

The Redevelopment Agency of the City of Hayward

DOWNTOWN FOCAL POINT MASTER PLAN

*Site Development Program
Urban Design Guidelines
Library Building Program*

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December 1991

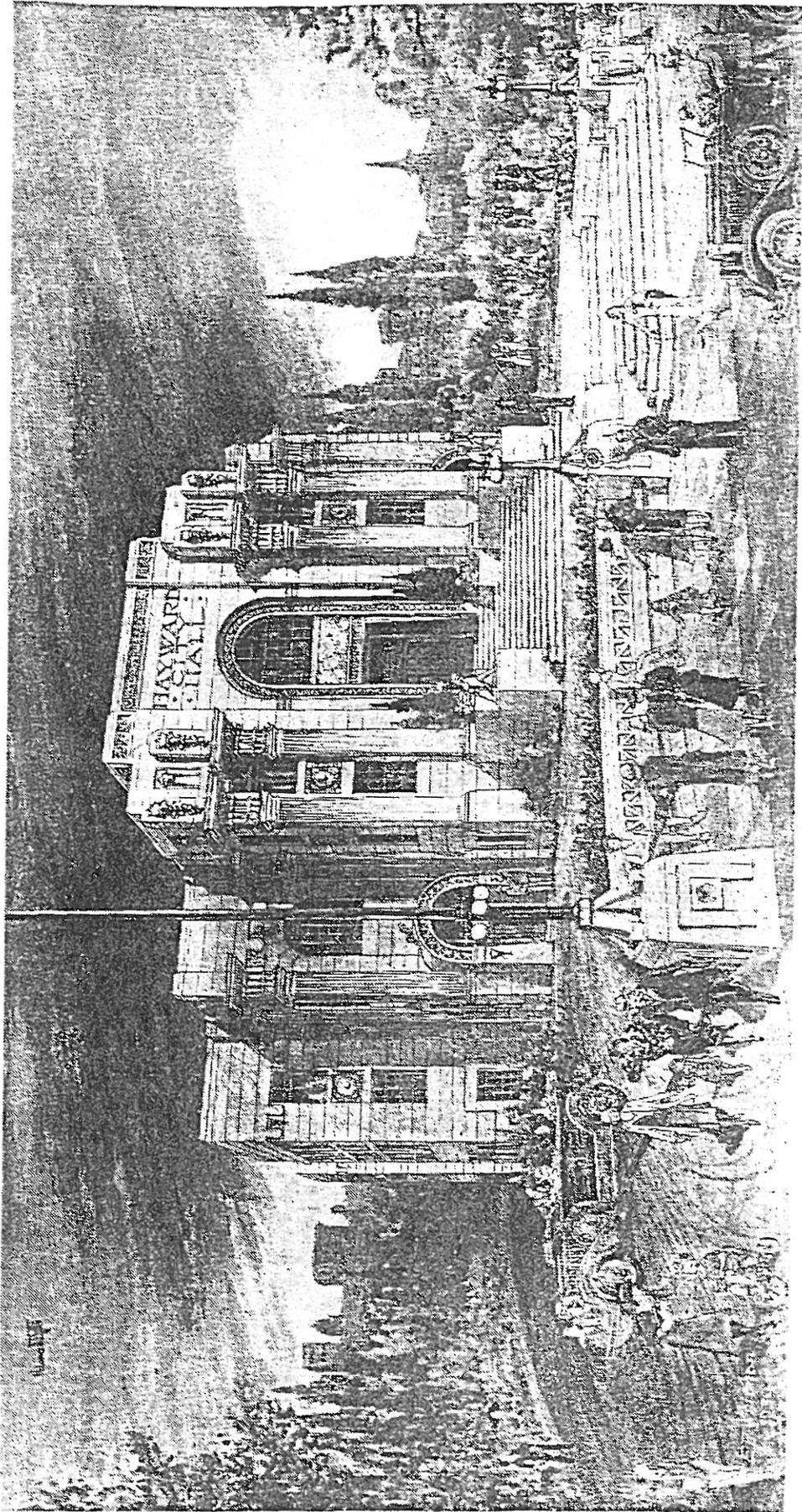


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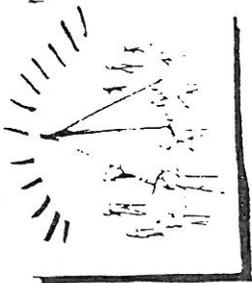
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Chapter I.

ORIENTATION



DOWNTOWN FOCAL POINT

FOCAL POINT LOCATION

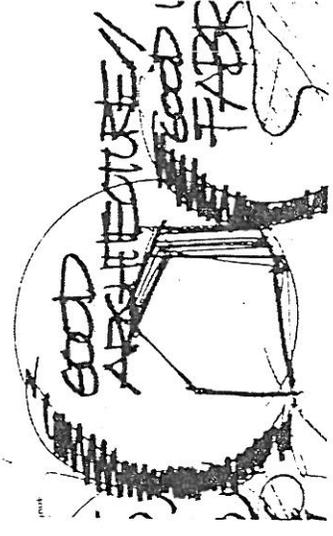
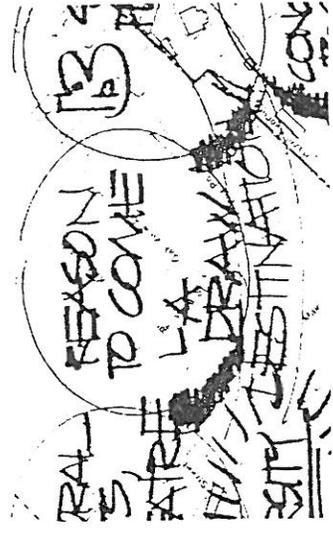
- CLOSE TO BART
- OPEN TO A, B, C-GO
- INCORP. LIBRARY
- MU

ACTIVITY

- GOVT
- OPEN SP
- MULT-USE

DESIGN

- PROMINENT, APPEAR
- INTERESTING
- VISIBLE FROM BART
- OPEN W/ PLAZAS
- UNIQUE



The Downtown Focal Point Master Plan was developed through a process organized around a series of public workshops.

ORIENTATION

The Role of the Focal Point in the Revitalization of the Downtown Core

The Downtown Focal Point Project is the first and most visible part of a district-wide revitalization effort to reestablish Hayward's Downtown Core as the functional and symbolic heart of the City. The intent of the project is to ensure that substantial and tangible results are achieved right at the beginning of the revitalization effort. By building a popular, distinctive and high-quality destination identified with downtown, the City will enhance the image and visibility of the district. It will spark the revitalization of B Street - the main public thoroughfare of the downtown - by creating a major new "anchor" for the district, and providing the critical activity link between the Bay Area Rapid Transit (BART) station and the downtown core. Library patrons, community room users and employees will frequent downtown business services, restaurants and shops, and add vitality to the area. Public festivities and events in the Plaza space will attract others to downtown on weekends and evenings as well.

Besides directly providing new investment, this dramatic demonstration of the City's commitment to its center is intended to stimulate new private investment that will result in an entirely revitalized core. The end result - described in detail in the new *Downtown Hayward Design Plan* - is envisioned as a compact and colorful cluster of businesses, public and private services, and pedestrian

amenities, with plenty of people living and working in upper stories and surrounding blocks.

The Purpose of this Document

This Master Plan is a tool for the implementation of the community's desire to get the maximum benefit for the downtown out of the portion of land now separating B Street from the BART Station. It will do so first by providing the City with sufficient information to commission an architect (or competing architects) to prepare design and construction documents for the new Hayward Public Library, parking structure and Town Plaza. Second, the Master Plan provides a policy framework to guide the development of the private buildings and spaces in the project - the commercial and residential buildings and related public streets and spaces - to insure the creation of a unified and memorable city center.

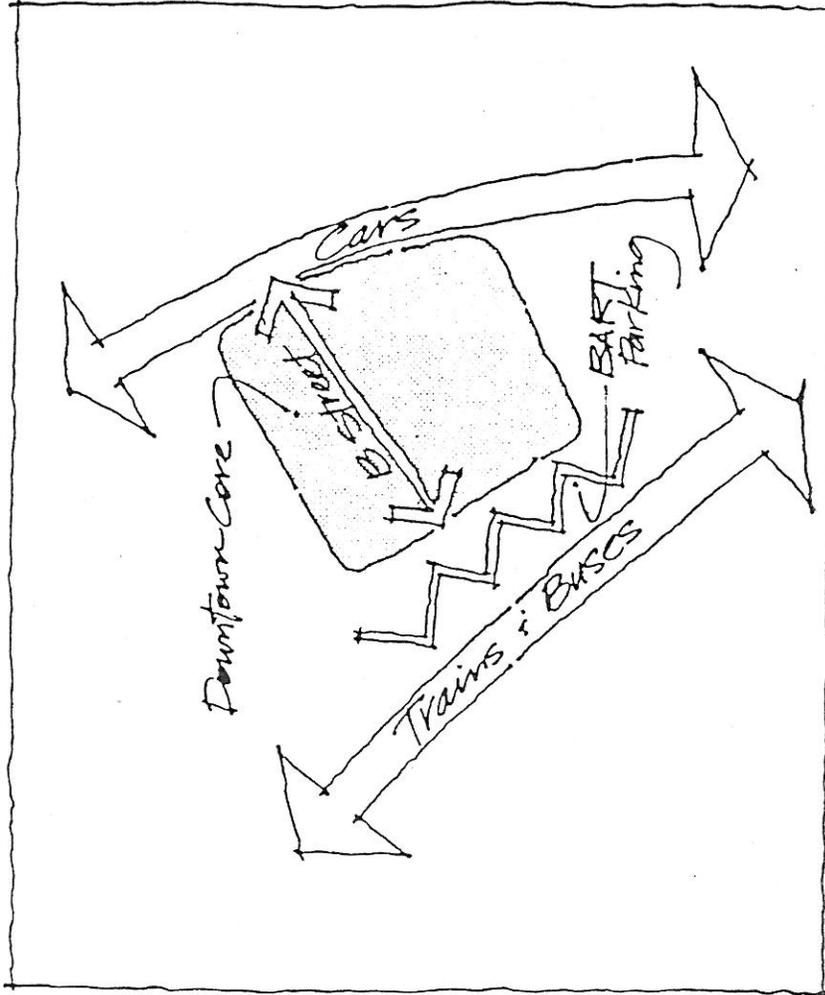
The Planning Process

The Downtown Focal Point Master Plan was developed through a process organized around a series of Public Workshops and City Council Study Sessions. Beginning in November, 1990, Hayward citizens participated in a series of five workshops to discuss their ideas related to construction of a major public building in the downtown core intended to catalyze revitalization activity and new investment. Workshop input was forwarded to the City Council Study Sessions held for the purpose of evaluating alternative site selection and site development scenarios. City Staff directed project consultants in the

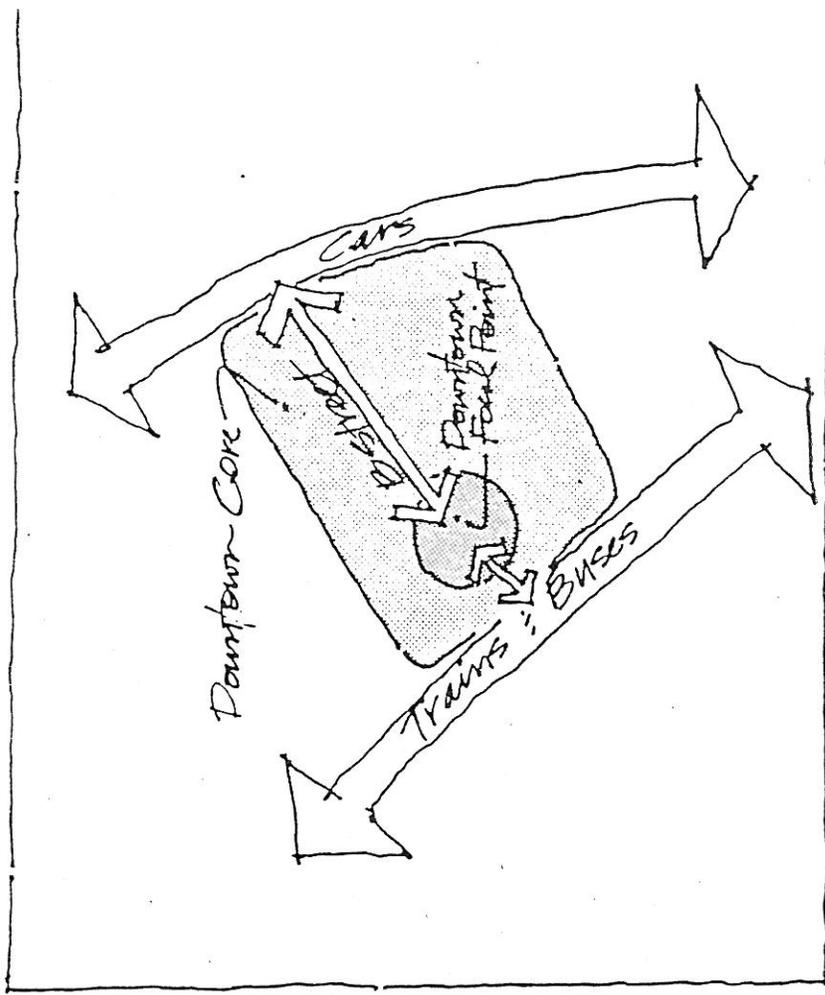
development and refinement of the plans in response to public comment and Council direction.

A series of different locations and site development options were evaluated (see Appendix A through D). A quick consensus was reached on the preferred location. Two alternatives were investigated: one using only land owned by the City, and one also incorporating the existing BART parking lot in a joint development effort. City Council, City Staff, and community workshop participants favored the larger site because it offered the greatest opportunity to establish a link between the Downtown Core and the BART Station. In response to encouragement from initial meetings between City Staff, BART Officials and Alameda County Transit (AC Transit), a plan for joint development on the larger site was prepared. (An interim site development plan has been prepared to insure that continued transit access is provided throughout the various potential phases of the joint effort).

Achieving consensus on the site development program was more difficult. Although workshop participants agreed that a new "Focal Point" for the Downtown Core should contain housing, shops, a public plaza, and a public facility, the precise type of public building(s) was a source of heated debate. Early schemes all focused on a new City Hall providing a powerful new landmark and infusing the Downtown with activity. In response to workshop input requesting that a Library and/or Cultural Center be considered, City Staff directed the development of an alternative site development plan focusing on a new City Library. The Library-focused



Existing Condition: Downtown Core Cut Off from Regional Transit Station



Future Condition: Completed Downtown Focal Point Links Downtown and Regional Transit Station

URBAN DESIGN CONCEPT

option was evaluated for its potential to catalyze downtown revitalization, as well as for its cost, and was found to be an excellent alternative public building component. After evaluation, the City Council ultimately chose the Library as the public building component for the new Downtown Focal Point.

Document Organization

This document is divided into six parts.

Part I - Orientation describes the City's intentions for the project, the process by which decisions were made, and the physical and cultural contexts within which the Downtown Focal Point must be sensitively fit.

Part II - Site Development Program describes the major development components, their relative locations, and their sizes.

Part III - Urban Design Guidelines contains the policy guidelines that will insure that the separate components result in a cohesive and high quality destination that bears strong relationship to the historic form of the downtown core and its important public buildings and spaces.

Part IV - Library Building Program and V - Construction Budget for Public Components facilitate the immediate commencement of the City's portion of the construction project by providing a construction budget and an inventory of library space needs and organizational requirements for that building's architect.

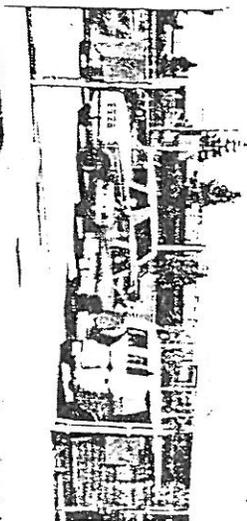
Part VI - Project Schedule provides an overall project schedule, outlining the coordination of the public and private components of the site.

The Site and Its Context

Revitalization Context

The economic vitality of Downtown Hayward has been a long-time concern to the community. Like many Bay Area communities, Hayward was transformed by the expansive pattern of postwar suburban development. What was a compact agricultural town a century ago became a sprawling landscape of freeways, industrial/office parks, residential subdivisions, and a regional mall today. Without direct access to freeways, downtown has had difficulty in attracting substantial new development and in maintaining a solid retail base. Many of the traditional mainstays of downtown's economic and social life - department stores, the old city hall, and Hayward High School - closed or left downtown. Adding to these problems are congested traffic conditions on nearby Foothill Boulevard and unreinforced masonry buildings on and adjacent to the Hayward Fault, which cuts through the center of downtown.

Downtown does have assets and opportunities, however. Most notably, development potential adjacent to the Hayward BART station is strong, due to BART's new emphasis on residential development for its station properties. In accordance with the *Downtown Hayward Design Plan*, connections between the Downtown Core and the station are to be

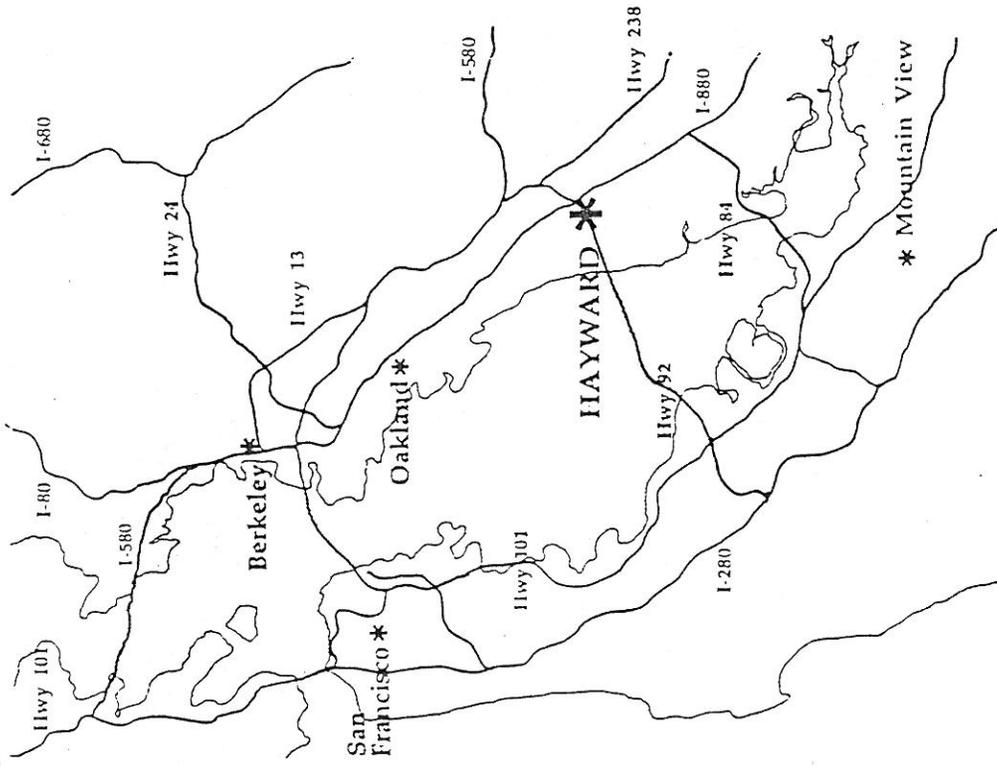


The Downtown Core is currently separated from BART by a large surface parking lot.

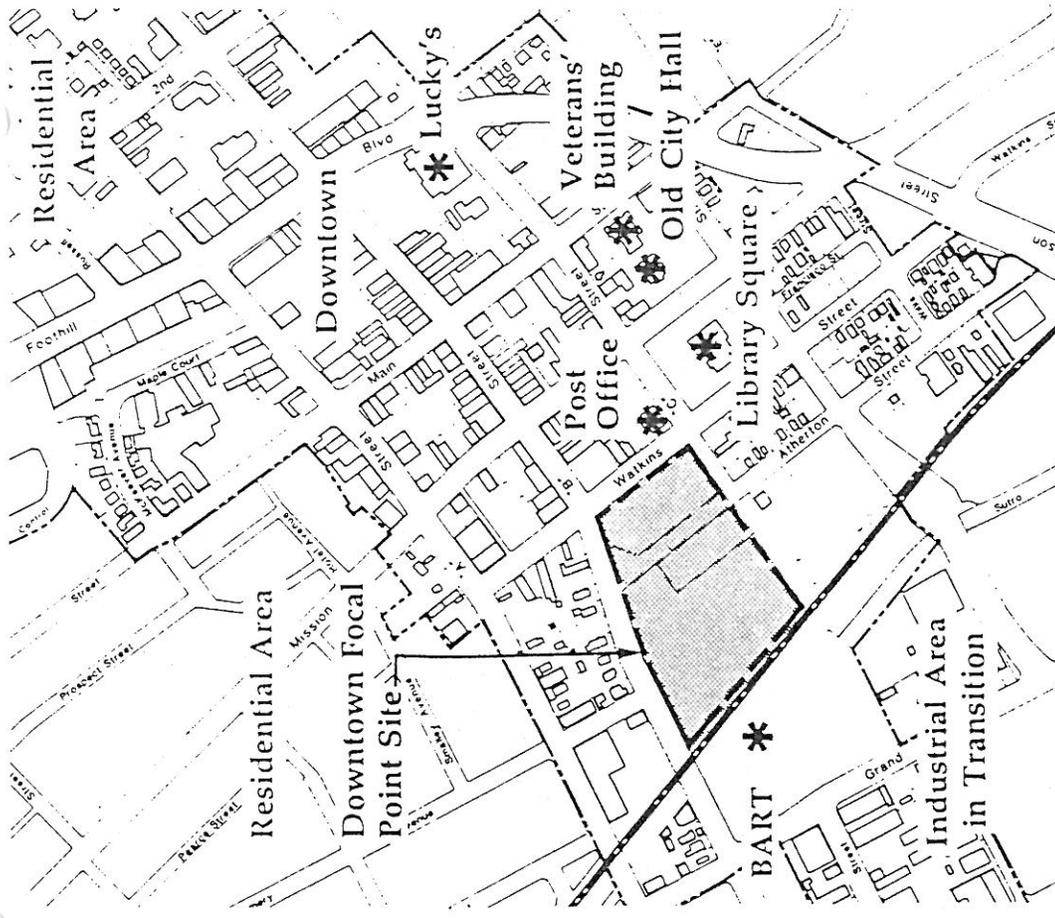
strengthened to attract the potential market of commuters and shoppers arriving from BART.

The heavy traffic volumes on Foothill Boulevard can be tapped as an asset in that they contain a significant population of potential customers. If the Downtown core becomes visible to this traffic and contains attractive destinations, some of this potential could be realized. There is no specially or entertainment district to serve East Bay communities between San Jose and Berkeley/Oakland. Downtown's collection of attractive historic buildings and the intact traditional "main street" form of B Street are key ingredients that can be built upon to help capture this market.

Finally, public comments at workshops and unexpectedly high attendance at downtown festivals indicate that there is substantial local community interest in strengthening downtown as the active heart of Hayward.



REGIONAL LOCATION

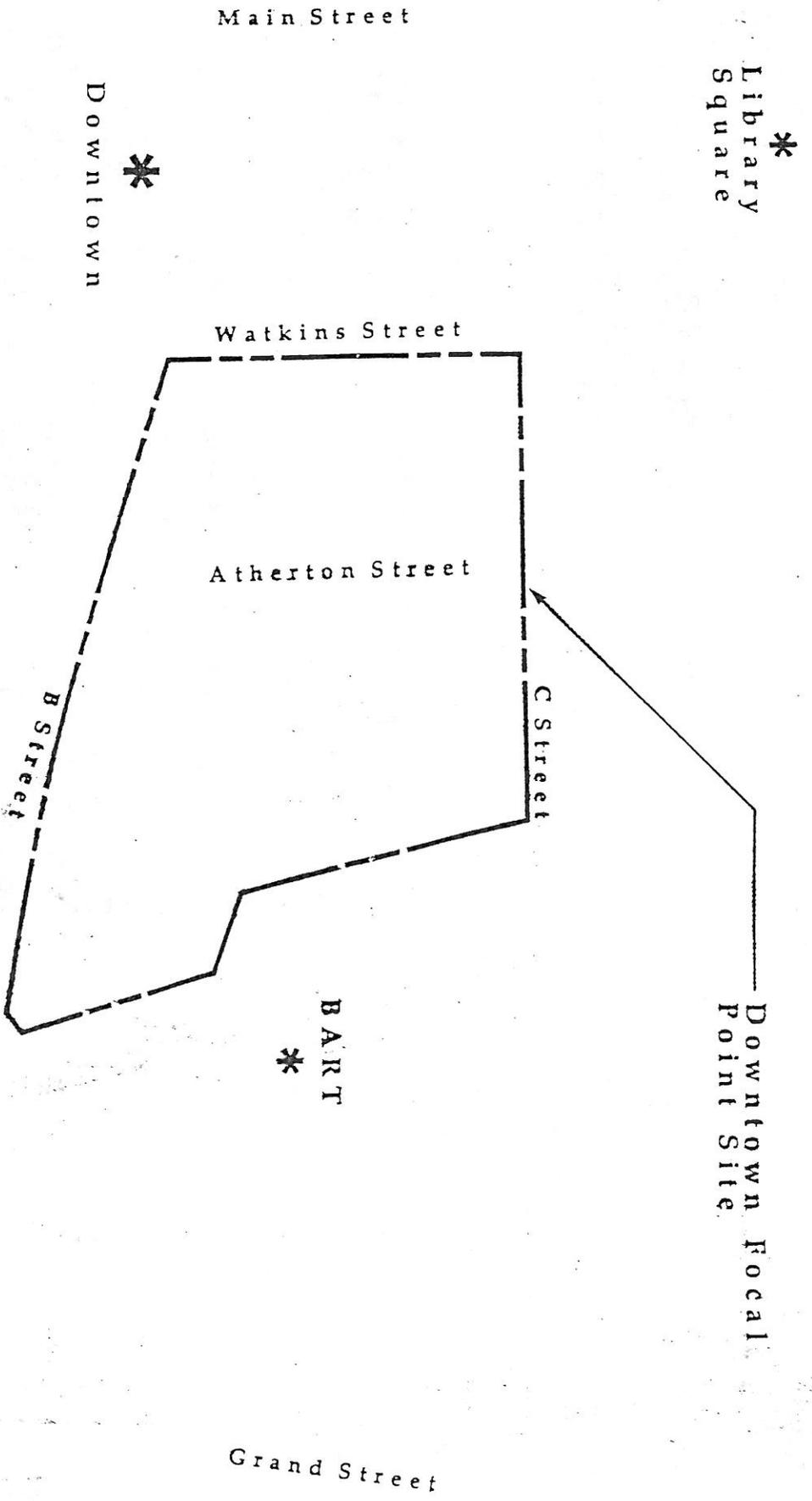


DOWNTOWN LOCATION



LOCATION MAP

AERIAL VIEW



Main Street

*
Library
Square

*
Downtown

Watkins Street

Atherton Street

B Street

C Street

Downtown Focal
Point Site

*
BART

Grand Street

Site Location and District Context

The Downtown Focal Point occupies a key location within the greater Downtown area (see Site Location Map). The existing core of downtown lies just to the east - a quadrant bounded by A Street, D Street, Watkins Street, and Foothill Boulevard, with major commercial activity also extending north on Foothill. The BART station and a proposed AC Transit "timed transfer" bus facility border the site on the west. Residential areas lie to the north and south. Major through-traffic corridors near the site are Foothill Boulevard/Jackson Street, Mission Boulevard, and A Street.

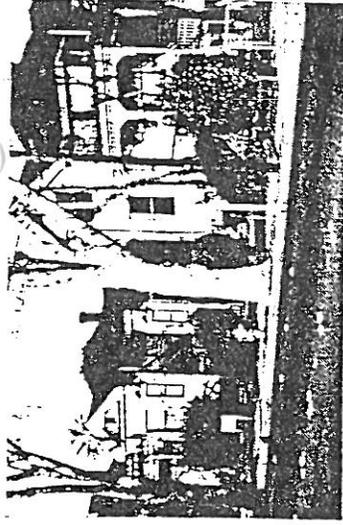
On-Site Conditions

The site is approximately 8 acres in size. It includes approximately 3.8 acres of land owned by the City of Hayward, including a portion of the Atherton Street right-of-way, and 4.2 acres owned by BART, excluding the bus station and commuter drop-off area (see Aerial View map). Hayward lands include the block bounded by B Street, C Street, Watkins Street, and Atherton Street. The block contains a small triangular park at the corner of B and Watkins, and buildings which were occupied by "Eden Express" and "McCullough Chevrolet". Also included in the city-owned lands is a surface parking lot along the westerly frontage of Watkins Avenue currently owned by McCullough Chevrolet. BART lands consist of the "East Parking Lot" for the Hayward Station, containing approximately 375 spaces. BART plans to consolidate this parking in a struc-

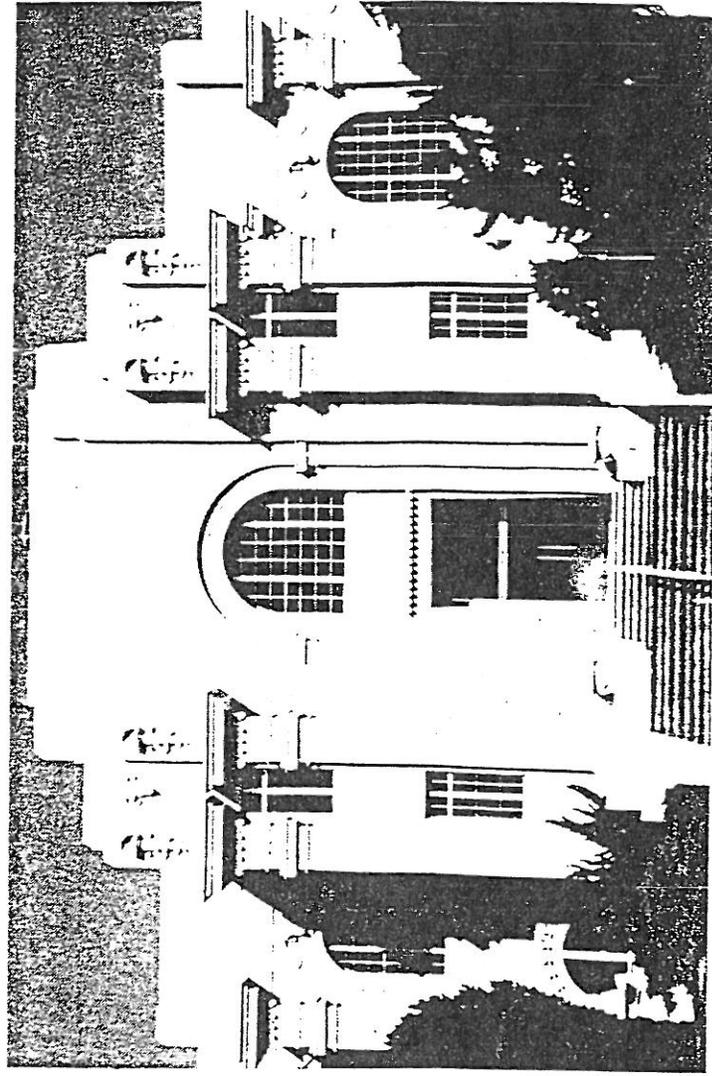
ture on the west side of the station. This would free the site for development.

Off-Site Conditions

Across B Street to the north is a row of historic Victorian houses and a row of majestic street trees. This frontage is an important feature of Downtown, and is planned for preservation by the *Downtown Hayward Design Plan*. (The City is considering the establishment of a Victorian Historic District to protect these buildings.) The



The row of historic Victorian houses and majestic street trees across B Street is an important feature of The Downtown Core.

