholder feedback throughout the development of the Plan. With input from the CEDC, staff identified a list of key stakeholders to include in the planning process. The list was comprised of business owners in the industrial and retail sector, the development community, workforce development organizations, community groups, and other jurisdictions.

ENVIRONMENTAL IMPACT

Adoption of the Plan will not have a binding effect on future City actions and is statutorily exempt from review under the California Environmental Quality Act (CEQA) guidelines, Section 15262, Feasibility and Planning Studies.

FISCAL IMPACT

The total additional appropriation being requested as part of this action for FY2014 is \$500,000. As stated, at a minimum, \$350,000 of the cost of the plan will be offset by the redistribution of

property tax increment due to the dissolution of the former Hayward Redevelopment Agency. In addition, the program requires the allocation of an additional \$150,000 from the General Fund in FY 2014 for program start-up costs – for a total FY 2014 program budget allocation of \$500,000.

By year five of the plan, staff is still projecting revenue generation of \$2 million to \$5.4 million annually if optimal uses are secured for the Opportunity and Catalyst sites. This is only one of many ways that increased revenue will be realized through the efforts of the Plan. Staff will revisit the actual generation of revenues as part of the FY2014 mid-year budget review and report to the Council accordingly.

Adoption of the Plan will not have a binding effect on the Council’s future budget decisions. If at any point in the five years of the Plan the General Fund is negatively impacted by unforeseen circumstances, Council has the authority to defund the Plan so those funds can be used elsewhere.

ECONOMIC IMPACT

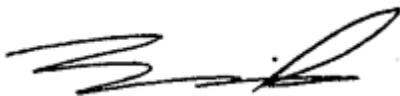
As the economy continues to improve in the coming years, it is essential that Hayward is recognized for its assets and is known as a welcoming and effective city in which to conduct business. The Economic Development Strategic Plan is a purposeful, concentrated effort to achieve this vision with an efficient use of the City’s resources. The Plan’s activities, including branding, site-specific marketing, business support and ombudsman activities in the industrial and retail areas will result in strategic projects that are intended to have a catalytic effect on Hayward’s overall economy and the City’s revenues. At the same time, the workforce development partnerships and the community events will help to ensure that economic growth is leveraged to create opportunities and lifestyle improvements for Hayward’s residents.

NEXT STEPS

Once the Plan is adopted and funds are appropriated, staff will begin recruitment for the Economic Development Specialists and the administrative support position. The new Economic Development Manager, who is expected to be hired within the next month, will be staff lead in reporting regularly on the Plan’s progress to the CEDC and Council.

Prepared by: Mary Thomas, Management Fellow
Fran Robustelli, Economic Development Project Team Leader
Kelly McAdoo, Assistant City Manager
Tracy Vesely, Director of Finance

Approved by:



Fran David, City Manager

Attachments:

- Attachment I: Resolution Approving the Economic Development Strategic Plan
- Attachment II: Economic Development Strategic Plan
- Attachment III: March 4, 2013 Staff Report to the CEDC: Council Feedback on the Draft Strategic Plan from the February 26, 2013 Council Work Session
- Attachment IV: Changes to the Economic Development Strategic Plan based on Council Feedback
- Attachment V: Analysis of Redistributed Redevelopment Property Tax Dollars

HAYWARD CITY COUNCIL

RESOLUTION NO. 13-

Introduced by Council Member _____

RESOLUTION FINDING THE ECONOMIC DEVELOPMENT STRATEGIC PLAN EXEMPT FROM CEQA REVIEW, APPROVING THE PLAN AND APPROPRIATING FUNDING FOR IMPLEMENTATION OF THE PLAN

WHEREAS, it is essential that Hayward is recognized for its assets and is known as a welcoming and receptive city in which to conduct business as the economy continues to improve in the coming years; and

WHEREAS, an integrated City staff team was formed in August 2012 to work with the Council Economic Development Committee (CEDC) and key stakeholders in the community to develop an Economic Development Strategic Plan (EDSP) to present to the City Council; and

WHEREAS, staff met with the CEDC eight times over six months to seek feedback on all major areas of the planning process, and the CEDC has recommended that the City Council approve the EDSP, which covers FY 2014- FY 2018; and

WHEREAS, the City Council reviewed and provided comment on the EDSP at a duly noticed Council work session held on February 26, 2013; and

WHEREAS, the City Council has considered the staffing and funding models of the EDSP and has determined that allocating funds towards the EDSP is in the best interests of the City; and

WHEREAS, the Economic Development Strategic Plan is categorically exempt from the requirements of the California Environmental Quality Act (CEQA), pursuant to Section 15262 of the CEQA Guidelines, Feasibility and Planning Studies.

NOW, THEREFORE, BE IT RESOLVED that the City Council finds that the Economic Development Strategic Plan is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15262 of the CEQA Guidelines, Feasibility and Planning Studies, and hereby approves the Economic Development Strategic Plan for FY 2014-FY 2018 and its associated goals and activities.

BE IT FURTHER RESOLVED that the City Council authorizes the appropriation of \$500,000 from the General Fund as part of the FY 2014 operating budget for the Economic Development Program in the Office of the City Manager, which appropriation will be used to pay for operating costs associated with the first year work tasks in the EDSP.

IN COUNCIL, HAYWARD, CALIFORNIA _____, 2013

ADOPTED BY THE FOLLOWING VOTE:

AYES: COUNCIL MEMBERS:

MAYOR:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ATTEST:
City Clerk of the City of Hayward

APPROVED AS TO FORM:

City Attorney of the City of Hayward

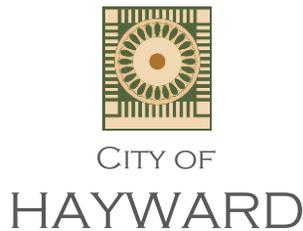


CITY OF
HAYWARD



Economic Development Strategic Plan

FY 2014 – FY 2018



A Message from the City Manager

Economic development is more than just infrastructure, jobs, and buildings. It is a pathway to self-sufficient neighborhoods and a fiscally sound City. As the economy continues to improve in the coming years, it is essential that Hayward is recognized for its assets and is known as a welcoming and effective city in which to conduct business, where staff understands and respects the critical business elements of time and certainty.



This Economic Development Strategic Plan is a purposeful, concentrated effort to achieve this vision with a focused use of the City's resources. The Plan's activities will result in strategic projects that are intended to have a catalytic effect on Hayward's overall economy and City revenues. At the same time, the workforce development partnerships and the community events will help to ensure that economic growth is leveraged to create opportunities and lifestyle improvements for Hayward's residents, and to develop good-paying jobs available to a well-prepared and educated workforce.

Economic Development is the responsibility of all Hayward staff. In order to have the strong presence needed to leverage the economic rebound, the City's personnel must include an economic development perspective in all that we do, and be willing to proactively coordinate efforts efficiently and effectively across departments. The Plan's staffing and funding model is a cost-effective and reasonable approach that will result in maximize economic return to the community and the municipal organization.

We look forward to working with all of our partners to make this Plan a reality, and to achieving a model outcome for others to emulate.

Fran David
City Manager



CITY OF
HAYWARD

Economic Development

VISION

The City of Hayward is recognized as the most desirable and business-friendly place in the East Bay in which to locate and conduct business.

Economic Development

MISSION STATEMENT

We will achieve our vision by:

- 1) Ensuring efficient and predictable business permitting processes;
- 2) Creating and sustaining a safe, clean, green, and fiscally sound business environment that benefits residents, businesses, and the region;
- 3) Promoting Hayward for its central location, prime business sites, great climate, and excellent transportation, water, and public safety services; as well as for other strengths that distinguish Hayward from other municipalities, such as its international community, civic participation, and history;
- 4) Actively recruiting and retaining businesses, especially supporting emerging sectors that create quality, good-paying jobs like biotech industries and food processing businesses; and
- 5) Fostering an educated and job-ready local workforce by driving the improvement of the academic performance of Hayward students and by connecting businesses, learning institutions, and community agencies.

STRATEGY AT A GLANCE

This Strategic Plan takes a three pronged approach to Economic Development. The three Focus Areas and associated goals are shown below. Each goal is supported by work tasks and performance measures, which are listed on pages five through nineteen. The work tasks include a shaded timeline that indicates the year in which each task will be executed. The performance measures are listed by priority in order to focus staff’s efforts on the most important metrics during the first years of the Plan.

The Plan includes a staffing and funding model, shown on pages twenty and twenty-one, which was created to match the needs of the three Focus Areas. Each work task has been assigned to a lead Economic Development staff person. This person will be responsible for overseeing the completion of the task, including coordinating efforts with other departments when needed.

The Plan also identifies opportunity sites in Hayward’s industrial areas and catalyst sites in Hayward’s retail areas, which are shown on pages twenty-two through twenty-four. The purpose of the sites is to focus limited city resources on the areas that will provide the most immediate positive economic results.

Focus Area 1	
BRANDING AND MARKETING	
Goal BM1	Identify and develop a brand that showcases Hayward’s strengths
Goal BM2	Develop and execute a comprehensive marketing program
Goal BM3	Strengthen and expand events and opportunities for community members, businesses, and visitors to engage with Hayward businesses

Focus Area 2	
INDUSTRIAL SECTOR	
Goal IS1	Support existing business, especially in the area of safety
Goal IS2	Recruit and secure new businesses in priority locations/industries that are a good fit for Hayward
Goal IS3	Maintain and expand the public infrastructure that businesses need to thrive
Goal IS4	Create proactive, site-specific land use policies that clearly convey information to potential businesses
Goal IS5	Ensure a timely and predictable permit process
Goal IS6	Improve Hayward’s education-to-job bridge through an active community partnership

Focus Area 3	
SERVICE & RETAIL INDUSTRY	
Goal SR1	Support existing business, especially in the area of safety
Goal SR2	Recruit and secure new businesses in priority locations/industries that are a good fit for Hayward
Goal SR3	Maintain and expand the public infrastructure that businesses need to thrive
Goal SR4	Create proactive, site-specific land use policies that clearly convey information to potential businesses
Goal SR5	Ensure a timely and predictable permit process
Goal SR6	Improve Hayward’s education-to-job bridge through an active community partnership

FOUNDATIONAL TASKS and MEASURES

The tasks and measures listed below apply to the whole plan. The foundational tasks lay the groundwork for the Plan, and are therefore all scheduled to be completed in the first year. The foundational measures pertain to all three Focus Areas and will be monitored on an ongoing basis throughout the five years.

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff*	Other Departments
FWT.A	Measure baselines and set targets for all metrics						ED Manager, Industrial Specialist, Retail Specialist	
FWT.B	Create a master flowchart for all surveys and future data collection						ED Manager, Industrial Specialist, Retail Specialist	
FWT.C	Secure funding and staffing						ED Manager	City Manger
FWT.C	Build on current successful economic development activities						ED Manager	City Manager
FWT.D	Report on Plan progress to the CEDC twice a year and to Council annually						ED Manager	

PERFORMANCE MEASURES		Baseline	Priority**
FPM.1	An annual sales tax growth above the sum of population growth and inflation	\$25,777,000 in FY 2012	I
FPM.2	An annual increase in the number of business licenses that is half of the increase in the labor force	Data available through Revenue, new staff will establish baseline during first year	I
FPM.3	95% occupancy rate of non-residential space	Measure baseline during first year	I
FPM.4	5% annual increase in property tax, both commercial and residential	\$35,960,000 in FY 2012	I
FPM.5	X% increase in the number of good-paying, quality jobs	Define good-paying, quality jobs and measure baseline during first year	I
FPM.6	The employment rate increases year over year	91.6% in November, 2012 ¹	I

*Key for Lead Economic Development Staff
See the Staffing Model on page twenty description of each of the positions

ED Manager	Economic Development Manager
C & M Relations	Community & Media Relations Officer
Industrial Specialist	Economic Development Specialist (Industrial Focus)
Retail Specialist	Economic Development Specialist (Service & Retail Focus)

**Key for Prioritization of Performance Measures

I	Greatest Importance
II	Very Important
III	Important

¹ Source: Bureau of Labor Statistics

BRANDING AND MARKETING

GOAL BM1: Identify and develop a brand that showcases Hayward’s strengths

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
BM1.A	Complete the recruitment process and hire the Community and Media Relations position						ED Manager	City Manager
BM1.B	Obtain consultant services to complete a brand assessment and identify opportunities to maximize the effectiveness of the City’s communication materials (logos, website, brochures, signage, etc)						ED Manager, C & M Relations	City Manager
BM1.C	Facilitate a discussion with the City Council in a work session to develop a shared definition of “business friendly”						ED Manager	
BM1.D	Complete a comprehensive training program for key staff in marketing activities and on how to promote the brand through the everyday performance of duties						C & M Relations	All Departments

PERFORMANCE MEASURES		Baseline	Priority
BM1.1	90% or more residents members have a positive image of Hayward in the Resident Satisfaction Survey	79% in the 2012 Resident Satisfaction Survey	I
BM1.2	100% of materials in the City’s annual portfolio demonstrate unified messaging and images that reinforce and are consistent with the City’s brand	Need to compile portfolio, then measure baseline during first year	I
BM1.3	90% or more of customers taking customer surveys state that that staff is embodying the themes of the City’s brand	Need to do brand assessment, then measure baseline during following year	II
BM1.4	Three of the top five positive characteristics listed by residents in the Resident Satisfaction Survey are reflective of the City’s brand	Need to do brand assessment, then measure baseline during next survey	II

BRANDING AND MARKETING

GOAL BM2: Develop and execute a comprehensive marketing program

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
BM2.A	Obtain consultant services to create a marketing plan including development of specific tasks to be included in the annual work plan						ED Manager, C & M Relations	
BM2.B	Compile an annual communications portfolio of material utilized by all departments and measure how well the messaging and images reflect the City's brand						C & M Relations	All Departments
BM2.C	Create and complete a biannual survey of brokers, developers and business organizations to measure awareness of Hayward and its attributes						Retail Specialist, Industrial Specialist	
BM2.D	Update the marketing plan annually in coordination with the economic development annual work plans based on outcomes from the prior year						ED Manager, C & M Relations	
BM2.E	Develop a list of desired service & retail business types that cater to college students and personnel to guide marketing activities in areas around the colleges						Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
BM2.1	By the end of year two, a 30% increase in inquiries from external businesses and developers that are interested in locating or doing projects in Hayward	Data currently being tracked manually, new staff will establish baseline during first year	I
BM2.2	By the end of year two, a 100% increase in social media activity and a 200% increase in website click-throughs	Measure baseline during first year	I
BM2.3	By the end of year two, a 50% increase in the number of positive media stories	Measure baseline during first year	II
BM2.4	By the end of year three, a 30% increase in awareness about Hayward's attributes, as measured through a biannual survey of brokers, developers and business organizations	Measure baseline during first year	III

BRANDING AND MARKETING

GOAL BM3: Strengthen and expand events and opportunities for community members, businesses, and visitors to engage with Hayward businesses