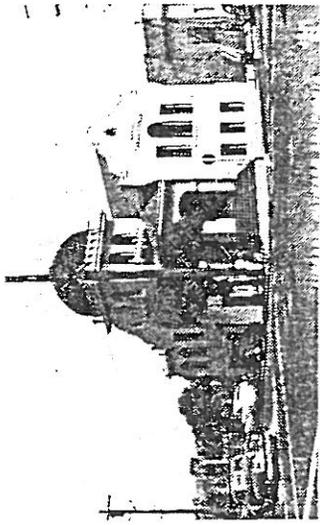


The old City Hall is a cherished symbol and one of the most important civic images in the City.

Hayward Downtown Core



BUILDING PATTERN



Hayward's first public library was Carnegie Library which stood at the corner of B Street and Foothill Boulevard.

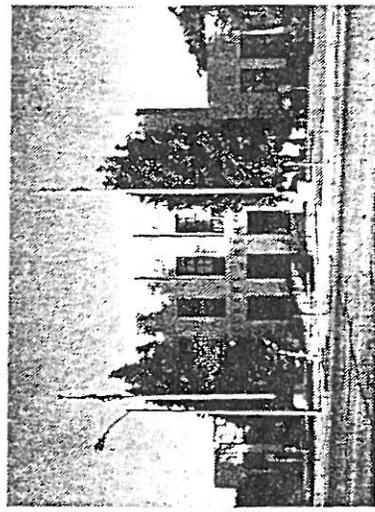
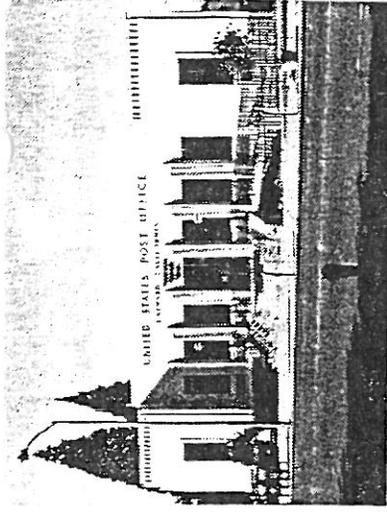
northwest corner of B and Watkins Streets is being considered as the site for a new Downtown Fire Station. East along B Street the downtown commercial district links to the site with a continuous frontage of one and two story commercial buildings. East across Watkins Street are parking lots, a small commercial building, and the side of the historic downtown Post Office (this block is currently being acquired by the Hayward Redevelopment Agency). To the southeast is "Library Square", containing the present library and landmark heritage trees. South

across C Street between Atherton and Watkins is a block of small houses, and between Atherton and BART is vacant city-owned land; the entire C Street frontage is proposed for new multi-unit infill development by *Downtown Hayward Design Plan*. A mixture of residential and light industrial buildings is located to the south of the BART station.

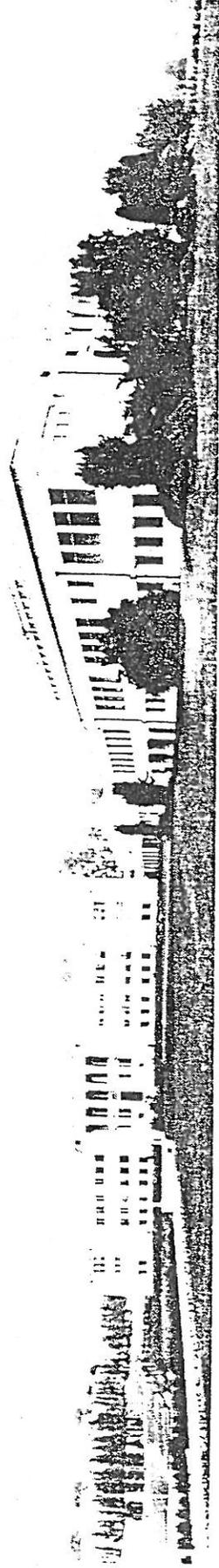
Architectural Context

Downtown Hayward's historic public buildings and spaces establish a local tradition of civic imagery, and contribute to downtown's character and pedestrian-oriented building scale. These buildings provide a design context for the new Main Library.

The Old City Hall (1930) at 22938 Mission Boulevard is a highly-ornamented, terracotta-clad "City Beautiful" building in an unusual, eclectic classical style. It is a cherished symbol and one of the most important civic images of historic Hayward for its residents. The building has a formal relationship to the street, partly set back from yet fully facing Mission Boulevard. The city outgrew the facility, and in 1969 it became a Police De-

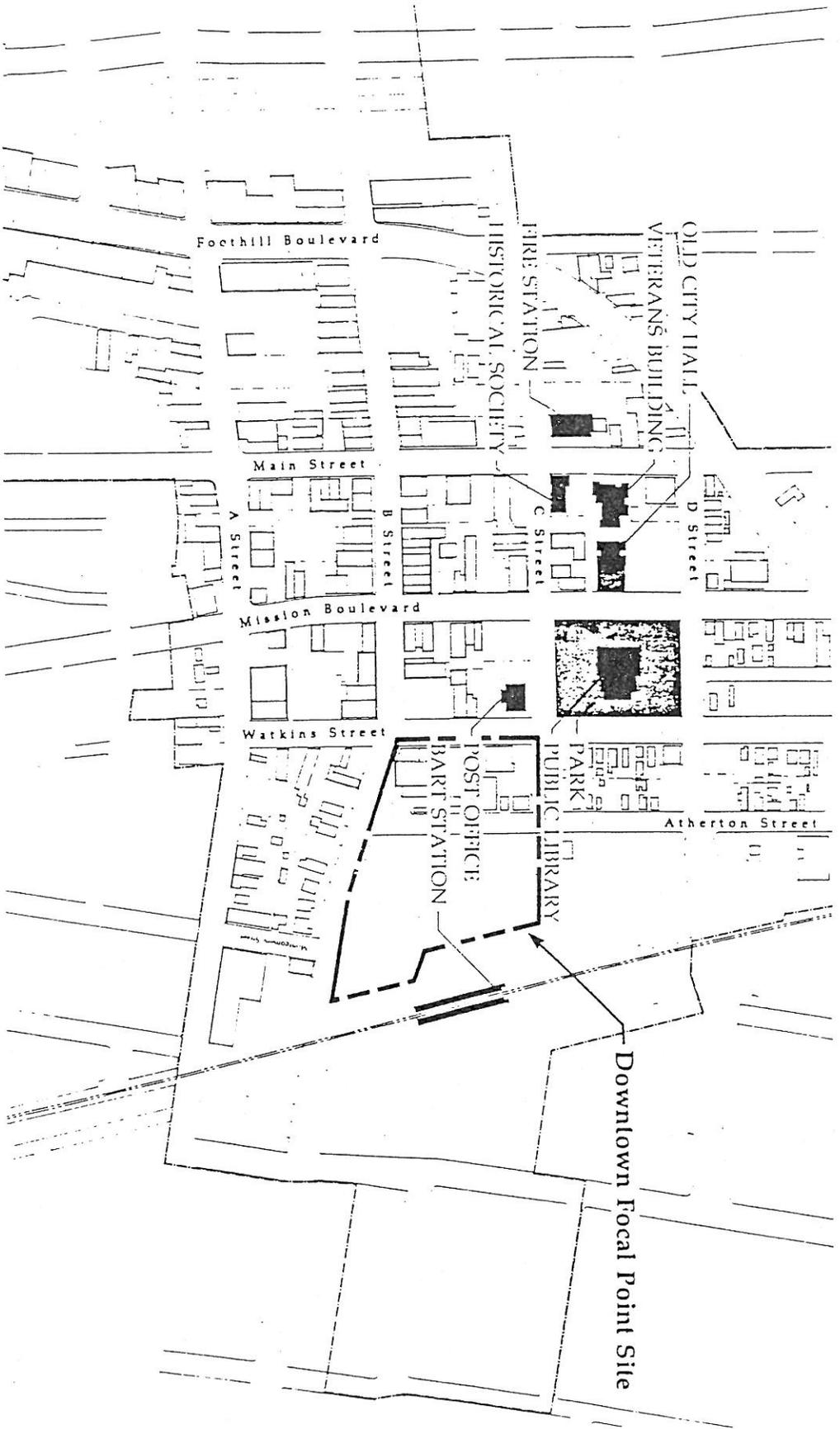


The Downtown Post Office and Veteran's Building were identified as important historical images.



The demolished Hayward Union High School remains an important image for long-time Hayward residents.

Hayward Downtown Core



EXISTING PUBLIC BUILDINGS

partment building. The building was vacated in 1975 because of its need for seismic reinforcement.

The Veterans Memorial Building (1932) is just behind the Old City Hall, facing Main Street. It also has a formal, frontal relationship with the street. While maintaining the traditional imagery of a colonnaded public building, its "WPA Moderne" architectural treatment is more restrained and streamlined than the ornate classicism of the Old City Hall. Its light colored masonry surface relates well to the Old City Hall.

The Downtown Post Office (circa 1934) faces C Street and Library Square. It is similar to the Veterans Memorial Building in its frontal relationship to the street, its architectural style, its masonry surface and light color, and its image of a symmetrical front with fluted pilasters and flanking building wings. The Old City Hall, the Veterans Memorial Building, and the Downtown Post Office form an identifiable "Civic Center" group of downtown public buildings with shared architectural characteristics.

Hayward's first public library (1905-1949) was the three-story Carnegie Library which stood at the corner of B and First Streets (First Street was replaced by the Foothill Boulevard right-of-way). The Library was a simple stucco building in a Spanish style. It was essentially domestic in character, in keeping with the small town scale of Hayward at the time it was built.

The present brick and concrete library was built in 1951 in Library Square, which was

deeded to the City for use as a park in the 1800's. While the library was carefully sited amid the park's heritage trees, its simple exterior and modest modern functional style did not reflect the civic qualities of Hayward's other public buildings. In 1958 it was enlarged to 9,300 square feet, and in 1981 it was expanded to 25,000 square feet, the maximum building footprint possible without removal or damage to the historic trees and other landscaping of the Square. Future uses under consideration for the building, such as an arts center, would preserve its civic function.

Other, now-demolished structures such as the classically colonnaded Hayward Union High School (1913-1967) form part of the civic imagery of downtown that remains important and meaningful to longtime Hayward residents.

Chapter II.

SITE DEVELOPMENT PROGRAM

SITE DEVELOPMENT PROGRAM

Full Buildout

The development program established during the public and staff planning process for the Downtown Focal Point includes a mix of civic, commercial, and residential development. It consists of the following elements (see "Site Development Program Illustration on the following page):

Program Summary

| Facility | Size |
|--|--------------------------------------|
| Main Library | 55,000 SF ± |
| Central Plaza | 22,000 SF ± |
| Parking Structure With Commercial Frontage | 140 cars @ 2 levels with 12,000 SF ± |
| Housing | 180 units ± |
| Retail Pavilion | 22,000 SF ± |

Note: Quantities of development listed above are approximate. They may vary somewhat to reflect changes in market conditions or construction cost parameters.

The "Master Plan" on the following page illustrates a configuration of development that meets the guidelines contained in Chapter III.

(Other configurations are possible.) It incorporates aspects of the surrounding street and block pattern of downtown to create a new precinct that joins the City and BART. The Library building and Plaza create a civic destination that links downtown to the BART station, and provides a focus for residential development on adjacent blocks.

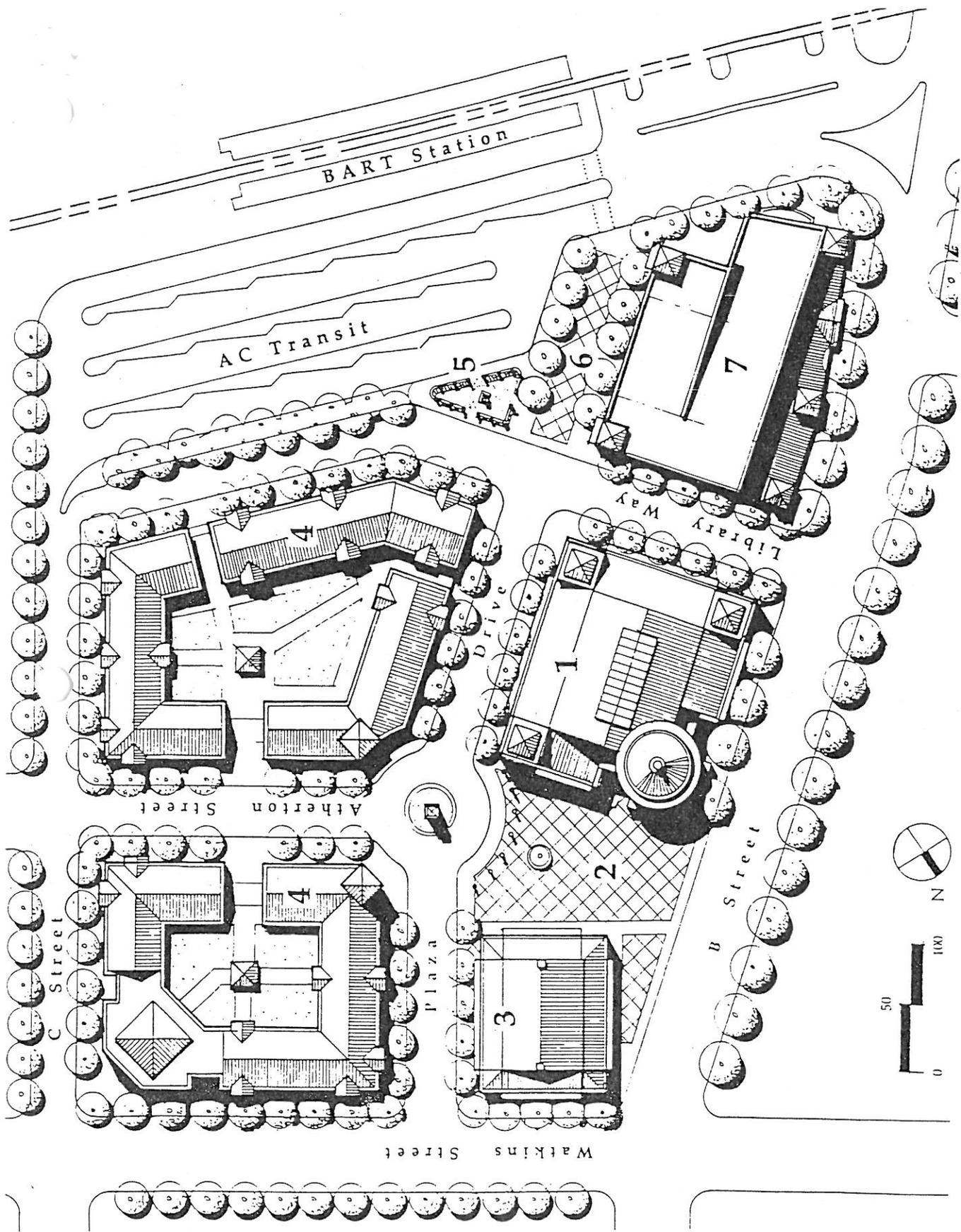
Parking for the Library and retail commercial uses will be provided in a structure to the west of the building adjacent to the access drive for the BART station. Parking for housing will be provided in subsurface garages. To conserve land and maximize development opportunities, no surface parking, with the exception of on-street curbside parking, will be provided on the site when it is built out.

Potential Interim Development

Over half of the Downtown Focal Point site will be needed to accommodate BART's surface parking spaces if construction of the commuter parking structure on the west side of the station is not completed prior to the commencement of construction of the residential portion. In that event, the new surface parking lot should be configured as indicated by Figure x during the interim period. This will accommodate approximately 383 spaces (375 exist today) on 4.1 acres, leaving 3.9 acres for development of the Library, Plaza, Parking Structure, and Retail Pavilion.

internal streets, including the proposed "Plaza Circle". This will allow street trees and possibly sidewalks to be established as part of Phase I. Residential development could then simply "fill in" the site once the commuter parking structure is completed.

To the extent possible, the layout of vehicular access lanes within the interim parking lot should reflect the final configuration of



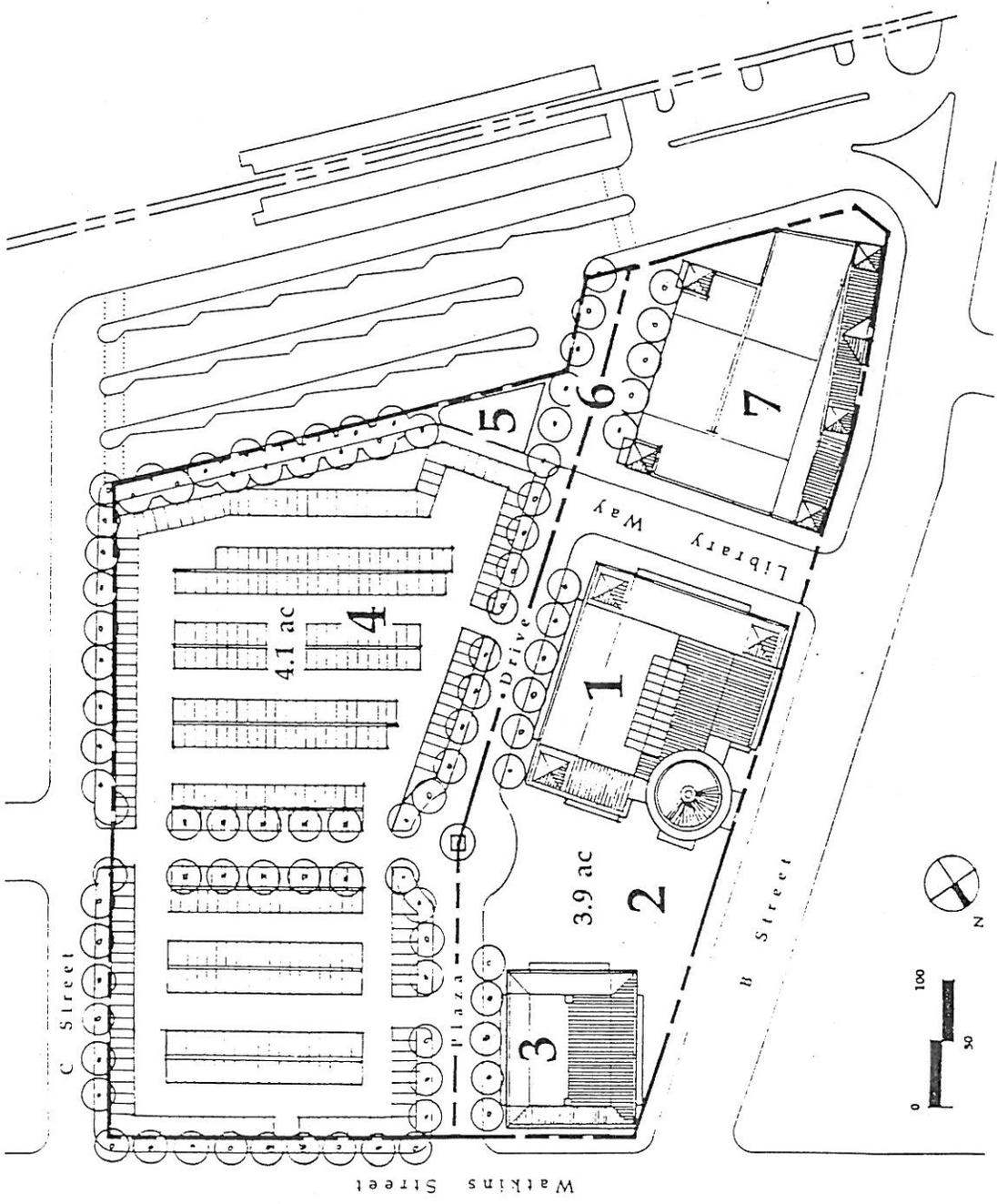
SITE DEVELOPMENT PROGRAM

L E G E N D

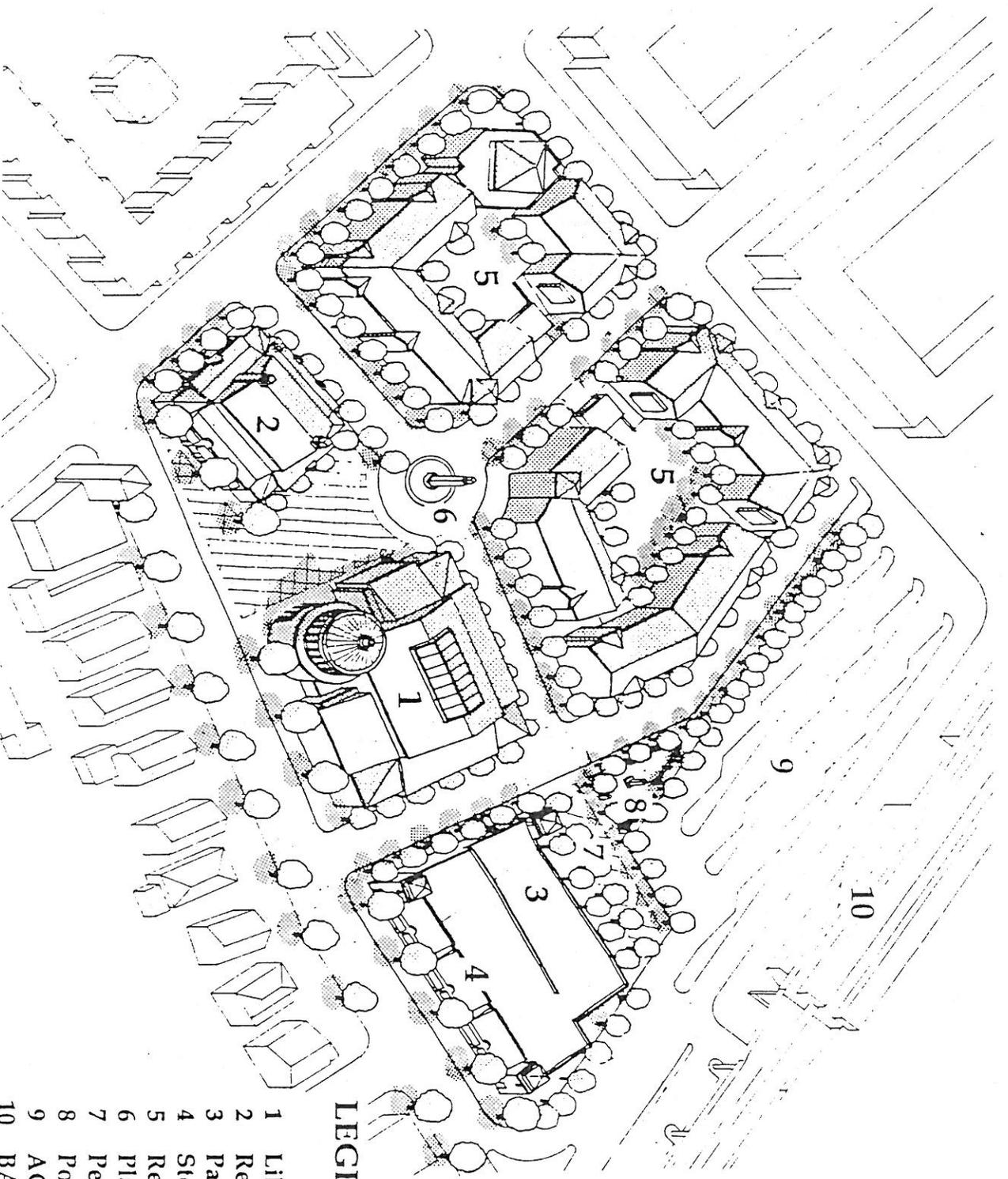
- 1 CITY OF HAYWARD
MAIN PUBLIC LIBRARY
55,000 sq.ft. \pm ;
2 stories
 - 2 PUBLIC PLAZA
22,000 sq.ft. \pm
 - 3 RETAIL PAVILION
22,000 sq.ft. \pm
 - 4 RESIDENTIAL DEVELOPMENT
180 units \pm ;
288 parking spaces
(avg. 1.6 spaces/unit)
 - 5 POCKET PARK
2,500 sq.ft. \pm
 - 6 PEDESTRIAN PROMENADE
10,000 sq.ft. \pm
 - 7 PARKING STRUCTURE &
STOREFRONT COMMERCIAL
42,000 sq.ft. \pm ;
140 spaces; 2 levels;
12,000 sq.ft. \pm ground
floor commercial
- TOTAL PARCEL SIZE:
8.0 acres

LEGEND

- 1 LIBRARY
 - 55,000 sq.ft. ±;
 - 2 stories
- 2 PUBLIC PLAZA
 - 22,000 sq.ft. ±
- 3 RETAIL PAVILION
 - 22,000 sq.ft. ±
- 4 BART PARKING
 - 383 spaces
 - (375 spaces existing)
- 5 POCKET PARK
 - 2,500 sq.ft. ±
- 6 PEDESTRIAN PROMENADE
 - 10,000 sq.ft. ±
- 7 PARKING STRUCTURE & STOREFRONT COMMERCIAL
 - 42,000 sq.ft. ± ; 140 spaces
 - 2 levels
 - 12,000 sq.ft. ± ground floor commercial



POTENTIAL INTERIM DEVELOPMENT PROGRAM



LEGEND

- 1 Library
- 2 Retail Pavilion
- 3 Parking Structure
- 4 Storefront Commercial
- 5 Residential Development
- 6 Plaza Circle
- 7 Pedestrian Promenade
- 8 Pocket Park
- 9 AC Transit Facility
- 10 BART Station

ILLUSTRATIVE DESIGN CONCEPT



DOWNTOWN PLAN CONTEXT

Chapter III.

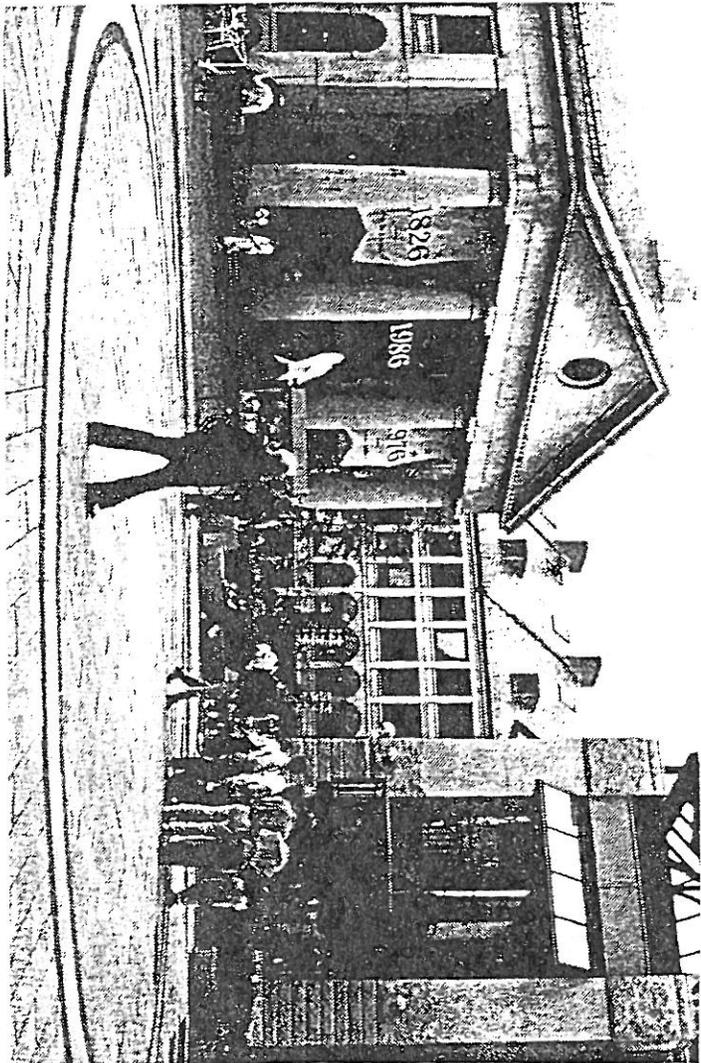
URBAN DESIGN GUIDELINES

URBAN DESIGN GUIDELINES

The Urban Design Guidelines focus on the external relationship of the Downtown Focal Point to the city. They address the layout and design of streets and spaces as well as issues related to building form, materials, and architecture in general. Their most important function is to make the Downtown Focal Point a special precinct that is part of the fabric of the Downtown Core, not just a well-designed project.

The Guidelines express the public's goals for the site as they emerged during the course of the workshop process. They serve as performance standards for the design and development of the Library and surrounding site, and they clarify key design parameters for the architects and other designers and decision-makers who will ultimately determine the final form of the Downtown Focal Point's buildings and spaces. "Should" indicates that a guideline is discretionary. "Must" or "shall" indicate that deviation from the guideline would seriously compromise the intent of the Master Plan.

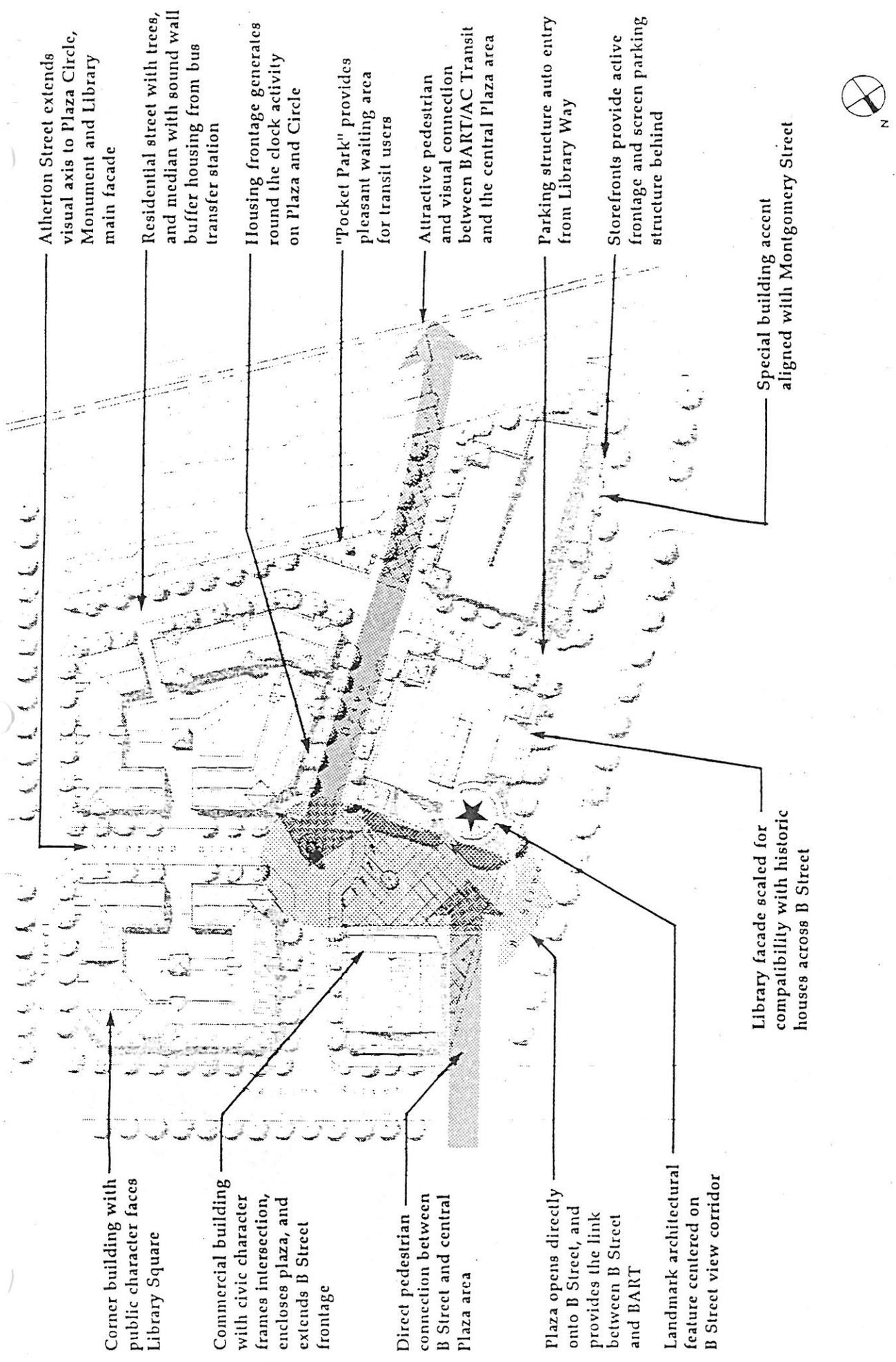
In addition to the programmatic and architectural guidelines contained in the *Downtown Focal Point Master Plan*, the project architect must adhere to building and safety requirements listed in the City of Hayward's Building Code and Fire Department Standards.