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
BM3.A	Conduct an effectiveness assessment of all City sponsored events and implement identified changes						ED Manager	
BM3.B	Develop mechanisms to measure event attendance and vendor sales during events						Retail Specialist	
BM3.C	Identify and grow signature events that the region associates with Hayward						Retail Specialist, C & M Relations	
BM3.D	Expand the events volunteer program to increase the number of events that are community led with staff support, rather than staff led						Retail Specialist	
BM3.E	Increase advertising of City and community partner events in accordance with the marketing plan						Retail Specialist, C & M Relations	
BM3.F	Review the community promotions grant program and develop a strategy to direct money to the promotions that have the most impact on economic development						ED Manager	Library & Community Services
BM3.G	Develop and execute a survey of Chamber members, BIA members, and other business owners to identify new events that would increase their exposure to community members						Retail Specialist	
BM3.H	Develop and execute a survey of key community consumer groups, such as student unions and parent organizations, to identify new events that would increase their exposure to Hayward businesses						Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
BM3.1	By the end of year four, a 100% increase in the retail sales of adjacent businesses during event days	Measure baseline during first year	I
BM3.2	By the end of year four, a 100% increase in the total annual attendance of events	Data currently being tracked manually, new staff will establish baseline during first year	II
BM3.3	By the end of year four, a 100% increase in the total annual event attendees that live in Hayward	Measure baseline during first year	III
BM3.4	By the end of year four, a 200% increase in the number of businesses taking part in Hayward events, through sponsorship, advertising, or participation	Measure baseline during first year	III

INDUSTRIAL SECTOR

GOAL IS1: Support existing business, especially in the area of safety

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS1.A	Identify key safety concerns and crime patterns in the industrial area, utilizing information from the new CADRMS and anecdotal evidence collected from businesses over a 12 month period						Industrial Specialist	Police
IS1.B	With the Police Department and Code Enforcement, establish a crime prevention and beautification program for property managers						Industrial Specialist	Police, Code Enforcement
IS1.C	Design and offer incentives to existing industrial businesses for expansions and improvements						ED Manager, Industrial Specialist	
IS1.D	Establish a comprehensive retention program for existing businesses						ED Manager, Industrial Specialist	
IS1.E	Expand the business visitation program						Industrial Specialist	
IS1.F	Complete a transit and amenity needs assessment for employees in the industrial areas and create an implementation plan based on recommendations						Industrial Specialist	Public Works E&T
IS1.G	Explore a Facilities Maintenance District to fund amenities in the industrial areas						ED Manager	Public Works
IS1.H	Develop a method to survey businesses that close or leave Hayward to learn from common issues						Industrial Specialist, Retail Specialist	
IS1.I	Identify a method to track how different ethnic groups are represented in the Hayward economy						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
IS1.1	Retain 100% of the top 25 industrial employers year-to-year	List available	I
IS1.2	Retain 100% of the top 25 industrial sales tax generators year-to-year	List available	I
IS1.3	Year over year decrease in crime in the industrial areas, broken down by crime category	Data available through Police Dept, new staff will establish baseline during first year	I
IS1.4	Year over year decrease in publically reported external code violations in the industrial areas	Data available through Code Enforcement, new staff will establish baseline in first year	II
IS1.5	Maintain current level of employment at existing industrial businesses	Data available through Revenue, new staff will establish baseline during first year	II
IS1.6	Maintain an annual increase in gross revenue by existing businesses equal to Bay Area wide growth, by industry	Data available through Revenue, new staff will establish baseline during first year	II

INDUSTRIAL SECTOR

GOAL IS2: Recruit and secure new businesses in priority locations/industries that are a good fit for Hayward

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS2.A	Establish a comprehensive attraction program for key business types						ED Manager, Industrial Specialist	
IS2.B	Design and offer incentives to new industrial businesses for site preparation and for those that offer good paying jobs						ED Manager	
IS2.C	Develop relationships with the property owners of the opportunity sites in the industrial area						ED Manager, Industrial Specialist	
IS2.D	Develop a Bioscience Council						ED Manager	
IS2.E	Expand the Food Manufacturing Council membership						Industrial Specialist	
IS2.F	Expand venture capital forums and investor round tables						ED Manager	
IS2.G	Develop marketing material for the opportunity sites in the industrial area that can be used by brokers						Industrial Specialist, C & M Relations	
IS2.H	Identify metrics to measure private investment in Hayward						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
IS2.1	95% occupancy rate of industrial sites	Quarter 2, 2012 ² : Manufacturing 93.4% Warehouse 87.4% R&D 80.6%	I
IS2.2	Maintain an annual growth in industrial jobs at new businesses at least equal to Bay Area wide growth, by industry category	Data available through Revenue, new staff will establish baseline during first year	II
IS2.3	Maintain an annual growth in the number of new businesses at least equal to Bay Area wide growth, by industry category	Data available through Revenue, new staff will establish baseline during first year	II
IS2.4	90% of businesses that take advantage of incentives indicate that they would not have located in Hayward without the incentive	Need to establish incentive program, then measure baseline	III

²Source: Cassidy Turley Commercial Real Estate Services, www.ctbt.com

INDUSTRIAL SECTOR

GOAL IS3: Maintain and expand the public infrastructure that businesses need to thrive

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS3.A	Complete an assessment of infrastructure deficiencies in the industrial area, as well as strengths that could be used for marketing, such as rail access and water supply						Industrial Specialist	Multiple Departments
IS3.B	Ensure water/sewer main capacity, especially for opportunity sites						ED Manager	Public Works U&S
IS3.C	Explore a public/private partnership to secure broadband/fiber optic network in the industrial area						ED Manager	
IS3.D	Complete the Whitesell/Cabot extension						Industrial Specialist	Public Works E&T
IS3.E	Create an implementation plan to address infrastructure deficiencies, especially the quality of roads						ED Manager	Public Works E&T
IS3.F	Create and implement a plan to link and expand existing biking and walking trails in the industrial area						Industrial Specialist	Public Works E&T

PERFORMANCE MEASURES		Baseline	Priority
IS3.1	Necessary sewer capacity available 100% of the time for opportunity sites	Need to identify desired uses for opportunity sites, then measure baseline during first year	I
IS3.2	Maintain a pavement index at 65 or above	In 2011: Industrial West = 61.4 Industrial Pkwy Mixed-Use Area = 60.3	II
IS3.3	Link all existing biking/walking trails and establish a 26.2 mile loop	Measure baseline during first year	III

INDUSTRIAL SECTOR

GOAL IS4: Create proactive, site-specific land use policies that clearly convey information to potential businesses

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS4.A	Identify desired uses for the opportunity sites in the industrial area with the CEDC and ensure that current zoning allows for the desired uses						ED Manager	Development Services
IS4.B	Confirm the City's zoning assessment with commercial brokers						ED Manager	
IS4.C	Update the City's Hazmat policies to address advances in the biotech industry						Industrial Specialist	Fire
IS4.D	Develop preferential zoning areas that support the desired business types						Industrial Specialist	Development Services
IS4.E	Assess opportunities for recreational uses in sections of the industrial area that are near retail areas						Industrial Specialist	Development Services

PERFORMANCE MEASURES		Baseline	Priority
IS4.1	Zoning for opportunity sites is appropriate for desired uses 100% of the time	Need to identify desired uses for opportunity sites, then measure baseline during first year	I
IS4.2	For new users, opportunity sites are used by desired business types 100% of the time	Need to identify desired uses for opportunity sites, then measure baseline during first year	I
IS4.3	Land use is a critical element in all marketing material that is targeted at industrial businesses	Need to create marketing material, then measure baseline	II

INDUSTRIAL SECTOR

GOAL IS5: Ensure a timely and predictable permit process

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS5.A	Develop an ombudsperson program to streamline industrial business applications and permits						ED Manager	Development Services, Fire, Public Works
IS5.B	Optimize the permit process by utilizing online technology and other available tools						Industrial Specialist	Development Services, Fire, Public Works, Police, Technology Services
IS5.C	Expand the permit performance measures to include other departments that are involved in permitting and inspections						Industrial Specialist	Development Services, Fire, Public Works, Police
IS5.D	Streamline the bioscience permitting						ED Manager	Development Services, Fire, Public Works

PERFORMANCE MEASURES		Baseline	Priority
IS5.1	100% of new businesses open by their target date	Measure baseline during first year	I
IS5.2	100% of permits are processed within the target timeframe	Currently being tracked for all projects, measure baseline for industrial projects in the first year	I
IS5.3	100% of planning applications are processed within target timeframe	Currently being tracked for all projects, measure baseline for industrial projects in the first year	I
IS5.4	100% of next day inspections scheduled before 4:00 pm are conducted the next day	Currently being tracked for all projects, measure baseline for industrial projects in the first year	I
IS5.5	100% of survey responses are excellent or good	Currently being tracked for all projects, measure baseline for industrial projects in the first year	II
IS5.6	By year three, 50% of minor permit applications are submitted electronically	Measure baseline during first year	II

INDUSTRIAL SECTOR

GOAL IS6: Improve Hayward's education-to-job bridge through an active community partnership

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
IS6.A	Encourage and support HUSD efforts to develop a mentoring/internship/skill-development program for students that are interested in specific careers						ED Manager	
IS6.B	Explore business sponsorships and employee volunteer programs to grow the City's existing afterschool programs						ED Manager, Industrial Specialist	Library & Community Services
IS6.C	Complete an inventory of desired skills from Hayward industrial businesses						Industrial Specialist	
IS6.D	Create an annual award to recognize businesses that support schools						C & M Relations	
IS6.E	Facilitate an adopt-a-classroom and employee volunteer program in partnership with HUSD, starting with the Promise Neighborhood middle and high schools						ED Manager, Industrial Specialist	Library & Community Services
IS6.F	Facilitate school tours of industrial businesses						Industrial Specialist	
IS6.G	Design and launch an annual Career Day at Chabot and Cal State that highlights the skills needed to work in Hayward industries						All	
IS6.H	Partner with the Chamber to incorporate a job fair element at the annual Business Expo						ED Manager	
IS6.I	Identify metrics that connect academic achievement to employment, such as job placement after graduation						Industrial Specialist, Retail Specialist	
IS6.J	Research and develop a local hire program to encourage and support businesses to hire Hayward residents						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
IS6.1	By year three, 10% of classrooms are adopted by businesses	Measure baseline during first year	I
IS6.2	By year three, 10% of middle and high school students are involved in mentoring or internship programs	Measure baseline during first year	I
IS6.3	Average district wide API scores of 900 or above	718 Average API Score in 2012	I
IS6.4	100% of HUSD schools have API scores above 800	List available	I

SERVICE & RETAIL INDUSTRY

GOAL SR1: Support existing business, especially in the area of safety

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR1.A	Identify key safety concerns and crime patterns in the retail areas, utilizing information from the new CAD/RMS and anecdotal evidence collected						Retail Specialist	Police
SR1.B	With the Police Department and Code Enforcement, establish a crime prevention and property improvement prgm for property managers						Retail Specialist	Police, Code Enforcement
SR1.C	Establish a comprehensive retention program for existing businesses						ED Manager, Retail Specialist	
SR1.D	Design and offer incentives to existing service/retail businesses for expansions and improvements						ED Manager, Retail Specialist	
SR1.E	Assist the Chamber, BIA, and workforce development organizations with starting a mentoring/consulting program for small businesses						ED Manager, Retail Specialist	
IS1.H	Develop a method to survey businesses that close or leave Hayward to learn from common issues						Industrial Specialist, Retail Specialist	
IS1.I	Identify a method to track how different ethnic groups are represented in the Hayward economy						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
SR1.1	Retain 100% of the top 25 retail employers year-to-year	List available	I
SR1.2	Retain 100% of the top 25 retail sales tax generators year-to-year	List available	I
SR1.3	Year over year decrease in crime in the retail areas, broken down by crime category	Data available through Police Department, new staff will establish baseline during first year	I
SR1.4	Sales tax in the downtown area grows at a higher rate than the citywide retail sales tax	Data available through Revenue, new staff will establish baseline during first year	I
SR1.5	Maintain current level of employment at existing retail businesses	Data available through Revenue, new staff will establish baseline during first year	II
SR1.6	Maintain an annual increase in gross revenue from existing bsns equal to Bay Area growth, by category	Data available through Revenue, new staff will establish baseline during first year	II
SR1.7	Year over year decrease in publically reported external code violations in the retail areas	Data available through Code Enforcement, new staff will establish baseline during first year	II
SR1.8	Year over year increase in Chamber-led events that are targeted at small business skill development	Measure baseline during first year	III

SERVICE & RETAIL INDUSTRY

GOAL SR2: Recruit and secure new businesses in priority locations/industries that are a good fit for Hayward

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR2.A	Establish a comprehensive retail attraction program for desired retailers						ED Manager, Retail Specialist	
SR2.B	Increase proactive code enforcement efforts around catalyst sites and other highly visible sites						Retail Specialist	Code Enforcement
SR2.C	Design and offer incentives to new retail businesses, such as small business loans and fee waivers						ED Manager	
SR2.D	Update psychographic data (example: Buxton Study)						Retail Specialist	
SR2.E	Grow relationships with retail brokers						ED Manager, Retail Specialist	
SR2.F	Develop relationships with the property owners of the catalyst sites in the retail area						ED Manager, Retail Specialist	
SR2.H	Develop marketing material for the catalyst sites that can be used by brokers						Retail Specialist, C & M Relations	
SR2.I	Identify metrics to measure private investment in Hayward						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
SR2.1	Maintain an annual increase in the number of new businesses at least equal to Bay Area wide growth, by retail category	Data available through Revenue, new staff will establish baseline during first year	I
SR2.2	95% occupancy rate of retail sites	Measure baseline during first year	I
SR2.3	Close sales tax leakage year over year in each retail category	2.4 – 21.4 stores leakage, depending on the category ³	II
SR2.4	90% of businesses that take advantage of incentives indicate that they would not have located in Hayward without the incentive	Need to establish incentive program, then measure baseline	III

³ Source: ADE, Inc 1/22/2013 Report for the City of Hayward; data from MuniServices LLC, and U.S. Economic Census

SERVICE & RETAIL INDUSTRY

GOAL SR3: Maintain and expand the public infrastructure that businesses need to thrive

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR3.A	Complete an assessment of infrastructure deficiencies in the retail area, as well as strengths that could be used for marketing, such as Hayward's unique downtown						Retail Specialist	Multiple Departments
SR3.B	Complete the way-finding sign program for the 238 Corridor						Retail Specialist	Public Works E&T
SR3.C	Complete parking study for downtown retail area						Retail Specialist	Public Works E&T
SR3.D	Create an implementation plan to address deficiencies identified in the infrastructure assessment						ED Manager	Public Works
SR3.E	Pre-install grease traps and other infrastructure to support restaurant development						Retail Specialist	Public Works U&ES
SR3.F	Work with Southland Mall to identify and address infrastructure needs						ED Manager	
SR3.G	Develop gateway program						C & M Relations	City Manager, Development Services

PERFORMANCE MEASURES		Baseline	Priority
SR3.1	By year three, 100% of way-finding signs are installed citywide	Need to establish way-finding program, then then measure baseline during first year	I
SR3.2	Maintain a pavement index at 65 or above in retail areas	Data available through Public Works, new staff will establish baseline during first year	II

SERVICE & RETAIL INDUSTRY

GOAL SR4: Create proactive, site-specific land use policies that clearly convey information to potential businesses

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR4.A	Identify desired uses for the catalyst sites with the CEDC and ensure that current zoning allows for the desired uses						ED Manager	Development Services
SR4.B	Confirm the City's zoning assessment with commercial brokers						ED Manager	
SR4.C	Revise the Sign Ordinance						ED Manager	Development Services
SR4.D	Monitor developments at Airport Land Use Commission and improve process to ensure appropriate influence						ED Manager	Development Services, Public Works E&T
SR4.E	Modify/create dining entertainment districts that include owner accountability systems						ED Manager	Development Services
SR4.F	Complete the Downtown Plan update						ED Manager	Development Services