The Downtown Focal Point precinct must be oriented around a Plaza.

1. RELATIONSHIPS OF BUILDINGS, STREETS, AND SPACES

- a) Relationship to B Street and Downtown
 - i) The Library building must incorporate a landmark architectural feature, such as a dome or tower, that provides a visual anchor for the western end of the B Street view corridor. This feature should also be highly visible from the BART station.
 - ii) A central Plaza must be provided that opens directly onto B Street.
 - iii) Public and private buildings associated with the Downtown Focal Point must continue the pedestrian scale street environment that exists downtown. Buildings must form a consistent frontage that is compatible in scale and character to downtown's larger older buildings.
 - iv) Massing and frontage articulation of buildings along B Street must reflect



Atherton Street extends visual axis to Plaza Circle, Monument and Library main facade

Residential street with trees, and median with sound wall buffer housing from bus transfer station

Housing frontage generates round the clock activity on Plaza and Circle

"Pocket Park" provides pleasant waiting area for transit users

Attractive pedestrian and visual connection between BART/AC Transit and the central Plaza area

Parking structure auto entry from Library Way

Storefronts provide active frontage and screen parking structure behind

Special building accent aligned with Montgomery Street

Corner building with public character faces Library Square

Commercial building with civic character frames intersection, encloses plaza, and extends B Street frontage

Direct pedestrian connection between B Street and central Plaza area

Plaza opens directly onto B Street, and provides the link between B Street and BART

Landmark architectural feature centered on B Street view corridor

Library facade scaled for compatibility with historic houses across B Street

ESSENTIAL RELATIONSHIPS

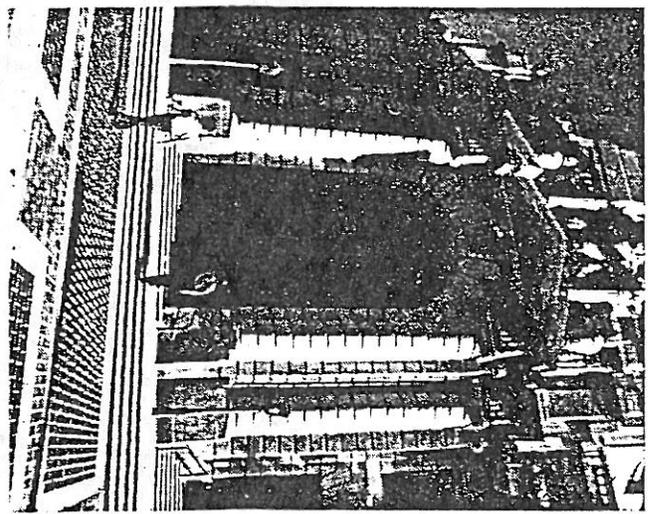
the scale and parcelization pattern of historic residential buildings across the street to the north; e.g. the Library and other buildings along the frontage should have a module of approximately 50 feet, consistent with the frontage of parcels across the street.

- v) A commercial building with active first floor uses must be located at the corner of Watkins and B Street to extend the fabric of the downtown commercial district and add life and services to the precinct; this "Retail Pavilion" must be set back from B Street to allow an unobstructed view of the Library from downtown.

- vi) The B Street facade of the Library must be second only to the Plaza facade in terms of prominence, level of articulation, and overall architectural quality (see next section).

- vii) The older existing brick storefront building at the corner of Watkins and B (currently "Eden Express") should be considered for preservation and inclusion as part of the Retail Pavilion; this would help to link new development to the existing fabric of downtown.

- viii) Active ground floor commercial uses should be extended along the remainder of the B Street frontage as feasible.



The primary entrance to the Library must open onto the Plaza.

b) Relationship to the Plaza

- i) The Downtown Focal Point precinct as a whole must be oriented around the Plaza, with representative building types, residential, commercial, and public, all fronting the space. Residential buildings are especially important in order to keep the space used and lively.

- ii) The Plaza must be oriented in such a way as to encourage pedestrian movement back and forth through it;

i.e. it should lie directly between downtown and the BART station entrance.

- iii) Two separate, free-standing buildings must define the Plaza space on at least two sides.

- iv) The primary and/or ceremonial entrance to the Library must open onto the Plaza, and it must be a highly visible and attractive architectural feature.

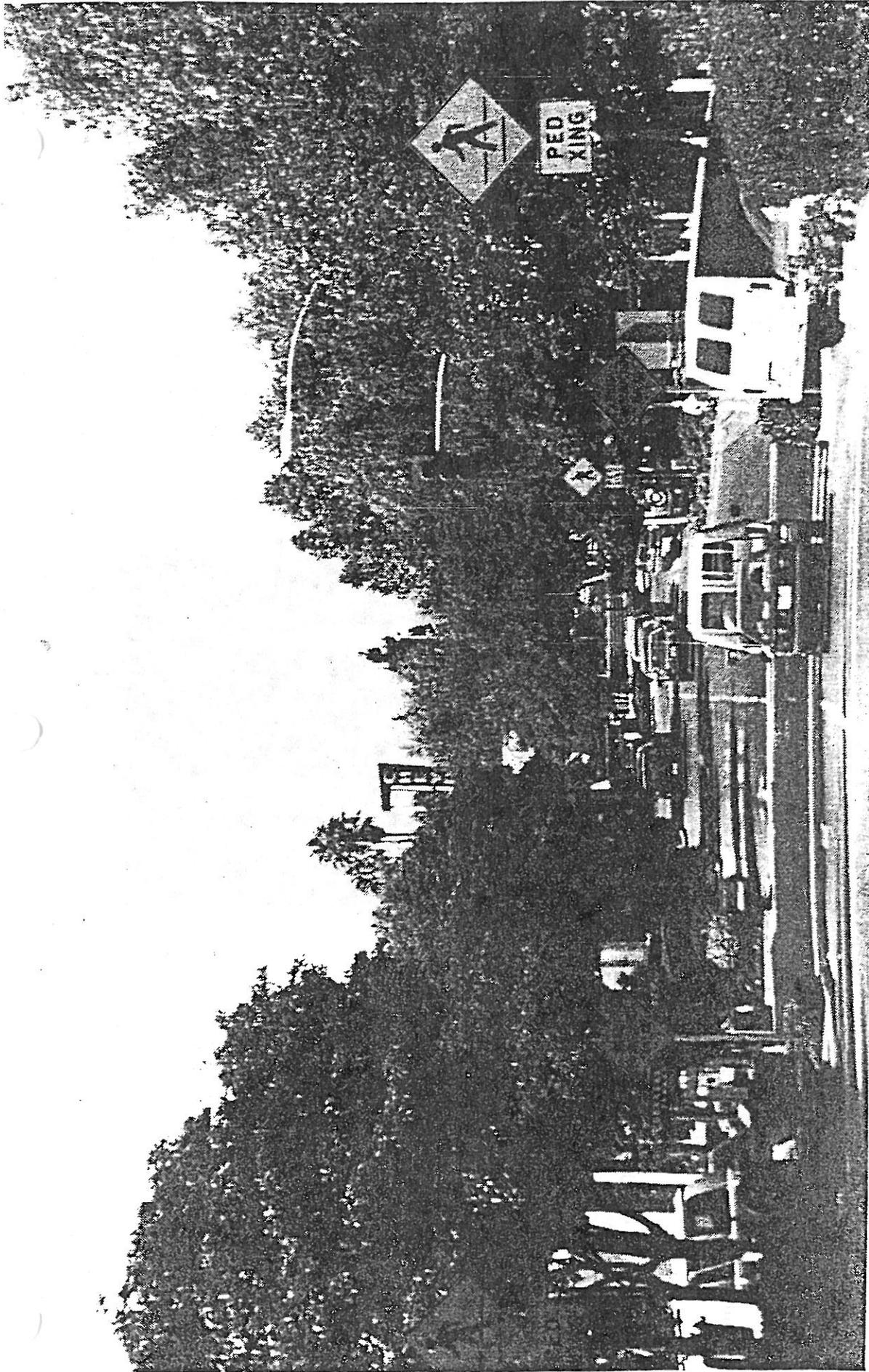
- v) The Plaza-facing facade of the Library must exhibit the building's highest level of architectural quality, materials, and detailing.

- vi) At least one food service business providing outdoor eating should be located on the Plaza. Additional pedestrian-oriented businesses, such as a newsstand, florist, etc., are encouraged.

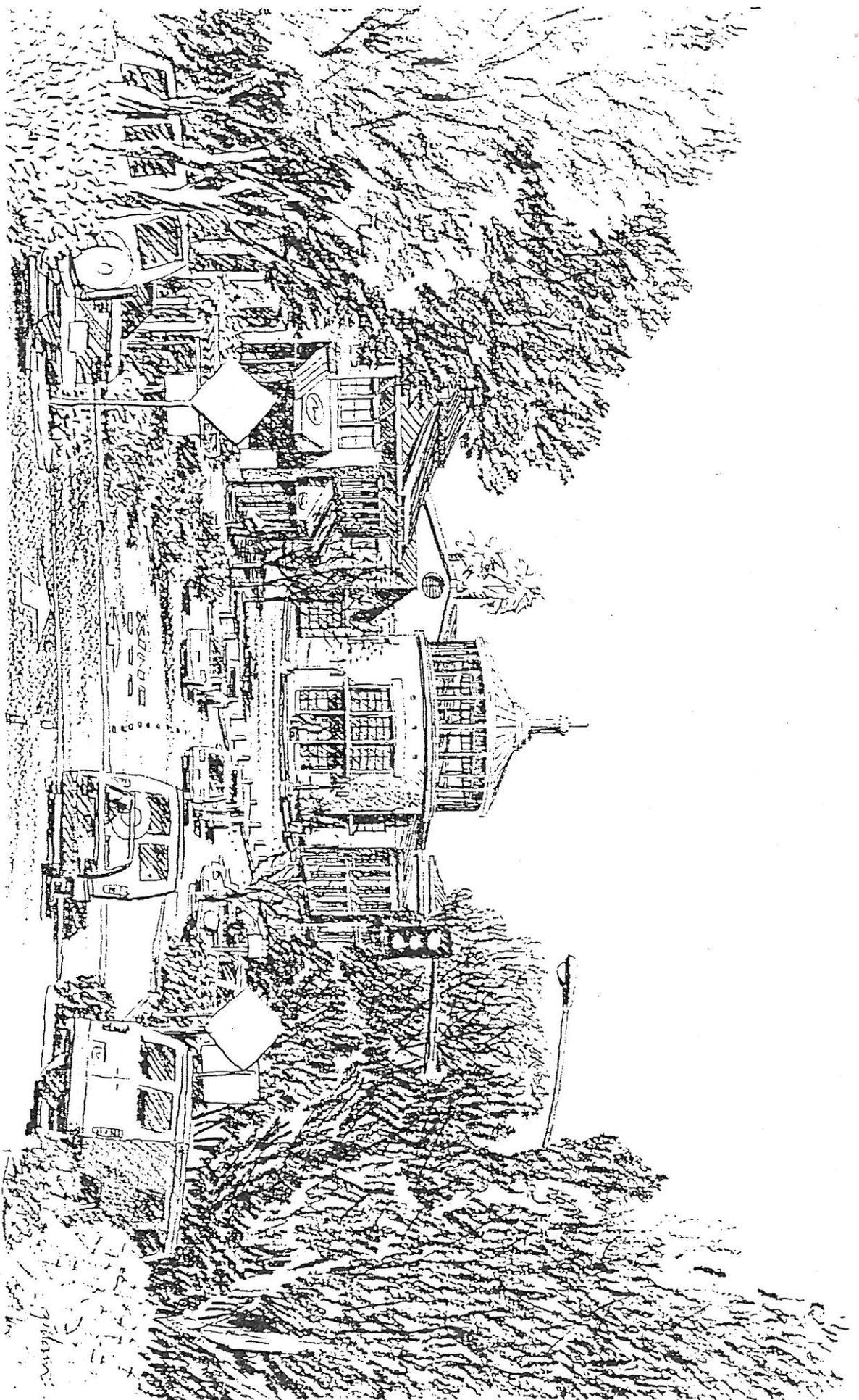
- vii) Buildings adjacent to the Plaza must provide attractive facades and major building entries on the Plaza.

c) Relationship to BART/A/C Transit Bus Transfer Station

- i) The Downtown Focal Point must provide a clear, attractive pedestrian linkage between the Station and the Downtown Core, beginning at the intersection of B Street and Watkins Avenue.



*Looking west along B Street
toward the Downtown Focal Point site.*



The Library's landmark architectural feature will provide a visual anchor for the B Street Corridor.

- ii) The pedestrian linkage should be in the form of a prominent street or pedestrian-only promenade that welcomes BART and AC Transit facility users to the city.
- iii) Special building facades, freestanding architectural elements and other forms of civic art should be used in combination to create a sequence of conspicuous visual "targets" that lead pedestrians back and forth through the site (see particularly Section 8-a-iv regarding "Library Circle").
- iv) An attractive open space or "green" should be provided to link new development on the Downtown Focal Point site to the BART/AC facility. The space should provide a pleasant place for bus riders to wait and for nearby residents to see and use.
- v) Because the BART station is not a particularly attractive structure, it should be screened from view from B Street, and at least partially screened from view from the Plaza.
- vi) Residential buildings should be buffered from noise from the proposed Bus Transfer facility. In order to allow the front of homes to face the station, street and fronting landscaping must filter and soften the view.
- vii) Building(s) adjacent to the BART vehicular entrance off of B Street should be massed and the facades designed to form a gateway to the station.
- viii) The roofscape of buildings within the Downtown Focal Point precinct should be attractive when viewed from the elevated BART station platform, downtown locations and the Hayward hills. Pitched roofs are recommended.
- d) Relationship to Existing Library Plaza
 - The Library Plaza is defined as the entire block occupied by the existing library building.
 - i) A special building or part of a building must be located at the corner of C Street and Watkins Avenue to link the Downtown Focal Point precinct to Library Plaza and the Old Post Office.
 - ii) The corner building should contain a use that is civic or public in character; e.g. meeting or recreational facilities for residents in adjacent housing developments.
 - iii) The corner building must have a prominent corner treatment; e.g. a tower, "cut" corner, major building entrance, etc. A symmetrical design is preferable to project a civic character.
 - iv) The corner building should be built to the back of the sidewalk to avoid being set back sufficiently to form a new public open space which would diminish the enclosure of Library Plaza.
- e) Relationship to C Street
 - i) Residential development shall be provided along the C Street frontage of the site.
 - ii) Buildings along the north side of C Street should be sited and massed to be compatible with the scale and character of housing proposed for the south side of C Street.
 - iii) C Street should be improved as an attractive residential street, with parallel parking, planting strip, street trees, and sidewalk. (See "Streets, Sidewalks, and Service and Emergency Access" for additional guidelines related to street improvements.) Building soundproofing in the form of additional insulation, glass block windows, etc., is recommended rather than soundwalls to buffer units from buses exiting the AC bus facility.

2. PLAZA DESIGN

- a) Size & Shape
 - i) The Plaza should be approximately 22,000 s.f. in area (1/2 acre ±). This would be large enough for public events yet small enough to be comfortable for individuals and small groups. It could accommodate a crowd of approximately 3,200 for a public ceremony or outdoor concert (approximately 7 s.f./person), or a "standing room only" crowd of approximately 7,300 (3 s.f./person).



The Plaza should accommodate formal public events.

- iii) Paving materials and color should contrast yet complement materials and color used for adjacent buildings. Using the same color and/or type of material for both the Plaza and adjacent buildings could be visually monotonous. Large expanses of white or very light paving surfaces create glare and should not occur.
- iv) The size, shape and texture of paving materials should complement the size and use of the plaza space. Smaller pavers such as bricks generally create a more intimate scale; larger stone slabs or concrete modules are more monumental. Cobbles or other unevenly surfaced materials are not appropriate for major pedestrian areas but may be considered for borders or accents.
- v) Paving patterns and material combinations should be relatively simple so that the Plaza can function as a medium to unify a number of different building types. In general, the pattern should resemble a carpet with two basic materials, one for the field and one for the edge adjacent to buildings and streets. A third, more decorative material, could be used to accent special locations within the space or adjacent to a special architectural feature. Large, high-contrast grids and other extremely bold figures are not recommended.
- ii) The plaza should be dimensioned to complement the Library building. It is anticipated that the Library will be wider than it is high, and the Plaza should generally appear wider than deep when viewing the Library's Plaza-fronting facade.
- b) Paving
 - i) The Plaza surface should be level to emphasize its accessibility to the public and encourage free-flowing pedestrian movement. Steep stairs or other dramatic forms of grade change should not occur.
 - ii) Paving surfaces should be durable and age gracefully. They must be capable of supporting the weight of emergency vehicles. Recommended materials are precast concrete pavers, stone, or brick.
- iii) The Plaza must serve a circulation as well as public gathering function. It should be somewhat irregular in shape, possibly employing a "forced perspective", to encourage everyday pedestrian movement between Downtown and the BART station.

c) Amenities: Seating, Lighting, Shade, Architectural Features, and Public Art

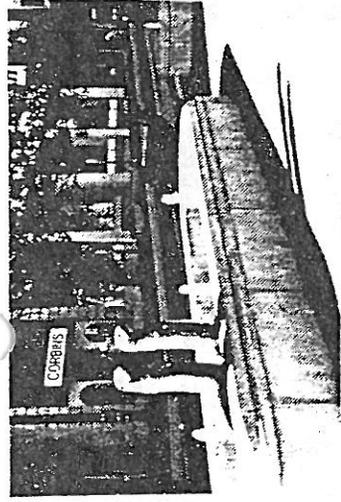
i) The Plaza must contain seating, lighting, and other amenities that create a comfortable "people place" to attract the public at-large as well as Library employees and visitors, downtown workers and shoppers, and nearby residents.

ii) The central portion of the Plaza should generally remain open and uncluttered to allow for civic events. Amenities should be concentrated along the perimeter of the space where people are comfortable sitting.

iii) A fountain or sculpture should be provided that gives the Plaza its own destination and focus. It should be sited to complement the space created by adjacent buildings, yet preferably not in the center of the space. The fountain and/or sculpture features should include some elements that are appealing to children.

iv) Seating should be plentiful and a variety of seating environments should be available. Where possible, fixed seating should be incorporated into the base of adjacent buildings, planters, and public art or freestanding architectural features. Movable seating should be provided by adjacent restaurants or cafes.

v) Freestanding architectural elements such as bollards, light standards, tree



The Plaza should include some features that are appealing to children.

guards and kiosks should be selected and/or designed to form a loosely related "family of objects" that complement both the Library and the Downtown commercial district.

vi) Canopies, awnings, and colonnaded arcades should be considered for the frontage of adjacent buildings to add scale and visual interest.

vii) Lighting should generally be intimate and pedestrian-scaled within the Plaza and along adjacent streets. Light standards should be approximately 13 feet in height, and spaced at approximately 60 feet on center for high activity areas.

viii) The library must be flood-lit on both the Plaza and the B Street frontages.

ix) Brighter light sources should be used to accent important areas such as entries and special architectural features. Lamps should be incandescent or color corrected HPS, metal halide, or fluorescent. Standard HPS, mercury vapor or fluorescent lamps should not be used.

x) Linear arrangements of freestanding elements such as trees, streetlamps, bollards, low walls and piers should be composed to establish edges along the open sides of the Plaza.

xi) Recommended materials for furnishings are precast concrete, brick, stone, cast-in-place concrete, cast iron, and painted steel.

xii) Metalwork should be rigid and substantial to sight and touch. Cast iron, steel, bronze, and cast aluminum are recommended. Extruded aluminum or glass fiber light poles and railing assemblies are not recommended.

xiii) Deciduous trees that reflect seasonal changes, create changing textures of shadows, and have a strong yet open branching structure should be used to provide shade within and adjacent to the Plaza. Attractive metal tree guards, tree grates, and uplighting should be provided.

3. LIBRARY DESIGN

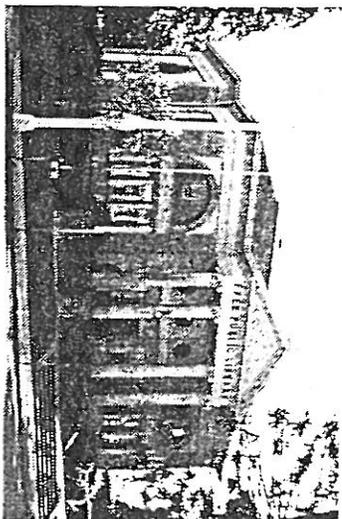
a) Image and Identity

i) The Library's architectural design should be an authentic and lasting interpretation of tradition for the pride and enjoyment of present and future Hayward citizens, using the best available craft and detailing today.

ii) The Library must be memorable in form and easily recognizable as one of Hayward's most important public buildings. It should be clearly distinguishable from office, residential, and other types of private sector buildings.

iii) The building should be prominent in size, height, and form, yet fit sensitively into its surroundings (see "Relationships" guidelines, above). It should convey a sense of stability and permanence through its siting, massing, architectural style, and quality of materials. It should be dignified, yet welcoming and open.

iv) The Library must provide a landmark civic architectural feature, such as a rotunda or tower, located on or as close as possible to the centerline that extends west from the segment of B Street between Watkins Street and Main Street. It must be visible and distinctive when seen from BART trains and from the station platform.



The Library must be recognizable as an important public building.

v) A secondary view of the landmark feature should be provided along the Alherton Street corridor.

vi) The main public entrance to the building should be on the Plaza, yet also be clearly visible and easily accessible from B Street. This entrance may be combined within the landmark architectural feature.

vii) The architectural expression and materials of the Library should relate to the tradition of public and civic buildings in Hayward and in the San Francisco Bay Area. Hayward's examples include the old City Hall, the Hayward Union High School (demolished), and the Downtown Post Office (pictures of these buildings are provided in Chapter I). These buildings provide a rich set of images and precedents that encompass classical,

Mediterranean, mission revival, and deco expressions that are familiar and accessible to many people.

b) Relationship of Ground Level Library Activities to Streets and Open Spaces

i) Public-oriented ground level functions within the Public Library should contribute to the life and activity of the Plaza and the B Street frontage.

ii) The main entry lobby should directly connect to the open space of the plaza and be a principal focus of the Library building. This lobby space should be welcoming and easy to navigate, leading the visitor to the major public functions of the building, including the book circulation counters and the principal vertical circulation core. (This space should also be designed to function as a public reception hall; see Library Building Program for a more detailed discussion of this space.)

iii) Additional public functions should be located to activate the plaza along its frontage, such as entrances and windows for community meeting rooms, staff entrances, and a library cafe.

iv) The ground floor should be designed to provide passing pedestrians with a pleasant walking experience and a sense of the library's presence. Major reading rooms should include large

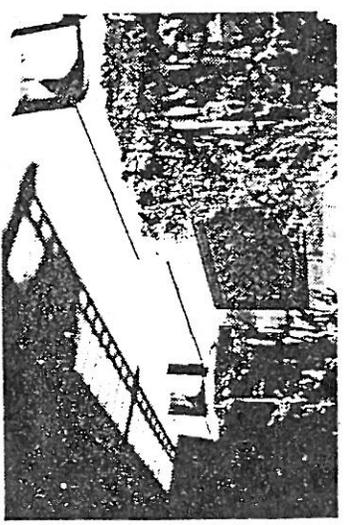
- windows that provide a strong public character (see Library Building Program).
- v) Pedestrians should be provided with nighttime views of illuminated lobbies and reading rooms to give these exterior spaces an additional sense of activity and human presence. Critical locations within the building are rooms that face the plaza, the room at the southwest corner that faces pedestrians walking from BART, and rooms along the south facade that line the street corridor where pedestrians walk between BART and the Plaza.
- vi) Loading and trash areas should be properly screened, attractive, and aesthetically integrated into building facades.
- c) **Height, Massing & Setbacks** (See Section 1 - Relationship of Buildings, Streets, and Spaces, for additional guidelines)
- i) The minimum height of the Library building, as measured from the grade of adjacent sidewalks to the top of wall cornice or eave line, should be 36 feet.
- ii) The landmark architectural feature recommended to terminate the B Street view axis can be either a broad form, such as a dome or rotunda, or a slender form, such as a clock tower or cupola. This feature should be massed to complement yet not dominate the Plaza and surrounding streets.
- iii) All street and Plaza corners of the building should be articulated with a corner tower or special corner treatment. At a minimum, a separate, recognizable roof articulation should be used at these locations.
- iv) Main building facades should be built parallel to, and set back no more than 10 feet from the B Street, Watkins Street, or any other street right-of-way lines.
- d) **Roof Treatment**
- i) The roof design should be configured to be attractive when viewed from the BART station.
- ii) The roof of the landmark architectural feature should be designed to add an attractive form to Hayward's skyline.
- iii) Roof penetrations such as dormers, monitors, cupolas, and skylights are encouraged to add interest and variation. These should be well-composed and linked with facade openings and other features below. Small tower roof treatments are encouraged to reinforce special corner masses.
- iv) All rooftop mechanical equipment should be screened. Vents and associated grillework should be minimized in appearance or should be ornamental and architecturally incorporated in the roof design.
- v) Recommended roof materials are clay tile, concrete tile, and standing seam or batten metal roofing.
- e) **Facade Treatment**
- i) Architectural composition and elements should be inspired by the traditions of civic building design. Facades should be composed of a regular pattern of windows, columns, wall panels, etc., punctuated by entrances, protruding room volumes, and other special features.
- ii) Major entries should be enhanced with traditional public building facade feature(s) such as a stoa, colonnade, arcade, etc.
- iii) Facades facing residential buildings and the BART station must be attractively detailed. Long unarticulated or blank expanses of building wall should not occur on any visible frontage.
- iv) All handicapped access ramps required by grade changes should be attractive and well integrated into the facade design. They should not appear relegated to the margins of the building or "tacked on".

- v) Ground level treatment of buildings should reinforce and extend the desired pedestrian scale of the downtown. Street-fronting facades should be continuous to define the street space. Large wall surfaces should be regularly punctuated by asters, lines, notches, cornices, light fixtures and other repeating wall elements, to articulate regular modules of building bays and windows.
- vi) Building walls should be specially articulated to provide a sense of anchorage and structure where they meet the ground and roof, respectively. At the ground, a "base" composed of a protruding or "sitable" foundation treatments, material changes, rustication, and/or paving borders should be created. At tops of walls (eave line or parapet), features such as cornices, brackets, and/or overhanging eaves should be provided.
- vii) The proportion of window opening area to wall area should be high to maintain a sense of openness, especially on walls that face active public spaces.
- viii) Window and doorway openings in walls should be inset generously to provide shade and shadow texture. Protruding sills, headers and other elements should be used for added emphasis.
- ix) Window and door components should be made of heavy-duty, high-quality materials. Large windows and doors should generally have thicker dimensioned frames and mullions than small windows and doors.
- x) Users should be "rewarded" with visual and tactile interest as they get closer to the building. Surface relief and texture are preferable to sheer wall surfaces. Structural joints, transitions, supports, and light fixtures should be used as opportunities for ornamentation. Rich materials, well-crafted forms, and human-scaled elements (railings, seating, etc.) should be given special attention.
- xi) Windowless walls visible from the street should have the same base, cornice, trim, material, and color treatments as the rest of the building.
- xii) Recommended materials are as follows:
Wall surfaces: brown, tan or yellow brick; precast concrete; cement plaster; terra cotta or similar tile; EIFS (exterior insulation and finish systems) if openings are well-trimmed.
Ornament: Ceramic tile, brick, stone, precast concrete, glass, plaster, metal.
Glazing: Clear glass is preferred. Reflective or bronze tinted glazing should not be used.

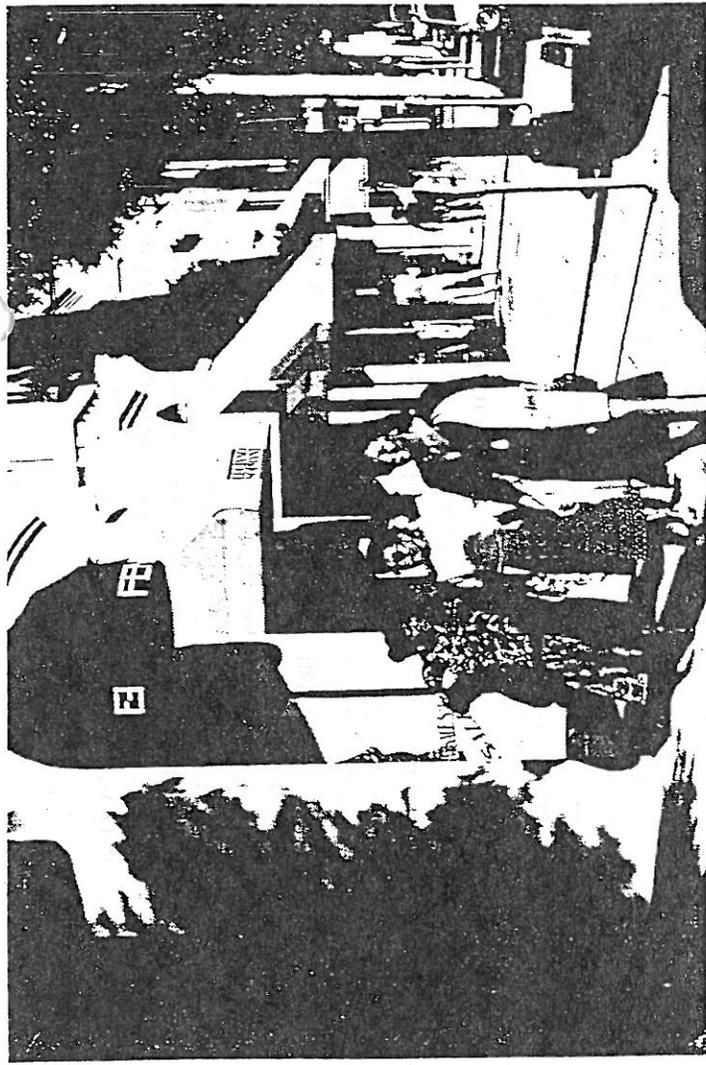
4. RETAIL PAVILION DESIGN

a) Preferred Uses

The intent of including retail uses in the Downtown Focal Point is to help establish the area as a "destination" for visitors and shoppers. A destination shopping district is one to which shoppers go without a planned itinerary, to browse and wander and enjoy the passing scene. Restaurants and a variety of small specialty goods' stores encourage that type of unstructured activity and are preferred as tenants for the Retail Pavilion. Large space users, such as appliance stores, furniture stores, grocery stores, garden supply stores, liquor stores and paint stores should not be sought as tenants for the Focal Point Retail Pavilion.



"Sitable" base designs can add additional seating opportunities around the Plaza.



Storefronts should have a strong pedestrian retail character.

Street and Plaza Street facades shall be built to and parallel with street rights-of-way.