PERFORMANCE MEASURES		Baseline	Priority
SR4.1	Zoning for catalyst sites is appropriate for desired uses 100% of the time	Need to identify desired uses for opportunity sites, then measure baseline during first year	I
SR4.2	For new users, catalyst sites are used by desired business types 100% of the time	Need to identify desired uses for opportunity sites, then measure baseline during first year	I
SR4.3	Land use is a critical element in all marketing material that is targeted at retail businesses	Need to create marketing material, then measure baseline	II

SERVICE & RETAIL INDUSTRY

GOAL SR5: Ensure a timely and predictable permit process

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR5.A	Develop and promote an ombudsperson program to streamline retail business applications and permits	█	█	█	█	█	ED Manager	Development Services, Fire, Public Works
SR5.B	Optimize the permit process by utilizing online technology and other available tools						Retail Specialist	Development Services, Fire, Public Works, Police, Technology Services
SR5.C	Update/review fire connection requirements						Retail Specialist	Fire
SR5.D	Facilitate the streamlining of ABC review while developing equally balanced accountability measures for problem users		█				Retail Specialist	Development Services, Fire, Public Works
SR5.E	Streamline restaurant permitting		█				Retail Specialist	Development Services, Fire, Public Works
SR5.F	Expand the permit performance measures to include other departments that are involved in permitting and inspections			█			Retail Specialist	Development Services, Fire, Public Works, Police

PERFORMANCE MEASURES		Baseline	Priority
SR5.1	100% of new businesses open by their target date	Measure baseline during first year	I
SR5.2	100% of permits are processed within the target timeframe	Currently being tracked for all projects, measure baseline for retail projects in the first year	I
SR5.3	100% of planning applications are processed within target timeframe	Currently being tracked for all projects, measure baseline for retail projects in the first year	I
SR5.4	100% of next day inspections scheduled before 4:00 pm are conducted the next day	Currently being tracked for all projects, measure baseline for retail projects in the first year	I
SR5.5	100% of survey responses are excellent or good	Currently being tracked for all projects, measure baseline for retail projects in the first year	II
SR5.6	By year three, 50% of minor permit applications are submitted electronically	Measure baseline during first year	II

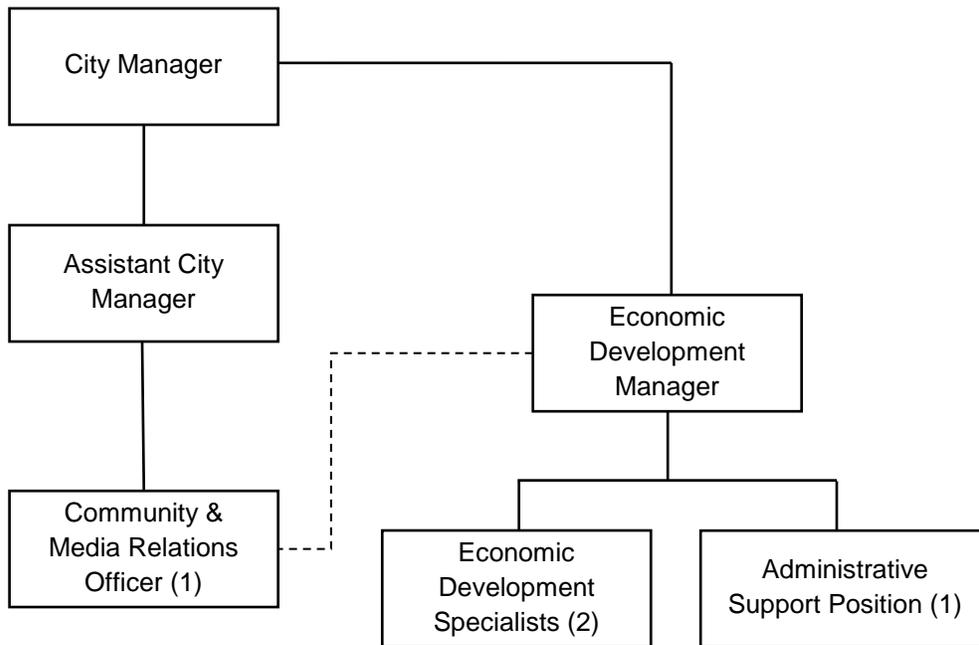
SERVICE & RETAIL INDUSTRY

GOAL SR6: Improve Hayward's education-to-job bridge through an active community partnership

WORK TASKS		FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	Lead ED Staff	Other Departments
SR6.A	Partner with applicable community organizations to create a job connection program for seasonal and part time entry jobs						Retail Specialist	
SR6.B	Expand the adopt a classroom and employee volunteer programs to service/retail businesses						Retail Specialist	Library & Community Services
SR6.C	Identify metrics that connect academic achievement to employment, such as job placement after graduation						Industrial Specialist, Retail Specialist	
SR6.D	Research and develop a local hire program to encourage and support businesses to hire Hayward residents						Industrial Specialist, Retail Specialist	

PERFORMANCE MEASURES		Baseline	Priority
SR6.1	By year three, 10% of classrooms are adopted by businesses	Measure baseline during first year	I
SR6.2	By year three, 10% of middle and high school students are involved in mentoring or internship programs	Measure baseline during first year	I
SR6.3	Average district wide API scores of 900 or above	718 Average API Score in 2012	I
SR6.4	100% of HUSD schools have API scores above 800	List available	I

STAFFING MODEL



Economic Development Manager – The Manager will direct, supervise, and coordinate the work of the Division in accordance with the five-year plan, including monitoring performance measures and preparing the budget. The Manager will be the lead for efforts related to the catalyst and opportunity sites, as well as tasks that involve coordination with outside groups and agencies and other City departments.

Community & Media Relations Officer – Half of the Officer’s hours will be dedicated to Economic Development efforts, the other half will be dedicated to public information, legislative, and neighborhood outreach efforts. The Officer will be the lead on all tasks related to branding and marketing. In addition, the Officer will work with the ED Specialists to develop and promote events and to recognize businesses that support schools.

Economic Development Specialist (Industrial Focus) – The Industrial Specialist will design, implement, and monitor programs that encourage economic development in the industrial area, including playing an ombudsman role and providing support for existing businesses. In addition, the Industrial Specialist will be the lead for many of the tasks related to improving Hayward’s education-to-job bridge.

Economic Development Specialist (Service & Retail Focus) – The Retail Specialist will design, implement, and monitor programs that encourage economic development in the retail areas, including playing an ombudsman role and providing support for existing businesses. In addition, the Retail Specialist will be the lead on assessing and expanding City-sponsored events.

Administrative Support Position – The Administrative Support Position will provide specialized clerical support for all Economic Development Programs. In addition to routine administrative duties, this person will assist with assembling the annual communications portfolio, compiling results of surveys and needs assessments, administering programs in the schools, and preparing for events.

FUNDING SOURCES

Proposed Operating Expenses for the Five-Year Plan (\$ in 1,000s)

Uses:	FY 2014	FY 2015	FY 2016	FY 2017	FY 2018
Employee Services	680	680	680	680	680
Events	168	168	168	145	145
Marketing Materials	70	70	70	70	70
Memberships	35	35	35	35	35
Supplies	30	30	30	30	30
Travel and Trainings	10	10	10	10	10
Planning Studies and Other Analyses	30	30	30	30	30
One-Time Startup Costs	150	120	90	0	0
Total:	1,173	1,143	1,113	1,000	1,000

Proposed Operating Funding Sources for the Five-Year Plan (\$ in 1,000s)

Sources:	FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	
Existing Sources	General Fund (Current)	595	595	595	595	595
	Community Promotions	23	23	23	0	0
	DBIA Fees	55	55	55	55	55
New Sources	Projected Revenue from Previous RDA Tax Increments	350	350	350	350	350
	General Fund (New)	150	120	90	0	0
Total:	1,173	1,143	1,113	1,000	1,000	

The table below displays projections for the Plan's most direct avenue for revenue creation, which is securing optimal uses on the Opportunity and Catalyst sites. Staff worked with the consultants at Applied Development Economics, Inc. to identify a possible optimal use for each site. These are preliminary concepts that may change with further analysis. The low projection assumes that only sites that are currently vacant turn over to an optional use over the course of the five years, with slower development in the early years. The high projection assumes that all twenty sites turn over to an optional use over the course of the five years.

Revenue Projections

	FY 2014	FY 2015	FY 2016	FY 2017	FY 2018	TOTAL
Low	\$200,577	\$501,443	\$902,597	\$1,404,039	\$2,005,770	\$5,014,425
High	\$356,523	\$891,308	\$1,604,354	\$3,229,943	\$5,400,933	\$11,483,061

Again, these projections focus on the most tangible avenue for revenue creation and therefore only include one of the many ways in which increased revenue will be realized through the efforts of the Plan.

KEY GEOGRAPHIC AREAS

The Strategic Plan includes two maps that identify and prioritize economic development areas and specific parcels. The purpose of the key geographic areas is to focus limited city resources on the sites that will provide the most immediate positive results related to the City’s business attraction and retention efforts.

Both maps are fluid documents that may be modified as needed, including the addition and removal of sites to account for ownership, the economic climate, and changes in interest from developers and businesses.

OPPORTUNITY SITES – Industrial Sector

See map on page twenty-three, **Opportunity Sites in the Industrial Areas**
 The opportunity sites were selected using the following criteria:

- Vacant or underutilized parcel/building
- Greater than five acres
- Single or few owners
- Proximity to major corridors/arterials
- Minimal CEQA concerns

CATALYST SITES – Service & Retail Sector

See map on page twenty-four, **Key Retail Areas and Catalyst Sites**
 The catalyst sites were selected using the following criteria:

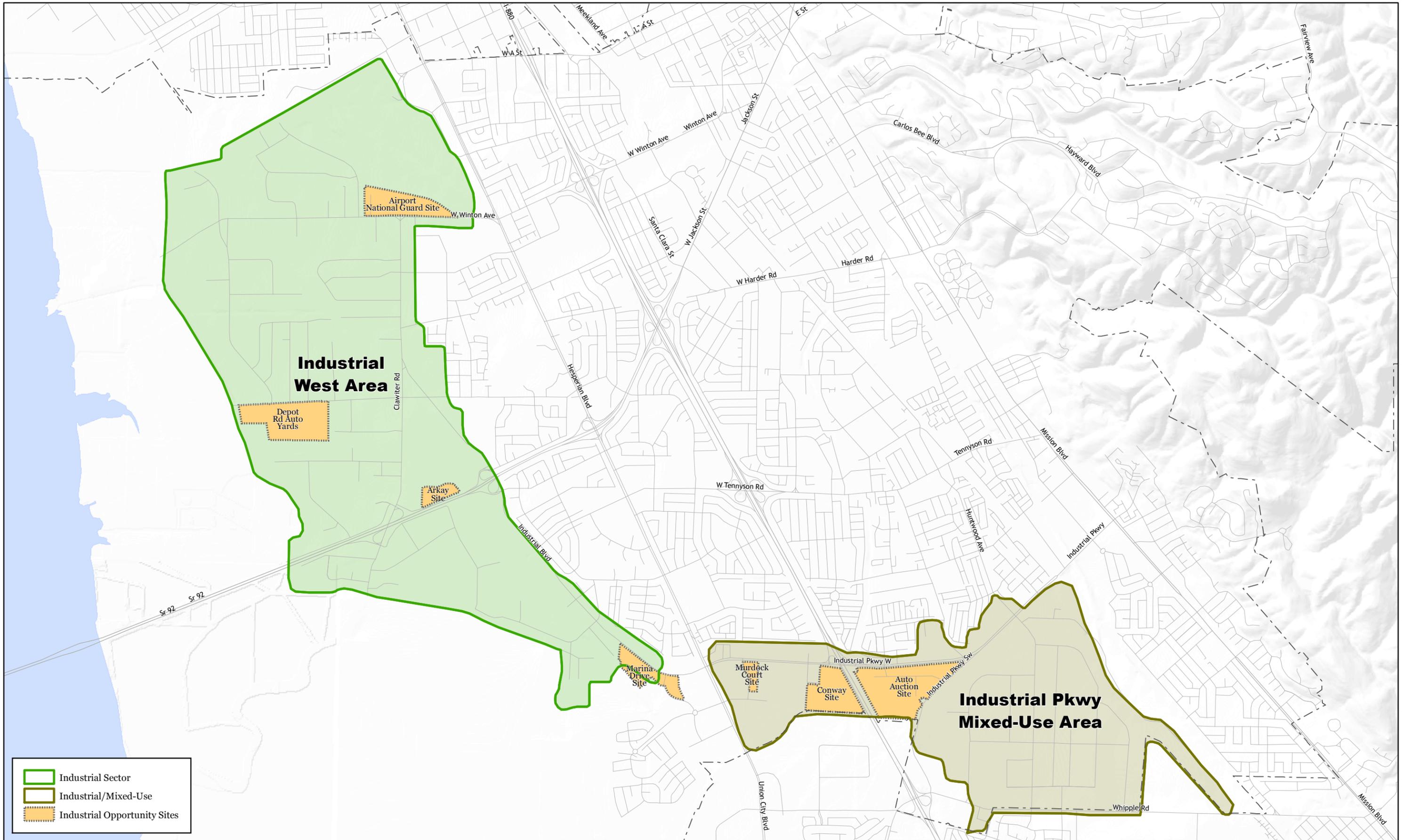
- The potential impact on the associated retail area
- High visibility
- Vacant or underutilized parcel/building
- Single or few owners
- Acreage

Summary of Work Tasks Related to the Catalyst and Opportunity Sites

Land Use	<ul style="list-style-type: none"> • Identify desired uses for the sites with the Council Economic Development Committee • Ensure that zoning allows for the desired uses, including confirming with brokers • Develop preferential zoning areas that support the desired business types
Relationships & Marketing	<ul style="list-style-type: none"> • Develop relationships with property owners • Grow relationships with retail brokers • Develop marketing material specific to the sites that can be used by brokers
Infrastructure & Site Readiness	<ul style="list-style-type: none"> • Complete an assessment of infrastructure deficiencies around sites, as well as strengths that could be used for marketing • Increase proactive code enforcement efforts around sites • Ensure water/sewer main capacity for the sites • Design and offer incentives to new businesses for site preparation