- iii) The Plaza facade of the retail building should generally be straight rather than sculpted, to provide a strong edge to the space.
- iv) The roof of the Pavilion shall be distinctive in form; a sloped roof with ornamental accents is recommended.
- v) To call attention to the special public character of the Downtown Focal Point precincts, all street and Plaza corners of the building should be articulated with a corner tower or other form of special corner treatment that accents adjacent street intersections and the Plaza space.

d) Facade Treatment

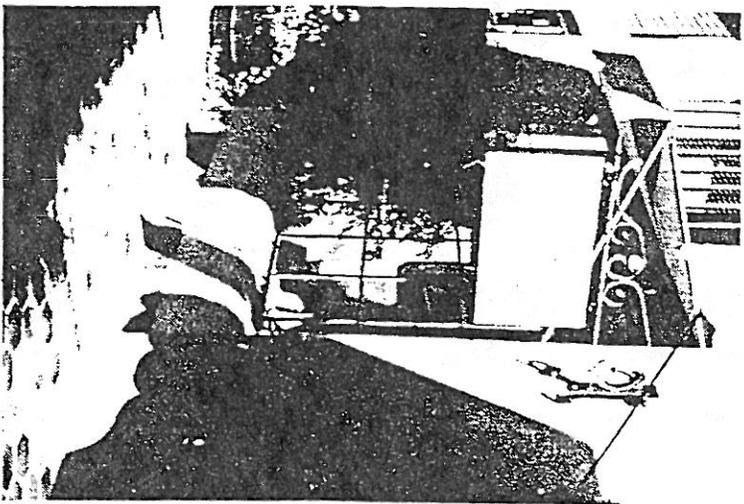
- i) A continuous frontage of storefronts should be located along the B Street and Plaza elevations; the Watkins Street and "Plaza Drive" elevations may have less but shall not be blank. Storefronts should have a strong pedestrian retail character.
- ii) Storefront entrances should be spaced at intervals of 30 feet or less.
- iii) Major facade divisions (piers, columns, break between storefronts) should articulate storefront facades at intervals of approximately 30 feet.

b) Image and Identity

The Retail Pavilion should be designed to be sympathetic, related, and deferential to the Library building. Massing and roof forms, facade composition, materials, colors, and ornamentation should complement the Library building, although these treatments need not be identical or create the impression of a Library "complex". They should be varied as appropriate to a commercial/retail character.

c) Height, Setbacks & Form

- i) The height of the retail building, as measured from the grade of adjacent sidewalks to the top of wall cornice or eave line, should be a minimum of 24 feet and a maximum of 36 feet.
- ii) The B Street facade of the retail building shall align with storefronts in the downtown commercial district immediately to the east to allow a clear view corridor of the landmark feature of the Library. The Watkins



Each storefront should have a projecting sign oriented to the pedestrian.

- iv) Glazing should be clear; dark tints and reflective glass shall not be used.
- v) Awnings should be canvas and should not be internally illuminated. Awnings should be divided at window or pier divisions, not run continuously along the building frontage. Awnings must be designed to meet City of Hayward Fire Department standards.
- vi) Signs should be attractive and in keeping with the overall quality of

the Downtown Focal Point precinct. Internally illuminated "can" signs should not be used.

vii) Flush-mounted and painted wall signs should align with major architectural elements, such as doors and windows. Ornamental elements such as moldings, pilasters, arches, clerestory windows, roof eaves, or cornice lines should be used as a frame.

viii) *Projecting Signs:* Each storefront should have a projecting sign oriented to the pedestrian. Projecting signs should be perpendicular to the building wall and within the ground floor facade area.

ix) Service access and facilities shall be unobtrusive and screened from adjacent residential uses.

x) See the *Downtown Hayward Design Plan* for additional guidelines.

5. PARKING STRUCTURE DESIGN

a) Image and Identity

i) The parking structure should serve as an important gateway and space-defining element for the BART station and bus transfer facility.

ii) The architectural treatment of the parking structure should be compatible with yet deferential to the Library building. It may derive facade composition, materials, colors, and

ornamentation from the Library building, but need not appear to be part of an integrated Library "Complex".

iii) Commercial or service uses must be located along the garage's B Street frontage. Uses could include a storefront cultural facility, a police substation, or small professional offices or retail businesses. These uses should be contained in storefronts that have a strong identity of their own, and are not overwhelmed by the garage structure above.

iv) The design of storefronts should conform to guidelines established for the Retail Pavilion -- see Section 4, above.

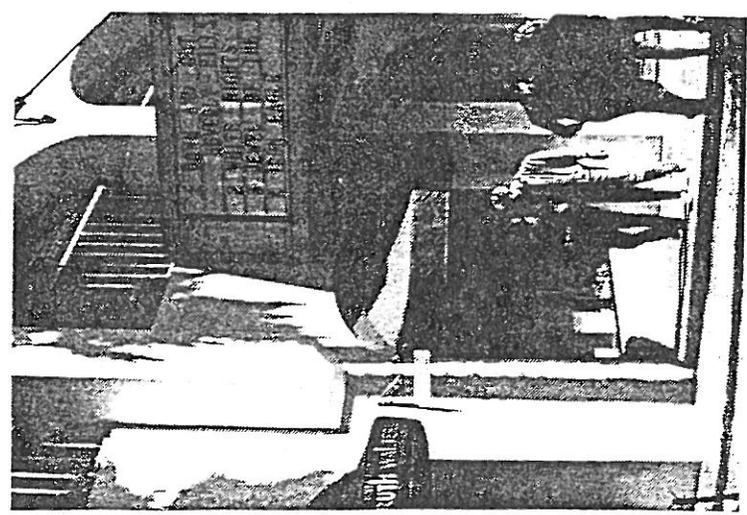
b) Relationship to Streets and Open Spaces

i) While all faces of the building are prominent and must be visually appealing, its south face is especially visible and must be attractive when seen from the BART trestle, station and pedestrian entrances. An arcade, colonnade, or other special architectural treatment should be provided to enhance the pedestrian promenade.

ii) Automotive and pedestrian entrances should be articulated as discrete and prominent facade elements along the Library Way frontage.

iii) On the south side of the building, a straight facade parallel to and aligned

with the south wall of the Library should help to funnel pedestrians along a promenade between BART and the Downtown Focal Point. On the B Street and Library Way sides, the building mass should parallel the lines of the street corridors. The northwest corner of the parking structure should contain a specially articulated architectural feature that marks the vehicular entrance to the BART station.



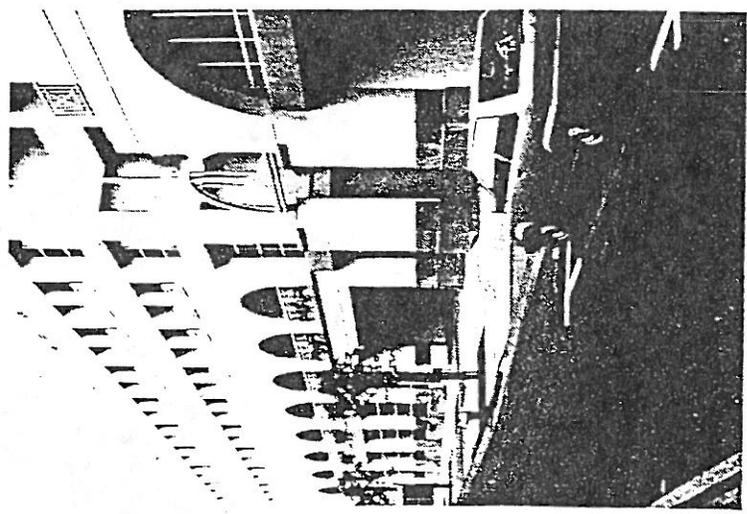
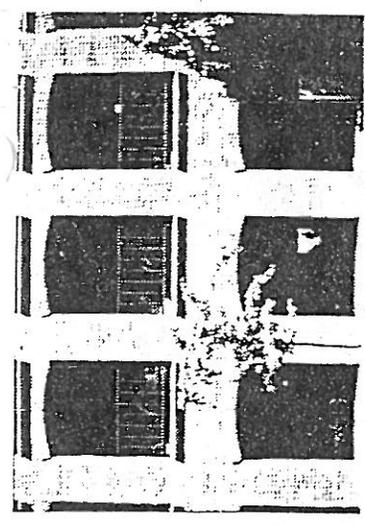
Ground-level commercial and service uses must be located along the B Street frontage of the parking structure.

c) Facade Treatment

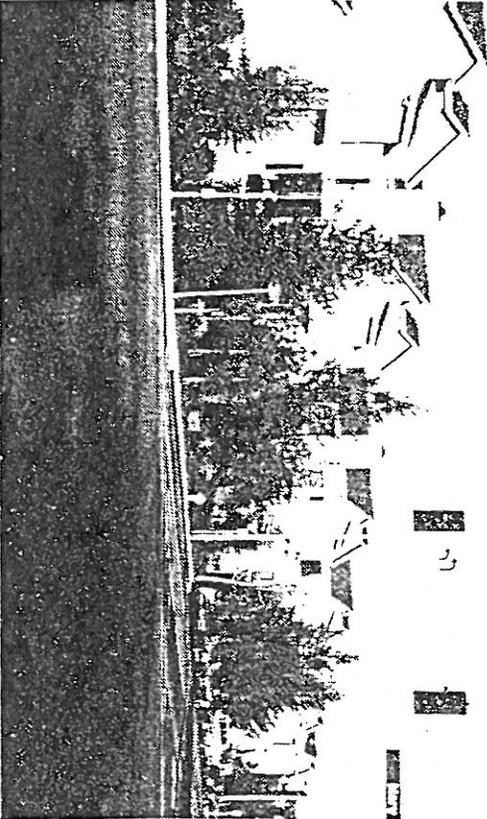
- i) The large span openings of the parking structure should be subdivided and designed as window openings, rather than treated as leftover space.
- ii) Stair and elevator towers should be used (e.g., projected or recessed) to help vary and break up large surfaces and masses.
- iii) If a ramped parking bay system is used in the structure, the effect of sloping floors on the exterior facade composition should be minimized.
- iv) Facades should be well articulated with elements such as roofline and intermediate cornices, bands, and a building base, similar to other buildings of the precinct.
- v) A strongly legible building base of different thickness, material, and/or color from the upper portions of the facade should be provided, to emphasize pedestrian scale. This base should be a minimum of four feet in height.

d) Roof Treatment

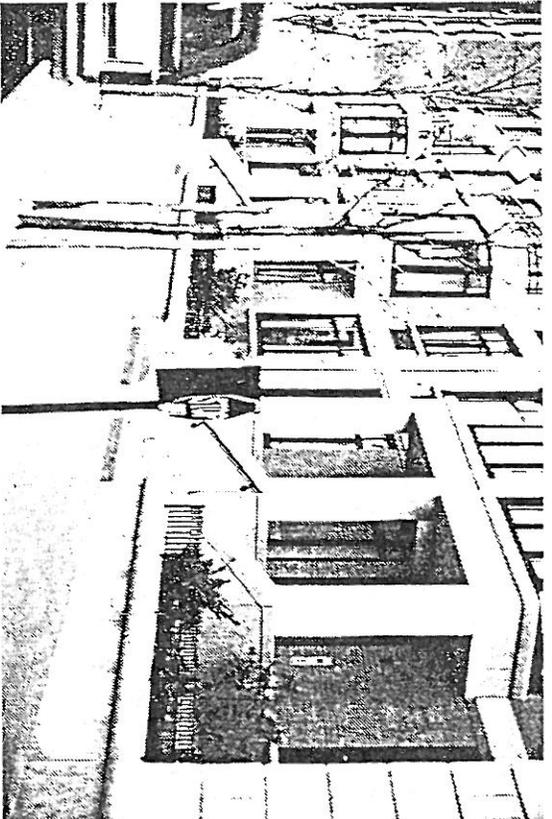
- i) The appearance of rooftop parking areas should be attractive when seen from the ground and from the elevated BART trains and platform. Edge treatments of the roof deck such as parapets, ornamental railings, or trellises should be provided. These



The large span openings of the parking structure should be designed as window openings.



To avoid presenting blank frontages along public streets...



... residential buildings must open directly onto public streets.

should be integrated with composition of the facade below.

- ii) Stair and elevator towers should be emphasized with different roof forms. Mechanical equipment should be unobtrusive and integrated architecturally with the rest of the building.

6. RESIDENTIAL BUILDING DESIGN

The following guidelines provide items essential to the special character and location of the Downtown Focal Point Project. Additional development standards and design guidelines for residential development are provided in the *Downtown Hayward Design Plan*.

a) Relationship to Streets and Spaces

- i) Buildings shall front publicly-accessible streets and ways whenever possible.
- ii) Frequent building entrances, transitional elements between public and private spaces such as low walls, stoops and porches, and varied facade compositions must be provided to create a neighborhood scale along streets and outdoor spaces.
- iii) One or more residential buildings must front onto the Plaza/Circle space to enclose it and generate pedestrian activity.
- iv) Parking should be provided in below-grade garages or partly-below-

grade podiums that are unobtrusive as viewed from adjacent streets and ways. Podiums should extend a maximum of five feet above finished grade to keep stoops and stairs relatively low, and should form an attractive architectural base.

b) Building Design

- i) Residential buildings shall adhere to the development standards and guidelines contained in the *Downtown Hayward Design Plan*.

7. RESOURCE EFFICIENCY

All buildings within the Downtown Focal Point should be designed for efficiency and convenience in energy and water consumption, and long term economy in life-cycle costing of materials and maintenance. The following energy and resource conservation strategies should be considered:

- (a) Passive solar design and solar control for heating and cooling, including operable windows.
- (b) Daylighting to reduce daytime lighting loads.
- (c) Task lighting to reduce ambient lighting requirements.
- (d) Long life, energy efficient light sources.
- (e) Multiple control/supply zones for HVAC and lighting systems.

(f) Excellent insulation.

(g) Recycling facilities for paper and other materials.

8. STREETS, SIDEWALKS, SERVICE AND EMERGENCY ACCESS

a) Street and Block Network

- i) Publicly-accessible streets shall be used to provide access to buildings throughout the site. Streets shall be arranged to create an intimately-scaled fabric of blocks linked to adjacent districts, rather than a single "superblock".
- ii) An internal street shall be used to separate the Library, Plaza, and Retail Pavilion from adjoining housing, while creating a cohesive public street environment. This street is identified as "Plaza Drive" in the Master Plan.

iii) Atherton Street shall be continued into the site and terminate at the Plaza.

iv) A circle containing a prominent civic landmark shall be provided to join Atherton Street and Plaza Drive and provide drop-off access to the Plaza. This "Library Circle" shall also function as one of the visual targets that lead pedestrians back and forth between the Downtown Core and the BART station.

v) An internal street shall be provided to link the interior of the site to B Street. This street is identified as "Library Way" in the Master Plan illustration.

vi) Streets should be aligned to terminate at attractive building facades, building entrances, other prominent architectural features, or civic landmarks.

vii) All new streets shall maintain a parallel or perpendicular relationship to surrounding Downtown streets.

b) Street Design

i) Internal streets shall provide curbside parking on both sides.

ii) Streets shall generally be narrow; i.e. approximately 36 feet from curb-to-curb. (Assumes two-way traffic with parallel parking on both sides.)

iii) Interior streets shall be planted with deciduous shade trees; trees shall be planted in tree wells approximately 5'X5' in dimension, at a spacing of approximately 30 feet on center.

iv) Sidewalks shall be provided along all streets. The sidewalk along B Street must be at least 15 feet in width. Sidewalks along all other streets must be 12 feet in width (except where planting strips are required -- see item ix. below). All Downtown Focal Point sidewalks should feature special paving materials or special color and scoring.

Atherton Street shall be extended into the site and terminate at the Plaza

Motorists always enter the Traffic Circle to the right of the center landmark

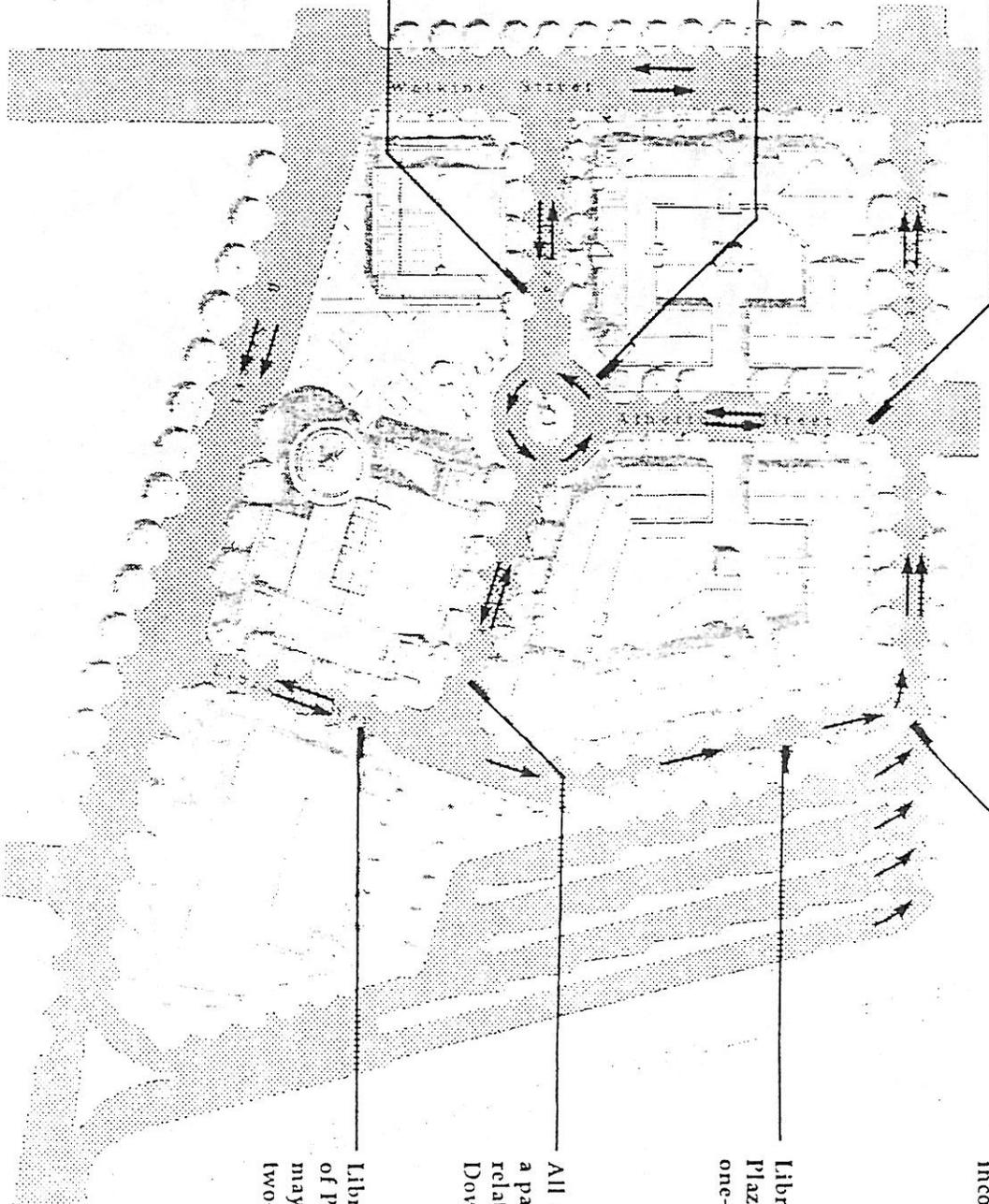
Plaza Drive will separate residential buildings from the more public portion of the Focal Point precinct

The design of the southern terminus of Library Way must incorporate a "forced left turn"

Library Way south of Plaza Drive must be one-way southbound

All new streets shall maintain a parallel or perpendicular relationship to surrounding Downtown streets

Library Way south of Plaza Drive may accommodate two-way traffic

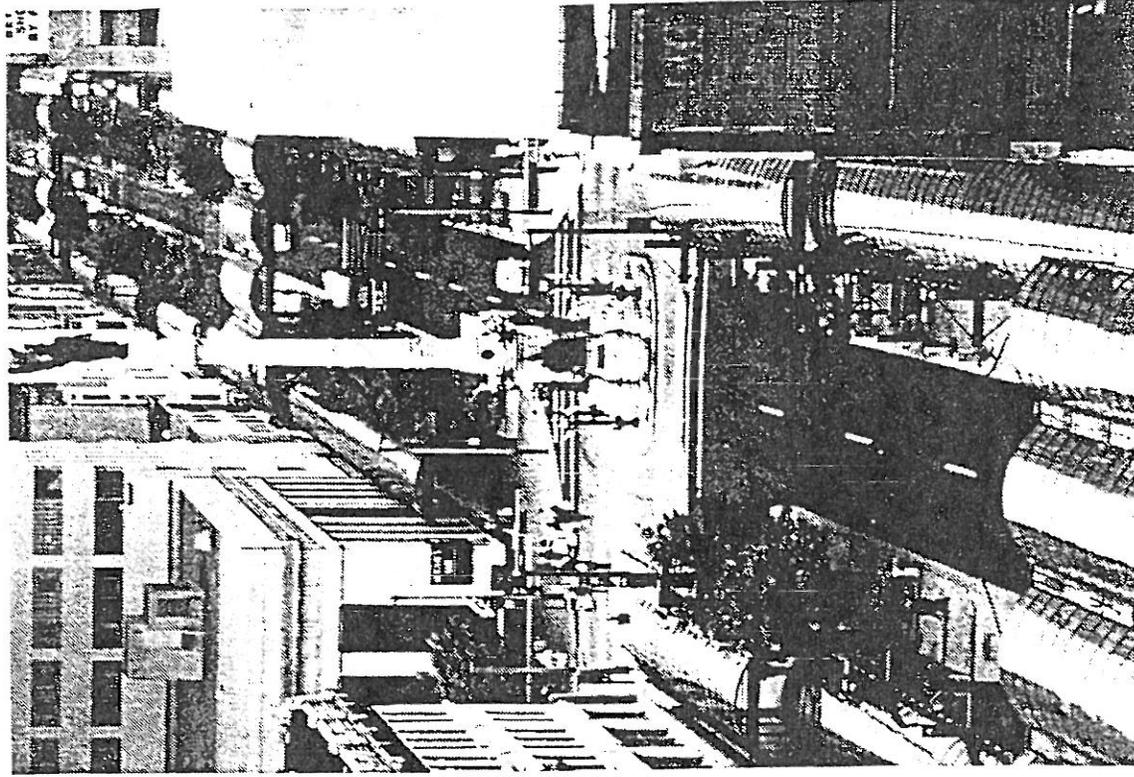


CIRCULATION PLAN

- v) Interior streets shall have pedestrian-scale decorative street lights; lights shall be approximately 14 feet in height, located 60 - 90 feet apart.
- vi) Perimeter streets should have street trees and lighting in accordance with the policies of the *Downtown Hayward Design Plan*.
- vii) "Parking pockets" created by trees located outboard of the curbline should be considered along housing frontages on interior streets, i.e. Alherton Street, Library Way, and Plaza Drive.
- viii) Corner curb "bow-outs" to enhance pedestrian circulation should be provided at all intersections that do not require a designated right turn lane.
- ix) Sidewalks located along residential buildings must include a 6 foot wide planting strip between the back-of-curb and sidewalk (the sidewalk itself must be approximately 6 feet wide).
- x) Corner curb radii should be 10 feet, except where bus turning movements require larger ones.

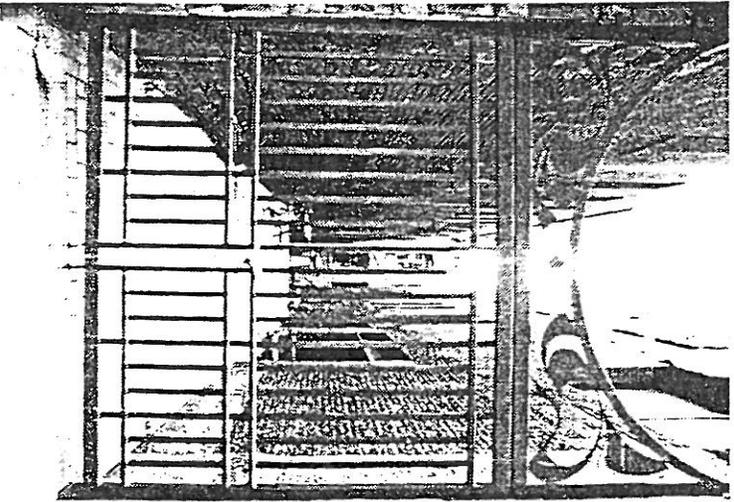
c) **Library Service, Parking Structure, and Emergency Access**

In addition to guidelines established for the *City of Hayward Building Code*, the following specific items are required:



A circle containing a prominent civic landmark shall function as a visual target that lends pedestrians back and forth between the Downtown Core and BART.

- i) Direct vehicular access to Library service and delivery areas should be provided from an interior street.
- ii) Library service access areas shall be located along Library Way.
- iii) Freight delivery and trash storage areas must be screened from public view, must not conflict with pedestrian or vehicular movement, and must be accessible to Fire Department vehicles.



Attractive metal fencing should be used wherever pedestrian access to outdoor spaces must be restricted.

- iv) Where pedestrian access to outdoor spaces must be restricted, attractive metal fencing should be used, such as wrought iron or steel. These may also be combined with masonry walls or piers. Chain link fencing shall not be used.

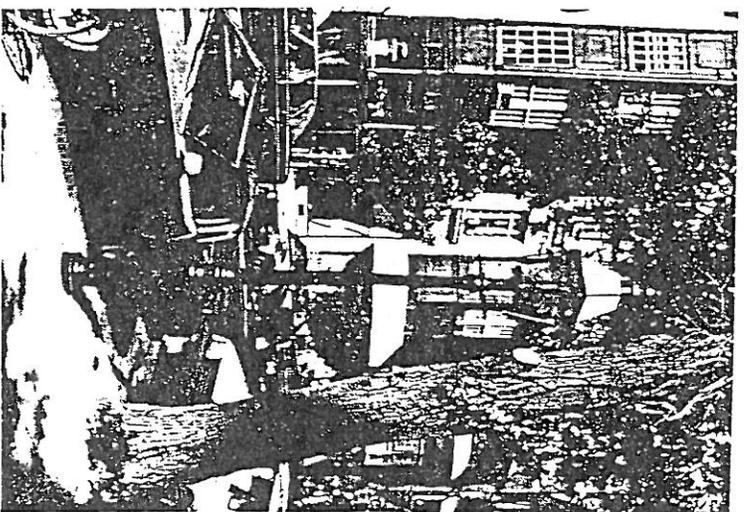
- v) Direct parking garage access shall be from an interior street, rather than from B Street.
- vi) Parking garage access shall not be located across the street from or adjacent to residential or commercial uses or the Plaza.

- vii) Emergency vehicle access curbscuts shall be provided on both sides of "the Downtown Focal Point" and "Station Promenade". Curbscuts and access-ways shall be unobtrusive and incorporated into the overall design of the spaces, yet access to the interior of these spaces shall not be obstructed by trees, bollards, or other amenities.

9. LIGHTING

a) Building Illumination

- i) Building-mounted illumination should be architecturally integrated into the facade composition, with equipment selected to harmonize with and augment building ornament.
- ii) Facade illumination should be selectively located to "wash" build-



Decorative, pedestrian-scaled light standards should be incorporated into the Focal Point design.

- ing masses and walls, in order to help define the Plaza space and the pedestrian paths that connect BART and the downtown.
- iii) Lamp color, power, beam spread and orientation should be selected to avoid harsh and unflattering illumination of surfaces and materials, and to prevent lighting spill-over onto adjacent residential buildings and properties. Incandescent-like lamps

such as "white" high pressure sodium and compact fluorescent lamps are recommended; color-uncorrected sodium, mercury vapor and fluorescent lamps should not be used.

b) Area Lighting

i) To create a comfortable, attractive and safe nighttime environment, low height, low brightness, closely spaced, decorative light standards should be employed rather than high height, high power, widely spaced light standards. The maximum height should be 14 feet, and the maximum intensity 100 watts.

ii) Light sources should be shielded from casting light to the sky; for area lighting, they should cast light indirectly or through an optical diffuser or translucent globe. Borosilicate glass globes are preferable to acrylic or polycarbonate globes for yellowing resistance and light quality.

c) Color

i) For area lighting, lamp type (color of light) should be warm in tone and minimize color rendering distortion, especially of skin tones. Incandescent, compact fluorescent, "white" or color corrected high pressure sodium are recommended lamp types. Standard high and low pressure sodium, standard mercury vapor, standard metal halide, and cool white fluorescent should not be used.

Chapter IV.

LIBRARY BUILDING PROGRAM

LIBRARY BUILDING PROGRAM

(The Library Building Program will be inserted here when completed.)

Chapter V.

**CONSTRUCTION BUDGET
FOR PUBLIC COMPONENTS**

CONSTRUCTION BUDGET FOR PUBLIC COMPONENTS

Summary

The City of Hayward has established a maximum budget for design and construction of the public sector components of the Downtown Focal Point. These costs reflect the level of quality in design and construction that the City requires for this important downtown civic project:

| | |
|----------------------------|----------------------|
| Main Library @55,000 GSF | \$ 12,265,000 |
| Building & Site | \$6,255,150 |
| Interior & Furnishings | \$6,009,850 |
| Plaza | 660,000 |
| Parking Structure | 1,925,000 |
| Retail Pavilion* | 1,630,000 |
| Commercial Frontage* | 890,000 |
| Streets, Walks, and Ways** | 300,000 |
| TOTAL | \$ 17,670,000 |

- * These structures will either be leased or sold.
- ** These are 1/2 the total costs; remaining 1/2 to be borne by BAKI or future developer of remainder of site.

Construction Cost Assumptions

Construction cost assumptions for the project are as follows:

Library: \$223/Gross Square Foot (GSF) - Composite of building cost @ \$155/SF, site

development @ \$20/SF, and an allowance of \$48/SF for library furniture. More specifically: Building estimate assumes steel frame; metal seam or concrete tile roof; concrete plaster with some masonry veneer; articulation of windows and doors. Typical interior finishes include limited wood panelling and casing; furniture allowance is for shelves, stacks, card files, tables, and chairs.

Plaza: \$30/SF - Allowance for 80% of plaza to be paved, 20% to be planted; allowance for precast concrete pavers with paving accent, high quality benches, planters, trash receptacles, 13' height pole lights, pedestrian level lighting, 24" box trees, and a \$150,000 allowance for civic element or sculpture.

Parking Structure: \$13,750/Parking Space - Ramp or "scissors" w/ reinforced concrete slab and beam construction; articulated concrete exterior for openings, corners, and entrances.

Retail Pavilion and Structure Frontage: \$74/GSF - Costs for shell and core only; no tenant improvements included; 12'-15' height ceilings; wood frame with masonry core walls; metal seam or concrete tile roof; stucco with some masonry veneer; some articulation of windows and doors.

Streets, Walks, and Ways: Composite of streets @ \$3/SF; walks @ \$17/SF (including street trees, lights, and paving); and the "Station Promenade" @ \$30/SF.

Construction cost assumptions reflect the anticipated bid amount, including construction cost prorates as well as materials and labor.

These costs vary depending upon the scope of the construction project, time of construction, the number of bidders, the level of City involvement during the construction phase, and other factors.

Construction cost prorates include the following:

- general conditions 6%
- contractor overhead and profit 6-10%
- bonds 2%
- design contingency 10%

Chapter VI.

PROJECT SCHEDULE

PROJECT SCHEDULE

(The final Project Schedule will be published simultaneously with the completion of The Library Building Program Chapter.)

Appendix A

PUBLIC WORKSHOP COMMENTS

APPENDIX A

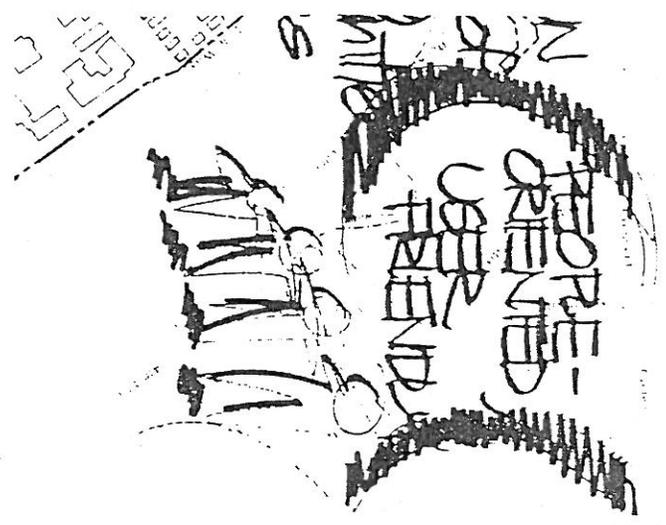
Public Workshop Comments

City Council Study Session -
OCTOBER 17, 1990

Public comments as follows:

DESIGN CRITERIA:

- uses: should change from bars to family oriented place; appeal to all age groups
- o Public buildings should look public:
 - Fault line: green belt through town but might be too much open space. Do not want to create division through town.
 - BART: not maintained. They want a parking structure. Let's use it to our advantage.
 - Music facility, arts, galleries.
 - Design: roots of this community are Mexican/Latino. Design needs to reflect that. Accents form different cultural backgrounds.
- o Opportunity for strong focal point e.g. museum, art gallery, theatre, need people in Downtown.
 - On fault line, public building should be safe.
 - BART, ugly, out of our control. Should be utilized, but not a focus. Should be visually pleasing. Do not want to be controlled by other agencies, e.g. BART.
 - Smaller scale architecture, sense of roots to the building, permanence, sense of community.
- o Concerns:
 - Importance of activity center.
 - accessibility: practicalness
 - welcoming atmosphere
 - theme: should cross all ethnic and cultural boundaries
- o To make civic buildings, need sense of permanence
 - through materials
 - through budget
 - there are trade-offs
- o Potential Assets
 - BART: as activator
 - Library Plaza: sense of history
 - Buildings should have integrity
 - democracy
 - greenery: not too much concrete
 - fault line park: opportunity and connector to other neighborhoods
 - building: should be timeless, variety: mass, colour
- o BART: Need to take advantage of BART, bus, AC transit as a transit hub. Replace view. Intensify use.



POTENTIAL PROGRAM COMPONENTS:

- o Front shops onto the park space.
- o Back door to City Hall. Should be grand entrance and also back door. Perhaps on a retail street.

DOWNTOWN-RELATED AND GENERAL:

- o Givens: historic significance
 - Library Plaza
 - Post Office

- Fault Line
- Intensify in areas we can build in.
- Utilize areas where we cannot build.
- Library Plaza: Intensify use on that site. Add on and focus to a side of plaza. There is too much open space.
- o Physical form of fault line park is important.
- o Whatever we do must relate to all of Downtown from an urban design standpoint. Transit, streets, development, retail all need to be a part of whole Downtown.
- o In designing Downtown:
 - BART
 - Fault line
 - Library building ugly

Community Workshop #1 - NOVEMBER 28, 1990

Public comments as follows:

DESIGN CRITERIA:

- o It's O.K. to over-build for earthquake safety due to our location. The location of Hayward is ideal to steal the entire region's popular concert and art activity on a large scale. Let's not make just a local concert and cultural arts center, but a regional one!
- o Bus access for field trips is important.
- o Encourage pedestrian usage for Civic

- Center and retail. Also need cultural center as part of focus. Do not build directly on fault line.
- o I am 36 yrs. old and have lived in Hayward for 21 years. Currently I live on "D" Street with my husband and two sons. We do use Hayward Downtown for the following reasons; Library, Book Shop, Co-op Antique Stores, Bus Depot, Luckys, Chinese Restaurants. My fantasy is to have a structure that will bring people together that have a lot in common. Concerts, (outside).
- o Keep City Center right here, build around what you have here with mall plaza effect for events. No need for another City Hall. Enlarge the City Council Chambers some way or another as professionals. You should know how!
- o Keep small town feeling. Bring retail back. Lots of trees because of heat. Keep building not too high because of vaults.
- o How will this project be funded? Will private investments be a part of this project?
- o Hayward needs the new Civic Center to lead the way for all of downtown Hayward. We need a Civic Center that reflects the Spirit of Hayward and not one that's an "office building".
- o The audience reaction to the slide presentation was favorable to the European style plazas. Lots of seating areas, small or broken up sub-plazas, canopy trees,
- o The next century will be "The Green Era" so our future City Hall should be abundantly surrounded with gardens, trees, recreational areas, aquariums/zoo's, a piazza, a structure enclosing a garden or park. Enhance the symbolic message, "Hayward is a Garden City".
- o City Center should be low-rise, at most, three stories. It should be built using lots of natural things: trees, water, wood, rocks, grass. Try to unite it to the downtown BART station.
- o Architecture: I like old City Hall or
- o fountains, even paving for ease to the elderly or wheel chairs, gradual ramps, food vendors and small cafes on fringes of the plaza. Wind screening with buildings and landscaping. City Center should have identity without being intimidating. Consider using Mediterranean influence in architectural planning, but not Greek Revival - too intimidating even though the community prefers it from the old High School. Where is the parking and vehicle access in Mountain View? I liked what you said about having a logical gathering place for the city events. The City Hall should adequately provide space for city services without being a monument to clerical workers. Spend the money where it will benefit the citizens. Also, city should strongly suggest downtown offices. Allow one hour lunch breaks - it allows people to get out and enjoy the downtown environment and regularly patronize restaurants.

Hayward High School. Need a new City

Hall, Library, Art Museum, Performing Hall, Meeting Room, Lecture Hall, Public Restrooms, Child Care Facilities, all beautifully landscaped.

Save the City Hall! Make the new Civic Center along the same lines.

Architecture like old Hayward High.

A landmark building should not be overly dramatic. Welcoming atmosphere more important than "awesome" structure.

I'd like to restore the Old Mission Building City Hall and add onto it. (like bookends - Davies Hall and the State Office building in San Francisco).

Use historic City Hall design.

Incorporate design features of extinct architecture (that which hides under the 1930, 40's, 40's stucco). Allow development rather than stagnating in design, both in New City Center and design guidelines. Build a Cultural Center in conjunction with the New City Hall/Center. Form an association, foundation, or whatever, to manage the downtown.

Retrofit and add to the Old City Hall on Mission Boulevard. Most beautiful building in Hayward.

Use the design of front of Old City Hall as "gateway" entrance to City Center.

POTENTIAL PROGRAM COMPONENTS:

I would like to see a new Civic Center that would offer the citizen a "one stop service center." For example, have a police substation, a place to pay your phone or/and your P.G.& E. bill. A service center that will feature Chabot College and Cal. State (a small substation). This will encourage people to come downtown Hayward while also shopping. It would also be interesting to have a "high tech" center as well as a historical building. The whole idea of having a police substation is to help develop and maintain a friendly and safe environment.

A new Civic Center should be user-friendly in terms of accessibility to the public. Accessibility of information should include a mixture of civic functions, cultural functions and business functions. The Civic Center should have a City Plaza where the public can mingle. The building itself should be expandable, something similar to the original City Hall.

We need multi-cultural uses located around the Library Plaza. They don't all have to be in the same building or same block, as long as they are within walking distance of one another, and of the new City Hall and the BART station.

Location first!

The new City Hall that will be built must be of such a size that in later years will not be obsolete. The meeting rooms, halls, facilities comfortably large, well lit, and acoustically sufficient. Rooms, halls, etc. that flow so they are a pleasure to be in. Most Hayward residents would really approve of incorporating architecture similar to the Old Hayward High School. That would make the New City Hall a landmark.

Arts, multi-use facilities, small scale, gateway to BART, pedestrian "user-friendly" -- are some of the ideas I like.

(1) Avoid high-rise buildings (with exception of a single domed tower, possibly) because of earthquake danger in the area and to keep a small-town feel, instead of large-scale city feel.

(2) Provide facilities for a active and varied night life -- not just gay bars like downtown currently has. Suggestions: dance clubs, theatres, restaurants, coffee houses.

(3) As part of this program, refurbish the original City Hall on Mission Blvd. It doesn't have to remain City Hall since it's too small, but it can serve some public function.

(4) Do not plow under the beautiful existing library and library grounds. If library people want a new library, go ahead and build a bigger one, but use the existing building for another

function. Could it be the museum or cultural center? Or even (now I'm dreaming) a child care center? If the old trees existing on library grounds are torn down, I may move from Hayward forever.

- Moving sidewalks, strong mix of busi-ness (shops, offices, residents, etc.).
- Please inform me of future community meetings. I would like to see performing arts center, new libraries, great retail shops (lower level). The "center" should maintain a theme of "communication". The best city I have been in is Vienna, Austria. It would be lovely to recreate even an ounce of that warmth here. The Old City Hall is just fine.

- A New City Center that would truly be centrally located and surrounded by a mixture of public and private uses could be a great way to revitalize our now-dead downtown. There should be easy areas for pedestrians and at least one good coffee shop with outdoor seating on a public plaza for people-watching.
- Incorporate a museum, including history of Hayward, local and traveling art exhibits, geology and natural history.

- For a City Center to survive, there needs to be a great deal of variety of shopping, cultural events, food and a variety of housing types that encourage and enhance pedestrian and like traffic. I would like to see co-housing as one housing type. There should be trees, benches, lots

of visual stimulation and public rest-rooms and other creature comforts -- well maintained, and "user-friendly". I like Spanish or "Victorian Funk" styles -- human-scaled with lots of curving lines -- not cold, hard angles. Hooray for 160 acre lots! Open space is in.

- Include a "transportation center" as part of this Civic Center project, e.g., under



ground parking garage, widened streets to accommodate bus stops. We should not leave it up to BART and AC transit. Civic Center project should set the tempo.

- Green plaza featuring drama theatre, concerts, playgrounds, game shows, fashion shows, etc. Activities sponsored by local merchants. Building design of adjoining shops fronts to match Civic Center.
- A square with a library included sounds great.
- Accessible by AC transit. C.S.P. phase II in effect in 1992. Most movers and

shakers will be dead in five to eight years.

- Old buildings should stay up. Service facilities for the homeless. Sleeping privi-leges. General funds for feeding homeless in evenings.

- (1) To utilize BART, we'd better make it some type of real gateway instead of set in a stupid parking lot.
- (2) Keep cultural center functions away from City Hall, use as anchors on an axis away from City Hall, much as shopping malls use major tenants as anchors.
- (3) Get a mailing list of those who attended so they can follow up.

My wild dream is to first of all support the idea of a new Civic Center. It should incorporate city services, the arts, a quality library, commercial and housing. Automobiles should be extremely limited. We should take advantage of Hayward's great climate by encouraging outdoor cafes, etc. However, it won't work unless we get traffic under control. We need to tell the State of California that they must get commute traffic out of our town. Commute traffic should utilize existing freeways including 880, 238, etc. Traffic from Pleasanton going to the San Mateo Bridge should get to the Nimitz to access 92. Traffic from 580 dumping into Hayward should utilize 238 to 880 in order to bypass Hayward. Millions of federal dollars are being spent to upgrade

880 to 238. Hayward should make Foothill and "B" Street neighborhood streets which would be designed specifically to discourage commute traffic. If necessary, we should take the steps

Berkeley took to discourage through traffic by blockades or whatever.

Hayward will have to reclaim their streets for local traffic if anything is to work in this town. The Mission, Jackson, Foothill intersection should become a traffic circle surrounding a landmark monument which would become a symbol of Hayward (another Arc d'Triomphe? a fountain, sculpture?). We need to make Jackson a beautiful avenue (Champs d'Elysees?) leading to the downtown - the same with Mission and Foothill. This would be for its residents - not commuters. Hayward would finally "get on the map".

- I work at Community Resources for Independent Living. I'm most concerned about plans for accessibility for persons with disabilities of all varieties.

- Near as possible to BART. Public bath rooms and other "resting places" encourage unique business i.e.: artists, designers, etc., encourage cultural tenants, galleries, theaters, etc.

- New, bigger library! Cultural arts and historical society as central to Civic Center. I love the old City Hall architecture, and old Hayward High -- it would be brilliant to use those motifs in the design of buildings in the center. Tear off Old City Hall facade bodily and move

it and use it. Built it into one of the buildings in the New Civic Center then turn the old City Center site into a Greenway).

- A new Civic Center is Hayward's opportunity to redeem itself for 30 years of disastrous urban planning. We need a new City Hall, a cultural arts center. See the Hayward arts council report!!!

DOWNTOWN-RELATED AND GENERAL:

- How can we in Hayward support these grand projects with the very poor demographics of our area? We are not Mountain View. We can't support the good stores, moderately expensive restaurants, etc. like the cities shown in the presentation. Our demographics support Mexican and Chinese restaurants, second-hand stores and cheap furniture stores. If our citizens can't afford to support the yuppie retail establishments, how do we attract them?

- I would like to know who owns the various locations.

- City "services" like planning, building and water offices need to be physically better organized for public access, for getting citizens through their business more efficiently. Won't necessarily keep them there for other activities (shopping), so the city must encourage and initiate facilities, space and programs which would bring people throughout the day

and evening. People of all ages and cultures should find a useful, central and encouraging location for regular activities. I like the Bank of America main branch building and the new Kaiser Sleepy Hollow building the most. Society and old High School structures are interesting too. The renovation of Centennial Hall seems a good compromise. The present City Hall and courthouse are yucky. I like the idea of the substation police services coming downtown (college, too)!

- The current downtown must be upgraded to complement the downtown work force (meaning) City Hall employees, etc.

The downtown is currently encircled by a major commute route which makes it virtually inaccessible for residents living outside the downtown area (most of the residents). What will be done to rid our city of this menace?

- (1) What ever happened to California State University at Hayward? Invite them to take part in downtown activities (e.g. drama, arts, etc.).

- (2) Have a market night like San Luis Obispo where families come down to shop in open shops, eat in restaurants that stay open past 2:00 p.m. This would be a weekly event to bring people downtown. Any public plaza should be designed to allow in part such activities. If you want people downtown then ban cars on certain streets so they don't conflict with

each other. Go for a new complex. Keeping the old has not worked well so in the best interest of downtown - get on with it and do it!

- We should start at Hayward BART, clean it up, start with an attractive mall with a good place to eat (a very good example is the mall around the train station in Washington D.C.).

- Great workshop!

- Keep downtown quaint! With no driving in downtown two-four blocks. Berkeley tiered parking lots. Within the town, place trees, benches in town center. I would like to see a Spanish - Old theme with a town clock on all four sides at the top.

- Bring a friendly, small town feeling to Hayward, where people can feel comfortable living. Bring part of 1908 back. I do my shopping in Castro Valley because I enjoy the cozy squares. I don't find that in Hayward. The disassociated feeling from large buildings, large, unused plazas in Hayward which discourages customers = money. We have a lot of interesting spots (Hayward Historical Society, Library, etc.) We need to some how bring them together. As a child I used to walk everywhere (school, library, church, grocery store, clothing shops, ice cream shops). Today there is too much traffic and not enough shops to browse into. I plan on staying here to raise my family and I'm concerned about the type of community it will be then. I want

downtown safe, friendly, and lots of social interaction.

- Do not feel the new City Hall will revive the downtown area. Housing and well-known stores mixed with the stores in existence now - plus keeping the post office. I do not like going across town. No one has talked about cost. What will it cost the tax payers? Why do we to have build a new library?

- Revitalize downtown, more affordable Housing, and safe to walk.

- There must be a better reason to invest in a new City Hall than to bring life to downtown. The present City Hall is already downtown. Who to pay for new center? Sell present building? Before getting too far into consultants, find out if people want a new City Hall.

- Downtown Hayward is a financial rat hole for a City Hall. Is there any location adjacent to Gardin Park that would be suitable for a City Hall location? All of the downtown sites lack potential as compared to our park. The current City Hall location cost us the most beautiful high school in the United States for little downtown improvement. There is no reason to believe that a downtown City Hall will mean a downtown revitalization. If a first, and possibly last, means we will have a City Hall in a dilapidated neighborhood from the beginning, a City Hall by Gardin Park would mean a site adjacent to beauty. The downtown businesses need less vehicular traffic for an improvement in trade. Reflecting on the



way the city ruined the creek, perhaps it is best to forget the Gardin Park proximity.

- Library Plaza already fills some of the needs for a Downtown Square. Be sure to preserve some of the very special trees in the plaza.

- Workshop process is helpful and constructive. Limit speaker's time and total time of workshop.

- Hayward has potential (schools, old buildings a lovely setting, etc.). However as G. Stein once said about Oakland, "there's no there there". She could have been talking about Hayward. We are fragmented without a center. We should really get behind this project and please, get on with it!

- I would like to know where the fault line is in downtown Hayward.

- Great meeting! Hope the next one will be soon to capture the momentum. Hay-

ward has a rich history. Its exciting to think of creating a Civic/Cultural Center that will build on the past and move into the future with a "pedestrian-friendly" downtown that will blend City Hall, library, art museum, etc. in a warm and welcoming Town Square concept.

- A new City Hall, well conceived and executed, would bring a synergism which would avoid (tend to avoid) the need for total involvement in a redevelopment agency type effort. Mixed use development in or near a City Hall nearer to BART. Would induce investment by private developers whose interests are needed to revitalize Hayward. Would like to see Spanish courtyard/Portico type development which would be attractive in inclement weather as well as good summer months. Mixed used area should be economically attractive to investors so, for that reason, architecture shouldn't be overwhelming or intimidating, thus two or three story Spanish Styling seems appropriate.

Community Workshop #2 - FEBRUARY 13, 1991

Public comments as follows:

SITE SELECTION:

- I am for site 2b. How about 2b & 3? It sounds great. I'm for underground parking.

(1) Forge ahead! A new Civic Center near BART and downtown is a great

- idea!
- (2) Keep a strong tie to library square if Site #2 is selected, which looks like the best so far.
- (3) We need a Cultural Arts Center!
- (4) Consultants doing a good job!

I found information presented by consultants and City Staff important. Perhaps at future meetings we could restrict comments and questions. Keep them to end of presentation. Time (to me) seemed to be wasted - need to watch for redundancy. Appreciated patience of Michael Freedman - Information clear and easy to follow. (Like 2b, 2d and put parking on other side of BART - visibility important.) I like the idea of a plaza.

- Site 2b, 2c or 2d. Keep going ahead to plan proposal.
- Keep working on Sites 2 & 3 for possible new City Center - but do remember the little theater, Sun Gallery, etc. Do something to complement them, not compete.

- (1) No confidence in the new plan. Old downtown has deteriorated despite all the talk and lack of planning to improve downtown.

- (2) How will it be refinanced? I am upset that Hayward voted down Hayward City Hall retrofit but city members and City Council voted a tax on business. A tax is a fee under another name.

(3) How could anyone build such a large

building on the type of soil and location of the old Hayward High School?

- (4) Why is part of downtown not included in overall plan? Should be in four areas not three redevelopment areas.

(5) What hold do you have for owners of buildings that have been targeted for retrofit. Some concerted hold should be offered. Officials state there is no money but money is found for other projects, consultant fee, generous raises for city employees, it appears to be more than cost of living raises.

- (6) Why was office space rented for temporary location of City Hall away from downtown instead of supporting downtown business?

(7) Develop other sites, between A & C and Foothill and BART.

◦ Go do it!

◦ We cannot deny that Hayward needs revitalization, whether it comes in the form of a Civic Center, town square or a plaza. (It does not necessarily need to incorporate a performing arts center). I am for Sites 2 or 3. I think B Street should remain a one-way street. We do have BART and a bus station (just a reminder). Also the new D Street. Hayward is a city without a plan. We call ourselves the heart of the bay, let's live up to it. Hayward has people living in it who are

willing to shape its future so the time to do it is now! Mountain View has tracks at end of their street too.

- I am not convinced that the existing City Hall should be replaced. I do not feel Hayward has the potential to be a "convention" center. No organization's members would permit a convention in Hayward with San Francisco and Oakland being as close as they are. I think the notion that the west end of Hayward can be "revitalized" is foolishness. The city is not big enough for another shopping mall.

- Hayward needs revitalization. I like Site 2 & 3. The City needs a plan. Time to do it is now.

- An expanded Site 2 is good. So is a co-development project. Retail should be high quality. Pedestrian walkways are important.

- In Site 2d: What happens to BART parking? It's all tied into BART-funded parking structure west of BART. Will parking spaces be lost? No.

- I like Sites 2b or 2d: Encourage outside employees to Downtown. Encourage people and restaurants. Need to make it attractive.

- Proximity to fault a problem. Zones within fault areas require essential civic buildings to have extra structural enhancement which means more money to build the structure. We should make

sure we do not fall into those zones. An extended site with 2 + 3 is an idea to be explored. Plaza was too small in most schemes for civic functions. How do we get people to the site? What about a site between A and B Street? Lots of cross traffic. Test the traffic first.

- I'm a commercial real estate broker. People downplay Hayward. We should make it a place to stop. We have an opportunity here. Need an anchor to the downtown. Development in downtown Oakland at the City Center project is something to look at. We have a good opportunity here with BART. Integrating Site 2b is good.

OVERALL PROJECT:

- Do we really need a new City Hall? This should be decided prior to spending any funds. Will the voters agree to pay for it?

- I am for the project, however, maybe you can put more pedestrian walking areas like Munich, Germany where the whole downtown is a pedestrian walk, with City Hall in the middle. I was there on a Friday night at 10 p.m. and there were lots of pedestrians in restaurants, window shopping, stores are on the ground level and residential is on top. We came by subway.

- I believe we should keep the present City Hall as our City Hall. I believe we should keep our Convention Hall and the parking garage. We have shops and a

grocery store close to the City Hall and the Convention Center. We have the Senior Center and theater near by. Traffic would be too heavy in the proposed area. We do not need a cultural center as we have Chabot College at California State at Hayward and the Senior Center. I also believe the library is fine where it is. It could be enlarged where it is. We should consider traffic much more than the consultant is doing. Having the sites so close to BART, it would be ugly to see BART trains. I felt Mr. Garcia and Mr. Freedman, head of consulting firm, wanted only their views believed. I do not know how the new City Hall would be financed. I do not want the Convention Center removed as it brings business to Hayward.

I'm all for these meetings. Having lived here all my life, (exception of three years a few years ago) it's the first time I have had the opportunity for input to help make our city a city we can be proud of and have input into something that 100 years from now, I hope, someone else takes pride in. I believe we should help move businesses that have been an anchor to our community, such as the Cobbler, if the anchor businesses would prefer to be close to the new City Hall.

If this goes as planned, when will it be completed?

I'm for the project! I'm glad the shopping projects are for small shops. The vacancies in Hayward are in the big properties, like on the strip. The small shops can be

Appendix A. Public Workshop Comments

- successful - and even expand! (Ichiban Sushi)
- We were given a snow job 25 years ago about tearing down the old High School to build a new City Hall. Now you say it is not earthquake-proof. Another snow job. Repair City Hall on Foothill Blvd. and move city offices back into it. Save money. Why was City Hall built in 1967-1969? Not properly reinforced against the damaging effects of an earthquake?
- I don't think we need a new City Center. Why are we wasting our tax money for studies on this project?
 - (1) We should reinforce and add to the original City Hall on Mission Blvd. -- tear down old Heald's building, use most of that block -- it is beautiful, in downtown, right across from beautiful Library Plaza.
 - (2) Do not even consider building a high rise -- nothing higher than three stories due to Hayward's being earthquake country -- stick to wood frame.
 - (3) Do not tear down present library - if a new library is built, use present library building for another city purpose. Do not harm or remove any existing trees around library plaza - this is the only pretty place downtown.
- I definitely am in favor of a City Center revitalization project, but why are we even considering building a performing arts center in it when we already have
 - auditoriums at Cal State and Chabot? Also, Fremont is further along than we are in plans for performing arts center at their City Center. They are only ten miles away. When and if we do go ahead with a new City Center, why can't we institute an architectural competition to design it. Hopefully, we will come up with one that is scaled more to our community size than the present one is.
 - We absolutely need to continue the process in order to formulate the right questions.
 - We appreciate the fact sheets. It was good background information to introduce this discussion. We agree downtown needs redevelopment and increased density near BART. We also agree sprawl should also be controlled. Both are vital to improving our quality of life.
 - I realize that it is important to listen to everyone's concerns about the process -- but next time let's talk more about what the project could be -- not whether we should do it.
 - I would like to suggest that minutes or a short summation of highlights be sent along with agenda items to Hayward citizens. I'm concerned that people are unaware of what's happening.
 - Get more money for B Street. Get rid of the types of shops they have now. Put something in worthwhile to go to. Make it safe for evenings.
- The sooner the better. The community needs this. There are a number of cultures here. Please give a portion of the City Hall to paint a mural for preservation of the cultures here. Library inside building is unique opportunity. No historical buildings should be demolished. Existing City Hall makes a statement. Within a block of City Hall buildings are empty. Why will new City Hall bring all this development. I'm against the project. Waste of money.
- City Hall offices downtown is a good idea. What about retail? Where do people end up because they have to be there. Which unreinforced masonry buildings can stay legally?
- I don't think we need a new City Hall. The information here tonight is useful to know we can afford it. I've kept an open mind. Let's get on it. Site 2b is a good option.
- The city can afford a City Hall with using partial General Funds to pay for a new City Hall. City is in good financial straits. We are a well-balanced city economically. We have substantial growth in retail base. As a city, it's good to buy low. Developers are hungry now with the recession. (L.G.)
- As a resident I'm not against the project, but I do not like the way it's being handled. Do residents really want it or not? I'm for the project, but brochures sent out did not reflect what's happening. People are talking down to us. We are

spending money when we might not need to. This area is in recession. Sales taxes going down. Revenues down. Running people out of Downtown because of unreinforced masonry building ordinance. I'm getting turned off from the standpoint of marketing to the residents.

- Several issues here:
 - Should it be Site 2 or 3?
 - Process may be wrong. Is it?
 - Should or should not it be downtown?
- The process should go to the people, but we have elected officials to act on our behalf. Hayward has an image problem. If you're not willing to support the downtown in revitalization, nothing will happen. Now's the time to do it.
- This is a great opportunity to put Hayward on the map. Everybody says we can't do this. Why can't we? Let's give City of Hayward a chance.
- There has been no agenda item. Does the community want it or not?
- California is a booming place. It's booming in the valley. Why? Because it's got natural beauty. We need to draw people/developers. We have to turn this town around.

Community Workshop #3 - MARCH 20, 1991

Public comments as follows:

SITE SELECTION

- I feel Site 2 is the best, but on Clawiter Street I think you don't need more houses. You need to enhance the ones that are there.
- City Hall Complex shift to Site 3 to include McCough Chevrolet to BART for Plaza.
- Site 2 and 2d connecting with BART. Housing limited to 3 stories.
- Whole purpose to City Hall downtown is to provide an ice breaker for private investment. Site 2.
- Like the ideas. Let's get on with it. Site 2d.
- Let's vote on a site. Then figure out whether or not we'll have a City Hall.
- Like Site 2, but not for City Hall. New blight at existing City Hall.
- If we have a City Hall Site 2d first, then Site 3.
- We do not necessarily need a City Hall. Hayward needs a spark. Solid interest in downtown. Site 2. Library Square is important. Site 2 is cutting off the con-

necting art facilities. How do we get in the process?

- Town meeting concept is good. To the current government I say let's do it right or not at all. What is the grand strategy? Housing by BART explored. Test site with BART good.

DOWNTOWN COMMENTS

- Hayward in 1946 and 1966, planning was in reverse. In 1946 Foothill Development destroyed "B" Street and in 1966 Southland Planning destroyed Foothill. Now this planning will make Foothill like "B" Street is today.
- Will "B" Street be widened for this project?
- We need an overall plan! Opportunity with City Hall in temporary facilities and with earthquake damage. City is being opportunistic to make Hayward a better town.
- Need the City Hall. Need a sense of community. Need revitalization.
- Why are we hanging on to "B" Street? Hayward's been chopped up enough.
- Not how to preserve a good downtown, but how do you bring investment back. A City Hall is one way. Whole series of actions are necessary to turn this town around.
- Do we want the downtown? Many towns do not have centers. Hayward is

Appendix A. Public W shop Comments

- going that way. Do the citizens of Hayward want a downtown or not?
- Manager from Bank of West: Downtown is disintegrating. City Hall is the vehicle, not the purpose. If something does not happen soon, it's going to be a slum. More publicity, more people. Downtown revitalization is needed, necessary and important.
- Are you eliminating BART parking? No. Interim plan and final plan maintains parking in total. Where does City Hall park? On site.
- Are you jumping from a blighted area to a yuppie area. Why didn't the City move into downtown now. Not enough appropriate facilities for the City to move downtown.
- Hayward is #5 in population. In income, it is not so. In ten years what if another area needs revitalizing. Do we move City Hall again.
- A good city has districts. City Hall is not the only public building. Other facilities; swim/gym, Fire Station, Library, Community Center.
- Can we revitalize downtown without City Hall? Yes, but Hayward has disinvestment. Investment is not coming here. The big, private investment types are not working, therefore, where you take City Hall away from will cause disinvestment there. Right? No. Existing City Hall once moved, office to fill in its place, and nothing will be lost.
- Old time Hayward. Post Office, School District Offices, Superior Court, etc. are not downtown. How vibrant is it around City Hall now? Where has Capwells gone. Private development should decide where/when development happens. If business doesn't want it, city government shouldn't either. I like one stop shop as is now. Let private enterprise worry about downtown.
- Downtown merchants don't have prosperity.
- Want something that stands out. Existing City Hall does. And will businesses move into existing City Hall?
- No commitments from developers, however, developers are interested!
- There are factors other than site or what it looks like. Burlingame and Palo Alto are thriving. They do not need a wonderful City Hall. Should some of the expense be used for other things. Maybe it should. How can we bring in economic development. Answers go way beyond "B" Street.
- Put something next to BART so people can see it.
- If we move it what is it going to do for us at City Hall? Image for downtown? Capwells couldn't survive. We need to anchor downtown or forget the whole area.
- Nobody with money in downtown. Beautiful place. Need to bring people. People with money. We can get people, people from Japan. They retire early. Officially start investigating bringing them in.
- Development of programs for youth. If kids want to be here so will parents. Team work is good. We are rehashing things. Like to spend money on traffic, coordination between city and the two colleges.
- Suggestion, highlights on feedback from developers. In continuation of Downtown Plan, this will come. Need more than back room meetings. More than information on conversations with developers.

OVERALL PROJECT

- The new City Hall is a "bonehead" idea. Move back into the second City Hall. We don't need three!
- Please send future meeting notices. Suggest City Council Workshops on them.
- Hayward High as prototype.
- I can't visualize it in plan. What does it look like?
- Process? In conjunction with revenue finance. I would like to see the whole process. Where does one get on the agenda?

town. What is the ratio of retail: office: housing that should exist to support downtown? Site 2C is a small land mass. Retail and a lively B Street will not revitalize all of downtown, but if a City Center shows enough public and private support, it will be a catalyst. Need to get people coming and going. Need to activate the plaza.

- Architecture should reflect old City Hall.
 - What is a rationale for dense housing?

o Keep the library on Library Square. Put the Cultural Center across the plaza from City Hall. These ideas are excluding finances. Consider financing for project spread throughout downtown in addition to the City Hall.

- o Has the type of retail been chosen? No.
- o City Center or not, the current building needs to be rented. We need to look at the whole downtown before we do anything. AAA generates lots of traffic. What about it in downtown.
- o City Hall or City Center, we should not skimp on design. We should tie into Library Square and have performing arts or a cultural facility.
- o I am in favor of Site 2C "B." The library generates traffic.

o I like retail below, condos above, as a multi-use building.

o Library: What about a physical overpass to link City Hall to library? These two uses should be considered together.

Room for museums, parking, etc. Master plan it all together. I do not like surface parking. I would prefer underground parking. We must have high standards for quality. I think we should connect City Hall to downtown and BART.

o Library square is a large draw. Expand the existing library. I am in favor of City Hall but a library is more important.

o I'm confused; there is no consensus on City Hall, let's call it City Center.

Design and Use:

o Plaza should be accessible from all sides.

o There's no particular style to the City Center.

o We need critical mass to revitalize down

(1) Downtown Revitalization Plan.

(2) Ice breaker Project: City Hall.

After the site selection, refine the pieces for the City Hall and write it all down to be utilized. Decisions to be made in fall by City Hall.

- o How could we spend money when we do not know if it's a bonehead idea.
- o Like to see some criteria to see if we really need it or not.
- o Pushing on decisions when we don't have a plan.

Community Workshop #4 - JUNE 10, 1991

Public Comments as Follows:

SITE ALTERNATIVE 2C:

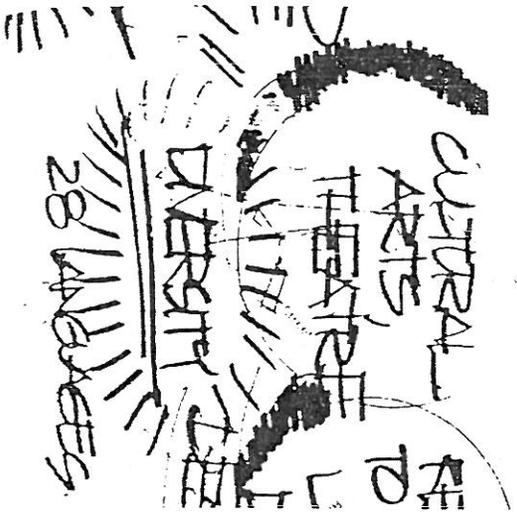
o You have not mentioned uses other than a library. Library floorplate can be used for another function. It does not have to be a library. What was happening to the other uses? Have we contemplated retail on second floor or in City Hall? The community needs community heritage, performing arts, library.

o Retail makes me nervous. I like the cultural things, but in retail people go in and out of business. It could be detracting.