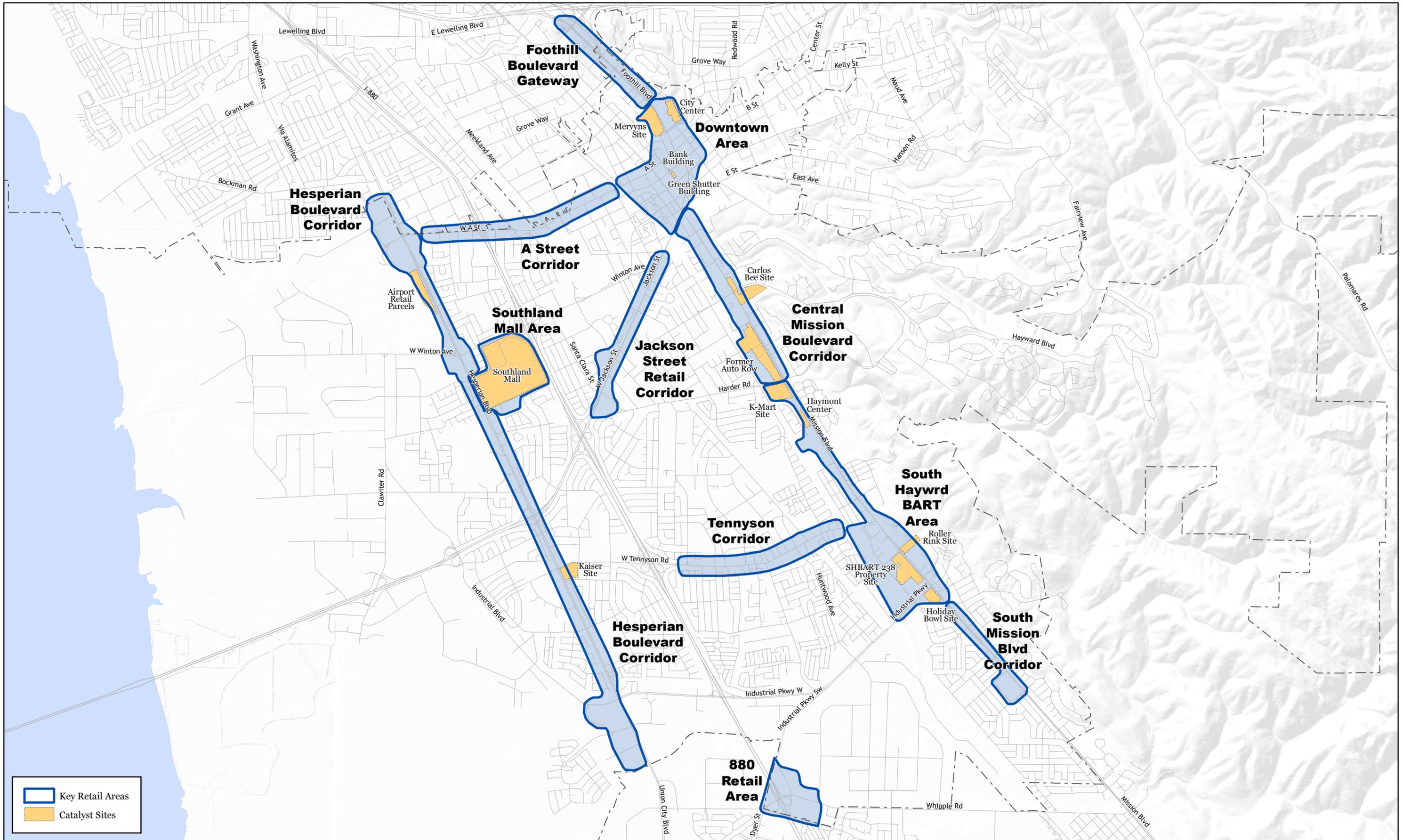
Opportunity Sites in the Industrial Area



	Industrial Sector
	Industrial/Mixed-Use
	Industrial Opportunity Sites



Key Retail Areas & Catalyst Sites





DATE: March 4, 2013

TO: Council Economic Development Committee

FROM: City Manager

SUBJECT: Council Feedback on the Draft Strategic Plan from the February 26, 2013 Council Work Session

RECOMMENDATION

That the Council Economic Development Committee (CEDC) receives this report and provides feedback.

BACKGROUND

The purpose of this report is to provide an overview of the feedback that the City Council provided to staff at the February 26, 2013, Council work session on the draft Economic Development Strategic Plan (Plan). Staff has met with the CEDC eight times over the past six months to seek feedback on all major areas of the economic development strategic planning process, including stakeholder engagement, the Vision and Mission Statement, Strategic Focus Areas, and key geographic areas. At all stages, staff incorporated CEDC feedback and returned to the Committee with updated drafts. The CEDC reviewed and provided comment on a final draft on February 11, 2013. This draft was updated to incorporate CEDC comments and presented to Council at a work session on February 26, 2013.

In addition to feedback on the draft Plan, staff sought Council feedback on uses for two catalyst sites that fall within the boundaries of the draft Mission Boulevard Corridor Specific Plan (MBCSP). Staff has been working with the CEDC to ensure that the desired uses for the two sites (Former Auto Row and the Carlos Bee Site) are consistent with the MBCSP before it is finalized. On February 11, 2013, the CEDC recommended to the City Council several changes to the MBCSP, including a commercial overlay that would prohibit residential units on the ground floor of the sites, but would allow residential units on the second floor and above.

DISCUSSION

At the Council work session on February 26, 2013, the City Council expressed general support for the draft Plan as well as appreciation for the effort that the CEDC and staff has put into the Plan's development. The Council expressed support for a commercial overlay for the two sites that fall within the Mission Boulevard Corridor Specific Plan. There were a couple of concerns about the changes proposed to Table 9 of the MBCSP, specifically around hospitals and

dance/nightclubs. Staff will return to Council for further discussions of the MBCSP in a work session later this spring.

Council provided the following comments on the draft Plan:

- The Plan needs to communicate to an outside audience that Hayward is open for businesses. It is not just an internal document.
- It would be valuable for the Council to develop a shared definition of “business friendly” at a future work session with the new Economic Development Manager once s/he is hired.
- Staff should report to Council as well as to the CEDC on Plan progress, albeit less frequently than to the CEDC. Staff is proposing that this be done annually.
- The Plan should include language about how Hayward’s educational institutions are economic generators through their purchasing power and staff should specifically support and attract local businesses that serve students, staff, faculty, and administrators.
- The Plan should include a task for the City to promote businesses hiring locally.
- The Plan should include a metric and target for online permit submission, tracking, and approval. For example: “50% of minor permit applications are submitted electronically.”
- The Plan should include a metric that tracks individual school performance, such as “100% of HUSD schools have API scores above 800.”
- Measure FPM.2 is confusing, which reads, “An annual increase in the number of business licenses that is half of the increase in the labor force.” Staff will discuss this more during Committee discussion.
- The City should add ethnicity to the permit and/or business license application, or develop some other method to track how different groups in Hayward are represented in the local economy. However, until the new business license ordinance and management system is in place, this is a not an achievable task. Staff suggests revisiting this as the new system is designed over the next three years.
- The City should develop a process for tracking businesses that close and learn from the issues that these businesses encountered.
- The City should continue to take the steps necessary to preserve commercial and industrial land for those uses.
- There is an auto storage yard just north of I-92 and east of Clawiter Road that is right next to the freeway and appears to be a large, sparsely utilized parking lot. With plans to

rebuild the Clawiter and I-92 overpass, this may be a good time to consider this as an opportunity site.

- The Auto Auction Site at I-880 and Industrial Blvd has great retail potential (auto dealers, big box retailers, a Union Landing style shopping center) in addition to being a good industrial opportunity site. This should be considered when the CEDC reviews the desired use for this site.
- The selection criteria for the retail catalyst sites should include locations that have a greater than average potential to import sales tax dollars.
- There is a concern that smaller neighborhood retail centers, such as the Fairway Park shopping center, are excluded from the maps.
- With great location comes great responsibility. Property owners on highly visible sites have a greater-than-average responsibility to maintain their properties and the City should target these sites for community preservation enforcement.

FISCAL IMPACT

The net new cost of the Strategic Plan to the General Fund did not change as a result of Council's comments, and will still be \$150,000 in the first year of the plan. As before, approximately \$350,000 of the cost of the plan will be offset by new property tax revenues from the dissolution of Redevelopment. By year five of the plan, staff is still projecting revenue generation of \$5 million to \$11 million if optimal uses are secured for the Opportunity and Catalyst sites. This is only one of many ways that increased revenue will be realized through the efforts of the Plan.

ECONOMIC IMPACT

As the economy continues to improve in the coming years, it is essential that Hayward is recognized for its assets and is known as a welcoming and effective city in which to conduct business. The Economic Development Strategic Plan is a purposeful, concentrated effort to achieve this vision with an efficient use of the City's resources.