- o A focal point is important.
- Cultural activities.

Let's inject some fiscal thought. Is it possible to build one building, then later when you have the funds, do the next phase? Many times the second phase does not happen. It makes sense, but one plans differently for one building rather than two.

Master Plan with Library Square. I do not like Site 2C "A" because of the surface parking.



SITE ALTERNATIVE 2D:

Eliminate the office scheme. Save the time and no interim parking lot!

- What about a YMCA or YWCA.
- I disagree with a YMCA. I like Site 2D "A" with housing. I do not like a lot of retail because it may be underutilized. Beyond plaza, there should be no retail.
- Retail beyond plaza is no good. I am concerned that housing is in between library and City Center.

- Fiscal responsibility:
 - What are you going to do with existing City Hall?
 - If you want to do what the consultants are envisioning, you need money.
 - We should sell the existing City Hall first. We can't sell it for the money we need, and it is hard to rent it.
 - If you are going to build something, build it with money the city has got.
- Observation: Why do we need government offices downtown? This space (2D) needs cultural things/performing arts.
- With the 1975 Planning Team there was controversy over planning downtown. Whatever we go with should have a cultural element. A model would be a useful tool.
 - High quality buildings if you're going to do anything, but that costs money.
- There is a possible compromise: Leave City Hall where it is, then have a Cultural Arts Center downtown.
- There are budget problems. The existing City Hall is fine. Leave the library where it is and expand it. Put a cultural center across from Library Square.
- I would like to keep costs down.
- It is not appropriate to have housing across from Library Square.
- I want to leave City Center where it is
- I would not want to live next to the bus station. Plus retail will not come to downtown. That's why we need a catalyst for downtown. To get public money to help private investment.
- Are you going to commit public money before private money is committed? 2D does both. Is this just another attempt to revitalize?
- What are the parallels with Mountain View? What's different, what's the same? What about funding? Demographics? What about the people that use it? It is a smaller city but it is ethnically diverse.
- In Mountain View the housing comes right up to downtown. Not true.
- Mountain View's income is probably higher than the basic income here. The problem is office building leasing is in a recession. How can we rent out the existing City Hall. Is it a potential to divert the money to downtown, not to City Hall.
- The library generates 35,000 people/month. It needs linkages to cultural activities. Hayward already has cultural diversity. I would like to start a foundation to help get more services for the library. In September a coordinated library plan will be completed. I do not like the existing City Hall. It is too bureaucratic now. I like the temporary

City Hall. I would like to see all those workers in downtown in a friendly City Hall. The existing library? Move it to a bigger site. Use library building for performing arts. There's a creative opportunity here!

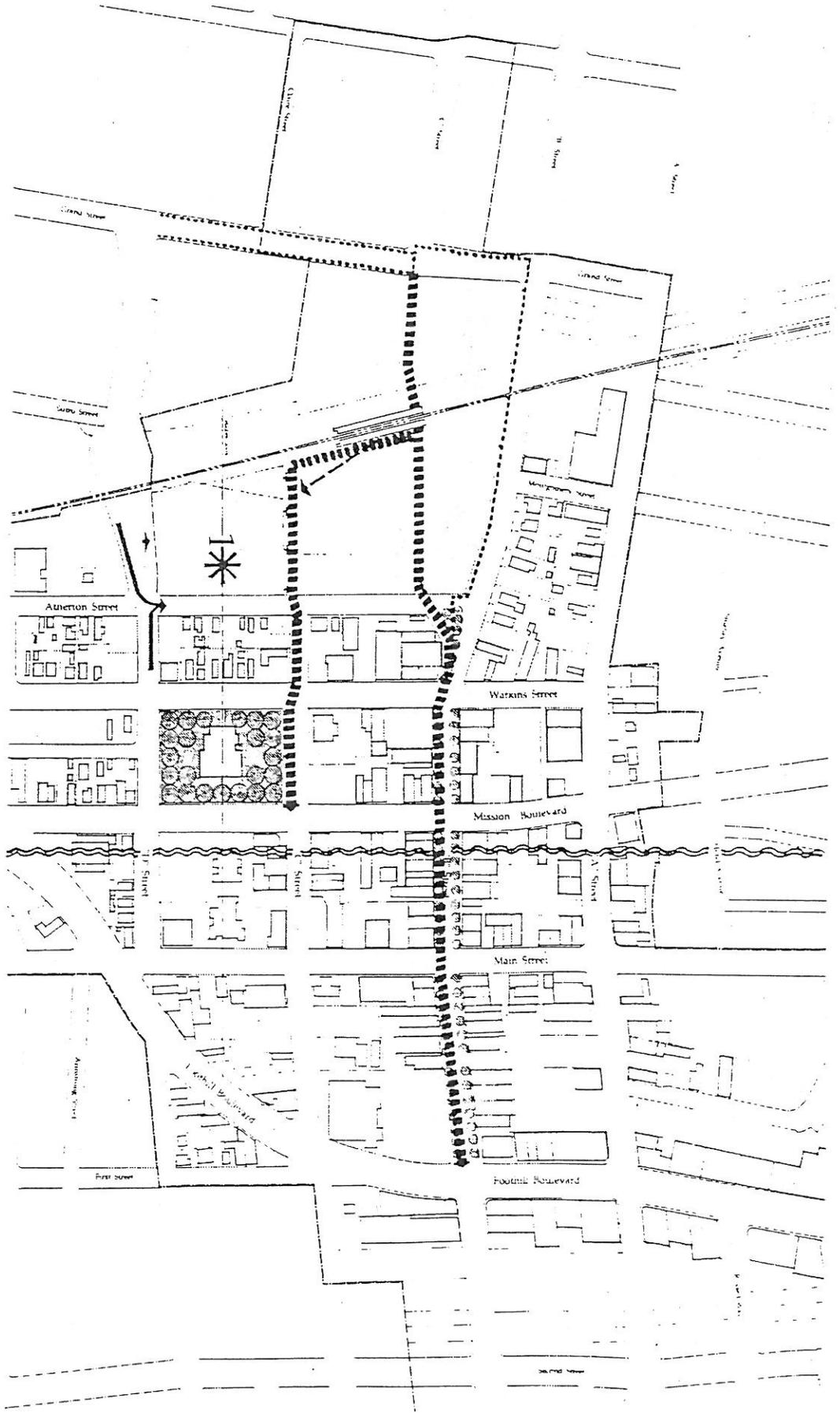
- Tonight has been a mish mash of ideas. Some ideas are clear though. 1. It should be seen from BART. 2. There seems to be a stand-off between a city Hall or a City Center. 3. Do one encompassing plan.
- Putting City Hall in downtown is difficult. The public has a negative outlook towards the City.
- The Cultural Center should be visible from BART. Also, we must think of the money and what we can do. Apartments: Why are we doing apartments? They should be condominiums.
- We need housing for purchasing power in the downtown. I don't know if the housing has to be that dense though.
- I like Site 2D "A": walkways and it brings neighbors together.
- People are going to drive to parking garages and not shop in downtown. Let's make it public transit oriented and spread out cultural activities.
- What about the fault? And the freeway running down to Fremont. It stops there. I go to the mall or Pleasanton to shop. Those brick buildings are moldy. Get them down.
- We need a focal point to encompass the

total plan. A small cultural building as a gateway. It should be a multi-use building with ground floor retail and office or housing above.

- It has to draw people from out of town for a seven day revenue base.
- We need something to bring people downtown, to use Library Square. The city owns part of the post office block, put parking there with other uses and de signs for the site.
- Let's change the format: Let the positive and negative people sit down and hash it out.
- On financing: It is not sound business to wait until you've got the cash. Phased development can work with proper backing from city and community.
- It looks like the walled City of Canterbury; it needs to be opened up.
- The business people in downtown? What is happening with them?
- In the process, we are getting somewhere. There is some passion, caring and concern.
- Working on the vision: Never a mistake to reach too high! The future should not be hostage to the past!

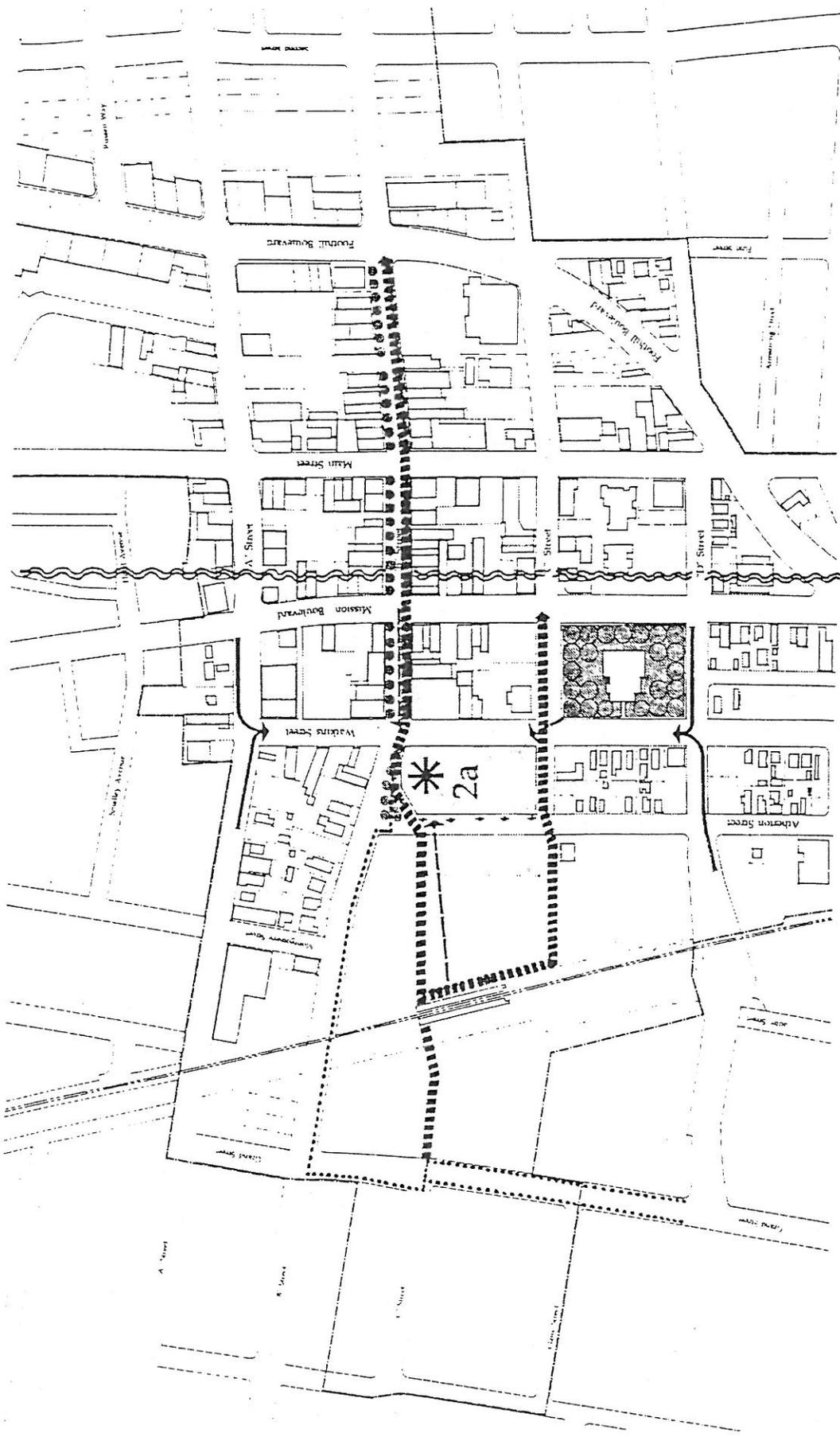
Appendix B

ALTERNATIVE SITES CONSIDERED



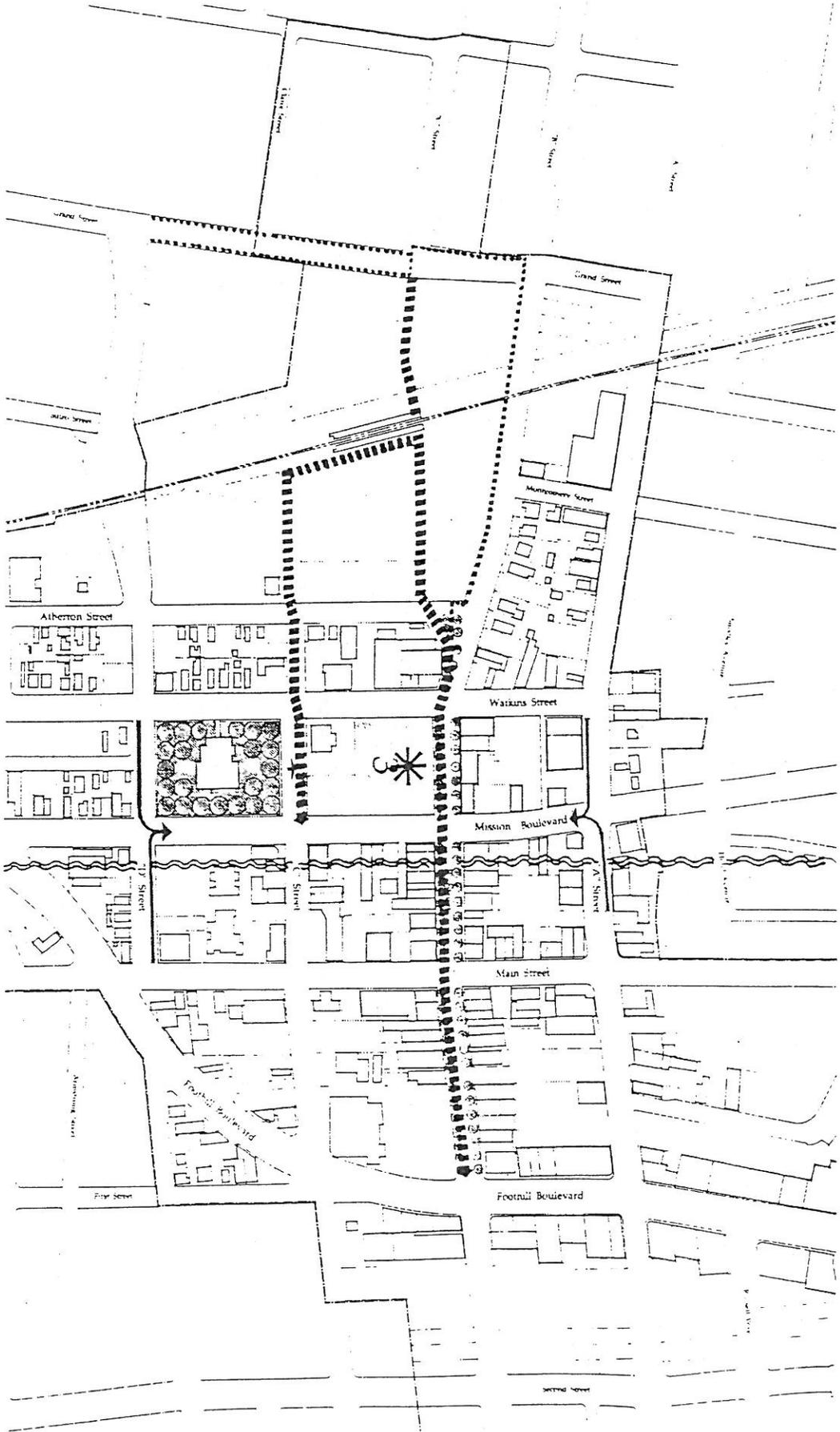
SITE ALTERNATIVE 1

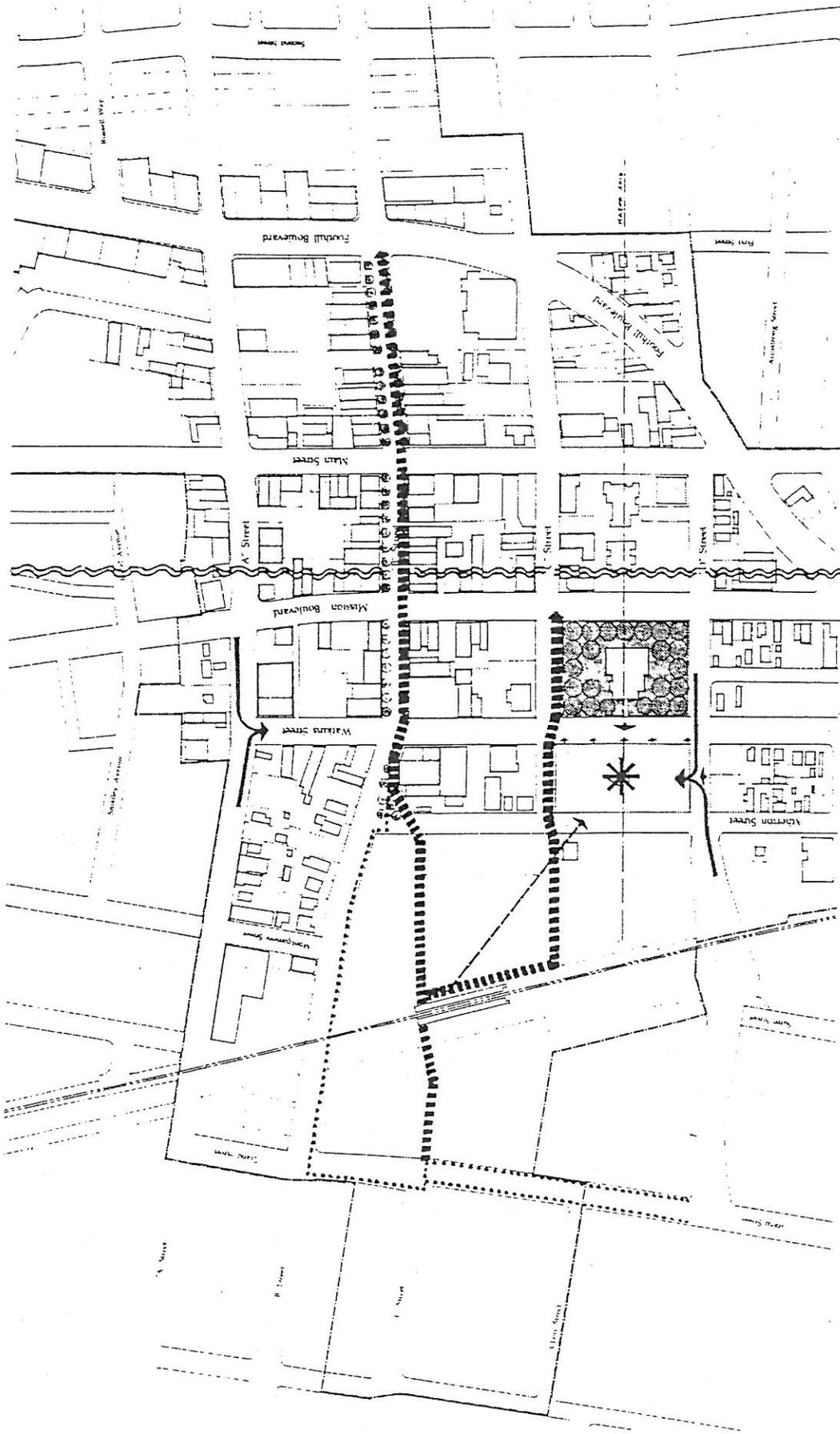




SITE ALTERNATIVE 2A

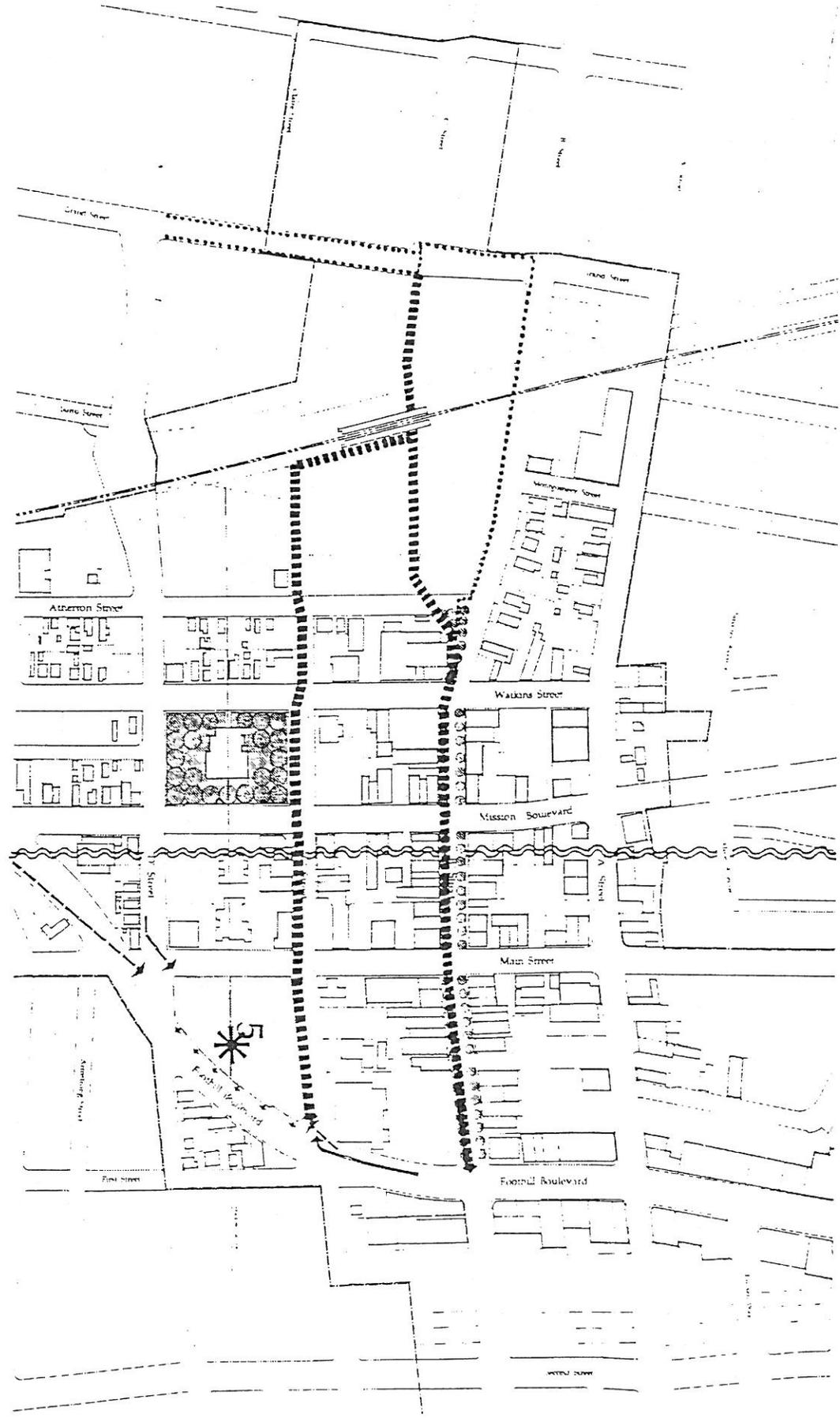
SITE ALTERNATIVE 3

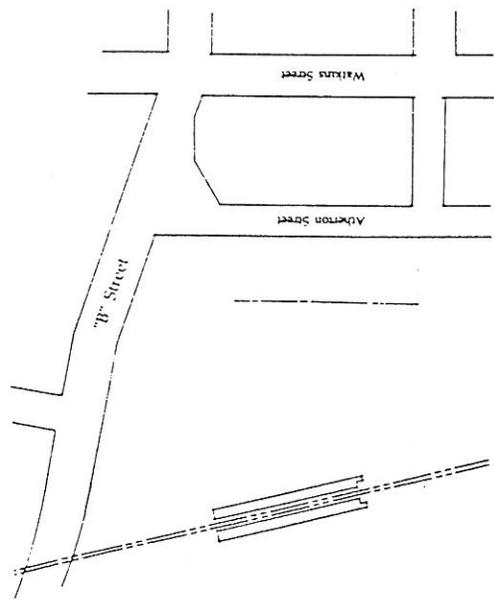




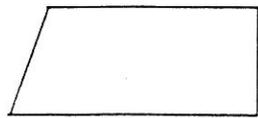
SITE ALTERNATIVE 4

SITE ALTERNATIVE 5

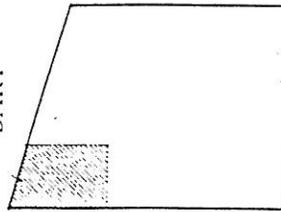




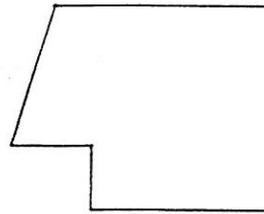
Existing Conditions



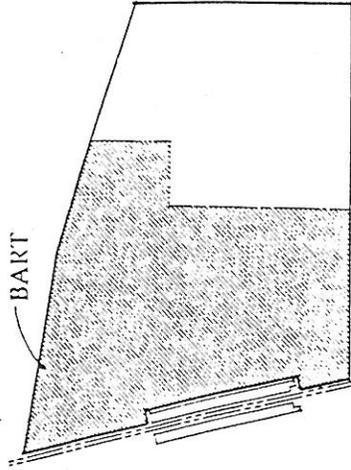
Site 2a



Site 2b



Site 2c



Site 2d

SITE 2 OPPORTUNITIES

Appendix C

**SITE SELECTION CRITERIA
AND ANALYSIS**

APPENDIX C

Site Selection Criteria and Analysis

MASTER PLAN GOAL: THE CIVIC CENTER PROJECT SHALL BE A CATALYST FOR THE REVITALIZATION OF THE DOWNTOWN BUSINESS ENVIRONMENT AND CREATION OF A WELL-USED AND MEMORABLE CENTER FOR THE CITY.

SITE SELECTION CRITERIA:

- A. THE CIVIC CENTER SHOULD ENHANCE THE IMAGE AND VISIBILITY OF DOWNTOWN.
- B. THE CIVIC CENTER SHOULD REVITALIZE B STREET AS THE MAIN PUBLIC THOROUGHFARE OF THE DOWNTOWN, CREATING AN ACTIVE AND VITAL PEDESTRIAN ENVIRONMENT.
- C. THE CIVIC CENTER SHOULD HELP LINK BART TO B STREET.
- D. THE CIVIC CENTER SHOULD ENABLE PRIVATE INVESTMENT IN DOWNTOWN.
- E. DEVELOPMENT OF THE CIVIC CENTER SITE SHOULD BE FEASIBLE IN THE SHORT TERM.
- F. THE CIVIC CENTER SHOULD CONSOLIDATE CIVIC FUNCTIONS IN THE DOWNTOWN AND LINK THEM TO B STREET.

SITE # 1

Positive Features

Good relationship to BART.

Fair relationship to D Street arterial.

Does not depend on making a deal with a private developer; does not require any relocation of existing businesses.

Negative Features

This site is one of the best housing sites in the entire BART system. Using it for the Civic Center would pre-empt using it to attract private investment.

Very poor relation to B Street.

SITE # 2 A

Positive Features

Makes Civic Link from BART to the downtown.
Helps catalyze the development of BART.
Does not depend on making a deal with a private developer.
Incorporates the little park triangle to draw additional visual attention to the presence of the Civic Center along B Street (from Foothill Blvd).
Good frontage across B Street to complement a new Civic Center.

SITE # 2 B

IN ADDITION TO THE POSITIVE FEATURES OF SITE 2A:

Creates a very powerful image for the revitalized downtown visible from BART and Foothill down B St.
Makes the revitalization of downtown not dependent on BART co-development.
Extends the possibility of making the link from BART to B Street through the Civic Center project.

Negative Features

Need to relocate the Chevrolet Dealer.
Invisible from all the arterials.
Requires the removal of a nice corner building (URM structure).
Revitalization of downtown still dependent on BART co-development.

IN ADDITION TO THE NEGATIVE FEATURES OF SITE 2A:

Need to purchase some land from BART and to vacate (move) a public right-of-way.

SITE # 3

Positive Features

- Replaces the only blighted block frontage along B Street.
- No good buildings would be lost.
- Easy to develop since City has control.
- No relocation necessary.
- Has a presence on Library Square.
- Post Office building is an asset.

Negative Features

- The Civic link of downtown to BART would still be dependent on the development of Site #2.
- Not visible from intersection of Foothill and B Street.
- Post Office parking lot a potential problem.
- Somewhat of an awkward shape (bending project around the Post Office).
- Invisible from all the arterials.
- Pre-empt's from private development the easiest and most attractive redevelopment site.
- Only site within the Alquist/Priola Zone.

SITE # 4

Positive Features

- Terrific connection to BART and Library Square.
- Best formal relationship to Library Square.
- Culvert not a problem.
- Visible from D Street; parking potentially directly accessible to D Street arterial.
- Saves City-owned and other developable sites for private investment.

Negative Features

- Relocation problems are an additional expense as well as a potential disaster for the project and the downtown.
- Assembly will undoubtedly cause delay.
- Does nothing for B Street
- Does nothing to link B Street and BART.

SITE # 5

Positive Features

Builds on presence (and value) of existing public buildings.

Very visible from arterials.

Negative Features

Overall, this is the least attractive site. It further disperses things and does not help to consolidate the downtown as a pedestrian precinct around BART. More Specifically:

Not visible from BART.

No direct benefit to B Street.

Will tend to be the least "urban" - thereby contributing to the further disintegration of the "fabric" of the downtown district.

Relocating the restaurant on the corner will likely be difficult, expensive, and time-consuming.

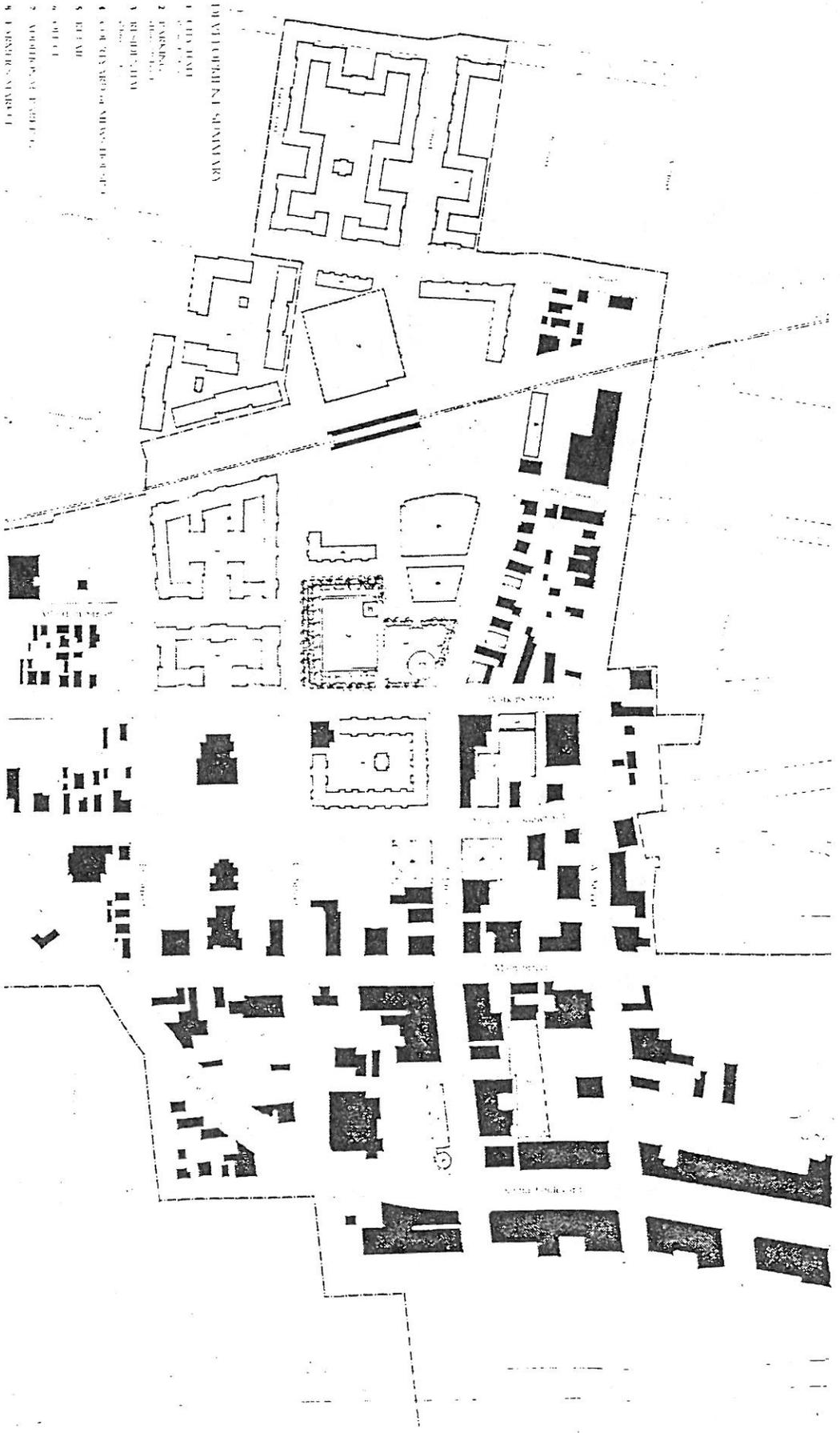
Would require funding the relocation of the Fire Station.

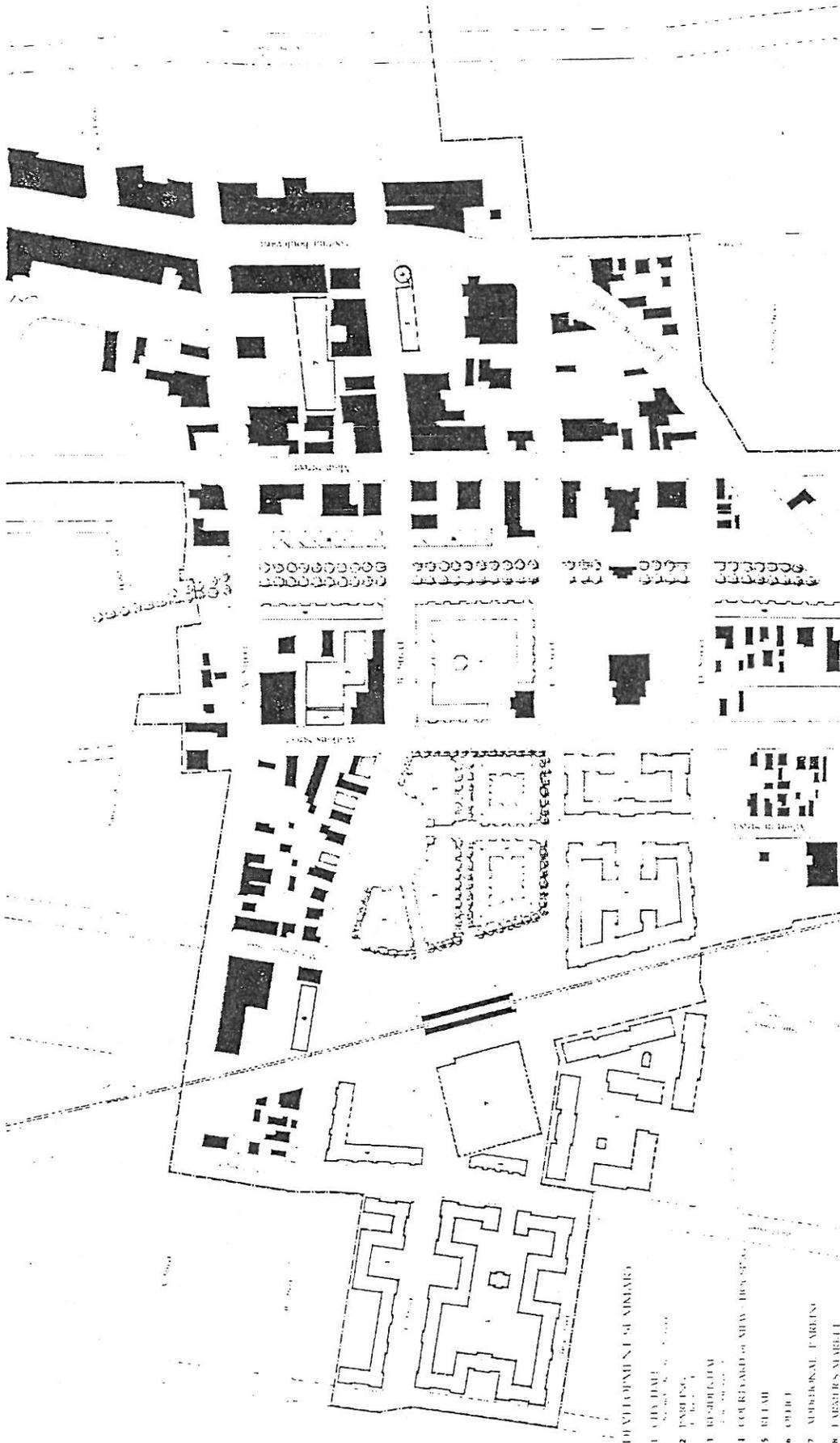
Builds on land that is potentially attractive to private sector investment.

Appendix D

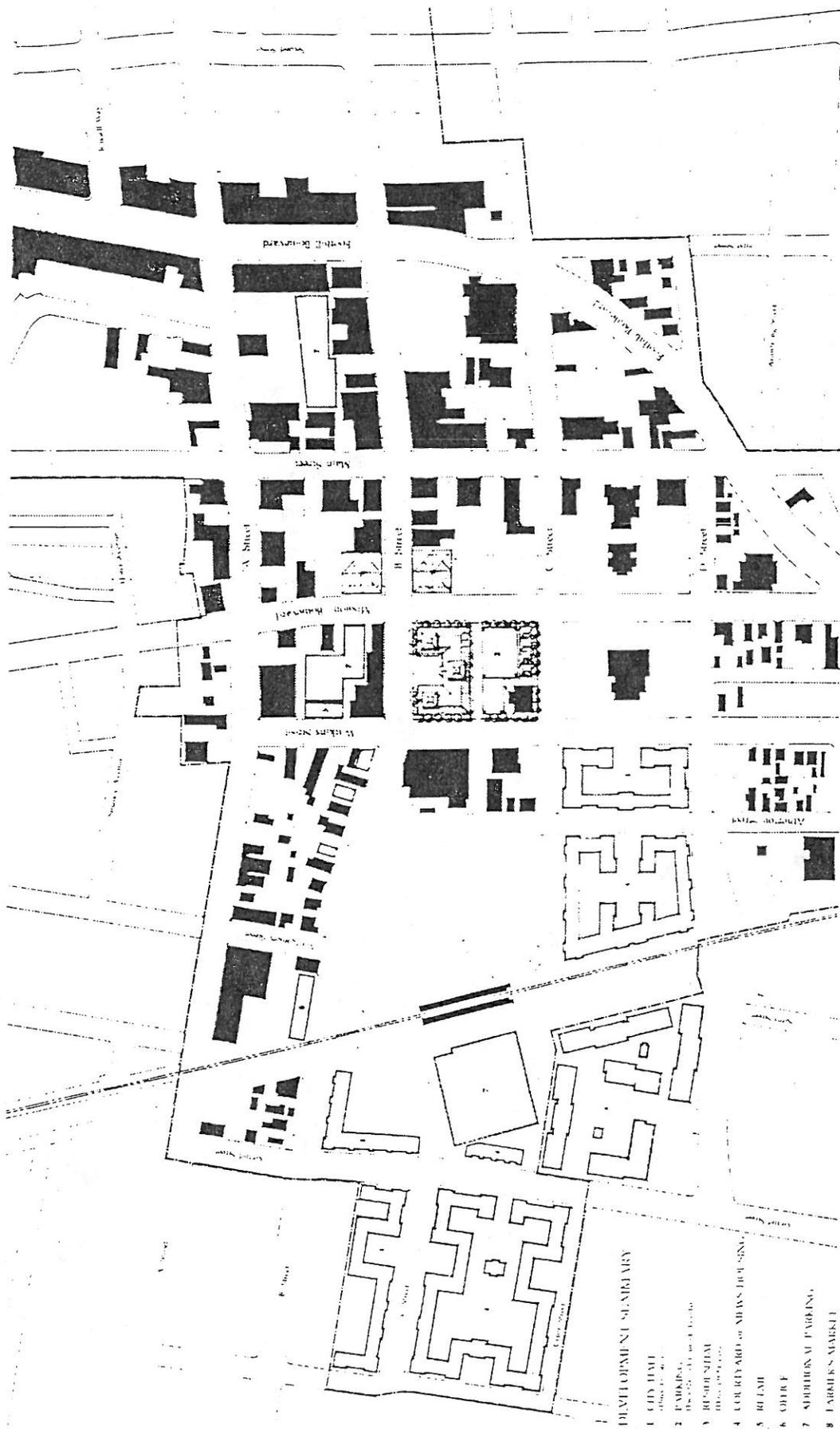
ALTERNATIVE DEVELOPMENT
CONCEPTS CONSIDERED

SITE ALTERNATIVE 2C



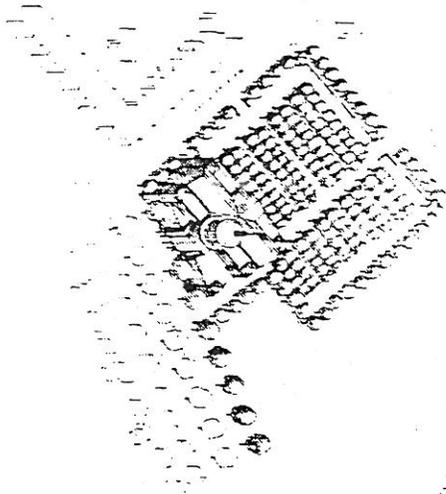


SITE ALTERNATIVE 2D

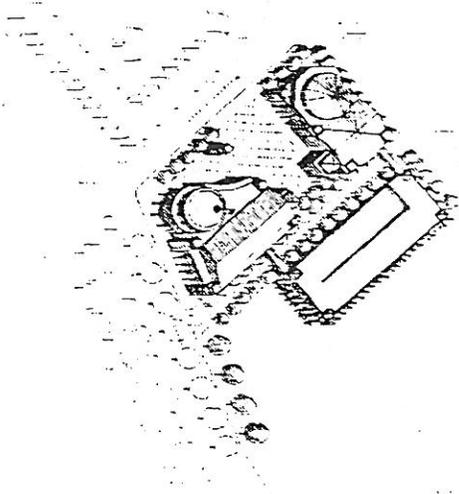


SITE ALTERNATIVE 3

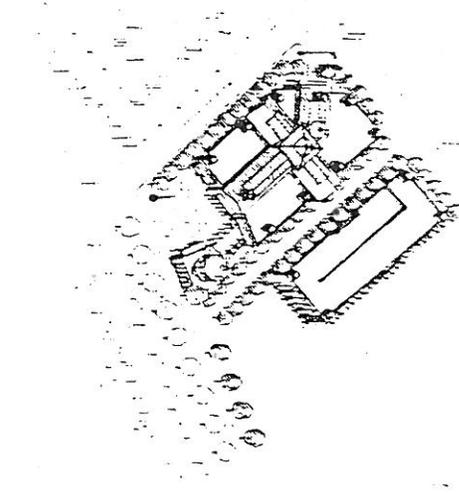
A



B

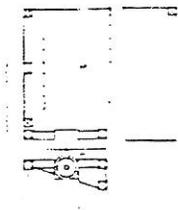


C



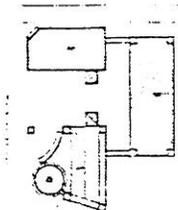
DEVELOPMENT SUMMARY

- 1. CITY HALL
- 2. PARKING
- 3. SURFACE PARKING
- 4. EXISTING CITY HALL AND SURFACE PARKING
- 5. EXISTING CITY HALL AND SURFACE PARKING



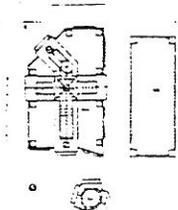
DEVELOPMENT SUMMARY

- 1. CITY HALL
- 2. PARKING
- 3. SURFACE PARKING
- 4. EXISTING CITY HALL AND SURFACE PARKING
- 5. EXISTING CITY HALL AND SURFACE PARKING



DEVELOPMENT SUMMARY

- 1. CITY HALL
- 2. PARKING
- 3. SURFACE PARKING
- 4. EXISTING CITY HALL AND SURFACE PARKING
- 5. EXISTING CITY HALL AND SURFACE PARKING



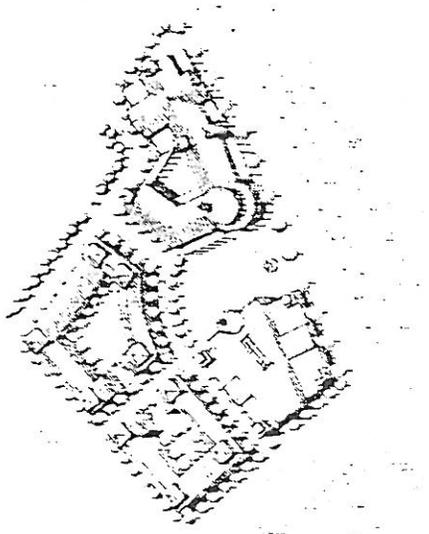
CITY HALL · SURFACE PARKING

CITY HALL · ADDITIONAL BUILDING

CITY HALL · STRUCTURED PARKING

SITE ALTERNATIVE 2C

A



DEVELOPMENT SUMMARY

CITY HALL

- 100,000 sq. ft. Library
- 100,000 sq. ft. Housing
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

RETAIL

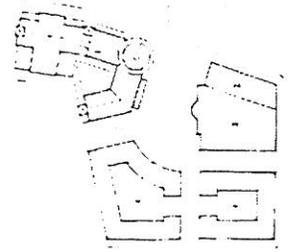
- 100,000 sq. ft. Retail
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

HOUSING

- 100,000 sq. ft. Housing
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

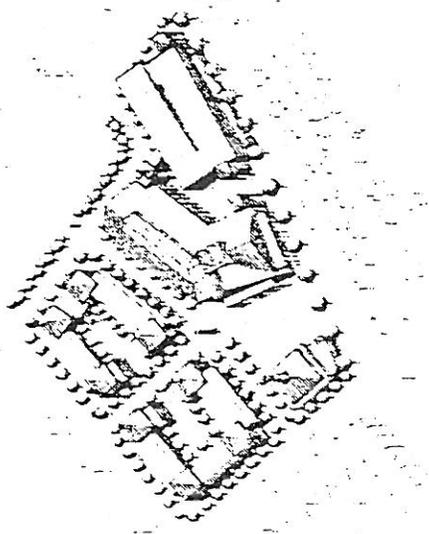
TOTAL SIZE

3,000,000 sq. ft.



CITY HALL · LIBRARY · HOUSING

B



DEVELOPMENT SUMMARY

CITY HALL

- 100,000 sq. ft. City Hall
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

RETAIL

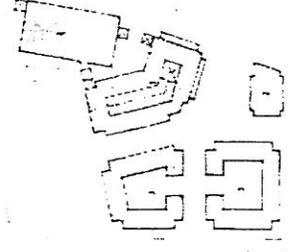
- 100,000 sq. ft. Retail
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

HOUSING

- 100,000 sq. ft. Housing
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

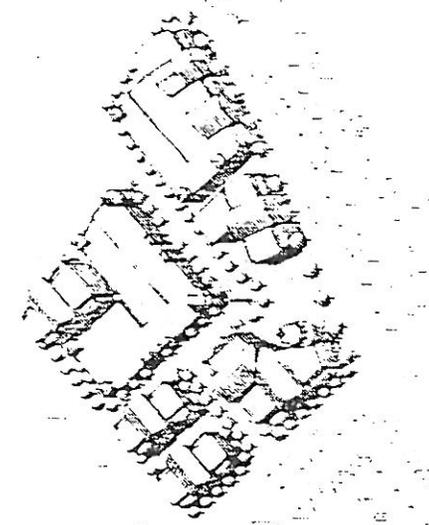
TOTAL SIZE

3,000,000 sq. ft.



CITY HALL · HOUSING

C



DEVELOPMENT SUMMARY

CITY HALL

- 100,000 sq. ft. City Hall
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

RETAIL

- 100,000 sq. ft. Retail
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

OFFICE

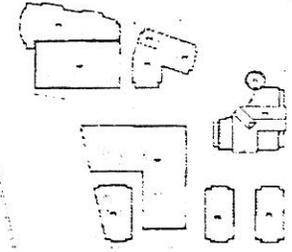
- 100,000 sq. ft. Office
- 100,000 sq. ft. Parking

PARKING

- 100,000 sq. ft. Parking

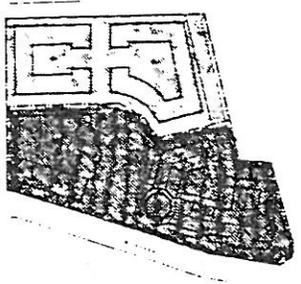
TOTAL SIZE

3,000,000 sq. ft.



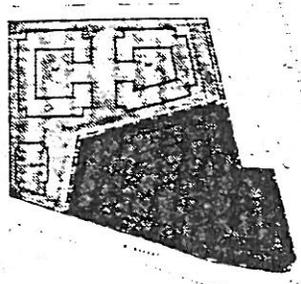
CITY HALL · OFFICE

SITE ALTERNATIVE 2D



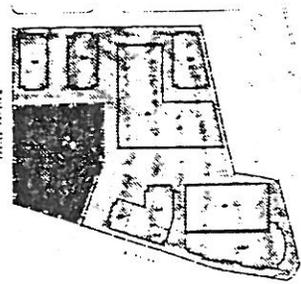
CITY HALL · LIBRARY · HOUSING

- DEVELOPMENT SUMMARY**
- 1 CITY HALL
 - 100,000 sq ft, 4 stories
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 2 RETAIL
 - 100,000 sq ft, 2 stories
 - 100,000 sq ft, 2 stories
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 3 HOUSING
 - 100 units, 100 units, 100 units
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 4 PARKING
 - 100 spaces
 - 5 CITY OF DENVER 27 ACRES
 - 6 RAIL STATION



CITY HALL · HOUSING

- DEVELOPMENT SUMMARY**
- 1 CITY HALL
 - 100,000 sq ft, 4 stories
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 2 HOUSING
 - 100 units, 100 units, 100 units
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 3 PARKING
 - 100 spaces
 - 4 CITY OF DENVER 27 ACRES
 - 5 RAIL STATION

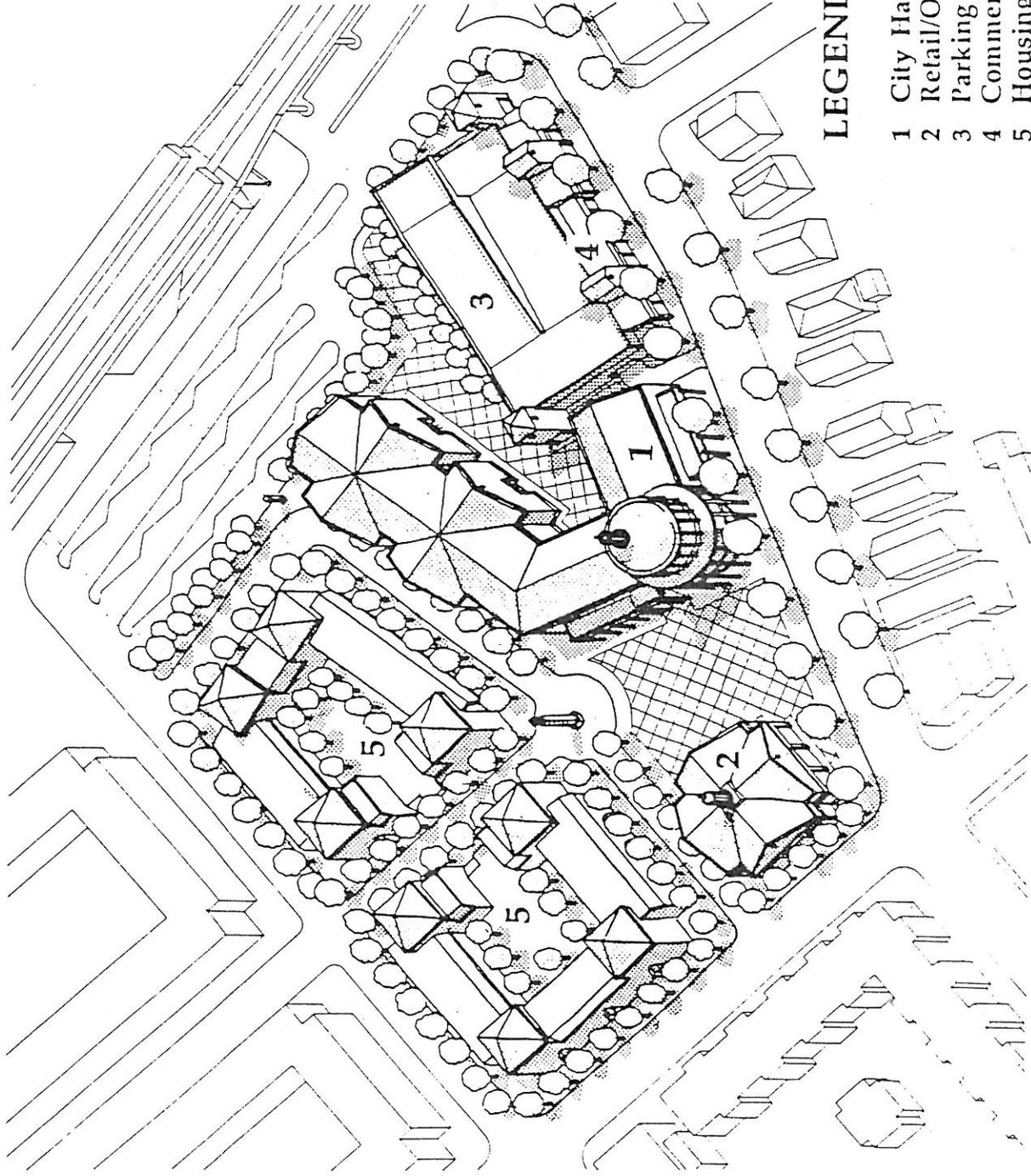


CITY HALL · OFFICE

- DEVELOPMENT SUMMARY**
- 1 CITY HALL
 - 100,000 sq ft, 4 stories
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 2 OFFICE
 - 100,000 sq ft, 4 stories
 - 100,000 sq ft, 4 stories
 - Parking 15 spaces on the 1st floor
 - 10 spaces from 2nd to 4th
 - 3 PARKING
 - 100 spaces
 - 4 CITY OF DENVER 27 ACRES
 - 5 RAIL STATION

SITE ALTERNATIVE 2D: PARCELIZATION

Illustrative Concept

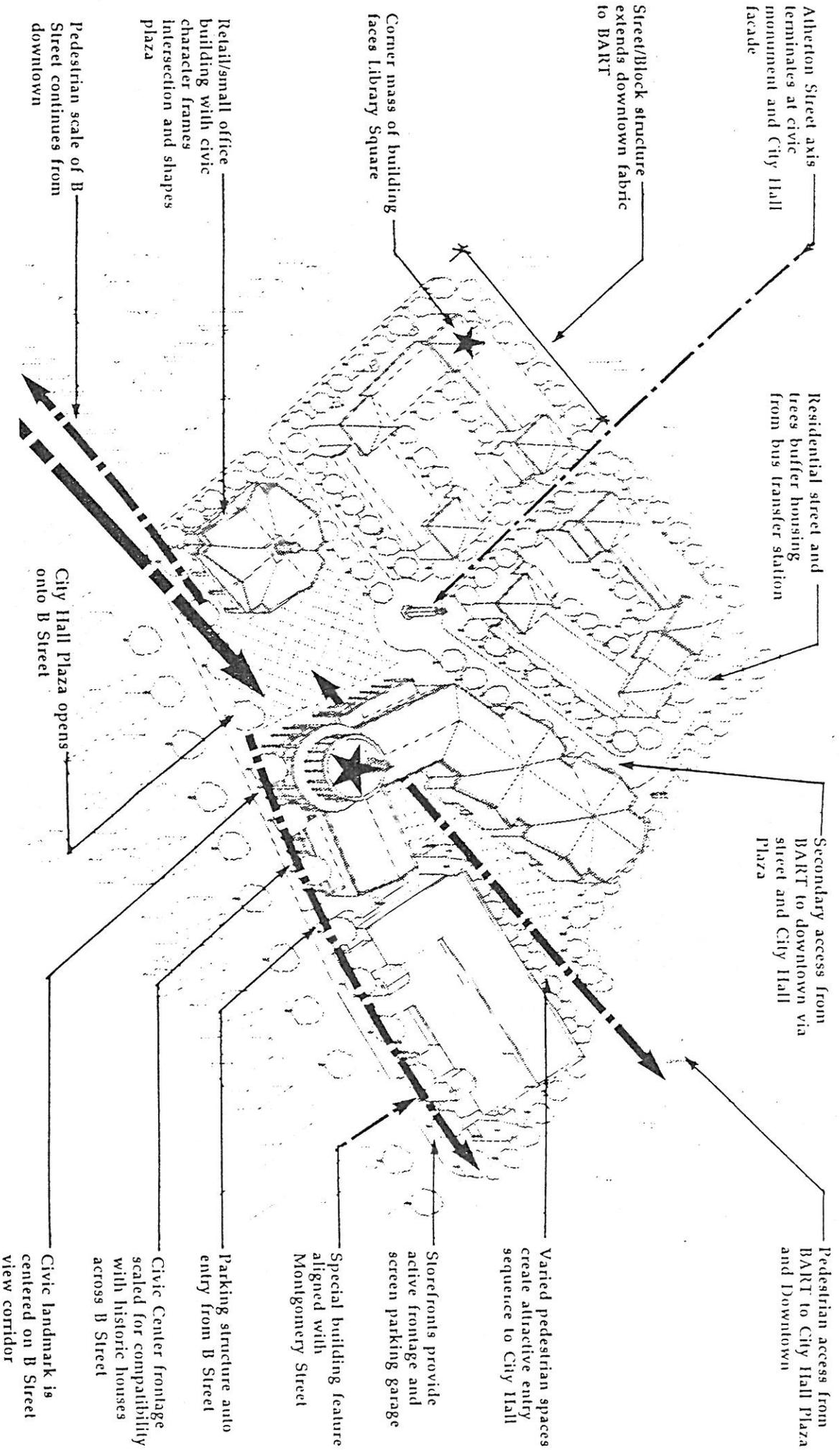


LEGEND

- 1 City Hall
- 2 Retail/Office Building
- 3 Parking Garage
- 4 Commercial Frontage
- 5 Housing

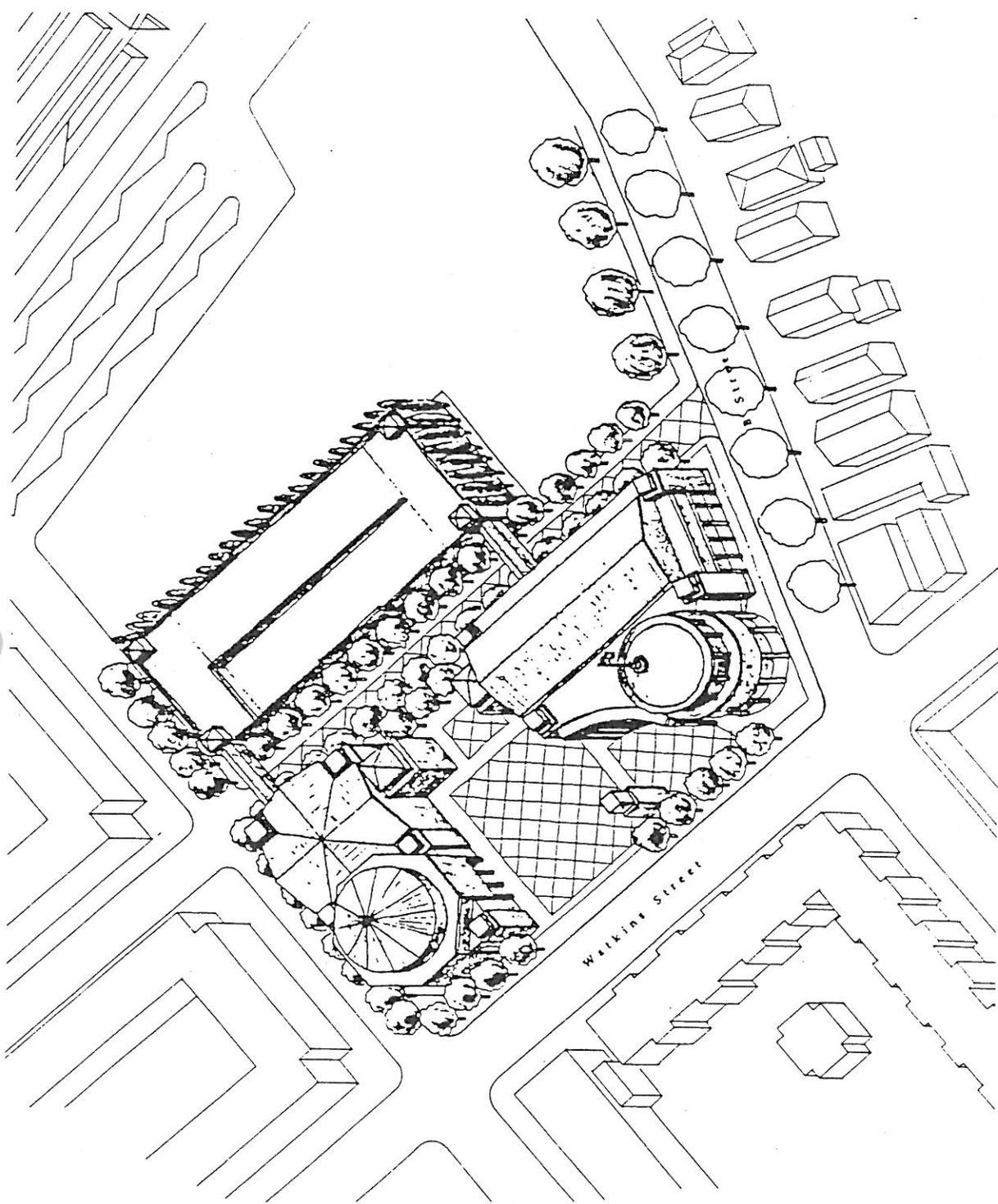
PREFERRED CIVIC CENTER CONCEPT

Urban Design Criteria



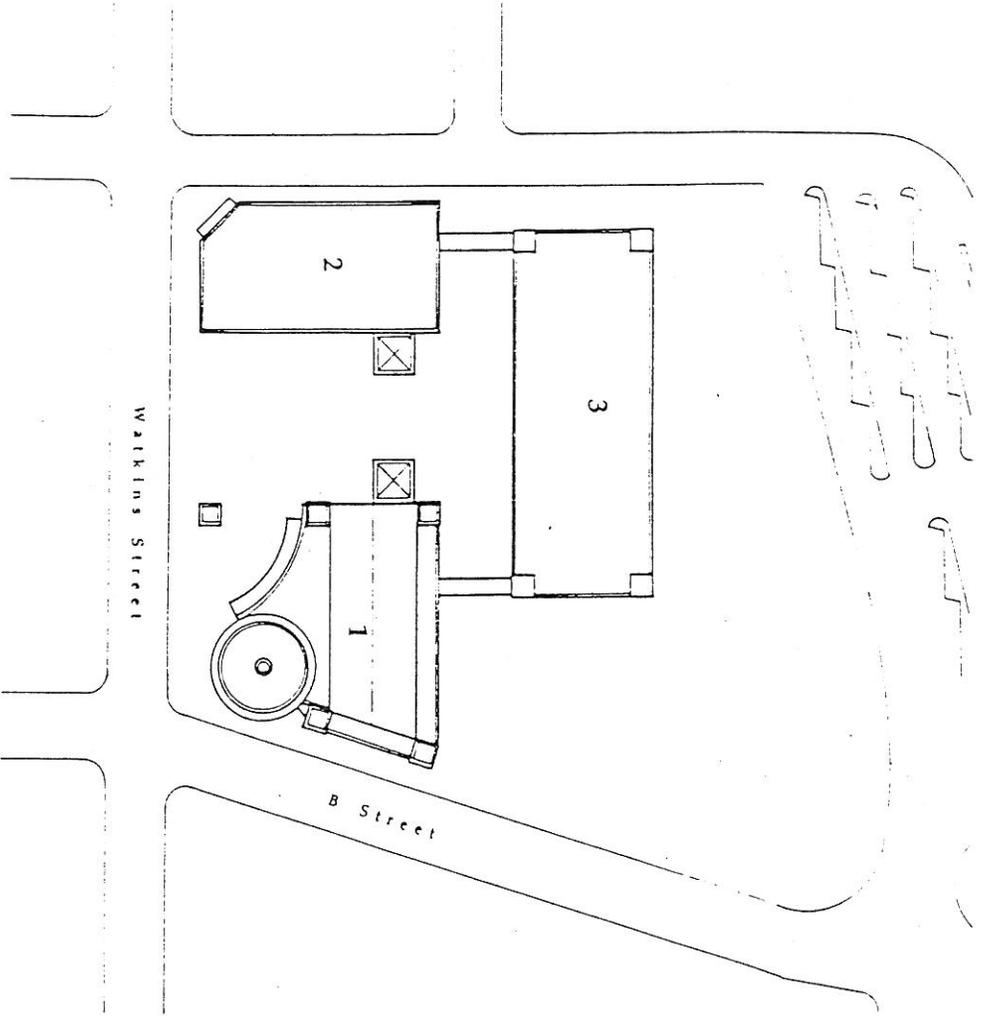
PREFERRED CIVIC CENTER CONCEPT

Site 2c: Development Concept B



PREFERRED ALTERNATIVE CIVIC CENTER CONCEPT

Site 2c: Development Concept B



DEVELOPMENT SUMMARY

- 1 CITY HALL
 - 100,000 sq.ft., 4 stories including Council Chambers
- 2 PUBLIC BUILDING OR OFFICE
 - 72,000 sq.ft., 4 stories
- 3 PARKING
 - 182,000 sq.ft., 580 spaces, 5 levels, 1 underground, 4 above ground (3 spaces/ 1000 sq.ft.)
- 4 PARCEL SIZE
 - 3.9 acres