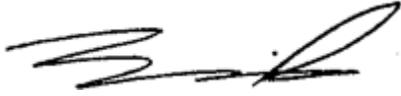
NEXT STEPS

After making final changes to incorporate Council's feedback, the Team will return Council with a final plan for adoption on March 19, 2013. At the February 4 CEDC meeting, staff proposed a timeline for the CEDC to receive presentations on each Catalyst and Opportunity site and discuss the desired future uses by the end of this fiscal year. At the next CEDC meeting, which is on March 18, Applied Development Economics will present an overview of their Retail Analysis findings.

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Changes to the Economic Development Strategic Plan based on Council Feedback

	Comment	Recommended modification or addition
1	The Plan needs to communicate to an outside audience that Hayward is open for businesses – It is not just an internal document	Amend the opening message so that it also addresses the business community
2	It would be valuable for the Council to develop a shared definition of “business friendly” at a future work session with the new Economic Development Manager once s/he is hired	Add as a first year Branding & Marketing task for the ED Manager
3	Staff should report to Council as well as to the CEDC on Plan progress	Modify Foundational Work Task D to include an annual report to the City Council
4	The Plan should include language about how Hayward’s educational institutions are economic generators through their purchasing power and staff should specifically support and attract local businesses that serve students, staff, faculty, and administrators	Add a task for BM2 that reads, “Develop a list of desired service & retail business types that cater to college students and personnel to guide marketing activities in areas around the colleges”
5	The Plan should include a task for the City to promote businesses hiring locally	Add a task for SR6 and IS6 that reads, “Research and develop a local hire program to encourage and support businesses to hire Hayward residents”
6	The Plan should include a metric and target for online permit submission, tracking, and approval – For example: “50% of minor permit applications are submitted electronically”	Add as a metric for IS5 and IS5 for year three
7	The Plan should include a metric that tracks individual school performance, such as “100% of HUSD schools have API scores above 800”	Add as a metric for IS6 and SR6
8	Measure FPM.2 is confusing, which reads, “An annual increase in the number of business licenses that is half of the increase in the labor force”	No change – Staff will review this metric after collecting data in year one to determine if it needs to be modified
9	The City should add ethnicity to the permit and/or business license application, or develop some other method to track how different groups in Hayward are represented in the local economy	Add a task for SR1 and IS1 that reads, “Identify a method to track how different ethnic groups are represented in the Hayward economy”
10	The City should develop a process for tracking businesses that close and learn from the issues that these businesses encountered	Add a task to IS1 and SR1 that reads, “Develop a method to survey businesses that close or leave Hayward to learn from common issues”

	Comment	Recommended modification or addition
11	The City should continue to take the steps necessary to preserve commercial and industrial land for those uses	No change – This comment will be brought to the CEDC when the Committee is discussing desired uses for the Catalyst and Opportunity Sites
13	The Auto Auction Site at I-880 and Industrial Blvd has great retail potential (auto dealers, big box retailers, a Union Landing style shopping center) in addition to being a good industrial opportunity site	No change – This comment will be brought to the CEDC for consideration when the Committee is discussing desired uses for the Auto Auction Site, which is currently scheduled for June
13	The selection criteria for the retail catalyst sites should include locations that have a greater than average potential to import sales tax dollars	No change at this time - Consider adding this criteria when the time comes to add new catalyst sites
14	There is a concern that smaller neighborhood retail centers, such as the Fairway Park shopping center, are excluded from the maps	Add an addition retail area to the base map called South Mission Blvd Corridor, which encompasses the Fairway Park Shopping Center
15	With great location comes great responsibility – Property owners on highly visible sites have a greater-than-average responsibility to maintain their properties and the City should target these sites for community preservation enforcement	Modify the work tasks in SR2.B to read, “Increase proactive code enforcement efforts around catalyst sites and other highly visible sites”
16	There is an site just north of I-92 and east of Clawiter Road that is right next to the freeway and appears to be a large, sparsely utilized parking lot – With plans to rebuild the Clawiter and I-92 overpass, this may be a good time to consider this as an opportunity site	Add as an Opportunity Site (see map and criteria table below)

Recommended Opportunity Site:



Opportunity Site Criteria	
Current Use	Vacant
Acreage	8.3 Acres
Number of Owners	2 Owners
Proximity to 880 or major arterials	Less than a mile

Actual and Projected Property Tax Dollars that will be Redistributed to the City from the Dissolution of the Redevelopment Agency

	Jan - Jun 2012 (actual)	Jul - Dec 2012 (actual)	Jan - Jun 2013 (actual)	Jul - Dec 2013 (estimate)	Jan - Jun 2014 (estimate)	FY 2014	FY2015	FY2016	FY2017	FY2018	FY2019	FY2020	FY2021
Available RPTTF for Distribution	4,721,491	4,957,239	5,076,366	5,032,795	5,032,795	10,065,590	10,266,902	10,472,240	10,681,685	10,895,318	11,222,178	11,558,843	11,905,609
County Admin Fees		89,585	67,281	80,000	80,000	160,000	160,000	160,000	160,000	160,000	160,000	160,000	160,000
Pass Through Payments		1,161,957	680,327	1,100,000	700,000	1,800,000	1,836,000	1,872,720	1,910,174	1,948,378	2,006,829	2,067,034	2,129,045
Enforceable Obligations													
2004 Bonds					2,482,678	2,482,678	3,368,582	3,372,325	3,371,182	3,370,042	3,368,362	3,365,962	3,367,260
2006 Bonds					361,650	361,650	635,006	636,606	638,006	639,088	634,838	635,538	635,813
Bond Admin Fees					7,500	7,500	7,500	7,500	7,500	7,500	7,500	7,500	7,500
Admin Allowance					125,000	125,000	250,000	250,000	250,000	250,000	250,000	250,000	250,000
Cinema Place Expenses					22,925	22,925	45,850	46,767	47,702	48,656	49,630	50,622	51,635
Misc. Expenses					112,556	112,556	51,335	52,361	53,408	54,477	56,111	57,794	59,528
Total Enforceable Obligations		2,868,913	4,216,021	2,308,884	3,112,309	5,421,193	4,358,272	4,365,559	4,367,799	4,369,762	4,366,440	4,367,416	4,371,735
Balance Remaining		836,784	112,737	1,543,911	1,140,486	2,684,397	3,912,630	4,073,961	4,243,711	4,417,178	4,688,909	4,964,393	5,244,829
Amount to be Distributed to Taxing Entities in Current FY			949,521	1,543,911	2,684,397	4,228,308	3,912,630	4,073,961	4,243,711	4,417,178	4,688,909	4,964,393	5,244,829
Increase in Distribution between Current FY and Base Year (FY13)					1,734,876	1,734,876	2,963,109	3,124,440	3,294,190	3,467,657	3,739,388	4,014,872	
Amount available for loan repayment (1/2 of increase between current FY and base year)					867,438	867,438	1,481,554	1,562,220	1,647,095	1,733,828	1,869,694	2,007,436	
Repayment of SERAF Loan Balance as of 3/2013 = \$3,876,516					867,438	867,438	1,481,554	1,509,524					
Repayment of GF Loan Balance to be repaid (less 20% set aside) = \$6,200,000								52,696	1,647,095	1,733,828	1,869,694	896,687	
RPTTF Distribution to All Taxing Entities		836,784	112,737	1,543,911	1,816,959	3,360,870	2,431,075	2,511,741	2,596,616	2,683,349	2,819,215	4,067,707	5,244,829
RPTTF Hayward Share (17.6%) <i>* Current Projection</i>		147,374	20,448	271,728	319,785	591,513	427,869	442,066	457,004	472,270	496,182	715,916	923,090
Typical City Share (15%)				231,587	272,544	504,130	364,661	376,761	389,492	402,502	422,882	610,156	786,724