PREFERRED ALTERNATIVE CIVIC CENTER CONCEPT

Appendix E

**ADDITIONAL CONSTRUCTION
COST DATA**

HAYWARD CIVIC CENTER: REFINED COST DATA*

SITE ALTERNATIVE 2C (3.9 acres)

| Option | City Hall @ \$175/s.f. | Council Chambers @ \$25/s.f. | Parking | Library @ \$223/s.f. | Office | Housing | Plaza @ \$30/s.f. | Streets @ \$3/s.f. | Pedestrian Areas: Walks @ \$17/s.f. | Totals** |
|--------|-----------------------------|--|---|----------------------------|--------|---------|--------------------------|-------------------------|---|--------------|
| A | \$16,625,000 95,000 s.f. | \$1,125,000 5,000 s.f. | \$1,299,000 300 spaces @ \$4,331/space | n/a | n/a | n/a | n/a | \$45,000 15,000 s.f. | n/a | \$19,094,000 |
| B | \$16,625,000 95,000 s.f. | \$1,125,000 5,000 s.f. | \$5,569,000 405 spaces @ \$13,750/space | \$8,920,000 40,000 s.f. | n/a | n/a | \$810,000 27,000 s.f. | \$54,000 18,000 s.f. | \$595,000 35,000 s.f. | \$33,698,000 |
| C | \$16,625,000 95,000 s.f. | \$1,250,000 5,000 s.f. (free standing) @ \$250/s.f. | \$3,938,000 300 spaces @ \$13,750/space | n/a | n/a | n/a | \$600,000 20,000 s.f. | \$54,000 18,000 s.f. | \$193,000 29,000 s.f. | \$22,960,000 |

- * Costs rounded to nearest \$1,000
- ** Excludes private sector development costs

NOTE: All costs listed are construction costs (i.e., bid price)

HAYWARD CIVIC CENTER: REFINED COST DATA*

SITE ALTERNATIVE 2D: (8.0 acres)

| Option | City Hall @ \$175/s.f. | Council Chambers @ \$225/s.f. | Parking for City Hall | Library @ \$223/s.f. | Office @ \$75/s.f. | Retail @ \$74/s.f. | Housing @ \$89/s.f. | Plaza @ \$30/s.f. | Streets @ \$3/s.f. | Pedestrian Arcas: Walks @ \$17/s.f. | Totals** |
|----------------------------|-----------------------------|-------------------------------------|--|---|------------------------------|----------------------------|--|--------------------------|--------------------------|--|--------------|
| A | \$16,625,000 95,000 s.f. | \$1,125,000 5,000 s.f. | \$3,120,000 200 under- ground parking spaces @ \$15,600 per space | \$10,438,000 40,000 s.f. Includes under- ground parking for 120 spaces @ \$12,650 per space | n/a | \$814,000 11,000 s.f. | \$18,651,000 171,000 s.f. 150 units with podium parking | \$630,000 21,000 s.f. | \$105,000 35,000 s.f. | \$1,997,000 117,500 s.f. | \$34,040,000 |
| B | \$16,625,000 95,000 s.f. | \$1,125,000 5,000 s.f. | \$3,150,000 300 spaces @ \$10,500 per space | n/a | n/a | \$592,000 8,000 s.f. | \$17,805,000 162,000 s.f. 154 units with podium parking | \$780,000 26,000 s.f. | \$135,000 45,000 s.f. | \$1,105,000 65,000 s.f. | \$22,920,000 |
| C | \$16,625,000 95,000 s.f. | \$1,125,000 5,000 s.f. | City portion \$4,125,000 of \$16,500,000 1200 spaces @ \$13,750 per space | n/a | \$22,500,000 300,000 s.f. | \$444,000 6,000 s.f. | n/a | \$750,000 25,000 s.f. | \$165,000 55,000 s.f. | \$1,343,000 79,000 s.f. | \$24,133,000 |
| Library- only option | n/a | n/a | n/a | \$10,570,000 40,000 s.f. Includes structured parking for 120 spaces at \$13,750 per space | n/a | \$1,184,000 16,000 s.f. | \$20,771,000 190,000 s.f. 180 units | \$660,000 22,000 s.f. | \$168,000 56,000 s.f. | \$1,793,000 105,500 s.f. | \$13,191,000 |

* Costs rounded to nearest \$1,000

** Excludes private sector development costs

NOTE: All costs listed are construction costs (i.e., bid price)

30 July 1991

Appendix F

**COMPARISON OF PUBLIC
BUILDING COMPONENTS**

**DOWNTOWN CORE FOCAL POINT PROJECT:
COMPARISON OF ALTERNATIVE
PUBLIC BUILDING COMPONENTS**

| PUBLIC BUILDINGS INCLUDED | NEW FACILITY'S CONSTRUCTION COSTS | FUTURE OF EXISTING CITY CENTER BUILDING | FUTURE OF EXISTING LIBRARY BUILDING | FINANCING STRATEGY | NEW ANNUAL OPERATING COSTS | IMPACT ON DOWNTOWN REVITALIZATION OBJECTIVES |
|---|--|--|--|--|--|---|
| CITY HALL (100,000 s.f.), LIBRARY (40,000 s.f.) (AND ASSOCIATED PARKING STRUCTURE) | 2D "A": \$34,040,000 2C "B": \$33,698,000 | Sell or lease for private use. (Simultaneously: promote new commercial investment in the City Center/Mervyn's area to create an attractive district that functions as a gateway into the city.) | Convert Library building to Cultural Facility. | 1. \$10m currently available to the City. 2. \$24m required from new revenues. (Also: Investigate potential private funding sources; Seek SB 1230 State Library Funds; Explore potential Assessment District for financing Public Improvements.) | 1. \$1.8m start-up and preparation cost and approximately \$936,000/year for new operating costs for the Library. 2. \$250,000 in new operating cost for converted Library/Cultural facility. | 1. Meets all focal point criteria and acts as the most powerful catalyst for private investment as well as for the generation of pedestrian activity downtown. 2. Requires the largest portion of land for public facilities (less land for private investment). |
| CITY HALL ONLY (AND ASSOCIATED PARKING STRUCTURE) (100,000 s.f.) | 2D "B": \$22,920,000 2C "C": \$22,960,000 | Sell or lease for private use. | No impact on Library. | 1. \$10m currently available to the City. 2. \$13m required from new revenues. (Also: Investigate additional funding through an Assessment District.) | n/a | Meets all focal point criteria and acts as a powerful catalyst for private investment and will help increase pedestrian activity downtown. 2. Requires a substantial portion of land reserved for public facilities. |
| LIBRARY (AND ASSOCIATED PARKING STRUCTURE) (40,000 s.f.) | \$13,191,000 | City offices move back to City Center building. | Convert Library to Cultural Facility. | 1. \$10m currently available to the City. 2. \$3m required from new revenues. (Also: Investigate additional funding through the establishment of a Library Fund; Seek SF 1230 State Library Funds; Explore potential Assessment District for financing Public Improvements.) | \$1.8m start-up and preparation cost and approximately \$936,000/year for new operations costs for the Library. | 1. Meets focal point criteria and is the most effective public use to generate pedestrian activity downtown as well as an effective catalyst for private investment. 2. Frees up more land for private investment. |
| CULTURAL FACILITY (44,000 s.f.) (This does not include library or theater facilities) | Approx. \$9,000,000 | City offices move back to City Center building. | No impact on Library. | Can be financed with existing City funds. | Approximately \$250,000/year for new operating costs. | 1. Can potentially meet focal point criteria but will provide a less effective catalyst for revitalization. 2. Frees up more land for private investment. |

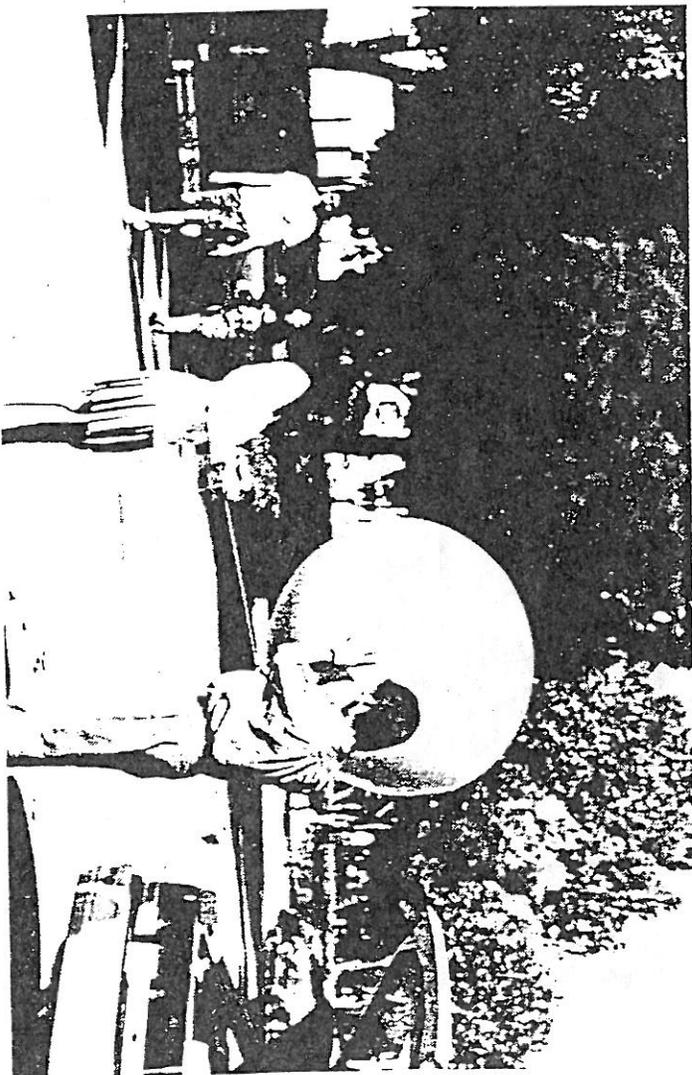
Appendix G

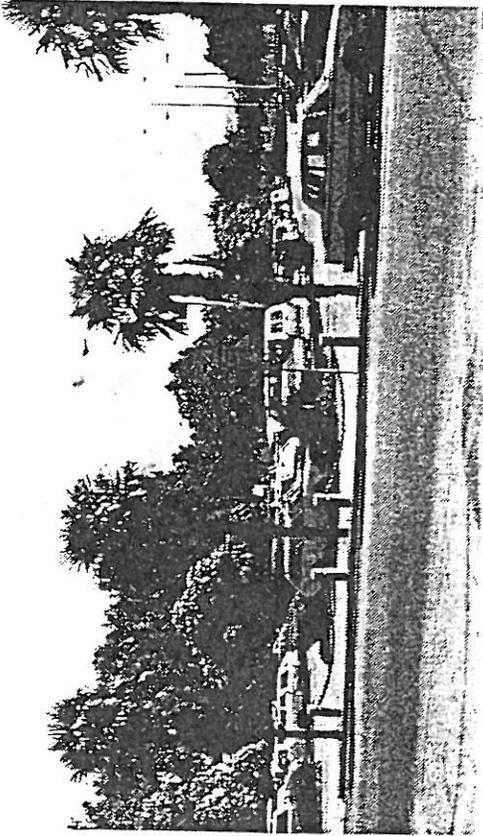
LIBRARY TOWER HEIGHT STUDIES

APPENDIX G

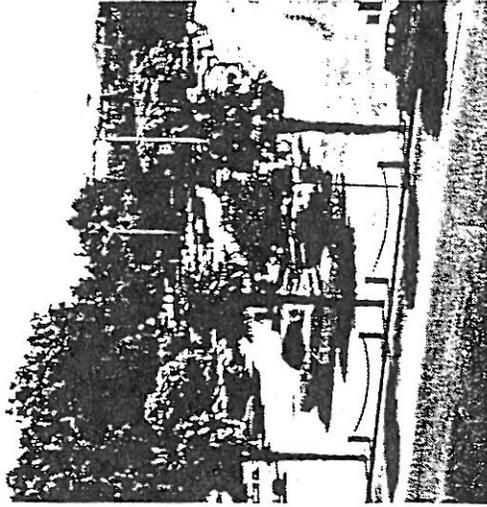
Library Tower Height - Field Test

A recurring theme in the Public Workshops was the importance of using the Downtown Focal Point to enhance the image and visibility of the Downtown Core. To accomplish this objective, the public building component of the Downtown Focal Point is required to incorporate a landmark architectural feature, such as a tower or dome, that provides a visual anchor for the western end of the B Street view corridor, and that is visible from the BART Station. To determine the appropriate height for the landmark - that is, a building feature that is sufficiently visible, yet not out of scale with its surroundings - a balloon test was conducted at the site. The following illustrations depict the results of that test.

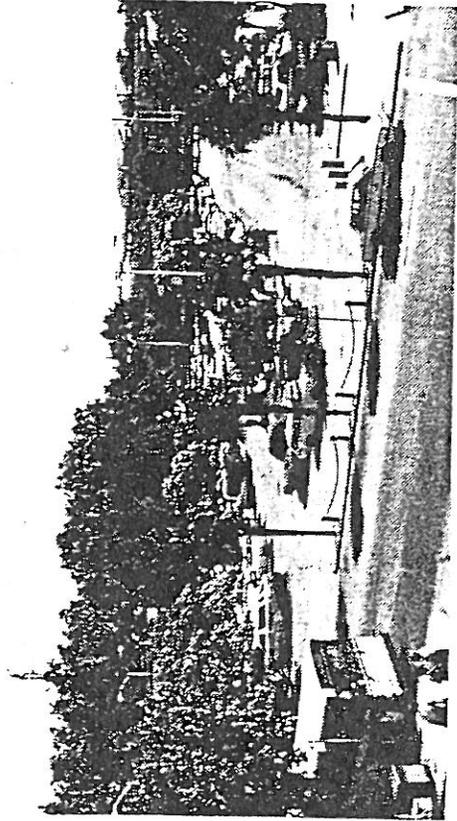




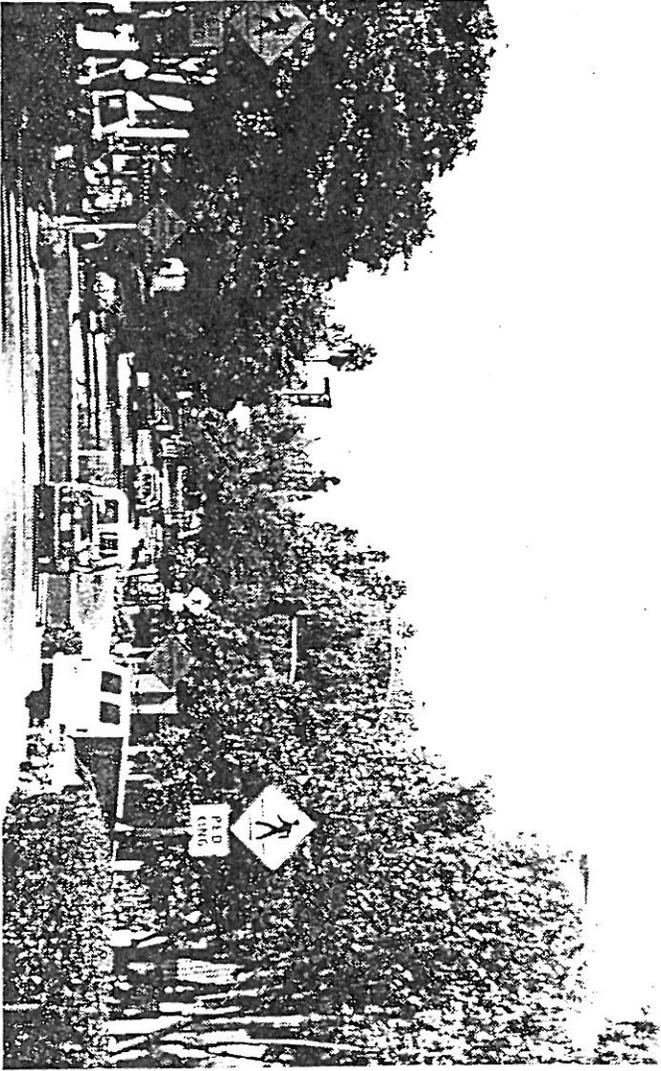
*Balloon at 50' height seen
from Bart Station at ground level*



*Balloon at 100' height
seen from BART Platform*



Balloon at 50' height seen from Bart Platform

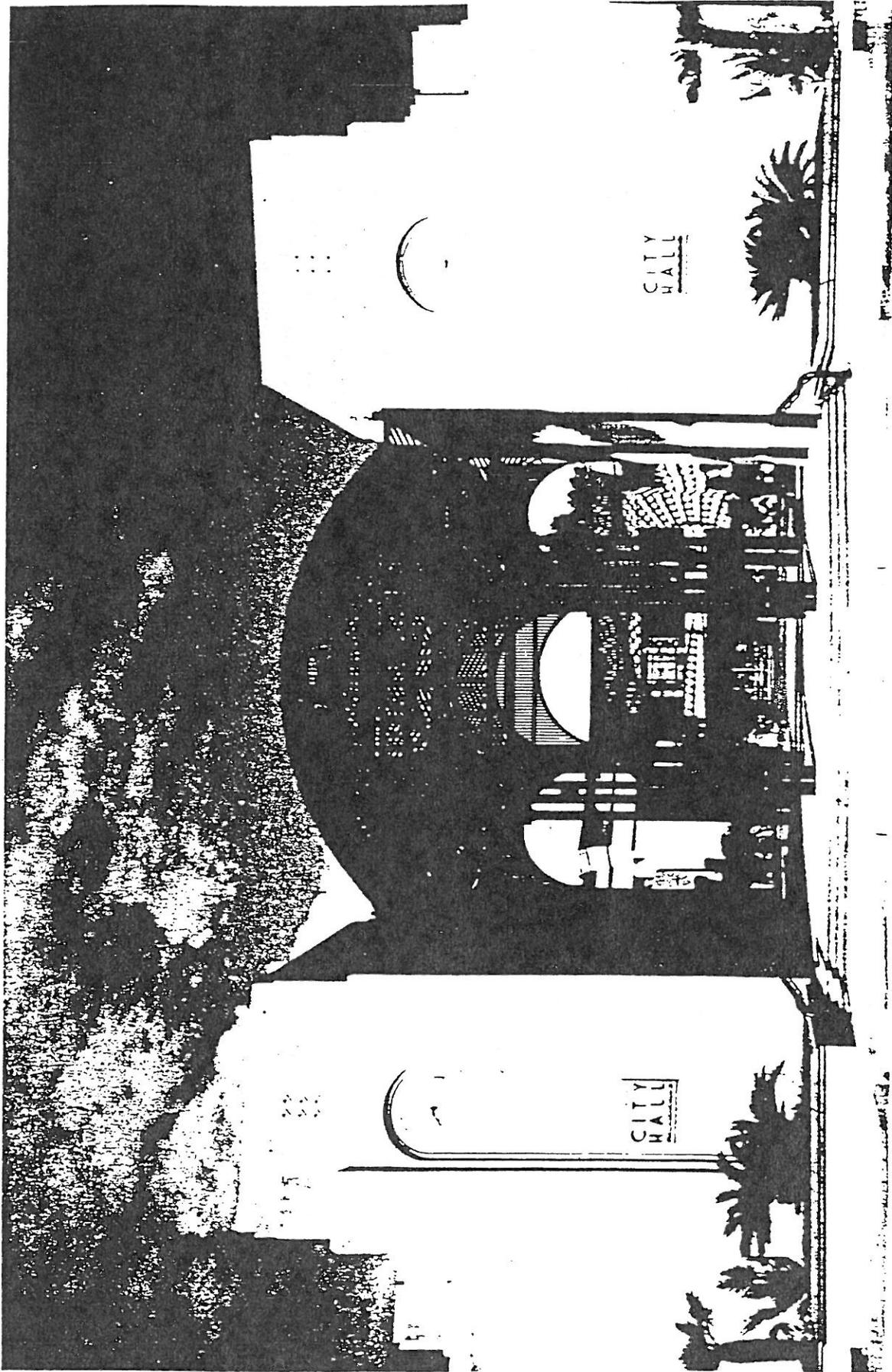


Balloon at 100' height seen down B Street

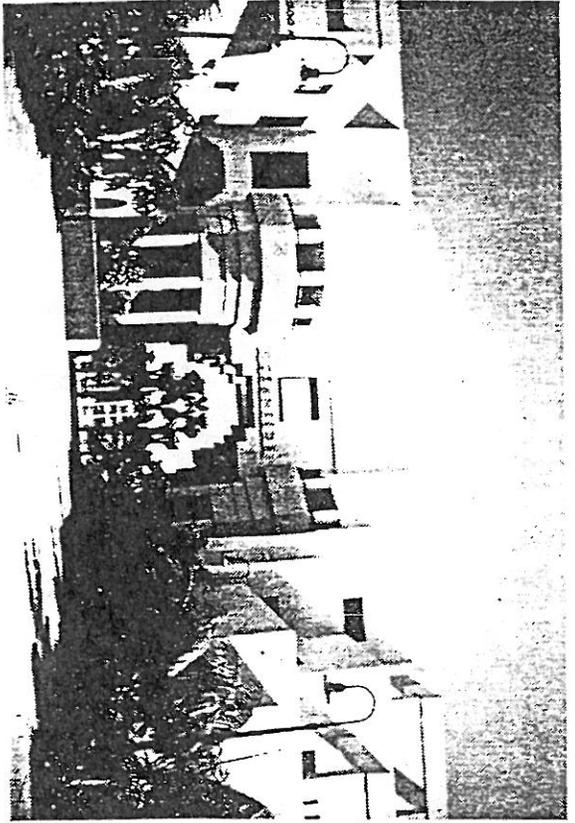
APPENDIX H

Public Building Design Survey

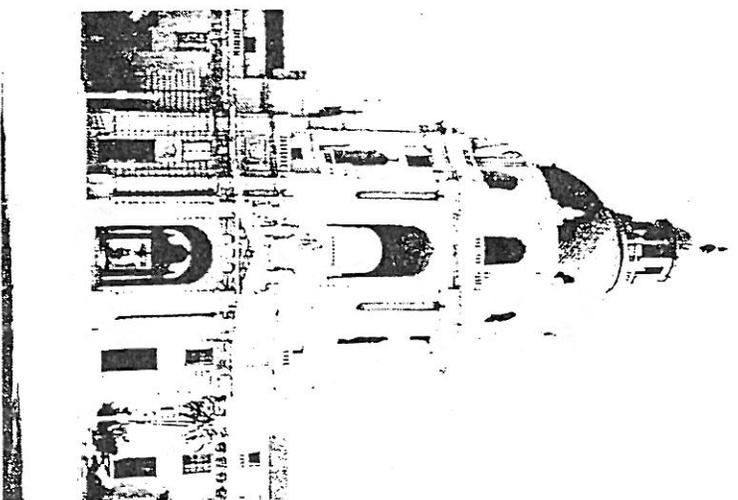
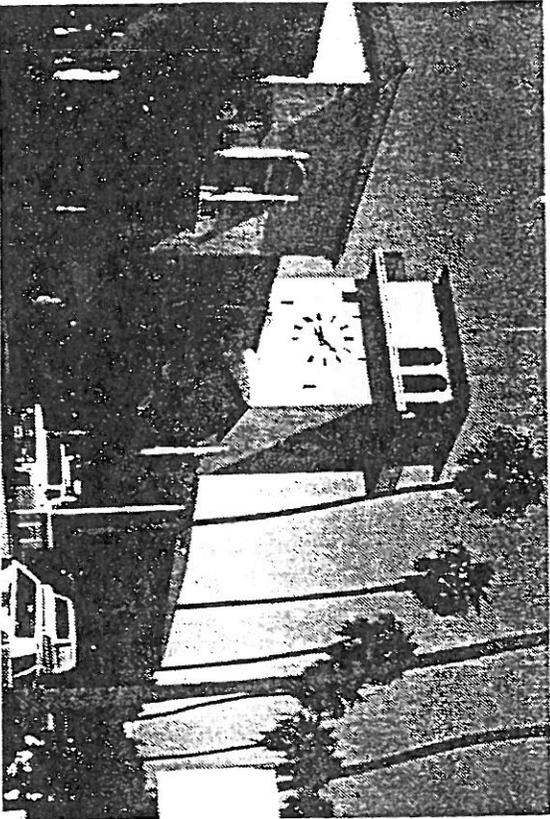
The City of Hayward sponsored a "Vision of Hayward" exhibit in November of 1990. As part of that exhibit, citizens were asked to comment on the design of existing city halls around the State. Participants were asked to focus their comments on the applicability of each design style to the design of new public buildings in Hayward. The following is a brief summary of the results of that survey.



Citizens generally preferred the WPA Revival design of the new Esccondido City Hall over the other examples.

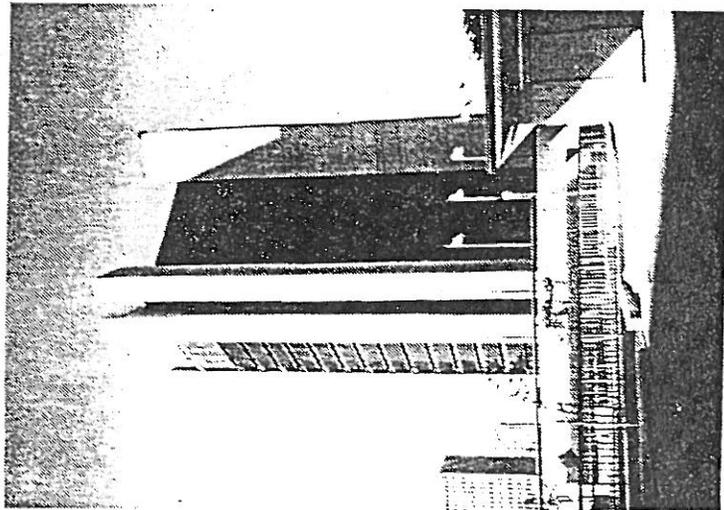


The Post-Modern design of the Oceanside City Hall was generally thought to be trendy and not an appropriate style for Hayward.

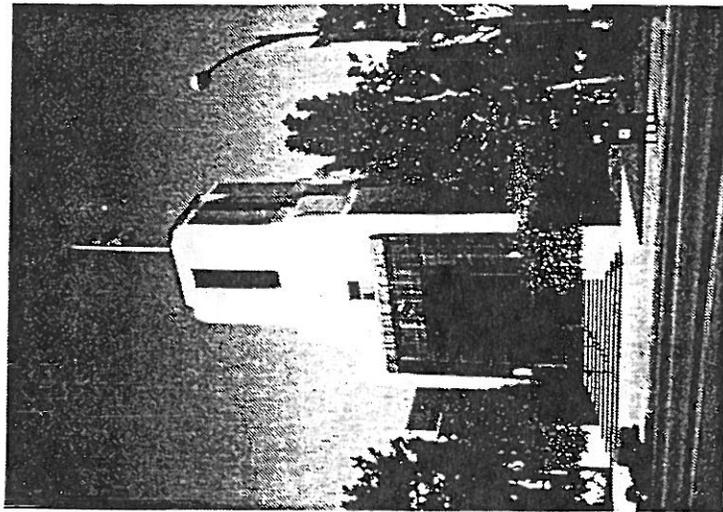
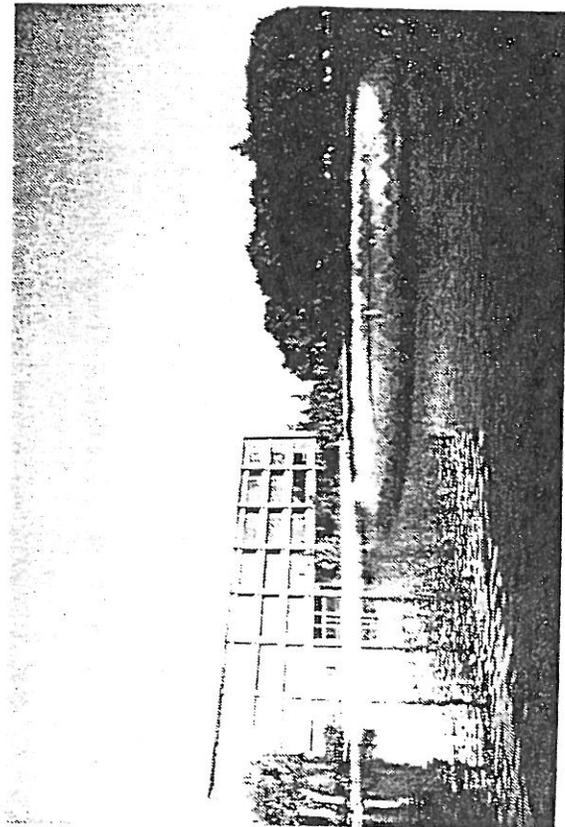


Participants valued the Beaux Arts design of Pasadena's City Hall for its traditional expression, but many participants felt that it was overly ostentatious.

The response to the design of the Seal Beach City Hall was mixed: some found the Spanish Mission Revival style appealing, but others commented that the style did not adequately represent Hayward's diverse cultural heritage.



The International Style design of the Long Beach and Fairfield City Halls was generally disliked by participants in the survey.



The response to the WPA Moderne Glendale City Hall was mixed: some commented that the style might fit well in Hayward, but that this particular building was a poor example of the style.

PROJECT PARTICIPANTS

